

**PLANNING COMMISSION****CITY OF OCEANSIDE**

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DATE: August 11, 2025

TO: Chairperson and Members of the Planning Commission

FROM: Development Services Department/Planning Division

SUBJECT: **CONSIDERATION OF A TENTATIVE MAP (T22-00004), DEVELOPMENT PLAN (D22-00009), AND DENSITY BONUS (DB22-00005) FOR THE CONSTRUCTION OF 83 SINGLE-FAMILY HOMES ON A 16.78-ACRE SITE ON GUAJOME LAKE ROAD, SOUTHEAST OF ALBRIGHT STREET (APN:157-412-15-00) – GUAJOME LAKES HOMES PROJECT – APPLICANT: RINCON CAPITOL GROUP, LLC**

**RECOMMENDATION**

Staff recommends that the Planning Commission by motion:

- 1) Certify the Environmental Impact Report (SCH No: 2022110028) and adopt the associated findings of fact and mitigation monitoring and reporting program by adopting Planning Commission Resolution No. 2025-P23; and
- 2) Approve Tentative Tract Map (T22-00004), Development Plan (D22-00009), and Density Bonus (DB22-00005) by adopting Planning Commission Resolution No. 2025-P20 with findings and conditions of approval attached herein.

**BACKGROUND**

The project site is located in the northeastern portion of Oceanside, just south of State Route 78 and consists of a 16.78-acre parcel located on Guajome Lake Road, southeast of Albright Street. Surrounding land use consists of single-family residential development to the north, south, and east and Guajome Regional Park directly to the west (across Guajome Lake Road). Access to the project site is provided from Guajome Lake Road, which is unpaved between Albright Street and the City of Vista municipal boundary to the southeast of the site. The project site (outlined in black) and surrounding area are depicted in Figure 1 below.

The project site has a General Plan land use designation of Single-Family Detached Residential (SFD-R) and a zoning designation of Single-Family Residential – Scenic Park Overlay – Equestrian Overlay (RS-SP-EQ).

Figure 1: Project Location

The project site is mostly undeveloped land consisting of previous disturbed areas and riparian habitat adjacent to a streambed. An abandoned house is located on the property within the defined habitat area and would be demolished as part of the proposed project.

An Environmental Impact Report (EIR) was prepared for the proposed project in compliance with the California Environmental Quality Act. As provided in the Biological Technical Report (Dudek 2024 – Attachment 3), the northern portion of the site contains a variety of habitat, including Diegan coastal sage scrub, disturbed riparian forest, non-native grassland, non-native riparian, a non-vegetated channel, and Southern arroyo willow riparian forest. The southern portion of the site, where development would occur, supports non-native grassland which has been previously mowed and maintained to control vegetation growth.



The project site is located within the Guajome Park Sphere of Influence (Special Management Area) as provided in the Land Use Element of the General Plan. Properties within the vicinity of Guajome Regional Park have the Scenic Park Overlay District designation which is combined with the base zoning district to ensure new development maintains compatibility with the park and does not impact natural resources and recreational and scenic areas in and adjacent to the Guajome Regional Park.

## **PROJECT DESCRIPTION**

The proposed project requires approval a Tentative Map and Development Plan pursuant to the City's Subdivision Ordinance and Zoning Ordinance, respectively. The applicant is requesting allowances under Density Bonus Law by reserving five percent (5%) of the proposed single-family dwellings as affordable to very low-income households.

**Tentative Map (T22-00004):** A request for the creation of 83 single-family lots (Lots 1-83) for residential purposes and six additional lots (Lots A – F) for a private road, bio-filtration basins, recreational open space, and natural open space on a 16.78-acre site.

The proposed lots would range in size from approximately 2,464 to 5,390 square feet with an average lot size of approximately 3,200 square feet.

**Development Plan (D22-00009):** A request to allow the construction of 83 detached single-family homes and associated site improvements including landscaping and recreational areas. The development footprint would encompass approximately 9.86 acres of the 16.78-acre site with the remaining 6.92 acres to be designated as open space.

Site Plan: The project has been designed with a private loop road extending around the interior of the site. Two separate access drives would connect the subdivision to Guajome Lake Road. Each single-family lot would front onto the private access road. The project would feature a centralized recreational open space area totaling approximately 35,151 square feet. All residential lots would also have private backyards.

**Figure 2: Site Plan**



The northernmost portion of the site would be maintained as open space with a 50-foot-wide biological buffer and a 50-foot-wide planning buffer from the southern edge of the riparian forest and southern willow riparian forest. Based on the elevation change, the slope at the northwest portion of the site requires the creation of a manufactured slope extending into the planning buffer.

Site Improvements: The proposed subdivision would include the installation of full site improvements along the Guajome Lake Road project frontage and the interior private street. Road improvements would include 40-foot curb to curb improvements including a

5.5-foot-wide parkway and a 4.5-foot-wide sidewalk along the project frontage. The internal private road would be 28–32 feet wide with 5-foot-wide sidewalks.

The proposed development would also construct off-site improvements, including paving an approximately 200-foot-long section of Guajome Lake Road between the project boundary and Albright Street and the construction of a 3-inch sewer force main that would connect to existing public sewer at the intersection of Old Ranch Road and Guajome Lake Road. Such improvements would occur within the existing right-of-way and would avoid any impacts to adjacent properties or Guajome Regional Park. A private sewer lift station would be constructed on the project site as well. No additional off-site paving would occur on Guajome Lake Road to the southeast of the project site. However, the project would install traffic calming features, such as speed cushions, and signage, in coordination with the City Traffic Engineer.

Engineering Improvements: The project site has an existing slope that descends from east to west across the proposed development footprint. Site grading and lot configurations were designed to align with the existing topography of the property. The project would utilize retaining walls along the project frontage and throughout the site to establish building pads and support proper storm water drainage. A landscaped manufactured slope and retaining wall would also be constructed on the northwest portion of the site.

Since the project does not contain slopes that have a natural gradient in excess of 20 percent with a minimal elevation differential of 25 feet, it is not subject to the Hillside Development Provisions provided in Article 30, Section 3039 of the Zoning Ordinance.

Architecture: The proposed project would involve the construction of 83 detached single-family homes on lots ranging in size from 2,464 to 5,390 square feet. Each home would be two stories and range in size from 1,869 to 2,220 square feet with floorplans that include four to five bedrooms and two and a half (2.5) bathrooms. All homes would include a two-car garage.

**Table 1**

PLAN TYPE	UNIT MIX	SQ. FT.	BDs / BAs
Plan 1	21	1,869	4 / 2.5
Plan 2	22	2,064	4 - 5 / 2.5 (loft option)
Plan 3	40	2,220	4 - 5 / 2.5 (loft option)
<b>Elevation Styles:</b> Ranch; Farmhouse; Progressive Prairie			

In accordance with the Scenic Park Overlay District, all exterior building finishes shall be painted in earth tones or other colors that are compatible with the surrounding landscape. The proposed homes would feature architectural design with three different styles

described as “ranch,” “farmhouse,” and “progressive prairie.” All three styles would utilize earth tone colors and feature concrete tile roofs, stucco siding, and intermittent board and batten siding or lap siding. As required by the Scenic Park Overlay District, all homes would not exceed the maximum permitted height of 25 feet.



**Landscaping and Open Space:** The landscape design for the proposed project would include a variety of trees, shrubs, accent plants, and ground covers throughout the site and along the Guajome Lake Road street frontage. The landscape palette would feature drought tolerant and low water use plants. A variety of vegetation deriving influence from Guajome Regional Park would be featured along the boundaries of the project site. The project would provide a 15 percent tree canopy that is approximately 57,332 square feet. Tree species would include, Desert Willow, Brisbane Box, Coast Live Oak, California Sycamore, and other varieties.

The 35,151 square foot private outdoor amenity area would feature a variety of recreational amenities including a playground, event lawn with natural turf, a horseshoe pit, shade structures, and an outdoor kitchen with furniture. As previously noted, the northernmost portion of the site (6.92 acres) along the riparian corridor would be preserved as open space. A wall would be constructed along the northern development perimeter where a manufactured slope would be created. Fencing would be placed around the remaining interface between the development and open space to prohibit access to the preserved riparian area.

**Vehicle Circulation and Parking:** Access to the site would be provided via two driveways on Guajome Lake Road. The internal private road would be 28 to 32 feet wide with five-foot sidewalks. The proposed subdivision was designed in consultation with the Oceanside Fire Department to ensure adequate ingress and egress. All fire access roads would be designed to accommodate fire apparatus trucks and other service vehicles. Parking on either side of Guajome Lake Road within the project boundaries would be prohibited.

Given the width of the internal loop road and the minimal space between driveways, on-street parking would be limited within the subdivision. A parking exhibit was provided with the Tentative Map (Attachment 3) that identifies areas that would accommodate up to 10 parallel on street parking spaces. Each home would have a two-car garage for a total of 166 spaces and a private driveway that could accommodate additional parking of two or

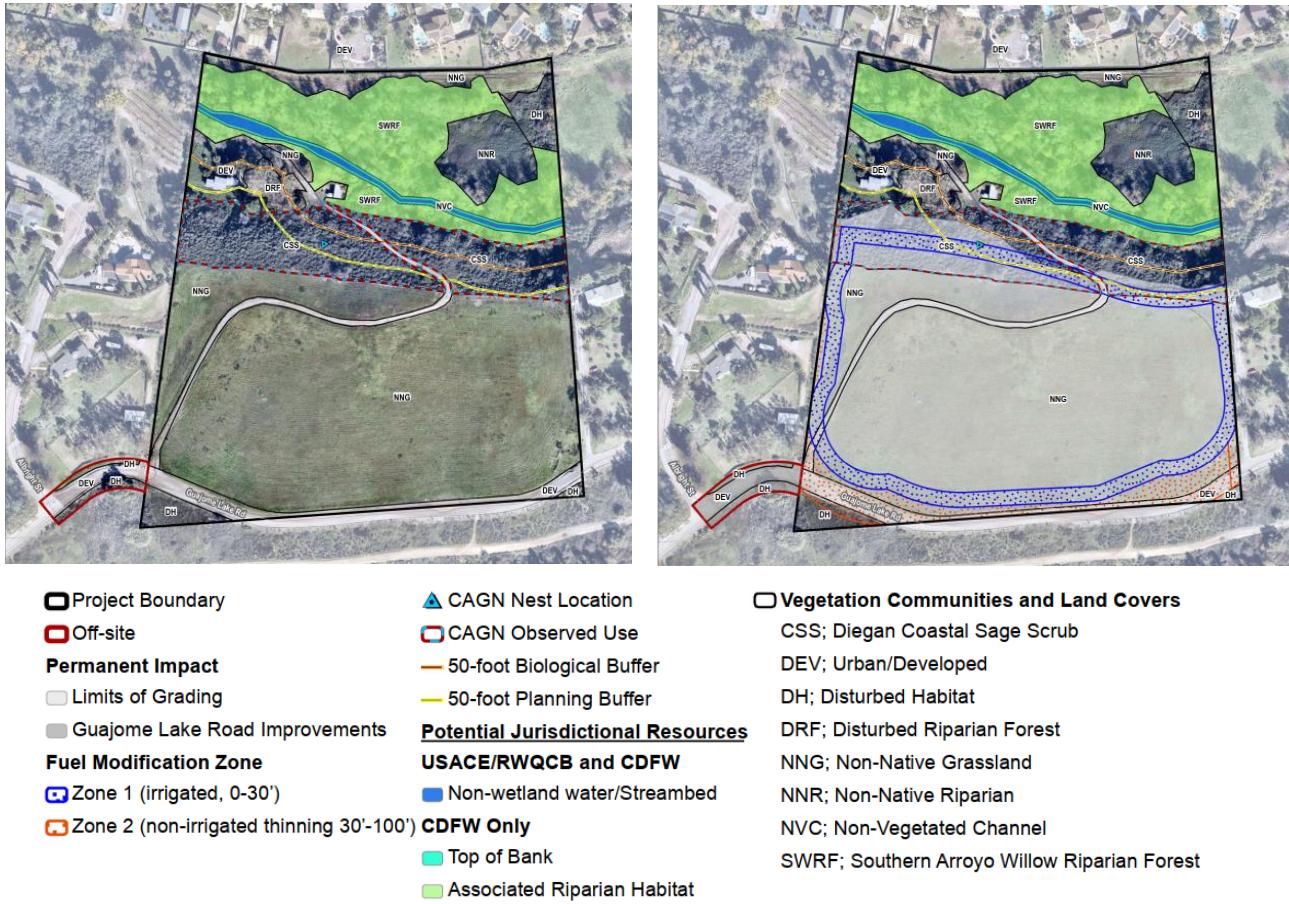
more cars. As a condition of approval, a clause shall be part of the Covenants, Conditions, and Restrictions (CC&Rs) that require garages to be kept available and useable for the parking of homeowner's or tenant's automobiles at all times. The homeowner's association shall be responsible for enforcing parking throughout the subdivision.

Traffic Analysis: A Local Transportation Assessment (LTA) was prepared for the project to analyze potential traffic impacts. Guajome Lake Road is classified as a Collector roadway in the Circulation Element of the General Plan. According to the study, the project would generate 830 average daily trips (ADT) based on 83 single-family residences. The LTA analyzed level of service (LOS) for existing conditions plus the project. Under existing conditions Guajome Lake road carries 744 ADT and operates at an LOS C (2,200 ADT for unpaved road), which is considered an acceptable level under the City's traffic guidelines.

With the planned roadway improvements along the project frontage, the Collector capacity under LOS C would increase to 7,500 ADT. The LTA concludes that paving this section of Guajome Lake Road would increase the capacity of the roadway to a Collector standard and would operate at LOS A. The project would not result in traffic impacts as defined in the City's Traffic Guidelines and no off-site improvements are required. However, the project would pave the offsite segment of Guajome Lake Road between the project boundary and Albright Street.

Habitat and Conservation Easement: A Biological Resources Technical Report ("BRT") was prepared for the project by Dudek (October 2024). The BRT was prepared to evaluate the project in accordance with CEQA as well as the City's Draft Subarea Plan. As shown in Figure 3, the project site has various vegetative communities, including Diegan coastal sage scrub and non-native grassland. The proposed development would have a direct permanent impact to 10.31 acres of the site with an additional 0.31 acres associated with off-site improvements. Onsite improvements would have a direct impact to 1.25 acres of Diegan coastal sage and 8.29 acres of Non-native grass. The loss of this sensitive vegetation would result in the need for 6.64 acres of off-site mitigation land.

Figure 3: Habitat Area



Per the Draft Subarea Plan, the northern portion of the project site overlaps with a hardline preserve zone. However, the project site is not located within the Wildlife Corridor Planning Zone. Approximately 0.03 acres of the proposed development would overlap with the hardline preserve boundaries due to grading and fuel modification zone. The project would modify the current proposed preserve boundary to conform with the site design and preserve all land to the northeast of the development footprint as open space and natural habitat. Furthermore, the existing abandoned house located in the biological buffer would be demolished.

As previously noted, the project would retain 6.92 acres of open space on the northmost portion of the site. The project would include a 100-foot buffer from the edge of the riparian forest consisting of a 50-foot biological buffer and a 50-foot planning buffer. The proposed development would impact 0.36 acres of the planning buffer based on the need to construct a manufactured slope and provide a fuel modification zone. Additional discussion of potential impacts to habitat and proposed mitigation is provided in the Environmental Review section of the staff report.

Since the Draft Subarea Plan has not been adopted, “take” of habitat for the coastal California gnatcatcher (i.e., coastal sage scrub) must be granted through the Section 10

consultation process with the U.S. Fish and Wildlife Service (USFWS) (MM-BIO-11). A Habitat Conservation Plan (HCP) would be prepared as part of the Section 10 consultation process and would include details regarding off-site mitigation. USFWS would review and must approve the HCP and associated mitigation proposal prior to the issuance of grading permits.

**Density Bonus (DB24-0000)** represents a request by the applicant for increased density and allowances under State Density Bonus Law (SDBL) by reserving five percent of the housing units as affordable to very-low income households. This reservation entitles the applicant to a density bonus of 20 percent as well as unlimited development waivers and one (1) incentive/concession pursuant to Government Code 65915.

As provided in Table 2 below, the proposed project would reserve 5 percent of the total number of units allowed under the base density (74 units) and is entitled to a 20 percent density bonus, resulting in 15 additional units for a project total of 83 dwelling units. Four of the dwelling units would be reserved as affordable to very-low income households. Base density was calculated by multiplying the allowable density in the RS District (5.9 du/acre) by the net acreage of developable land (12.45 acres) as defined in the Land Use Element of the General Plan. Staff notes that a recent HCD memorandum clarified that density calculations under SDBL shall be calculated based on gross site acreage, regardless of whether the land is considered undevelopable (i.e. habitat, steep slope).

**Table 2**

<b>Density Bonus Item</b>	<b>Calculations</b>	<b>Proposed for Project</b>
<b>Base Density Permitted by Current General Plan / Zoning</b>	74 units (per SDBL)	74 units
<b>Proposed Affordable Unit Income Level</b>	5% at Very Low-Income Level	4 units
<b>Proposed Density Bonus Percentage</b>	20.0%	15 units
<b>Total Dwelling Units Proposed on Site</b>	Maximum of 89 units allowed per SDBL	83 units proposed
<b>Resulting Density in Units / Acre</b>	83 units / 12.45 acres	<b>6.67 du/ac</b>

SDBL entitles projects to certain number of incentives or concessions and also provides for waivers from development standards that would physically preclude the project at the density proposed. The granting of waivers does not reduce the number of incentives allowed on a project, and the number of waivers that may be requested and granted is unlimited.

**Incentive/Concessions:** None requested

**Waivers:** In order to accommodate the project at the proposed density and as allowed under SDBL, the project cannot physically comply with all applicable development standards for the Single-Family Residential (RS) District and Equestrian Overlay District. The applicant has thus requested waivers from the following development standards pursuant to State Law:

1. Reduction of lot sizes
2. Reduction of lot width
3. Increase lot depth to width ratio
4. Reduction of building setbacks
5. Increase lot coverage percentage
6. Increase retaining wall heights
7. Equestrian development standards waived

The following table is provided to illustrate the development standards applicable to the project and to identify the standards proposed to be waived as a part of the Density Bonus application:

**Table 3: Development Standards**

DEVELOPMENT STANDARD	RS DISTRICT STANDARD SP OVERLAY STANDARD	PROJECT	NOTES
<b>Lot Size (sf) (OZO Sec 1050)</b>	6,000 sf (min)	Lots Range From: 2,464 – 5,390 sf	SDBL Waiver
<b>Lot Width (OZO Sec 1050)</b>	65 feet (min)	Lot Widths Range From: 32ft – 59ft	SDBL Waiver
<b>Maximum Lot Coverage</b>	45% (max)	Coverage Ranges From: 20% - 54%	SDBL Waiver
<b>Lot Depth to Width Ratio (OZO Sec 1050(F))</b>	2.5 : 1	Various lots will exceed standard up to a ratio of 3.7:1	SDBL Waiver.
<b>Building Setbacks:</b>  Front Side Corner Side Rear	20 ft (min) 7.5 ft (min) 10 ft (min) 15 ft (min)	F: 6 – 20 ft S: 3 – 7.5 ft CS: 6 – 10 ft R: 5 – 15 ft	SDBL Waiver
<b>Building Height (OZO Sec 1050)</b>	25 ft (max)	25 ft	Complies with Code
<b>Parking (OZO Sec 3103)</b>	2-car garage per single-family homes < 2,500 sf.	2-car garages provided for each home	Complies with Code
<b>Landscaping (OZO Sec 1040(T))</b>	Minimum 50% of yard adjoining street shall be planting or landscape.	Landscaped front yard areas provided for each lot.	Complies with Code

DEVELOPMENT STANDARD	RS DISTRICT STANDARD SP OVERLAY STANDARD	PROJECT	NOTES
<b>Fences and Walls (OZO Sec 3040(D))</b>	Maximum height of a fence or wall, including retaining walls shall be 6'. Retaining walls over 4' in height shall be planted and irrigated.	retaining walls are not plantable / irrigated with wall sections exceeding 6' in height: <ul style="list-style-type: none"> <li>• Exterior Facing – up to 9'</li> <li>• Interior Facing – up to 12.1'</li> <li>• Interior SW Basin – up to 10'</li> </ul> *Decorative vinyl fencing up to 6' high and ornamental iron fencing up to 5' high may be added to the top of residential lots and basin walls, respectively.	SDBL Waiver
<b>Usable Open Space (OZO Sec 1040(Q))</b>	Total useable space shall be at least 300 sf per unit (300 * 83 = 24,900 sf)	Common Open Space (35,151 sf) and Rear Yard areas for each lot provide usable open space greatly exceeding minimum sf.	Complies with Code
<b>Equestrian Overlay District (OZO Article 28)</b>	Article presents specific criteria and development regulations for the EQ Equestrian Overlay District	Project lots are not designed to meet equestrian development regulations, consistent with other residential subdivisions on Guajome Lake Road.	SDBL Waiver.

\*SDBL: State Density Bonus Law

## ANALYSIS

### 1. Planning Considerations

**Guajome Lake Road Improvements:** As previously noted, the proposed project includes full improvement of Guajome Lake Road within the project boundaries, including pavement, curb, gutter, and sidewalk. Off-site improvements would also include paving Guajome Lake Road between Albright Street and the project boundary. The improvement would be necessary to ensure the project is served by an all-weather access road as required by the Fire Department. All work would occur within the existing right-of-way and would not disturb any land within Guajome Regional Park.

Community members have expressed concern about the safety of Guajome Lake Road given the fact it is one of the few remaining unpaved roads within the City. Speeding, pedestrian and equestrian safety, and vehicle accidents were some of the concerns raised by residents and users of Guajome Regional Park. Although not required pursuant to the EIR prepared for the proposed project, in response to these concerns staff coordinated with the project applicant to require traffic calming measures to be

implemented with the project. Measures may include speed cushions/tables and advisory signage.

Several comments were received requesting the City to improve the entire length of Guajome Lake Road between Albright Street to the Oceanside/Vista city limit. Staff acknowledges that future road improvements are warranted; however, there is no legal nexus requiring the completion of such additional off-site roadway improvements due to the proposed project. As such, the burden of installing such an improvement cannot be placed on the developer. The project would be required to pay its fair share of thoroughfare and traffic fees. Future roadway improvements to the southeast of the project site would require funding and implementation as a future City Capital Improvement Program project.

**Equestrian Overlay District:** On December 11, 1991, the City Council adopted the Equestrian Overlay District as Article 28 of the Zoning Ordinance and applied the EQ designation on properties in an 889 acre area generally located east of Guajome Regional Park and south of the San Luis Rey River to the southern and eastern City boundaries. The purpose of the EQ is to:

- a) Provide for recreational opportunities by establishing an equestrian trail network around Guajome Regional Park and the San Luis Rey River within the Guajome Neighborhood Planning Area.
- b) Provide appropriate design standards and criteria for the keeping and protection of horses on private property.
- c) Create, enhance, and protect the equestrian and rural atmosphere within the overlay area by integrating open space areas and urban areas with horse trails.
- d) Achieve a visually pleasing and compatible relationship between buildings and accessory structures, horse trails, open space and planting areas, and the natural environment by providing appropriate buffers between equestrian and non-equestrian uses.

The EQ Overlay District may be combined with any underlying base zoning district and establishes equestrian development standards for all development projects located within the EQ. Regulations include minimum horse yard areas, providing equestrian trails, locational requirements for structures, and buffers. The applicant is requesting the waiver of all EQ development standards with the SDBL request because compliance with such standards would physically preclude the development from constructing residential units at the proposed density and level of affordability.

While there are many horse properties in the general vicinity, a comprehensive equestrian trail network never materialized. Many residential subdivisions, such as Jefferies Ranch, developed under the EQ Overlay District standards and include horse trails along each parcel frontage. However, the trails lack regional connectivity and provide no direct trail access to Guajome Regional Park. A waiver of the EQ Overlay standards for the project would not result in impacts to existing equestrian facilities in the vicinity or lead to the removal of equestrian trails.

The project site is located directly adjacent to an equestrian property. The applicant has coordinated with the property to address concerns, including potential impacts on horses related to construction noise, dust, and odor as well as compatibility concerns with the future single-family homes. Large animals are not considered a sensitive receptor under CEQA. However, the EIR fully analyzed potential impacts of the project, including temporary impacts related to construction. Proposed mitigation includes provisions for dust control and noise necessary to reduce impacts to less than significant. As a condition of approval, the Covenants, Conditions, and Restrictions (CC&Rs) shall include a disclosure advising all future homeowners in the project that the site is located in an Equestrian Overlay District that grants surrounding property owners the right to own and keep horses on their properties. Residents acknowledge that equestrian areas are subject to potential noise, odor, and dust associated with horse properties.

**Guajome Regional Park:** Concerns were raised regarding potential impacts of the project to Guajome Regional Park. The park is owned and managed by the County of San Diego (“County”). During preliminary review of the application, City staff contacted the County of San Diego’s Park and Recreation Department to inform the County of the pending development application. The County also received the Notice of Preparation of the and EIR and the Notice of Availability of the Draft EIR. Comments were received from the County in response to the DEIR. City staff also conducted a meeting with County staff to discuss details and any concerns regarding the project, during which the County stated that the proposed project would not impact park operations and indicated that a trail connection to the project would not be requested. Additional responses to the County are provided in the Response to Comments section of the Final EIR.

## 2. General Plan Conformance

Staff has evaluated the project for consistency with the following goals, objectives, and policies of the City’s General Plan as follows:

### A. Land Use Element

#### Community Enhancement

Goal: The consistent, significant, long term preservation and improvement of the environment, values, aesthetics, character, and image of Oceanside as a safe, attractive, desirable, and well-balanced community.

##### 1.12 Land Use Compatibility

Objective: To minimize conflicts with adjacent or related land uses.

Policy A: Adequate setbacks, buffering, and/or innovative site design shall be required.

Policy B: The use of land shall not create negative visual impacts to surrounding land uses.

*The project is designed to fully comply with the Scenic Park Overlay, including maximum height and architectural design to maintain compatibility with Guajome Regional Park. All homes would not exceed the maximum height of 25 feet and exterior building finishes would be painted in earth tones or other colors that are compatible with the surrounding landscape. Approximately 6.92 acres of the 16.78-acre site would be designated as open space, including the preservation of an existing riparian corridor. Additionally, the subdivision design would align with the site's existing topography with finished pads that step down towards Guajome Lake Road.*

#### 1.16 Housing

Objective: To ensure that decent, safe and sanitary housing is available to all current and future residents of the community at a cost that is within the reach of the diverse economic segments of Oceanside.

Policy A: The City shall strive to maintain a reasonable balance between rental and ownership housing opportunities, between senior and family housing, and encourage a variety of individual choices of tenure, type and location of housing throughout Oceanside.

Policy B: The City shall strive to produce housing opportunities for decent and affordable housing in a pleasant environment for all of Oceanside's citizens.

Policy D: The City shall encourage development of a variety of housing opportunities, with special emphasis on providing:

1. A broad range of housing types, with varied levels of amenities and number of bedrooms.

Policy E: The City shall protect, encourage, and where feasible, provide housing opportunities for persons of low and moderate income.

*The proposed project would construct 83 detached single-family homes with four homes reserved as affordable to very low-income households. The small lot, detached housing product would provide ownership housing opportunities for a variety of household sizes.*

#### 1.21 Common Open Space

Objective: To provide and maintain common open areas for a wide range of uses.

Policy A: Common open space must be accessible and usable by potential users of the common open space.

*The proposed project would include a 35,151 square foot private recreational area that would be centrally located within the subdivision and would provide residents with a wide range of amenities including a playground, event lawn, a horseshoe pit, shade structures, and an outdoor kitchen with furniture.*

## 1.22 Landscaping

Objective: The enhancement of community and neighborhood identity through landscaping requirements.

Policy C: Drought-tolerant materials, including native California plant

Policy F: A buffer of landscaping shall be required between the build environment and lands left in a natural or open state.

*The proposed project would preserve 6.92 acres of open space, including avoidance of the northernmost portion of the site containing the riparian corridor. A landscaped manufactured slope would be located between the developed area of the parcel and the open space to provide a buffer. In addition, a wall and fencing would separate the open space and the development in efforts to deter residents from recreating in the avoidance areas. As previously noted, the northernmost portion of the site would be maintained as open space with a 50-foot-wide biological buffer and a 50-foot-wide planning buffer from the southern edge of the riparian forest and southern willow riparian forest.*

## 1.37 Guajome Regional Park Sphere of Influence / Special Management Area

Objective: To protect the valuable natural and cultural resources of Guajome Regional Park by insuring that future development in areas adjacent to or visible from Guajome Regional Park will be compatible with its recreation and scenic areas.

Policy D: Building exteriors shall have textured surfaces and extensive use of natural building materials for accents and treatments

Policy E: The colors of exterior surfaces shall be tones compatible with the surrounding landscape and not bright, glossy, or otherwise visually out of character with the natural setting.

Policy H: Structures shall be oriented to preserve views from Guajome Regional Park, the development, and surrounding properties.

*The proposed project was designed to adhere to the SP Overlay District, including grading limitations, view preservation, building height, and building materials and finishes consistent with the policies of the Guajome Regional Sphere of Influence/Special Management Area.*

## **B. Housing Element:**

The Regional Housing Needs Assessment (RHNA) for the Sixth Housing Element Cycle (2021-2029) estimates that the City of Oceanside will experience demand for 5,443 new dwelling units, including 1,268 very-low income units, over the next eight years. By contributing four affordable very-low income dwelling units to the City's existing housing stock, the project would help meet the City's projected housing demand. Additionally, the Housing Element establishes the following relevant goals, and policies applicable to the project.

**Goal 1:** Produce opportunities for decent and affordable housing for all of Oceanside's citizens.

**Policy 1.3:** Promote a high, stable rate of homeownership in Oceanside.

*The project would offer a for-sale housing product that would increase the rate of homeownership in Oceanside.*

**Goal 3:** Protect, encourage, and provide housing opportunities for persons of low and moderate income.

**Policy 3.7:** Encourage the disbursement of lower and moderate-income housing opportunities throughout all areas of the City.

*The project would reserve four single-family dwellings as affordable to those in the "very low-income" category, in an area of the City that otherwise may be unattainable for lower income households. All of the 83 dwelling units, including the affordable units, would include four bedrooms which would be ideal for the needs of large families.*

## **3. Subdivision Ordinance Conformance**

The proposed project is subject to the Subdivision Map Act and the Oceanside Subdivision Ordinance, *Article IV (Tentative Subdivision Maps)*. Pursuant to Section 401 of the Subdivision Ordinance, the Tentative Map (T22-00004) has been prepared in a manner acceptable to the Engineering Division and the Subdivision Map Act. Approval of a tentative map is subject to findings for approval as provided in Section 406 of the Subdivision Ordinance and in the attached Planning Commission Resolution (Attachment 1).

## **4. Zoning Ordinance**

The project site is zoned RS-SP-EQ (Single-Family Residential – Scenic Park Overlay – Equestrian Overlay) and is subject to the provisions of Articles 10, 22, and 28. As previously discussed, the project is reserving five percent of the units for very low-income households and qualifies for unlimited waivers from development standards in accordance with SDBL. Table 2 of this staff report outlines all applicable development standards and identifies all requested waivers or reductions of development standards. The project would comply with

all development standards of the RS District not specifically requested through SDBL waivers. In addition, the project would fully comply with all SP Overlay District standards, including height and design requirements to ensure compatibility with Guajome Regional Park.

The applicant is requesting a waiver of all EQ Overlay District development regulations provided in Article 28, Section 2806 of the Zoning Ordinance. Based on the proposed lot sizes necessary to construct the project at the permitted density under SDBL affordability, the project would not provide equestrian facilities onsite as typically required by the EQ District. A waiver of the EQ standards would not impact existing equestrian uses in the surrounding area. While a key goal of the EQ District is to require public equestrian trails on all lots within a new subdivision, the project site is not located in proximity to an existing trail network that would warrant a connection.

The proposed project would have a realized density of 6.67 du/acre which is slightly above the RS Districts maximum density of 5.9 du/acre (74 units). By reserving five percent of the units as affordable to very low-income households, the project receives a 20 percent density bonus (additional 15 units). The applicant is entitled to a maximum of 89 dwelling units on the 12.45 acre developable portion of the site; however, the project proposes the construction of 83 dwelling units.

Since the project does not contain slopes that have a natural gradient in excess of 20 percent with a minimal elevation differential of 25 feet, it is not subject to the Hillside Development Provisions provided in Article 30, Section 3039 of the Zoning Ordinance.

The project was also reviewed for compliance with the City's Climate Action Plan (CAP) Ordinances as provided in Article 30 of the Zoning Ordinance. The project would meet the following requirements:

Table 1: Climate Action Plan Compliance

<b>Development Standard</b>	<b>Required</b>	<b>Proposed</b>
Renewable Energy	25 units or more shall install renewable energy facilities to supply a minimum of 50% of forecasted energy demand	Photovoltaic will be installed on each home to provide 50% of forecasted energy demand
Permeable Surface Area	22% (minimum)	26% (103,161 sf)
Tree Canopy	12% (minimum)	15% (57,332 sf)

Article 43 of the City's Zoning Ordinance establishes specific findings which must be made in order for the City to approve a Development Plan. The required findings pursuant to Article 43, Section 4305 are provided in the Planning Commission Resolution No. 2025-P20 (Attachment 1).

## **ENVIRONMENTAL DETERMINATION**

In accordance with CEQA, an EIR was prepared for the proposed project (SCH No: 2022110028) due to potentially significant environmental impacts.

The Draft EIR was made available for public review between November 20, 2024 and January 10, 2025. During the public review period the City received 37 comment letters (including the following state agencies and organizations: Buena Vista Audubon Society, California Department of Fish and Wildlife, County of San Diego, and the San Diego County Archaeological Society). All comments received on the Draft EIR during the public review period were evaluated and responded to in the Response to Comments (RTC) section of the Final EIR. Minor revisions were made to the text of the Final EIR. The revisions do not constitute “significant new information” as set forth in CEQA Guidelines Section 15088.5; therefore, recirculation of the EIR is not required.

As described in the Draft EIR, the proposed project would result in no impact or less-than-significant impacts to aesthetics, agriculture and forestry resources, energy, greenhouse gas emissions, hazards and hazardous materials, hydrology and water quality, land use and planning, mineral resources, noise, population and housing, public services, recreation, traffic and circulation, Tribal Cultural Resources, utilities and service systems, and wildfire.

The Draft EIR identified impacts to be significant associated with air quality, biological resources, cultural resources, and geology and soils. However, the implementation of recommended mitigation measures would reduce impacts to a less-than-significant level for all identified topic areas.

In accordance with CEQA, an EIR must consider a range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project. The Draft EIR analyzed three project alternatives: No Project (No Build) Alternative, Reduced Development Footprint Alternative, and Townhome (Coastal Sage Scrub Impact Avoidance) Alternative.

The Draft EIR identified the Townhome Alternative as the environmentally superior alternative because it would provide a reduced level of impact in some environmental analysis areas, including biological resources, cultural resources, and geology and soils. The Townhome Alternative would significantly decrease the disturbance area on site from 9.86 acres under the proposed project to 5.98 acres. However, this alternative would introduce 90 town home units. Approximately 2.98 additional acres of open space would be incorporated north of the proposed disturbance limits, maintaining the natural state of the surrounding environment and enhancing the buffer area between development and coastal sage scrub. By preserving a larger portion of the site as open space and avoiding impacts to coastal sage scrub, a regulatory “take” of the federally listed coastal California gnatcatcher

would be avoided. As such, no take permits would be required from the U.S. Fish and Wildlife Service.

The Townhome Alternative would meet proposed project objectives with the exception of Objectives 1 and 4, because this alternative would not be consistent with the existing single-family land use and zoning designation of the site and surrounding land uses. Additionally, the alternative would not include any recreational amenities. The Townhome Alternative would also require similar waivers of development standards including minimum lot sizes, setbacks, and removal of the EQ Overlay District. Under this alternative, impacts to biological resources, cultural resources, and geology and soils would still remain as less than significant with mitigation incorporated, similar to the proposed project.

1. Ensure both visual and functional compatibility with other nearby land uses.
2. Provide new, high-quality for-sale residential units on an infill development site.
3. Maximize affordable and market-rate housing opportunities on a site that can be served by existing utilities, services, transit, and street access.
4. Provide new market-rate and affordable housing on a site that is consistent with the City's General Plan, Housing Element, Zoning Ordinance, and affordable housing objectives, as well as the state Density Bonus Law, to help satisfy the City's Regional Housing Needs Assessment current and future demand for housing.
5. Preserve the riparian corridor in the northern portion of the project site.

While the Townhome Alternative is considered the environmentally superior alternative, staff is recommending consideration of the proposed project. The Townhome Alternative conflicts with existing General Plan land use and zoning and would introduce a multi-family housing product and higher density in an area characterized by the detached single-family homes. As documented in the response to comments by the CA Department of Fish & Wildlife, the proposed project would adequately mitigate the loss of habitat by creating 6.64 acres of coastal sage scrub at the Quarry Creek mitigation site.

The Final EIR, including the Response to Comments and the Mitigation, Monitoring, and Reporting Program (MMRP) is provided as Attachment 1.

Staff has reviewed the proposed project and determined that with the implementation of project conditions and mitigation measures contained within the MMRP, no significant impacts are anticipated. Therefore, staff recommends that the Planning Commission certify the FEIR with the Findings of Fact and MMRP.

## **PUBLIC NOTIFICATION**

In accordance with City Council Policy 300-14 "Enhanced Notification Program," a "Notice of Application" sign was posted at the project entrance and has been maintained on site. The City also mailed a "Notice of Application" letter to all property owners of record within a 1,500-foot radius of the subject property, tenants within a 100-foot radius of the subject property, and other interested parties.

Additionally, the applicant held two community outreach meetings at the El Corazon Senior Center to solicit input from the public. The first meeting was held on May 23, 2024. There were approximately 80 people, including City staff and the project team, in attendance at this meeting. A second meeting was held on December 17, 2024 and had approximately 30 people in attendance. As detailed in the Community Outreach Report (Attachment 6), the applicant conducted additional outreach throughout the application review process, which included 28 meetings and 7 phone calls with a total of 49 neighbors and community members.

Legal notice was published in the San Diego Union Times and notices of the Planning Commission Public Hearing were sent to property owners of record within a 1,500-foot radius of the subject property, tenants within a 100-foot radius of the subject property, individuals and/or organizations requesting notification, the applicant, and other interested parties.

It should be noted that the applicant has complied with all the requirements of the City's Enhanced Notification Program Policy.

A significant amount of correspondence has been received regarding the proposed project. Letters and emails addressed to staff, the City Council, and the Planning Commission (20 letters) are included as an attachment to the staff report (Attachment 6). Primary concerns raised by opponents include site access, increased traffic, emergency ingress/egress, preservation of biological resources, soil conditions, noise and air quality.

## SUMMARY

Staff finds that the proposed project is consistent with the applicable objectives and policies of the City's General Plan, as well as the applicable standards of the City's Zoning Ordinance.

Therefore, staff recommends that the Planning Commission by motion:

- 1) Certify the EIR and adopt the associated findings of fact and mitigation monitoring and reporting program, by adopting Planning Commission Resolution No. 2025-P23; and
- 2) Approve Tentative Map (T22-00004), Development Plan (D22-00009), and Density Bonus (DB22-00005) by adopting Planning Commission Resolution No. 2025-P20 with findings and conditions of approval attached herein.

PREPARED BY:

  
Marlyn Baeza

Principal Planner

SUBMITTED BY:

  
Darlene Nicandro

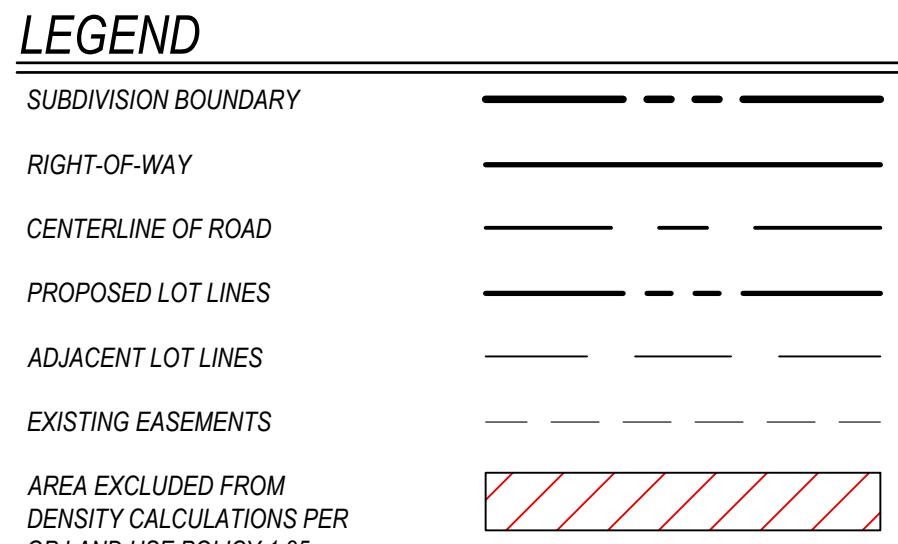
Development Services Director

Attachments:

1. Planning Commission Resolution No. 2025- P23 Certifying the Final EIR
2. Planning Commission Resolution No. 2025- P20 approving the Project
3. Final Environmental Impact Report (Including Mitigation Monitoring and Reporting Program, Technical Appendices, and Response to Comments) online at  
<https://www.ci.oceanside.ca.us/government/development-services/planning/ceqa/guajome-lake-road-subdivision-eir>
4. Project Plans
5. Other Attachments
  - a. Description and Justification
  - b. Community Outreach Report
6. Public correspondence regarding the project

# TENTATIVE MAP DENSITY BONUS (UNASSIGNED) GUAJOME LAKE RD

## CITY OF OCEANSIDE T22-00004 / D22-00009 / DB22-00005



### EXISTING EASEMENT INFORMATION

TITLE COMMITMENT PROVIDED BY CHICAGO TITLE COMPANY AS ORDER NUMBER 00163216-987-001-D4, DATED APRIL 26, 2022.

ITEMS 1-4 ARE NON-MAPPING ITEMS AND OMITTED HEREON.

⑤ EASEMENT FOR INGRESS EGRESS, PUBLIC UTILITIES AND INCIDENTAL RIGHTS, IS NOT PLOTTABLE AND IS NOT SHOWN HEREON **TO REMAIN**.

⑥ EASEMENT IN FAVOR OF THE PUBLIC OVER ANY EXISTING ROADS LYING WITHIN SAID LAND, IS NOT PLOTTABLE AND IS NOT SHOWN HEREON **TO REMAIN**.

⑦ EASEMENT GRANTED TO THE COUNTY OF SAN DIEGO FOR PUBLIC ROAD PURPOSES, RECORDED JANUARY 3, 1900, BOOK 257, PAGE 493, OFFICIAL RECORDS **TO REMAIN**.

⑧ MATTERS CONTAINED IN COVENANTS, CONDITIONS AND RESTRICTIONS, RECORDED NOVEMBER 2, 1946, BOOK 2278, PAGE 214, OFFICIAL RECORDS. NON MAPPING AND OMITTED HEREON.

⑨ DISCREPANCIES AND ANY OTHER MATTERS SHOWN ON ROS 2272

ITEMS 10-11 INTENTIONALLY DELETED.

⑫ MATTERS CONTAINED IN THAT CERTAIN DOCUMENT ENTITLED "OPTION AGREEMENT", RECORDED APRIL 9, 2008, AS DOC. NO. 2008-0186802, OFFICIAL RECORDS.

⑬ MATTERS CONTAINED IN COVENANTS, CONDITIONS AND RESTRICTIONS, RECORDED SEPTEMBER 24, 2008, AS DOC. NO. 2008-0505519, OFFICIAL RECORDS.

ITEM 14 INTENTIONALLY DELETED.

ITEMS 15-16 NON MAPPING ITEMS AND OMITTED HEREON.

### STATE DENSITY BONUS LAW CONCESSIONS / INCENTIVES

1. THE PROJECT IS ALLOWED ONE (1) INCENTIVES / CONCESSIONS FOR INCLUDING AT LEAST 5 PERCENT OF TOTAL UNITS FOR "VERY LOW" INCOME HOUSEHOLDS PURSUANT TO STATE DENSITY BONUS LAW (SDBL), CA GOVERNMENT CODE 65915. AT THIS MOMENT, THE PROJECT DOES NOT REQUEST TO USE ANY INCENTIVES.

2. WAIVER OF "DEVELOPMENT STANDARDS" AS IDENTIFIED IN SECTION 1050 OF THE CITY OF OCEANSIDE COMPREHENSIVE ZONING ORDINANCE, ARTICLE 10 RESIDENTIAL DISTRICTS, INCLUDING NET LOT AREA, LOT WIDTH, AND FRONT, SIDE AND REAR YARD SETBACKS

### SETBACKS

FRONT YARD	(FYSB)	20' (PER ZONE)	*SEE TABLE ON SHEET 2 FOR SETBACKS
REAR YARD	(RYSB)	15' (PER ZONE)	
STREET SIDE YARD SETBACK	(SYSB)	7.5' (PER ZONE)	REQUESTED WAIVERS OF DEVELOPMENT STANDARDS UNDER STATE DENSITY BONUS LAW
INTERIOR SIDE YARD	(SYSB)	7.5' (PER ZONE)	

### DISTURBED AREA CALCULATIONS

TOTAL EXISTING GROSS SITE AREA: 731,283 SF / 16.788 AC

\*AREA DISTURBED BY PROJECT: 458,900 SF / 10.535 AC

\*INCLUDING RIGHT-OF-WAY



VICINITY MAP  
NOT TO SCALE

### SHEET INDEX

SHEET 1-	TENTATIVE MAP TITLE SHEET AND NOTES
SHEET 2-	PROPOSED LOT CONFIGURATION AND LOT GEOMETRY
SHEET 3-	PRELIMINARY GRADING PLAN
SHEET 4-	PRELIMINARY GRADING PLAN
SHEET 5-	PRELIMINARY GRADING PLAN
SHEET 6-	PRELIMINARY GRADING PLAN
SHEET 7-	PRELIMINARY UTILITY PLAN
SHEET 8-	PRELIMINARY PARKING EXHIBIT
SHEET 9-	SECTIONS AND DETAILS
SHEET 10-	SECTION AND DETAILS
SHEET 11-	SECTION AND DETAILS

### DENSITY CALCULATIONS

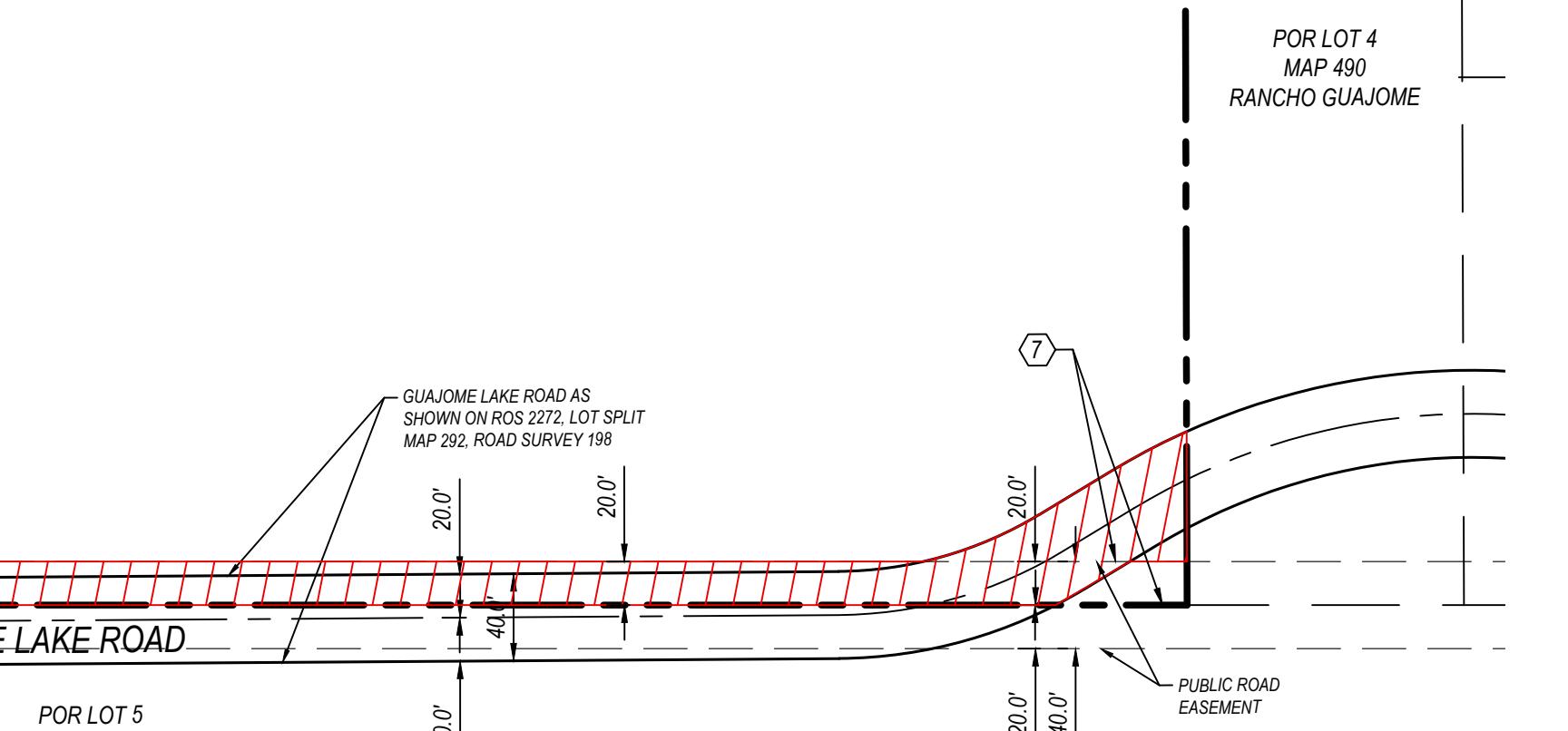
GROSS ACREAGE	16.788 AC (731,283 SF)	PER SECTION 1010 OF CITY OF OCEANSIDE ZONING ORDINANCE, MAXIMUM POTENTIAL DENSITY IN SFD-R IS 5.9 DWELLING UNITS PER GROSS ACRE; PUBLIC ROW AND RIPARIAN AREAS EXCLUDED FROM THE DENSITY CALCULATION BELOW
LESS PUBLIC ROAD EASEMENTS	0.569 AC (24,784 SF)	
LESS RIPARIAN AREAS	3,770 AC (164,109 SF)	

LAND USE DESIGN. (SFD-R)	AREA (AC)	DENSITY (DU)	DWELLING UNITS
BASE DENSITY PER DENSITY BONUS	=	74.0	
5% OF UNITS AS "VERY LOW" AFFORDABLE	=	4.0	TOTAL UNITS PER DENSITY BONUS ROUND UP = 89 PROPOSED MARKET RATE UNITS = 79 PROPOSED "VERY LOW" AFFORDABLE UNITS = 4 TOTAL PROPOSED UNITS = 83
20% DENSITY BONUS PER GOVT. CODE 65915 FOR 15% "VERY LOW" AFFORDABLE UNITS	=	88.8	

SUBJECT PROPERTY  
AREA = 731,283 SF  
= 16.788 AC (GROSS)

PARCEL 2 PM 9787

7



### IMPERVIOUS AREA CALCULATIONS

TOTAL EXISTING GROSS SITE AREA:	731,283 SF (16.788 AC)
EXISTING IMPERVIOUS AREA (ONSITE):	~6,500 SF
PROPOSED IMPERVIOUS AREA HEREON (OVERALL):	281,300 SF (6.46 AC)

### ZONING INFORMATION

GENERAL PLAN DESIGNATION:	SFD-R (SINGLE-FAMILY DETACHED RESIDENTIAL)
PRESENT ZONING REQUIREMENTS:	RS-SP-EQ (SF RESIDENTIAL, SCENIC PARK OVERLAY, EQUESTRIAN OVERLAY)
PRESENT USE:	RESIDENTIAL
PROPOSED USE:	RESIDENTIAL

PROPOSED PARALLEL ON STREET PARKING:	10.0
PROPOSED ENCLOSED OFF STREET PARKING:	2.0 / UNIT (2-CAR GARAGE)
PROPOSED UNENCLOSED OFF STREET PARKING:	2.0 / UNIT (EXCLUDING LOT 19) (18'-FT WIDE X 18'-FT DEEP DWY)

### TOPOGRAPHY

TOPOGRAPHY OBTAINED BY FIELD SURVEY  
TOPOGRAPHY OBTAINED ON: JANUARY 25, 2022  
PREPARED BY: PASCO, LARET, SUITER & ASSOCIATES

### OWNER INFORMATION

WE HEREBY CERTIFY THAT WE ARE RECORDED OWNERS OF THE PROPERTY SHOWN ON THE ATTACHED TENTATIVE MAP, DEVELOPMENT PLAN, AND DENSITY BONUS ENTITLEMENT APPLICATION AND THAT SAID APPLICATION SHOWS THE ENTIRE CONTIGUOUS OWNERSHIP. I UNDERSTAND THAT PROPERTY IS CONSIDER CONTIGUOUS EVEN IF IT IS SEPARATED BY ROADS, STREETS, UTILITY EASEMENTS, OR RAILROAD RIGHT OF WAYS.

OWNER: SAM DIMENSTEIN AND SANDRA DIMENSTEIN  
CO-TRUSTEES OF THE DIMENSTEIN FAMILY TRUST

### SUBDIVIDER INFORMATION

THE SUBDIVIDER AGREES TO DEFEND, INDEMNIFY, AND HOLD HARMLESS THE CITY OF OCEANSIDE AND ITS AGENTS, OFFICERS, AND EMPLOYEES, TO ATTACK, SET ASIDE, VOID, OR ANNUAL AN APPROVAL FROM THE CITY OF OCEANSIDE CONCERNING THE SUBDIVISION WHEN SUCH ACTION IS BROUGHT WITHIN THE TIME PERIOD SPECIFIED IN GOV. CODE SECTION 66499.37. THIS CERTIFICATION IS CONDITIONED UPON THE CITY OF OCEAN SIDE PROVIDING PROMPT NOTICE TO THE SUBDIVIDER AS PROVIDED BY THE ACT (GOV. CODE 66474.9).

SUBDIVIDER: KEVIN DUNN FOR: RINCON HOMES INC.  
5315 AVENIDA ENCINAS SUITE 200  
CARLSBAD, CA 92008  
PH: (888) 357-3553

### ABBREVIATED LEGAL DESCRIPTION

PARCEL 1 (OWNED-IN-FEE):  
THAT PORTION OF LOT 4 OF THE RANCHO GUAJOME, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AS SHOWN ON THE PARTITION MAP MADE AND FILED IN THE OFFICE OF THE COUNTY CLERK OF SAID COUNTY, ATTACHED TO AND MADE A PART OF THE REFFEE'S REPORT IN ACTION IN PARTITION SUPERIOR COURT OF SAN DIEGO COUNTY, NO. 19201, MORE FULLY DESCRIBED IN GRANT DEED RECORD IN THE OFFICE OF THE SAN DIEGO COUNTY RECORDER AS DOCUMENT NO. 2010-0110502

② PARCEL 2 (EASEMENT):  
A PERPETUAL EASEMENT AND RIGHT OF WAY UTILITY PURPOSES, INCLUDING BUT NOT LIMITED TO SEWER CONNECTION AND APPURTENANCES, ON OVER AND ABOVE TEH SOUTHEASTERLY 15 FEET OF LOT AS DESCRIBED IN CERTIFICATE OF COMPLIANCE RECORDED OCTOBER 26, 2011 AS FILE NO. 2001-0708137 OF OFFICIAL RECORDS. A PORTION OF LOTS 13 AND 14 OF CREEK NO. 4E, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO MAP THEREOF NO. 10548, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY, DECEMBER 28, 1982 (MORE FULLY DESCRIBED IN GRANT DEED RECORDED IN THE OFFICE OF THE SAN DIEGO COUNTY RECORDER AS DOCUMENT NO. 2010-0110502)

### BENCHMARK

ELEVATIONS SHOWN HEREON ARE BASED ON 2.5" DISK, IN ASPHALT OF ALBRIGHT STREET, AS SHOWN ON RECORD OF SURVEY 21787  
POINT NO: 1068  
ELEVATION: 202.66 (NAVD 88)

### BASIS OF BEARINGS

THE BASIS OF BEARING FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM, NAD 83 (CCS83) EPOCH 2011, ZONE 6, AS DETERMINED LOCALLY BY A LINE BETWEEN FIRST ORDER CONTROL STATIONS 1067 BEING A GRID BEARING OF N 22°52' E AS DERIVED FROM GEODETIC VALUES SHOWN ON RECORD OF SURVEY 21787, CITY OF OCEANSIDE SURVEY CONTROL, FILED ON AUGUST 21, 2014 AS FILE NUMBER 2014-036129 IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

### EARTHWORK / PROJECT GRADING

CUT:	84,500 CY	MAX CUT HEIGHT: 24 FT
FILL:	17,500 CY	
EXPORT:	67,000 CY	
REMEDIATION:	45,725 CY	

\*ESTIMATES DOES NOT INCLUDE STRIPPING OR UTILITY AND TRENCH VOLUMES, IF REQUIRED BY SITE CONDITIONS

CONTRACTOR SHALL SELF SATISFY THAT ESTIMATES ARE CORRECT PRIOR TO COMMENCEMENT OF WORK

EARTHWORK QUANTITIES ARE ESTIMATED FOR PERMIT PURPOSES ONLY. THESE QUANTITIES ARE APPROXIMATE AND ARE SUBJECT TO CHANGE BASED ON FINAL ENGINEER DESIGN AND DETAILING

### UTILITIES

WATER	CITY OF OCEANSIDE WATER UTILITIES
FIRE	OCEANSIDE FIRE PROTECTION DISTRICT
SEWER	CITY OF OCEANSIDE
ELEMENTARY SCHOOL	OCEANSIDE UNIFIED SCHOOL DISTRICT
HIGH SCHOOL	OCEANSIDE HIGH SCHOOL DISTRICT

### FEMA ZONE

ZONE:	ZONE
-------	------

## LEGEND

PROPERTY BOUNDARY	
RIGHT-OF-WAY	
EXISTING CENTERLINE OF ROAD PER ROS 2272, MAP 292	
PROPOSED CENTERLINE OF ROAD	
PROPOSED LOT LINES	
ADJACENT LOT LINES	
EXISTING EASEMENTS	
PROPOSED EASEMENTS	

## **PROPOSED EASEMENT INFORMATION**

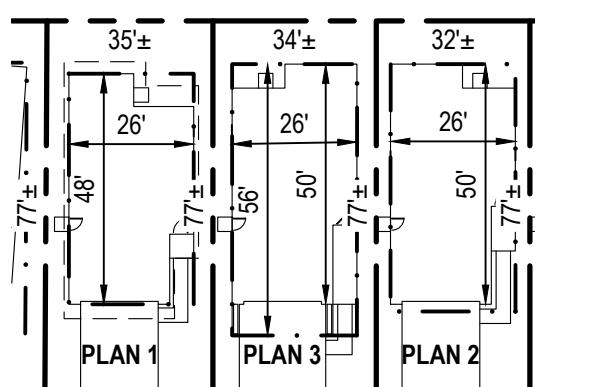
- 1 PROPOSED PUBLIC ROAD DEDICATION TO THE CITY OF OCEANSIDE FOR PUBLIC STREET RIGHT-OF-WAY
- 2 PROPOSED PEDESTRIAN ACCESS EASEMENT FOR THE BENEFIT OF ALL LOTS
- 3 PROPOSED GENERAL UTILITY EASEMENT
- 4 PROPOSED PUBLIC WATER EASEMENT TO THE CITY OF OCEANSIDE
- 5 PROPOSED PRIVATE SEWER EASEMENT FOR THE BENEFIT OF ALL LOTS
- 6 PROPOSED DRAINAGE EASEMENT
- 7 PROPOSED PEDESTRIAN ACCESS EASEMENT FOR THE BENEFIT OF THE CITY OF OCEANSIDE
- 8 PROPOSED PRIVATE ACCESS EASEMENT

## *EXISTING EASEMENT INFORMATION*

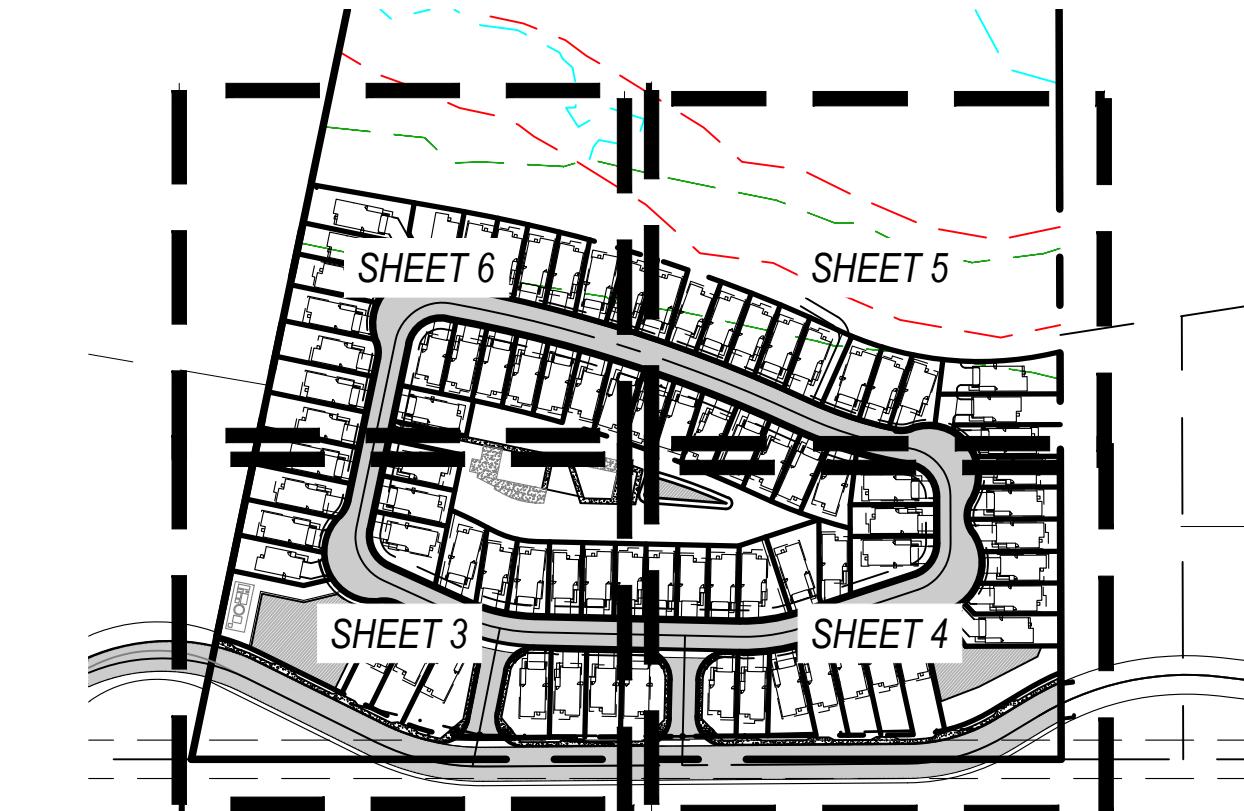
⑦ EASEMENT GRANTED TO THE COUNTY OF SAN DIEGO FOR PUBLIC ROAD PURPOSES, RECORDED JANUARY 3, 1900, BOOK 257, PAGE 493, OFFICIAL RECORDS **TO REMAIN.**

*\*SEE SHEET 1 FOR PLOTTING OF ALL EXISTING EASEMENTS*

ZONE REQUIREMENTS	RS
NET LOT AREA (SF)	6,000
LOT WIDTH (FT)	65 FT
FRONT YARD SETBACK	20 FT
CORNER SIDE YARD SETBACK	10 FT
SIDE YARD SETBACK	7.5 FT
REAR YARD SETBACK	15 FT
**BUILDING FOOTPRINT	-
LOT COVERAGE	45%
PLAN TYPE	-



## *DETAIL - TYPICAL LOT LAYOUT*



## *PLAN VIEW - PROJECT KEY MAP*

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SCALE: NOT TO SCALE

---

1 RANCHO GUAJOME  
PLAN VIEW - LOT LAYOUT CONFIGURATION

PROPOSED CENTERLINE GUAJOME  
LAKE ROAD PER CITY OF OCEANSIDE  
PUBLIC ROAD STANDARDS

0 40 80 120

ZONE REQUIREMENTS		RS	LOT1	LOT2	LOT3	LOT4	LOT5	LOT6	LOT7	LOT8	LOT9	LOT10	LOT11	LOT12	LOT13	LOT14	LOT15	LOT16	LOT17	LOT18	LOT19	LOT20	LOT21	LOT22	LOT23	LOT24	LOT25	LOT26	LOT27	LOT28	LOT29	LOT30	LOT31	LOT32	LOT33	LOT34	LOT35	LOT36	LOT37	LOT38	LOT39	LOT40	LOT41	ZONE REQUIREMENTS	RS	LOT1	LOT2	LOT3	LOT4	LOT5	LOT6	LOT7	LOT8	LOT9	LOT10	LOT11	LOT12	LOT13	LOT14	LOT15	LOT16	LOT17	LOT18	LOT19	LOT20	LOT21	LOT22	LOT23	LOT24	LOT25	LOT26	LOT27	LOT28	LOT29	LOT30	LOT31	LOT32	LOT33	LOT34	LOT35	LOT36	LOT37	LOT38	LOT39	LOT40	LOT41
NET LOT AREA (SF)	6,000	4,299	3,332	3,372	4,527	5,380	4,032	3,560	3,567	3,447	3,887	3,937	3,938	3,939	3,901	3,498	3,206	3,637	5,390	3,160	2,734	2,528	2,648	2,571	2,932	2,607	2,683	2,701	2,647	2,569	2,650	3,575	2,981	2,573	2,906	4,522	4,136	3,825	2,724	3,064	2,983	2,695																																												
LOT WIDTH (FT)	65	51	37	37	51	59	37	33	38	38	38	38	38	38	38	38	33	33	42	45	35	32	34	33	37	33	34	34	34	34	34	38	33	39	37	32	33	33	34	34	33																																													
FRONT YARD SETBACK	20	10	18	16	13	13	17	11	20	11	12	11	18	11	17	11	15	20	20	13	10	16	11	16	11	16	11	11	16	14	20	14	9	17	10	14	17																																																	
CORNER YARD SET BACK	10	10	-	-	10	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																																
SIDE YARD SETBACK	7.5	3	6/5	6/5	5	3	6/5	4/3	6/6	3/7.5	3/7.5	3/7.5	3/7.5	3/7.5	3/7.5	3/7.5	3/4	3/4	5/7.5	5/3	5/3	3/3	4/3	3/3	6/4	3/3	4/3	3/3	4/3	3/3	3/3	3/3	3/3	4/3	3/3	4/3	4/3	4/3	4/3																																															
REAR YARD SETBACK	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	15	10	10	10	10	9	10	10	10	12	10	10	15	15	15	15	15	15	15	15	15	15																																													
**BUILDING FOOTPRINT	-	1,322	1,090	1,175	1,322	1,322	1,090	1,322	1,090	1,322	1,175	1,322	1,090	1,322	1,090	1,322	1,175	1,322	1,090	1,322	1,175	1,322	1,175	1,322	1,090	1,322	1,175	1,322	1,175	1,322	1,090	1,322	1,175	1,322	1,090	1,322	1,175	1,322	1,090																																															
LOT COVERAGE	45%	31%	32%	34%	29%	28%	30%	41%	31%	38%	30%	34%	28%	34%	28%	38%	37%	36%	25%	34%	48%	46%	46%	50%	45%	45%	50%	46%	50%	33%	44%	46%	38%	29%	28%	40%	40%	33%	39%	40%																																														
PLAN TYPE	3	1	2	3	3	1	3	1	3	2	3	1	3	1	3	2	3	2	3	1	3	2	3	2	3	2	3	1	3	2	3	1	3	2	1																																																			
ZONE REQUIREMENTS		LOT42	LOT43	LOT44	LOT45	LOT46	LOT47	LOT48	LOT49	LOT50	LOT51	LOT52	LOT53	LOT54	LOT55	LOT56	LOT57	LOT58	LOT59	LOT60	LOT61	LOT62	LOT63	LOT64	LOT65	LOT66	LOT67	LOT68	LOT69	LOT70	LOT71	LOT72	LOT73	LOT74	LOT75	LOT76	LOT77	LOT78	LOT79	LOT80	LOT81	LOT82	LOT83																																											
NET LOT AREA (SF)	2,971	3,857	4,194	3,959	3,618	3,278	3,553	2,846	3,345	3,446	2,464	2,464	2,541	2,464	2,618	2,678	2,950	2,582	3,466	4,043	2,926	2,926	2,927	2,995	2,539	2,444	2,562	2,554	2,582	2,610	2,581	2,468	2,701	2,865	2,616	2,475	3,170	3,629	2,937	3,394	2,538	3,765																																												
LOT WIDTH (FT)	32	33	33	33	33	33	40	33	41	43	32	32	33	32	34	35	38	33	43	54	38	38	38	42	33	32	33	33	34	34	32	35	37	34	32	41	45	33	46	33	44																																													
FRONT YARD SETBACK	13	20	9	16	11	16	11	16	15	16	11	18	16	16	11	18	11	17	11	18	11	18	11	16	11	16	11	16	11	16	11	16	11	17	11	18	10	16	6	16	11																																													
CORNER YARD SET BACK	-	-	-	-	-	-	-	-	-	10	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-																																														
SIDE YARD SETBACK	3/3	4/3	4/3	4/3	4/3	4/3	4/6	4/3	3	3/6	3/3	3/3	4/3	3/3	4/4	4/4	4/4	3/4	3/4	7.5/3	7.5	7.5/3	7.5/3	7.5/4	3	4/3	3/3	3/4	3/3	3/4	3/3	3/4	3/3	3/4	3/3	3/4	7.5/3	3/4	3/4	3/4	4/4	4/4	3/3	7.5/5																																										
REAR YARD SETBACK	15	15	15	15	15	15	15	15	15	15	8	9	11	9	9	11	8	10	6	8	9	11	9	8	9	8	8	7	10	8	8	7	9	8	10	11	15	13	9	15																																														
**BUILDING FOOTPRINT	1,175	1,322	1,322	1,175	1,322	1,175	1,322	1,175	1,090	1,175	1,322	1,090	1,322	1,175	1,322	1,090	1,322	1,090	1,322	1,175	1,322	1,090	1,322	1,090	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175	1,322	1,175																																											
LOT COVERAGE	40%	20%	33%	30%	37%	36%	37%	41%	33%	34%	54%	44%	44%	52%	48%	50%	41%	45%	42%	38%	29%	45%	37%	45%	36%	46%	54%	44%	46%	42%	42%	54%	44%	46%	42%	53%	34%	36%	40%	33%	43%	35%																																												
PLAN TYPE	2	3	3	2	3	2	3	2	1	2	3	1	3	2	3	1	3	1	3	2	3	1	3	1	3	2	3	1	3	2	3	1	3	2	3	1	3	2	3	1	3	2	3																																											

**SEE SHEET 7 FOR CONTINUATION**

PARCEL 2  
PM 9787

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## SEE SHEET 7 FOR CONTINUATION

## LEGEND

PROPERTY LINE			
RIGHT-OF-WAY			
CENTERLINE OF ROAD			
PROPOSED LOT LINES			
ADJACENT LOT LINES			
PROPOSED EASEMENTS			
PROPOSED SETBACKS			
PROPOSED LIMIT OF GRADING			
PROPOSED CONTOUR			
EXISTING CONTOUR			
PROPOSED 6" CURB & GUTTER			
PROPOSED 6" CURB			
PROPOSED HARDCAPE			
PROPOSED MASONRY RETAINING WALL			
PROPOSED BMP			
EXISTING WATER MAIN (SIZE PER PLAN)			
EXISTING SEWER MAIN (SIZE PER PLAN)			
EXISTING STORM DRAIN (SIZE PER PLAN)			
EXISTING GAS MAIN			
PROPOSED SEWER MANHOLE			
PROPOSED 4" PVC STORM DRAIN			
PROPOSED 18" RCP PRIVATE STORM DRAIN			
PROPOSED 12" AREA DRAIN			
PROPOSED 8" PRIVATE PVC SEWER MAIN			
PROPOSED 8" PVC PUBLIC WATER MAIN			

## CONSTRUCTION NOTES

- ① EXISTING SURVEY MONUMENT TO BE PROTECTED IN PLACE; CORNER RECORD OR RECORD OF SURVEY TO BE FILED WITH THE COUNTY IF DISTURBED OR DESTROYED
- ② PROPOSED 6" PCC CURB PER SDRSD G-1 W/ CLASS II BASE
- ③ PROPOSED 6" PCC CURB & GUTTER PER SDRSD G-2 W/ CLASS II BASE
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- ⑥ PROPOSED WING-TYPE PCC HEADWALL WITH 4'X4' RIP RAP ENERGY DISSIPATER PER SDRSD D-34 & 40; ROCK CLASS = No.2 BACKING T = 1.1 FT
- ⑦ PROPOSED 36" X 36" BROOKS BOX OUTLET STRUCTURE; SEE BIOFILTRATION BASIN DETAIL SHEET 11
- ⑧ PROPOSED MASONRY RETAINING WALL SYSTEM; DESIGN BY OTHERS
- ⑨ PROPOSED 4" AC PAVEMENT OVER 6" CLASS II AB OR PER GEOTECH RECOMMENDATIONS
- ⑩ PROPOSED PCC BROW DITCH PER SDRSD D-75, TYPE B
- ⑪ PROPOSED PCC CROSS GUTTER PER SDRSD G-12
- ⑫ SIGHT DISTANCE VIEW CORRIDOR PER CALTRANS HIGHWAY DESIGN MANUAL AND CITY OF OCEANSIDE STANDARDS
- ⑬ PROPOSED MASONRY RETAINING WALL PER SDRSD C-03
- ⑭ PROPOSED PCC PED RAMP PER SDRSD G-27
- ⑮ PROPOSED PCC PED RAMP PER SDRSD G-29
- ⑯ PROPOSED TREE WELL BMP (4' X 10'); SEE DETAIL SHEET 11
- ⑰ PROPOSED ROLLED / MOUNTABLE PCC CURB AND GUTTER PER SDRSD G-04A W/ CLASS II BASE
- ⑱ PROPOSED RIP RAP ENERGY DISSIPATER PER SDRSD D-40; ROCK CLASS = 2 TON T=5.4'
- ⑲ PROPOSED 6" PCC ROLLED CURB PER SDRSD G-04A W/ CLASS II BASE; MODIFIED WITHOUT GUTTER

## CONSTRUCTION SITE NOTES

---

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PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER OF  
RECORD OF DISCREPANCIES UPON DISCOVERY.

## EXISTING EASEMENT NOTES

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SEE SHEET 1 FOR PLOTTING AND SUMMARY OF EXISTING EASEMENTS

## PROPOSED EASEMENT NOTES

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SEE SHEET 2 FOR SUMMARY OF PROPOSED EASEMENTS

# **PLAN VIEW - PRELIMINARY GRADING PLAN**

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SCALE: 1" = 20'

0                    20                    40

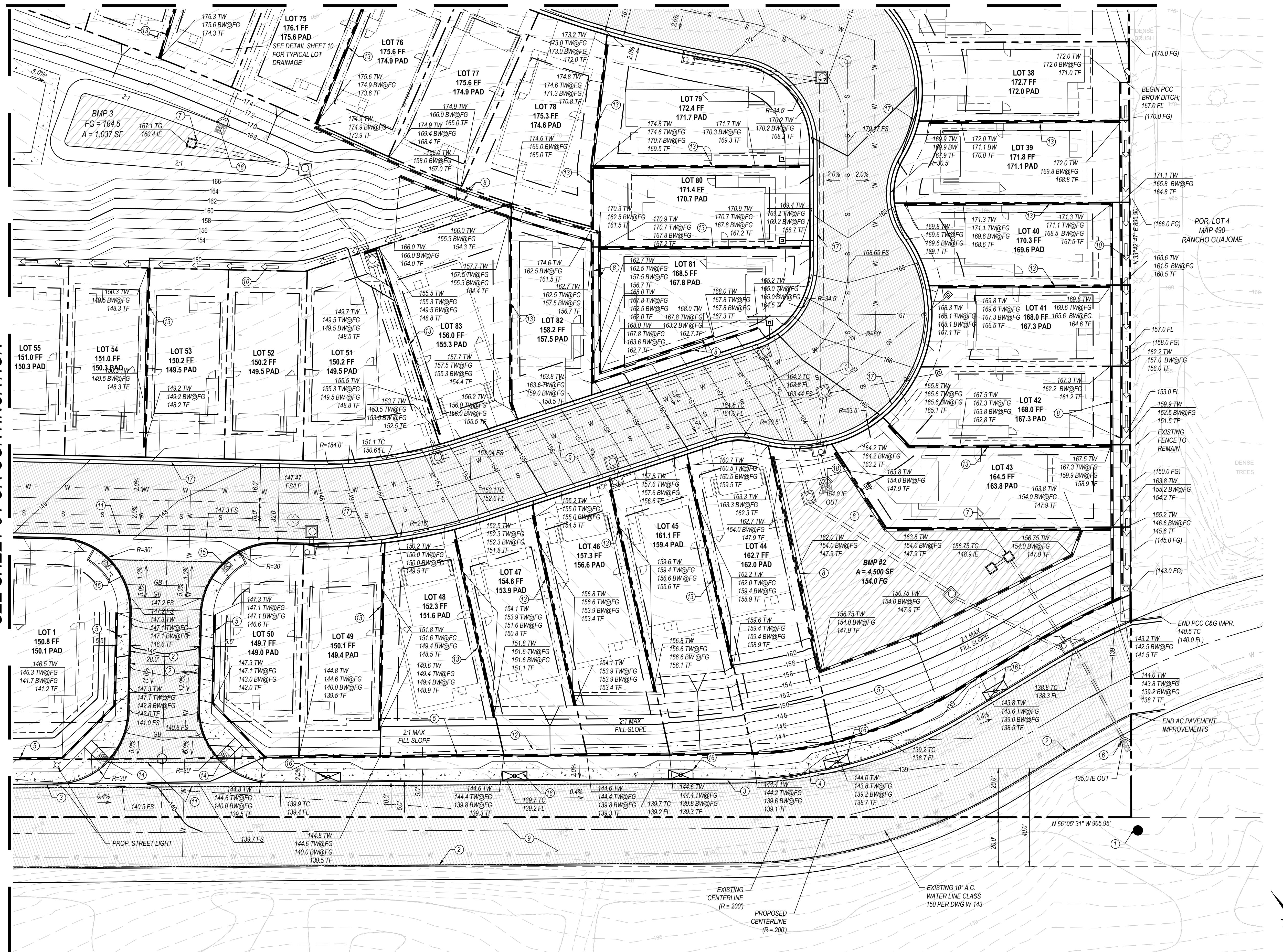
GRAPHIC SCALE: 1" = 20'

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REGISTERED PROFESSIONAL ENGINEER  
TYLER G. LAWSON  
No. 80356  
Exp. 12/31/24  
CIVIL  
STATE OF CALIFORNIA

**SEE SHEET 3 FOR CONTINUATION**



# PLAN VIEW - PRELIMINARY GRADING PLAN

---

SCALE: 1" = 20'

## LEGEND

PROPERTY LINE	_____	—	—	_____
RIGHT-OF-WAY	_____			
CENTERLINE OF ROAD	_____	—	—	_____
PROPOSED LOT LINES	_____	—	—	_____
ADJACENT LOT LINES	_____	—	—	_____
PROPOSED EASEMENTS	_____	—	—	_____
PROPOSED SETBACKS	_____	—	—	_____
PROPOSED LIMIT OF GRADING	_____	▼	▼	▼
PROPOSED CONTOUR	_____	140	_____	
EXISTING CONTOUR	_____	140	_____	
PROPOSED 6" CURB & GUTTER	_____			
PROPOSED 6" CURB	_____			
PROPOSED HARDSCAPE	_____	△	△	△
PROPOSED MASONRY RETAINING WALL	_____			
PROPOSED BMP	_____			
EXISTING WATER MAIN (SIZE PER PLAN)	_____	W	_____	W
EXISTING SEWER MAIN (SIZE PER PLAN)	_____	S	_____	S
EXISTING STORM DRAIN (SIZE PER PLAN)	_____			
EXISTING GAS MAIN	_____	G	_____	G
PROPOSED SEWER MANHOLE	_____	○		
PROPOSED 4" PVC STORM DRAIN	_____	SD	_____	SD
PROPOSED 18" RCP PRIVATE STORM DRAIN	_____			
PROPOSED 12" AREA DRAIN	_____	□		
PROPOSED 8" PRIVATE PVC SEWER MAIN	_____	S	_____	S
PROPOSED 8" PVC PUBLIC WATER MAIN	_____	W	_____	W

## CONSTRUCTION NOTES

- ① EXISTING SURVEY MONUMENT TO BE PROTECTED IN PLACE; CORNER RECORD OR RECORD OF SURVEY TO BE FILED WITH THE COUNTY IF DISTURBED OR DESTROYED
- ② PROPOSED 6" PCC CURB PER SDRSD G-1 W/ CLASS II BASE
- ③ PROPOSED 6" PCC CURB & GUTTER PER SDRSD G-2 W/ CLASS II BASE
- ④ PROPOSED 5' WIDE, 4" THICK PCC SIDEWALK PER SDRSD G-7 W/ CLASS II BASE
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- ⑪ PROPOSED PCC CROSS GUTTER PER SDRSD G-12
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- ⑯ PROPOSED TREE WELL BMP (4' X 10'); SEE DETAIL SHEET 11
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## CONSTRUCTION SITE NOTES

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## **EXISTING EASEMENT NOTES**

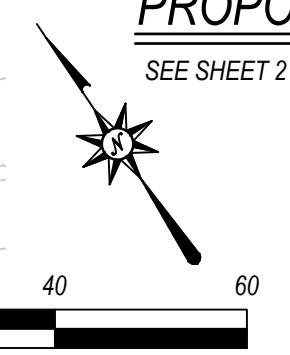
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SFF SHEET 1 FOR PLOTTING AND SUMMARY OF EXISTING FASMENTS

## **PROPOSED EASEMENT NOTES**

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SEE SHEET 2 FOR SUMMARY OF PROPOSED EASEMENTS



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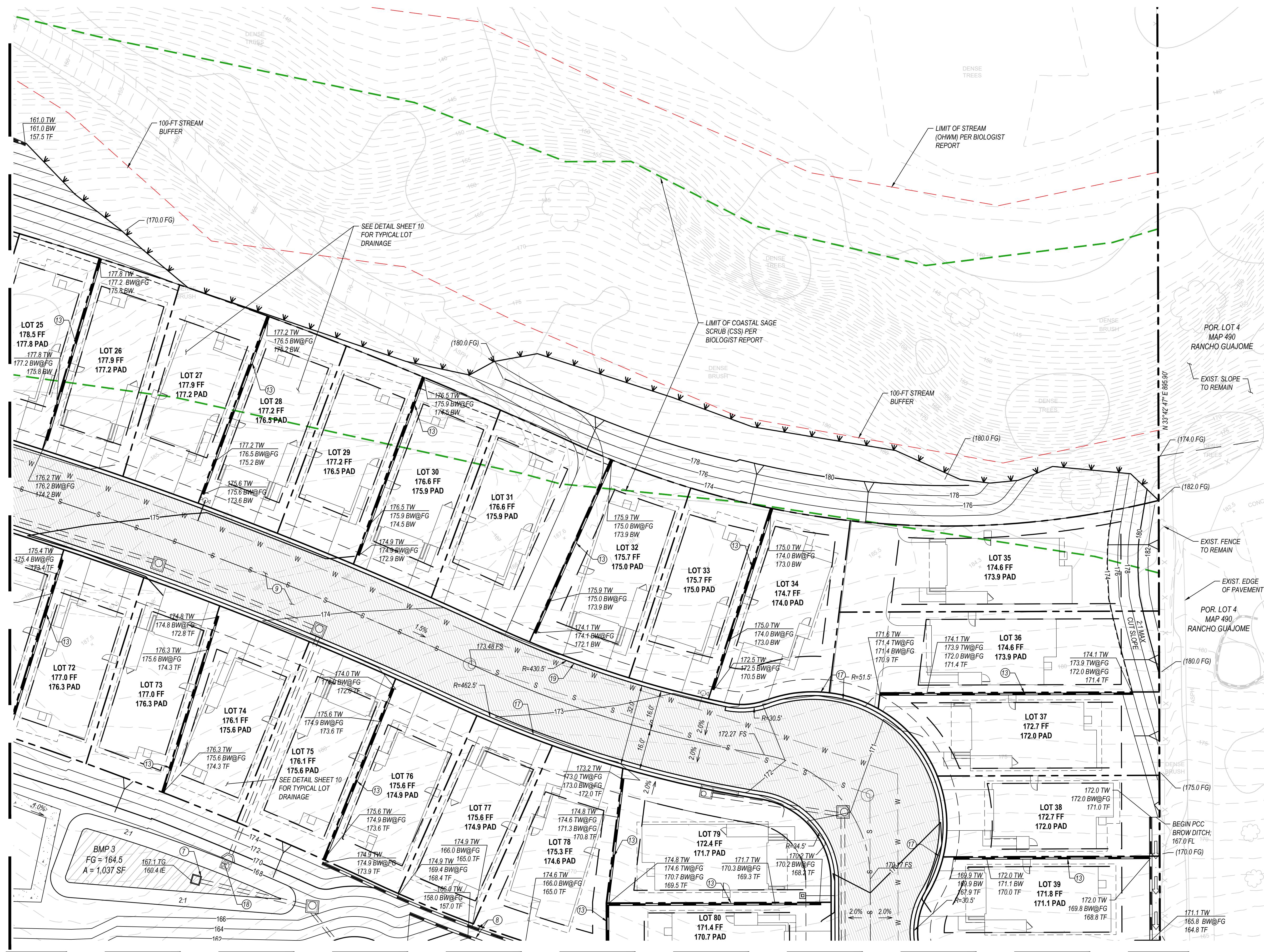
REGISTERED TRADE MARK  
No. 80356  
Exp. 12/31/24

SEE SHEET 6 FOR CONTINUATION

**SEE SHEET 4 FOR CONTINUATION**

# PLAN VIEW - PRELIMINARY GRADING PLAN

SCALE: 1" = 20



0                    20                    40

GRAPHIC SCALE: 1" = 20'

## CONSTRUCTION SITE NOTES

---

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## *EXISTING EASEMENT NOTES*

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SEE SHEET 1 FOR PLOTTING AND SUMMARY OF EXISTING EASEMENTS

## *PROPOSED EASEMENT NOTES*

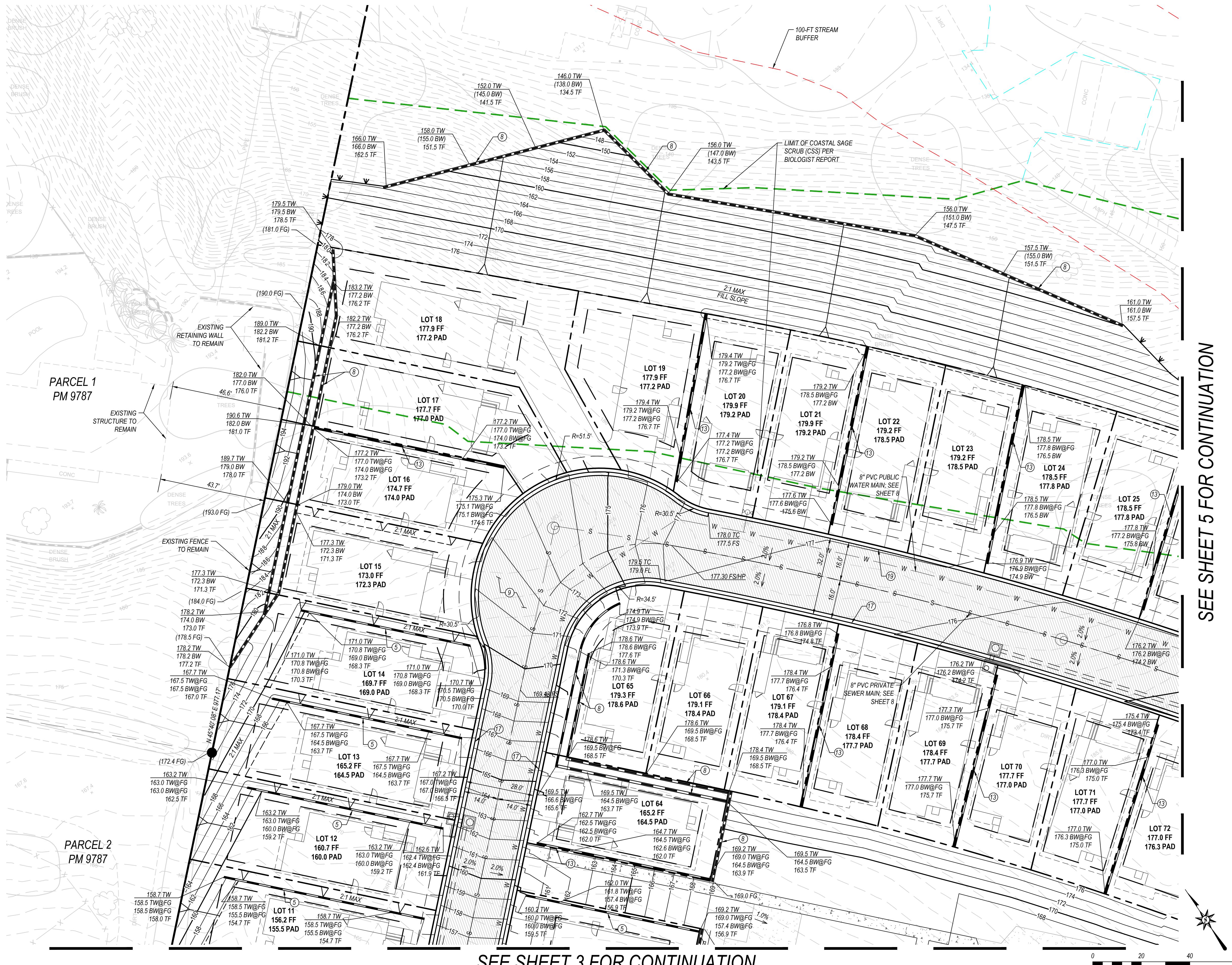
SEE SHEET 2 FOR SUMMARY OF PROPOSED EASEMENTS

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- ⑯ PROPOSED TREE WELL BMP (4' X 10'); SEE DETAIL SHEET 11
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## LEGEND

PROPERTY LINE	
RIGHT-OF-WAY	
CENTERLINE OF ROAD	
PROPOSED LOT LINES	
ADJACENT LOT LINES	
PROPOSED EASEMENTS	
PROPOSED SETBACKS	
PROPOSED LIMIT OF GRADING	
PROPOSED CONTOUR	
EXISTING CONTOUR	
PROPOSED 6" CURB & GUTTER	
PROPOSED 6" CURB	
PROPOSED Hardscape	
PROPOSED MASONRY RETAINING WALL	
PROPOSED BMP	
EXISTING WATER MAIN (SIZE PER PLAN)	
EXISTING SEWER MAIN (SIZE PER PLAN)	
EXISTING STORM DRAIN (SIZE PER PLAN)	
EXISTING GAS MAIN	
PROPOSED SEWER MANHOLE	
PROPOSED 4" PVC STORM DRAIN	
PROPOSED 18" RCP PRIVATE STORM DRAIN	
PROPOSED 12" AREA DRAIN	
PROPOSED 8" PRIVATE PVC SEWER MAIN	
PROPOSED 8" PVC PUBLIC WATER MAIN	

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## EXISTING EASEMENT NOTES

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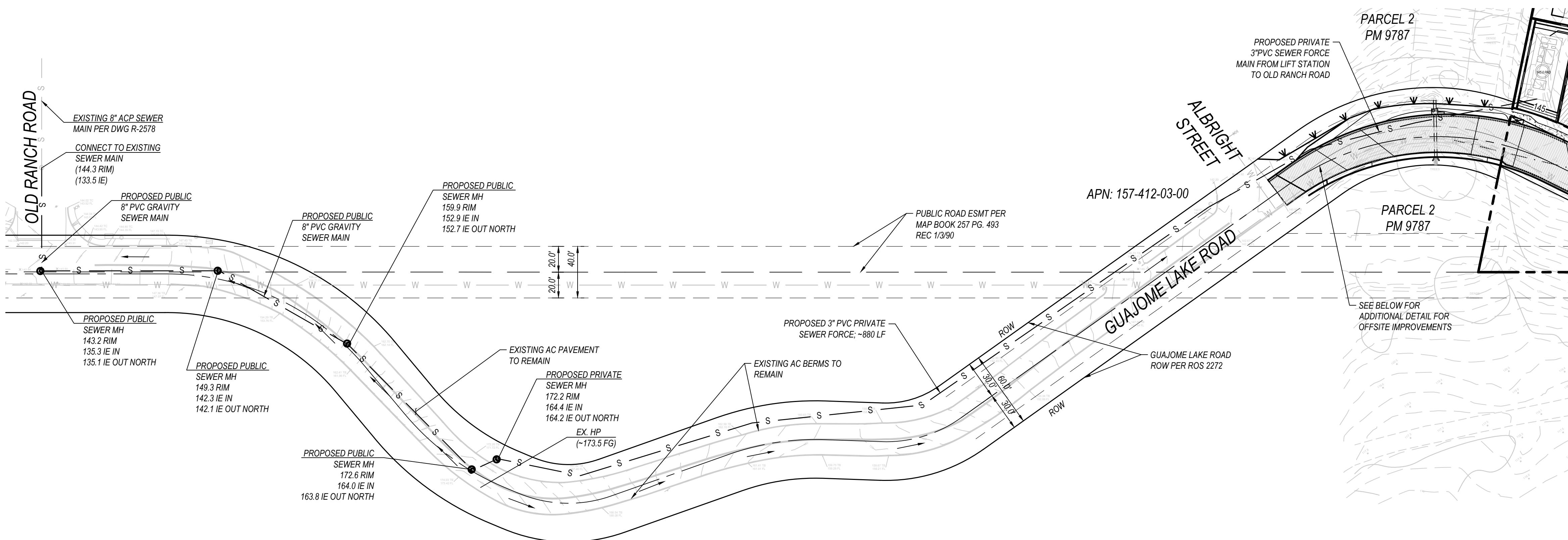
## PROPOSED EASEMENT NOTES

SEE SHEET 2 FOR SUMMARY OF PROPOSED EASEMENTS



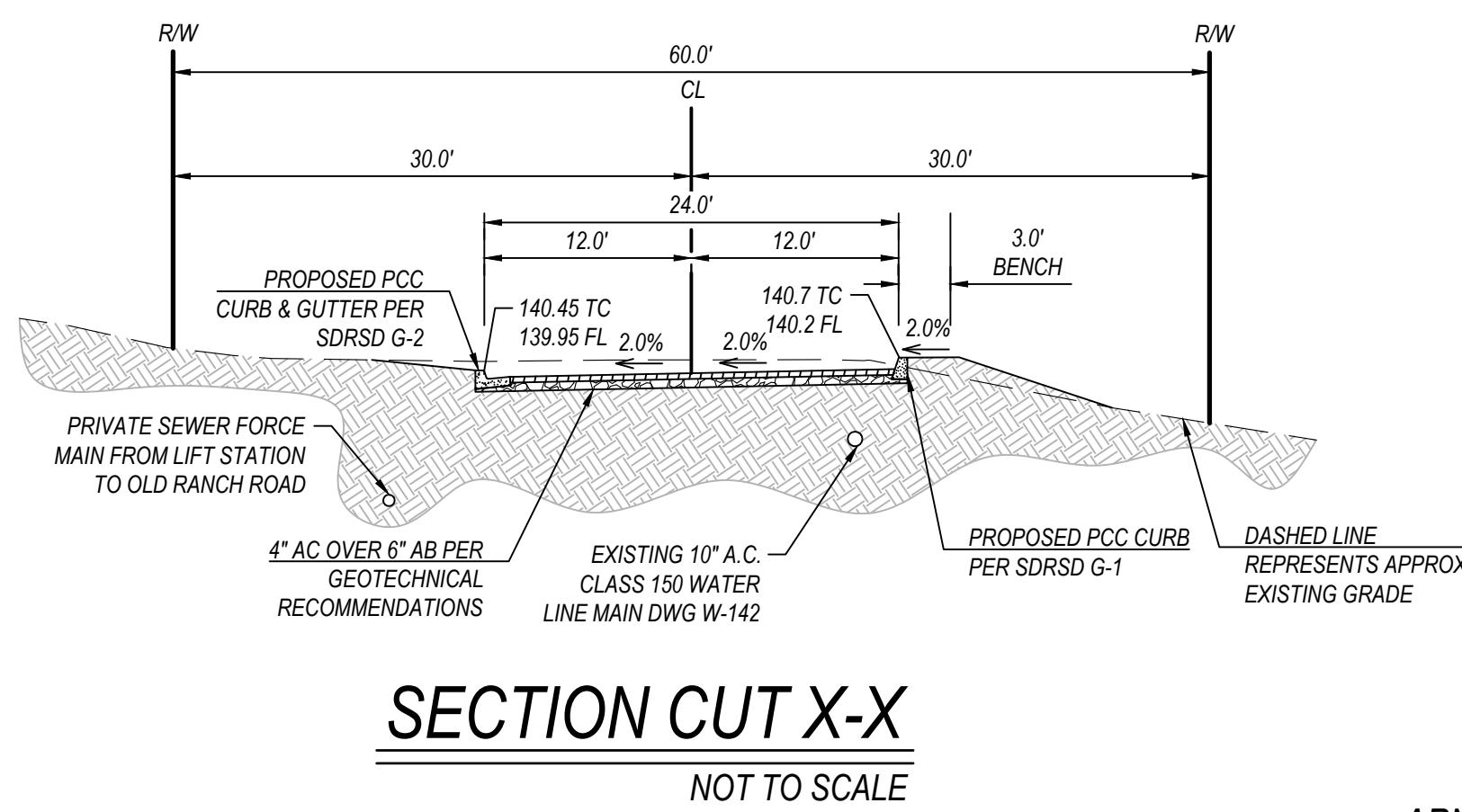
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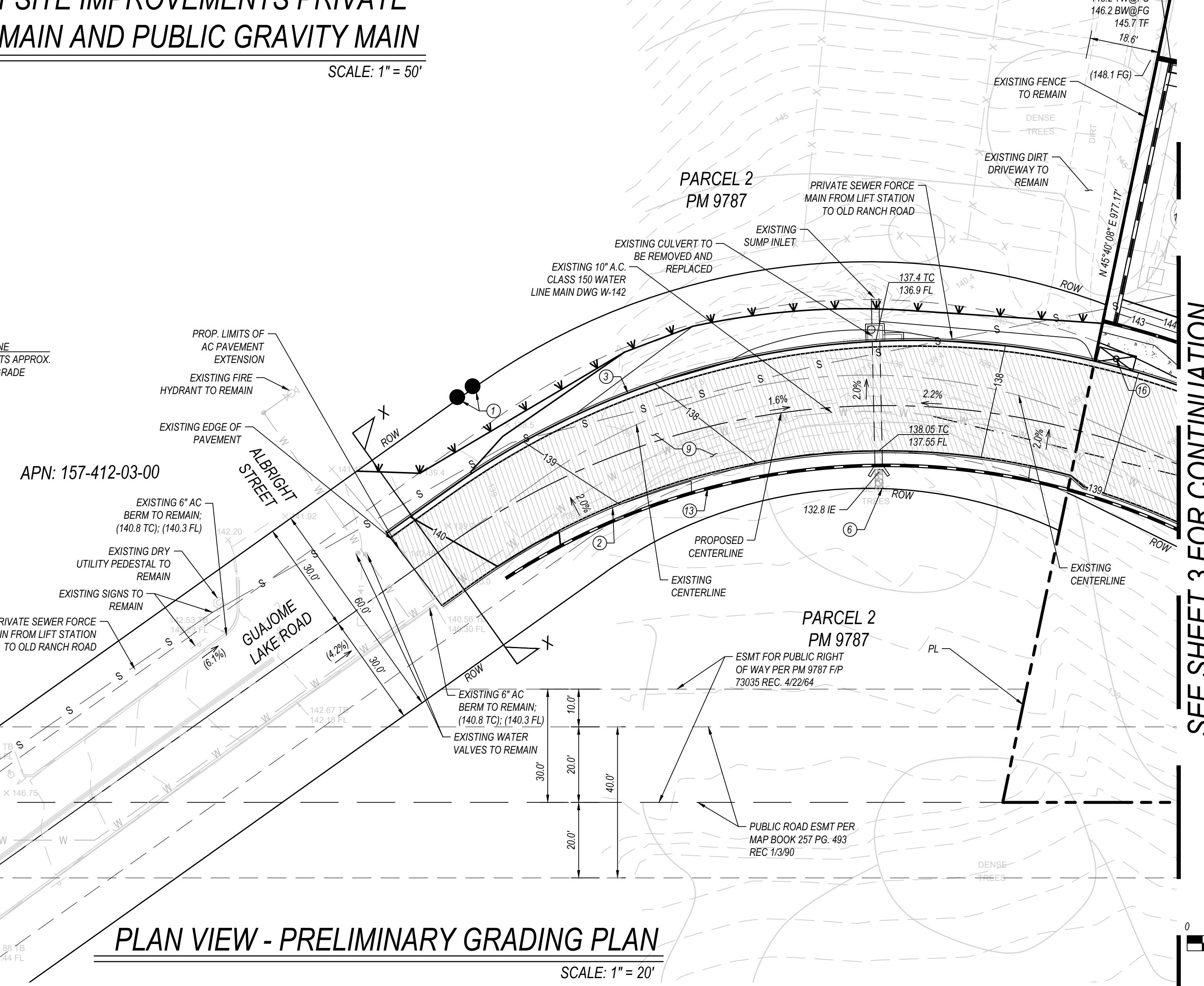


PLAN VIEW - OFFSITE IMPROVEMENTS PRIVATE SEWER FORCE MAIN AND PUBLIC GRAVITY MAIN

SCALE: 1" = 50'



SECTION CUT X-X



PLAN VIEW - PRELIMINARY GRADING PLAN

SCALE: 1" = 20'

## LEGEND

PROPERTY LINE	
RIGHT-OF-WAY	
CENTERLINE OF ROAD	
PROPOSED LOT LINES	
ADJACENT LOT LINES	
PROPOSED EASEMENTS	
PROPOSED SETBACKS	
PROPOSED LIMIT OF GRADING	
PROPOSED CONTOUR	
EXISTING CONTOUR	
PROPOSED 6" CURB & GUTTER	
PROPOSED 6" CURB	
PROPOSED Hardscape	
PROPOSED MASONRY RETAINING WALL	
PROPOSED BMP	
EXISTING WATER MAIN (SIZE PER PLAN)	
EXISTING SEWER MAIN (SIZE PER PLAN)	
EXISTING STORM DRAIN (SIZE PER PLAN)	
EXISTING GAS MAIN	
PROPOSED SEWER MANHOLE	
PROPOSED 4" PVC STORM DRAIN	
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PROPOSED 12" AREA DRAIN	
PROPOSED 8" PRIVATE PVC SEWER MAIN	
PROPOSED 8" PVC PUBLIC WATER MAIN	

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## PROPOSED EASEMENT NOTES

SEE SHEET 2 FOR SUMMARY OF PROPOSED EASEMENTS

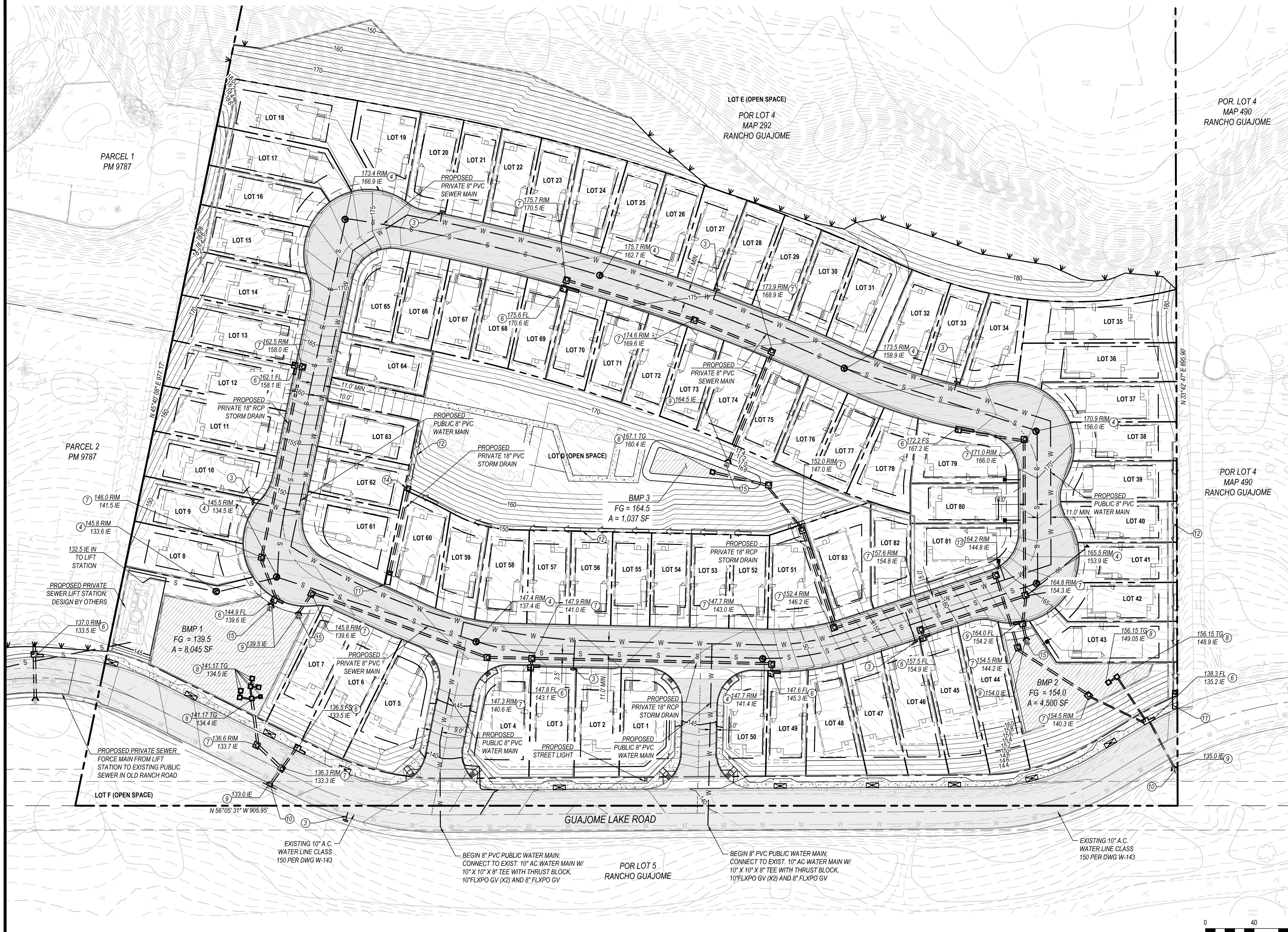


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## LEGEND

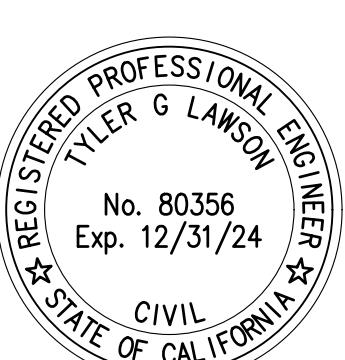
PROPERTY LINE	
RIGHT-OF-WAY	
CENTERLINE OF ROAD	
PROPOSED LOT LINES	
ADJACENT LOT LINES	
PROPOSED EASEMENTS	
PROPOSED SETBACKS	
PROPOSED LIMIT OF GRADING	
PROPOSED CONTOUR	
EXISTING CONTOUR	
PROPOSED 6" CURB & GUTTER	
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PROPOSED MASONRY RETAINING WALL	
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PROPOSED 4" PVC PVT STORM DRAIN	
PROPOSED 18" RCP PRIVATE STORM DRAIN	
PROPOSED 12" AREA DRAIN	
PROPOSED 8" PRIVATE SEWER MAIN	
PROPOSED 8" PUBLIC WATER MAIN	

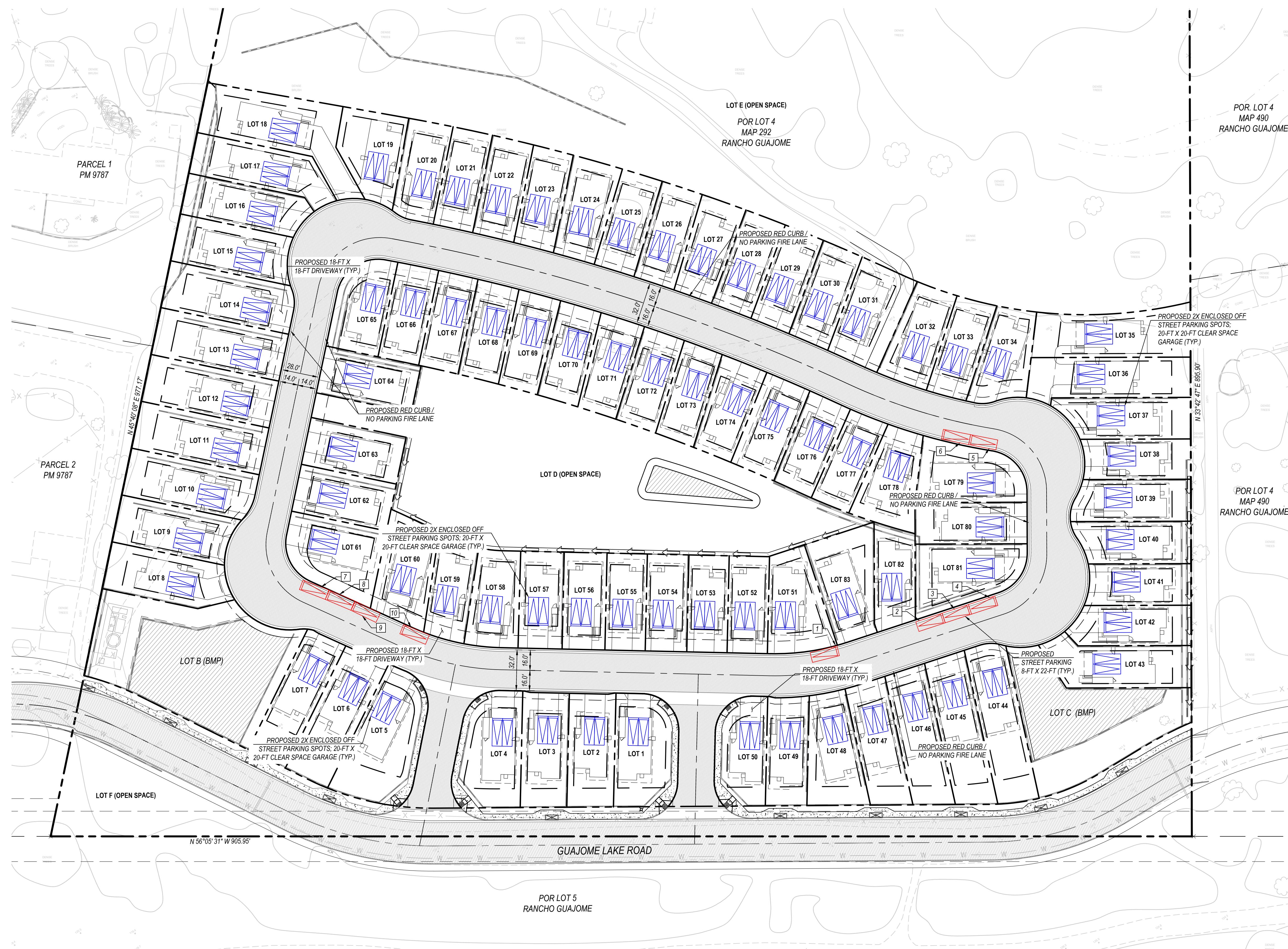
## UTILITY CONSTRUCTION NOTES

- ① PROPOSED 1" WATER SERVICE AND METER PER CITY OF OCEANSIDE STD W
- ② PROPOSED 4" PVC SEWER LATERAL PER SDRSD SS-01
- ③ PROPOSED FIRE HYDRANT ASSEMBLY PER CITY OF OCEANSIDE STD. W-01
- ④ PROPOSED 60" SEWER MANHOLE PER SDRSD SM-01
- ⑤ PROPOSED 18" PRIVATE RCP STORM DRAIN (1350-D)
- ⑥ PROPOSED TYPE B CURB INLET PER SDRSD D-02
- ⑦ PROPOSED TYPE A-4 STORM DRAIN CLEANOUT PER SDRSD D-09
- ⑧ PROPOSED 36" X 36" BROOKS BOX OUTLET STRUCTURE; SEE BIOFILTRATION BASIN DETAIL SHEET 11 FOR LOW-FLOW ORIFICE INFORMATION
- ⑨ PROPOSED WING-TYPE HEADWALL PER SDRSD D-34
- ⑩ PROPOSED 4'X4' RIP RAP ENERGY DISSIPATER PER SDRSD D-40; ROCK CLASS = No. 2 BACKING T=1.1FT
- ⑪ PROPOSED CURB OUTLET PER SDRSD D-25
- ⑫ PROPOSED PCC DRAINAGE DITCH PER SDRSD D-75, TYPE B
- ⑬ PROPOSED TYPE B-5 STORM DRAIN CLEANOUT PER SDRSD D-10
- ⑭ PROPOSED TYPE-F CATCH BASIN PER SDRSD D-07
- ⑮ PROPOSED 4' X 4' RIP RAP ENERGY DISSIPATER PER SDRSD D-40; ROCK CLASS = 2 TON BACKING T=5.4'

## UTILITY SITE NOTES

1. ALL UTILITIES SHOWN HEREON PER BEST AVAILABLE RECORDS. CONTRACTOR SHALL VERIFY EXACT HORIZONTAL AND VERTICAL LOCATION PRIOR TO CONSTRUCTION. CONTRACTOR SHALL NOTIFY ENGINEER OF RECORD OF DISCREPANCIES UPON DISCOVERY.





## LEGEND

PROPERTY LINE	
RIGHT-OF-WAY	
CENTERLINE OF ROAD	
PROPOSED LOT LINES	
ADJACENT LOT LINES	
PROPOSED EASEMENTS	
PROPOSED SETBACKS	
PROPOSED LIMIT OF GRADING	
PROPOSED CONTOUR	
EXISTING CONTOUR	
PROPOSED 6' CURB & GUTTER	
PROPOSED 6' CURB	
PROPOSED HARSCAPE	
PROPOSED MASONRY RETAINING WALL	
PROPOSED BMP	
EXISTING WATER MAIN (SIZE PER PLAN)	
EXISTING SEWER MAIN (SIZE PER PLAN)	
EXISTING STORM DRAIN (SIZE PER PLAN)	
EXISTING GAS MAIN	
PROPOSED PARALLEL ON STREET PARKING (8-FT WIDE X 22-FT DEEP PER CITY OF OCEANSIDE ZONING ORDINANCE 3109)	
PROPOSED ENCLOSED OFF STREET PARKING (20-FT WIDE X 19-FT DEEP 2-CAR GARAGE MIN IN SFR ZONING PER CITY OF OCEANSIDE ZONING ORDINANCE 3109)	

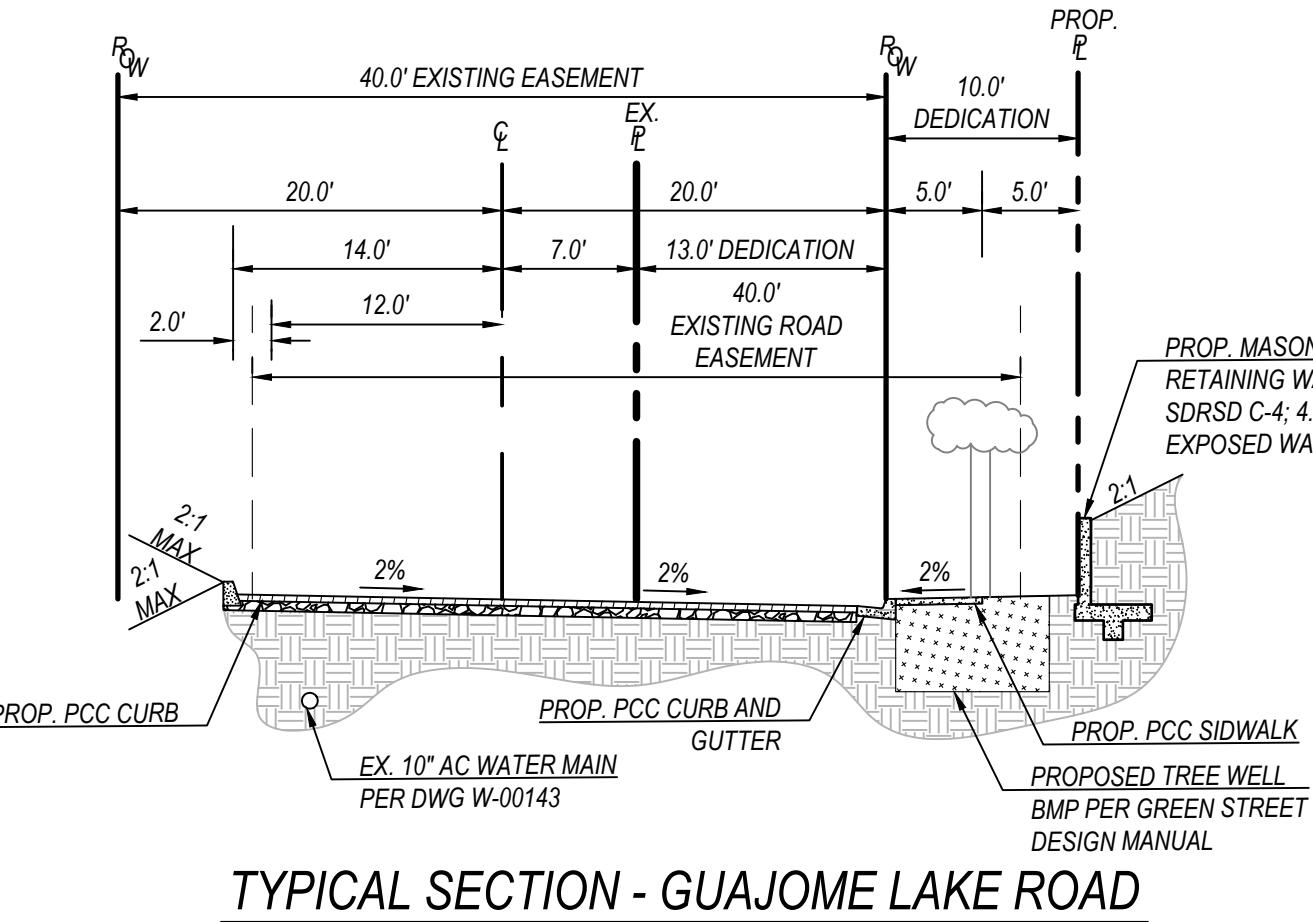
## PROPOSED PARKING

PROPOSED PARALLEL ON STREET PARKING:	10.0
PROPOSED ENCLOSED OFF STREET PARKING:	2.0 / UNIT (2-CAR GARAGE)
PROPOSED UNENCLOSED OFF STREET PARKING:	2.0 / UNIT (EXCLUDING LOT 19) (18-FT WIDE X 18-FT DEEP DWY)



**PASCO LARET SUITER**  
& ASSOCIATES

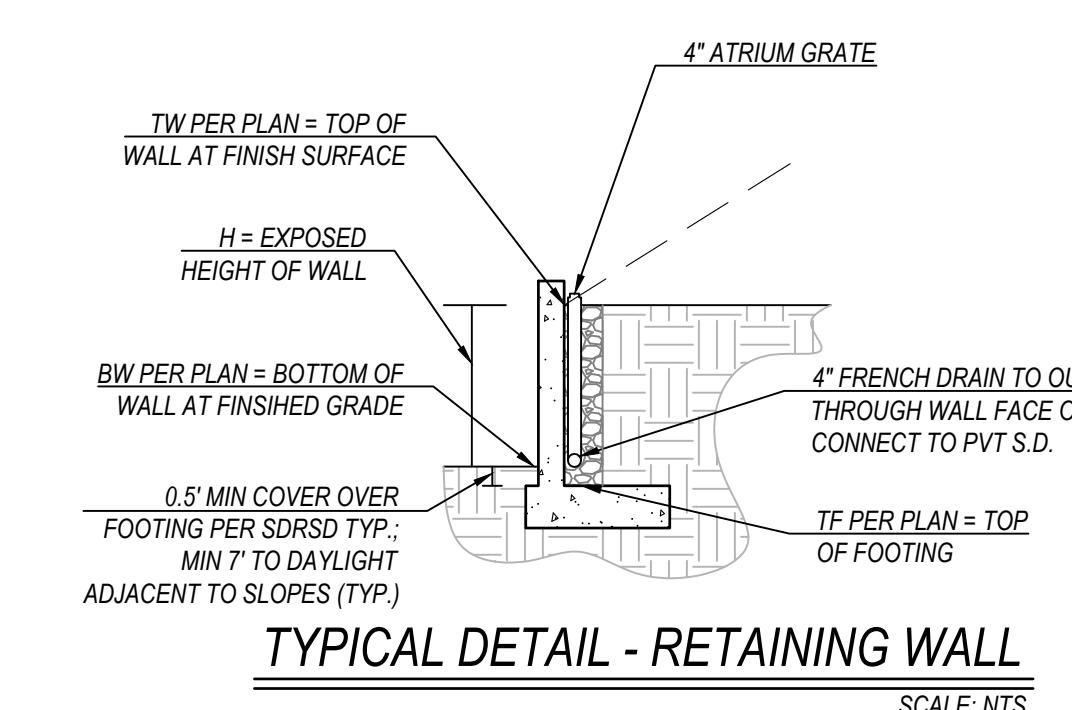
San Diego | Solana Beach | Orange County  
Phone 858.259.8212 | [www.plsaengineering.com](http://www.plsaengineering.com)



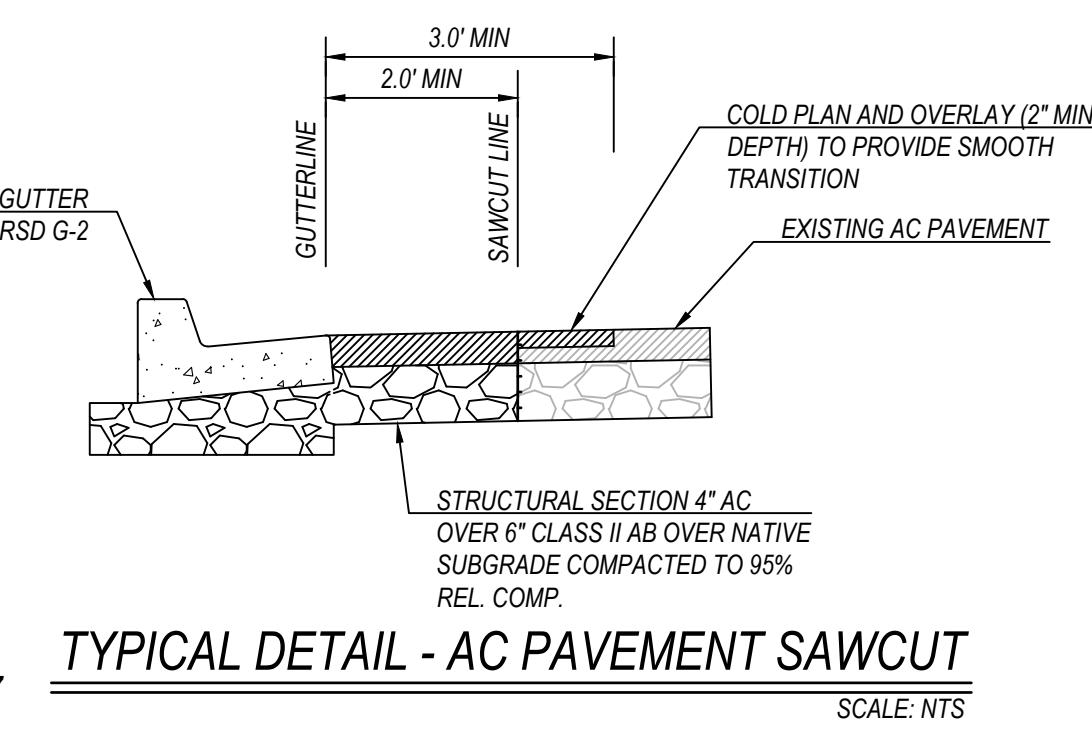
**TYPICAL SECTION - PRIVATE ROAD NORTH**  
SCALE: NTS

EX. 10" AC WATER MAIN  
PER DWG W-00143

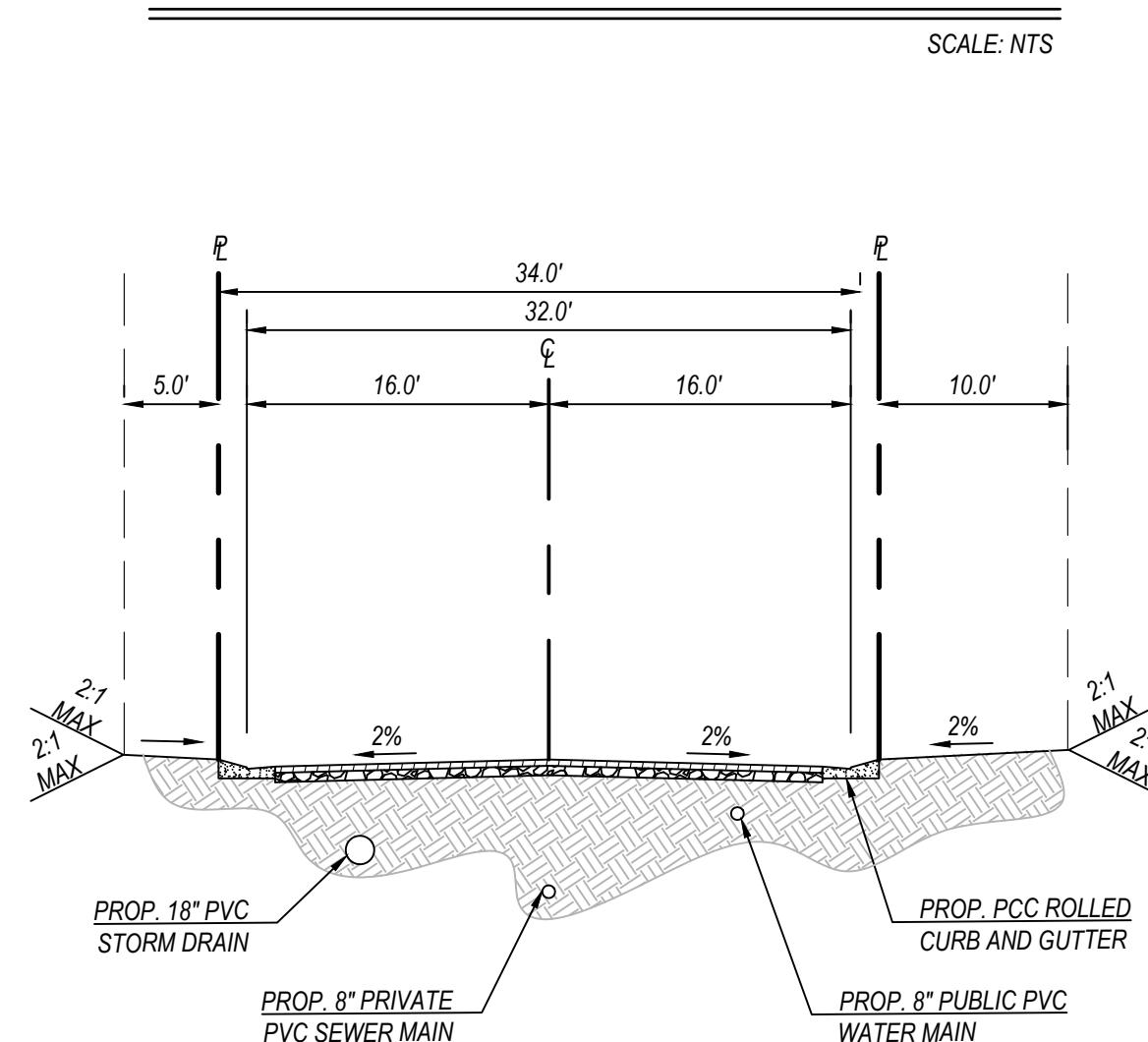
PROPOSED TREE WELL  
BMP PER GREEN STREET  
DESIGN MANUAL



PM 9787



**TYPICAL SECTION - GUAJOME LAKE ROAD**  
SCALE: NTS



**TYPICAL SECTION - PRIVATE ROAD WEST**  
SCALE: NTS

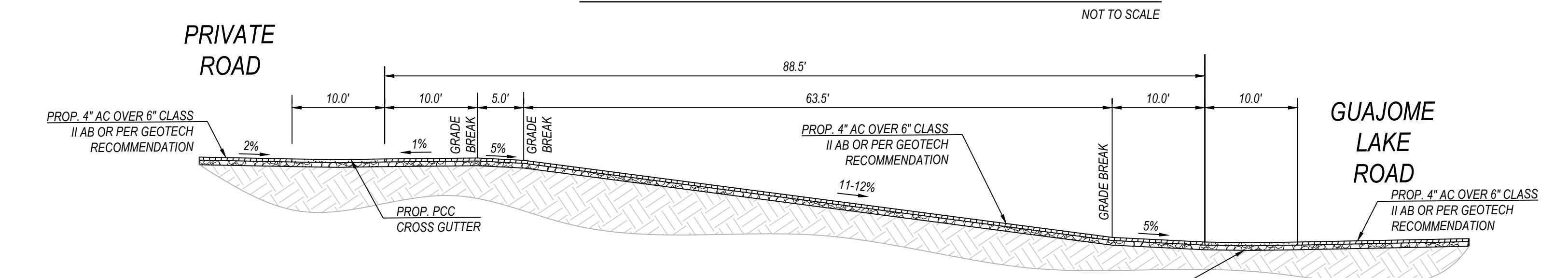
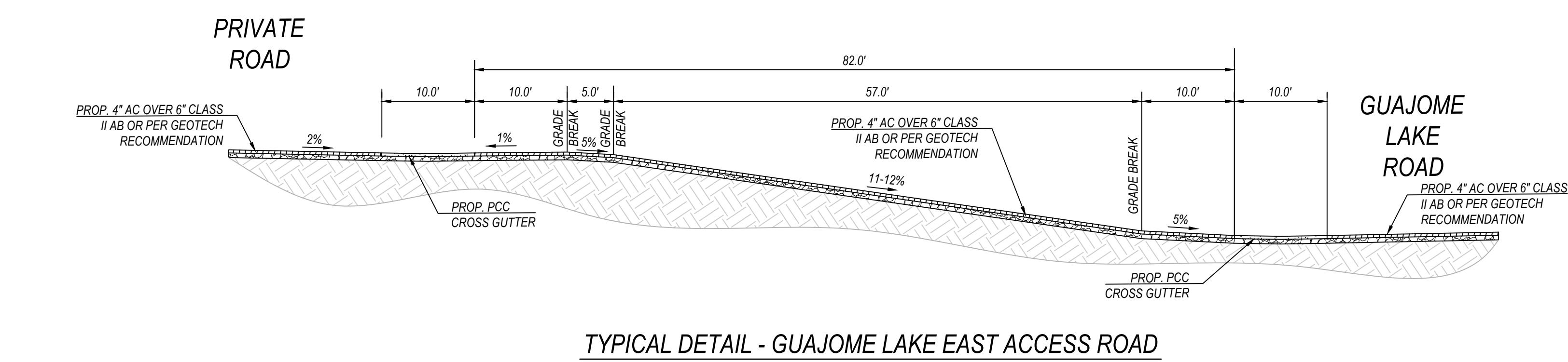
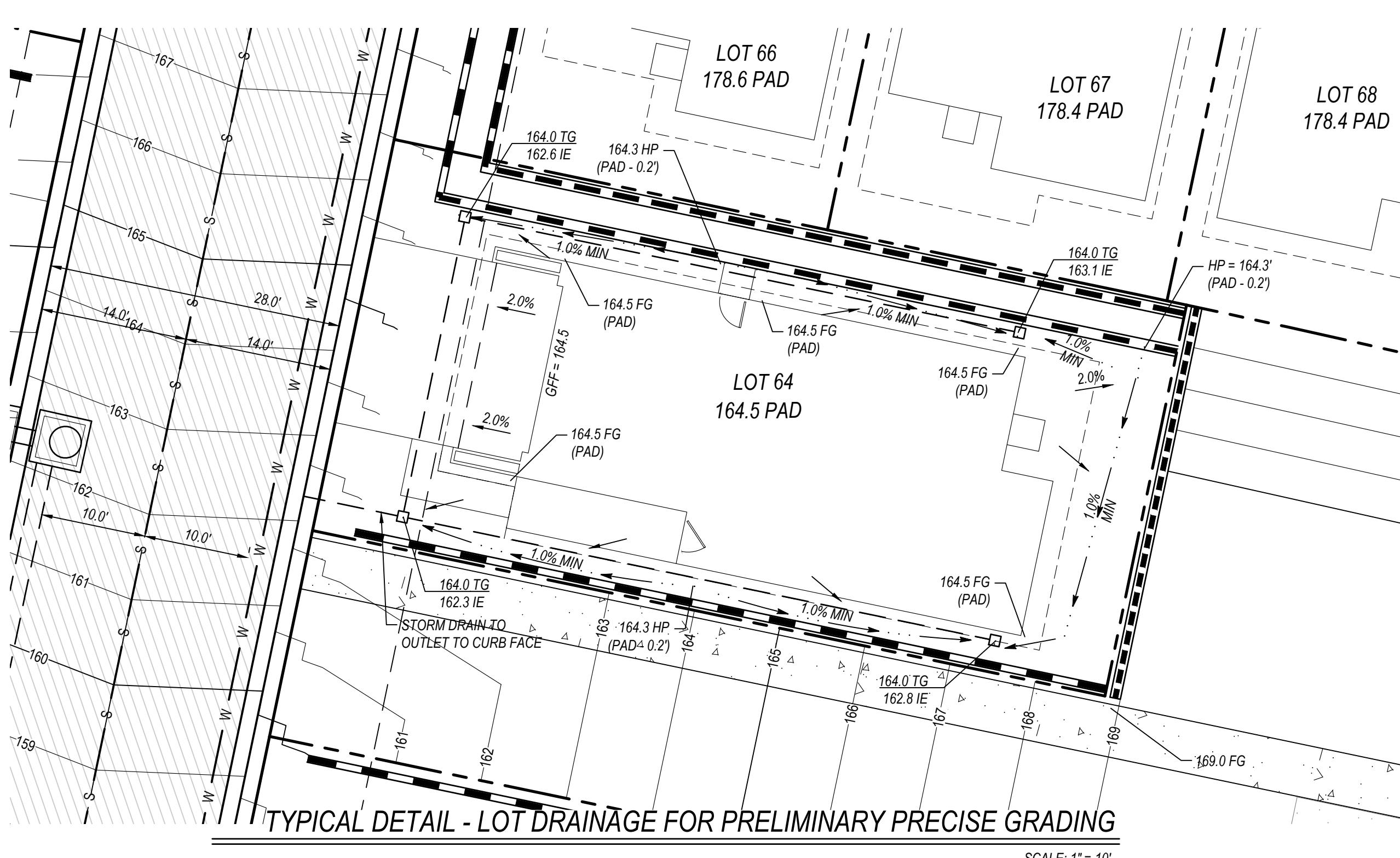
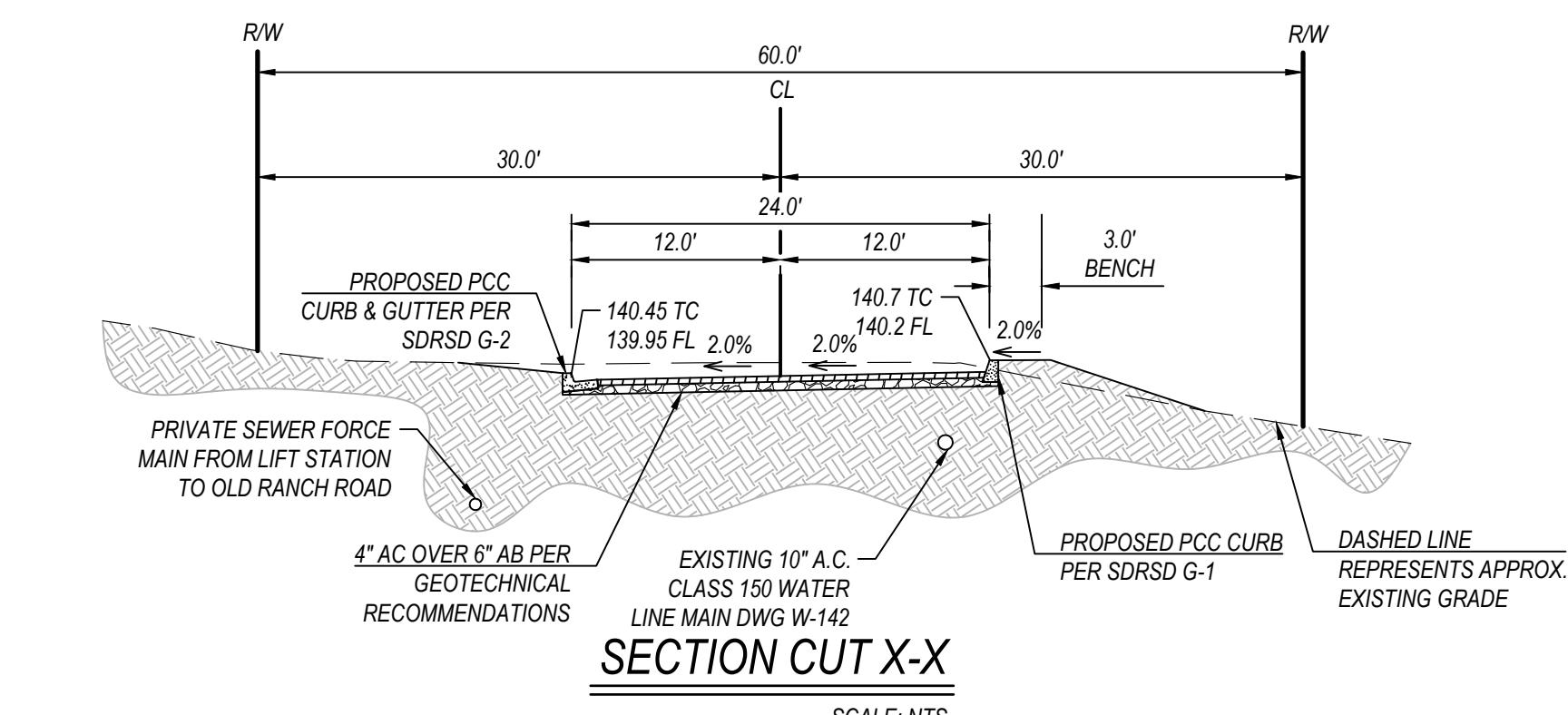
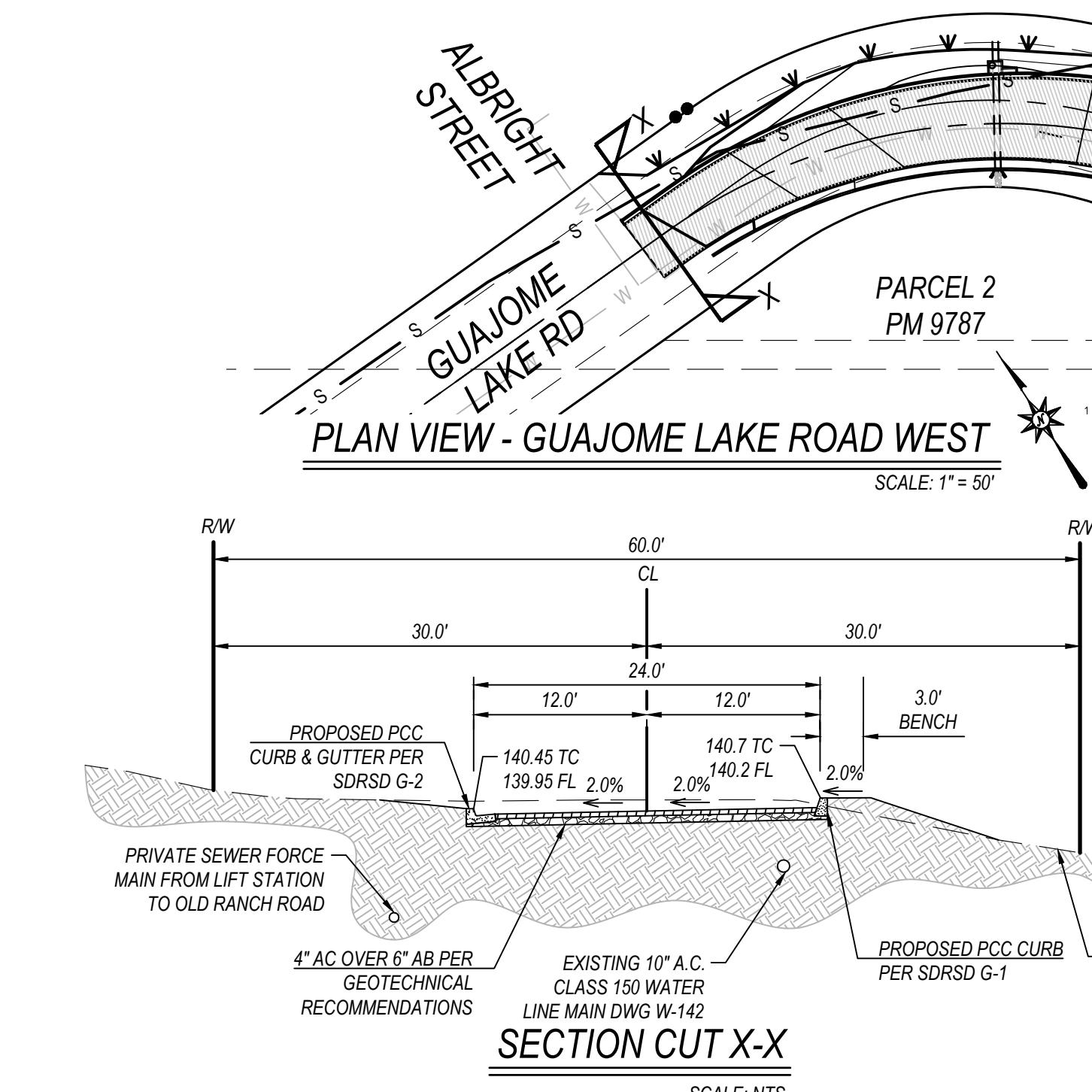
PROPOSED PCC ROLLED CURB AND GUTTER

PROPOSED 8" PVC WATER MAIN

PROPOSED 18" PVC STORM DRAIN

PROPOSED 8" PRIVATE PVC SEWER MAIN

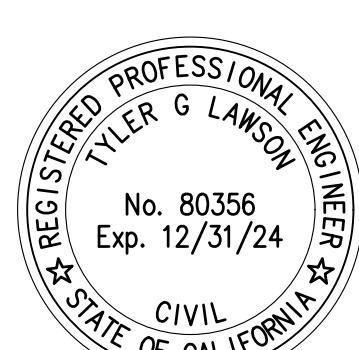
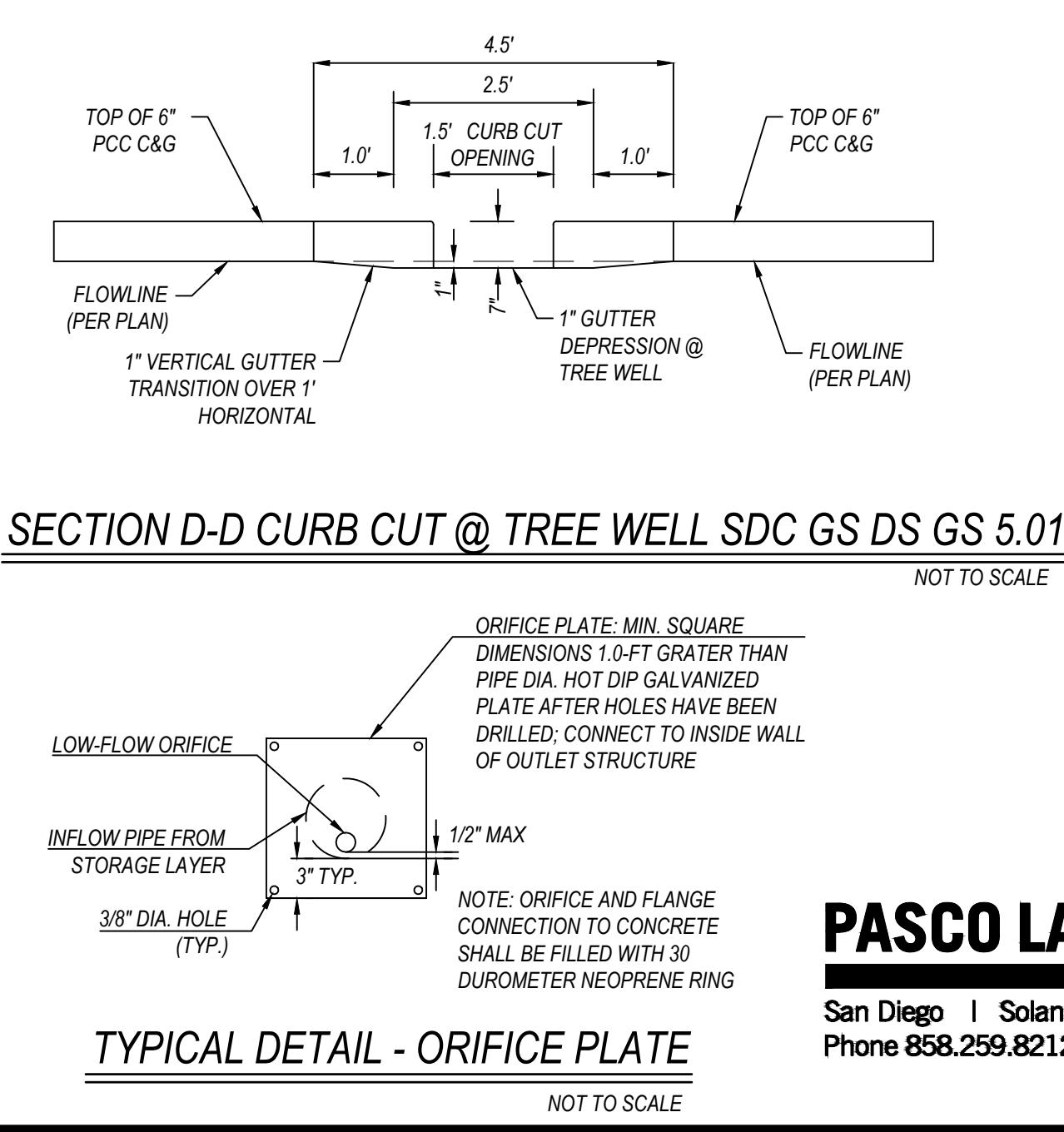
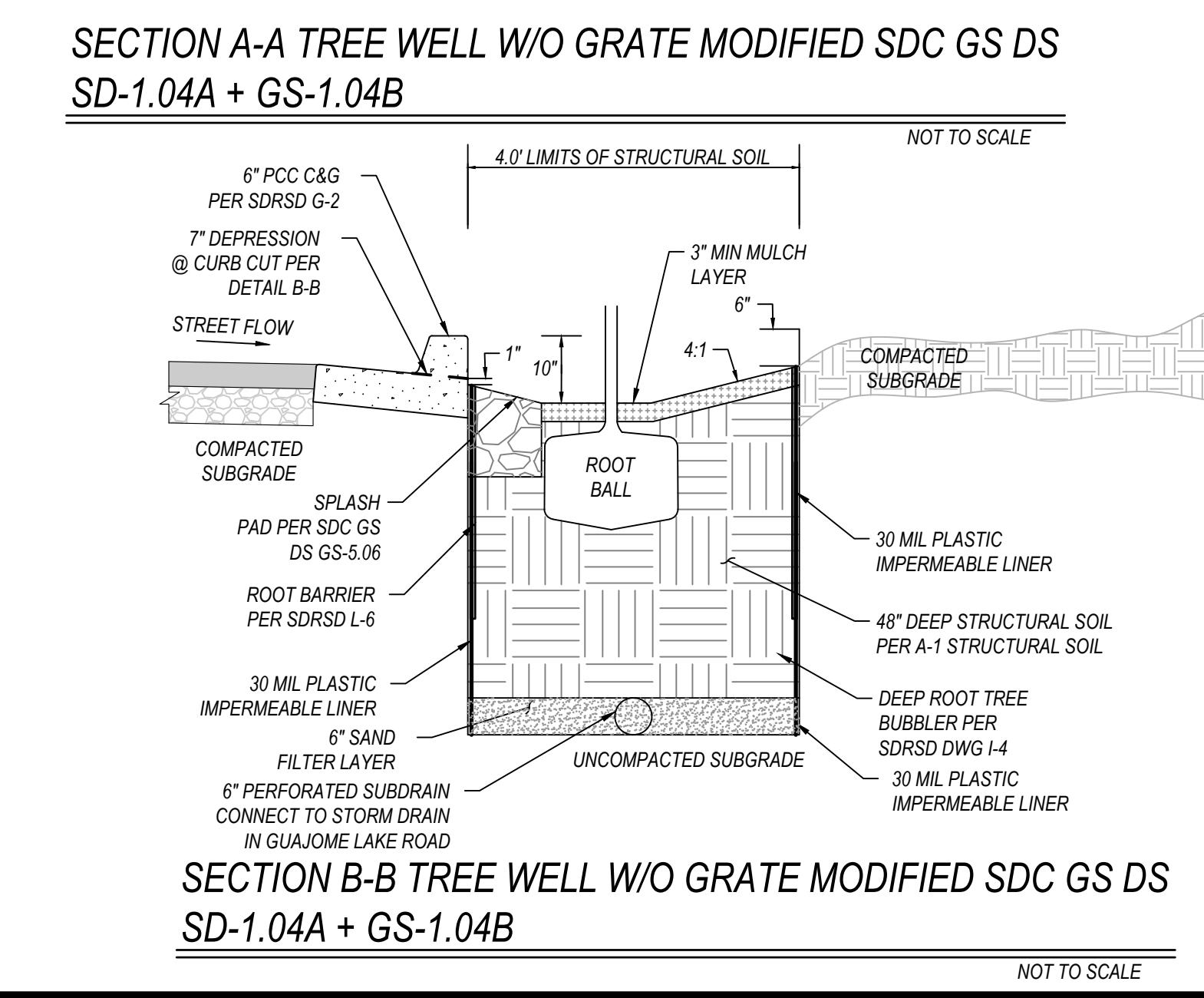
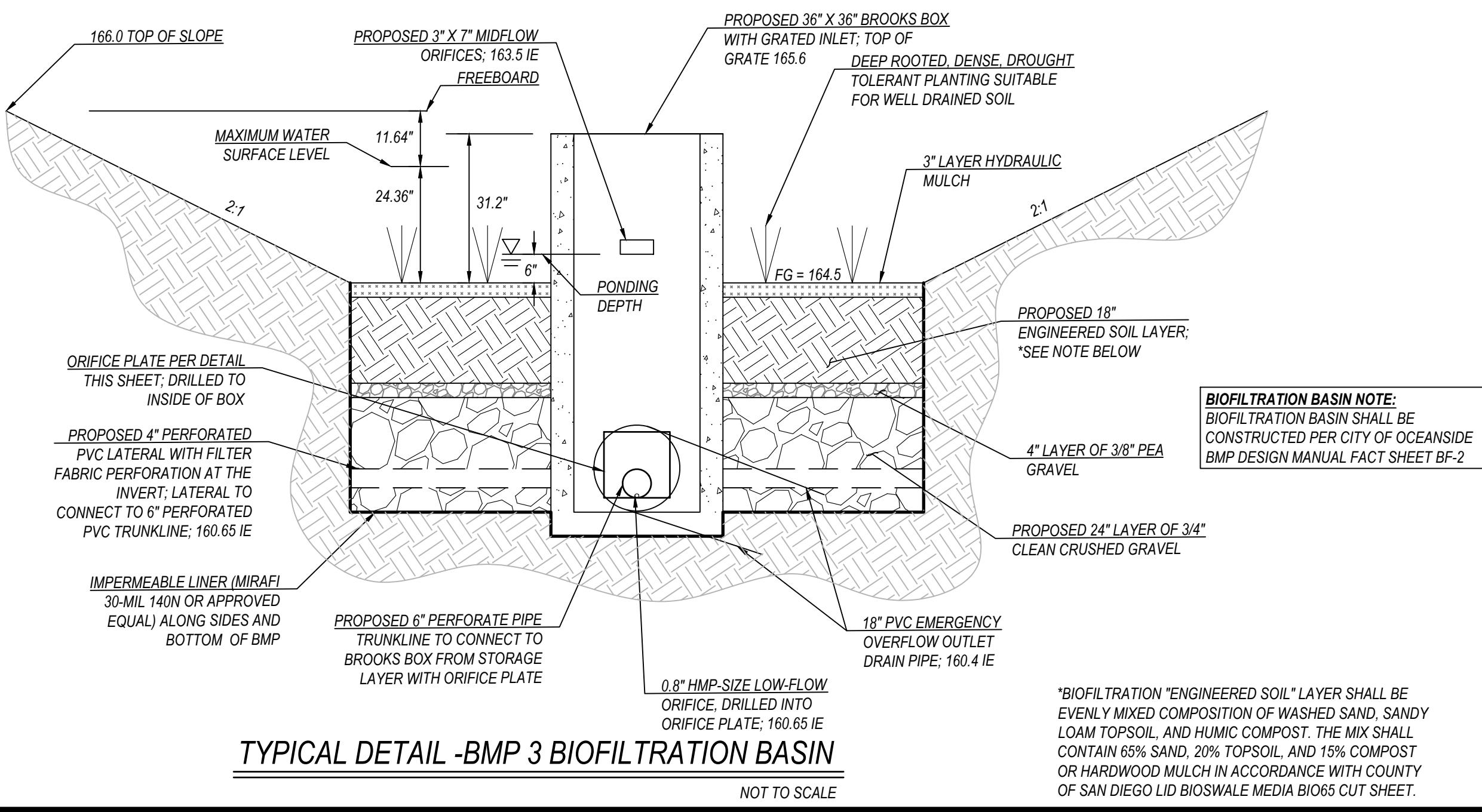
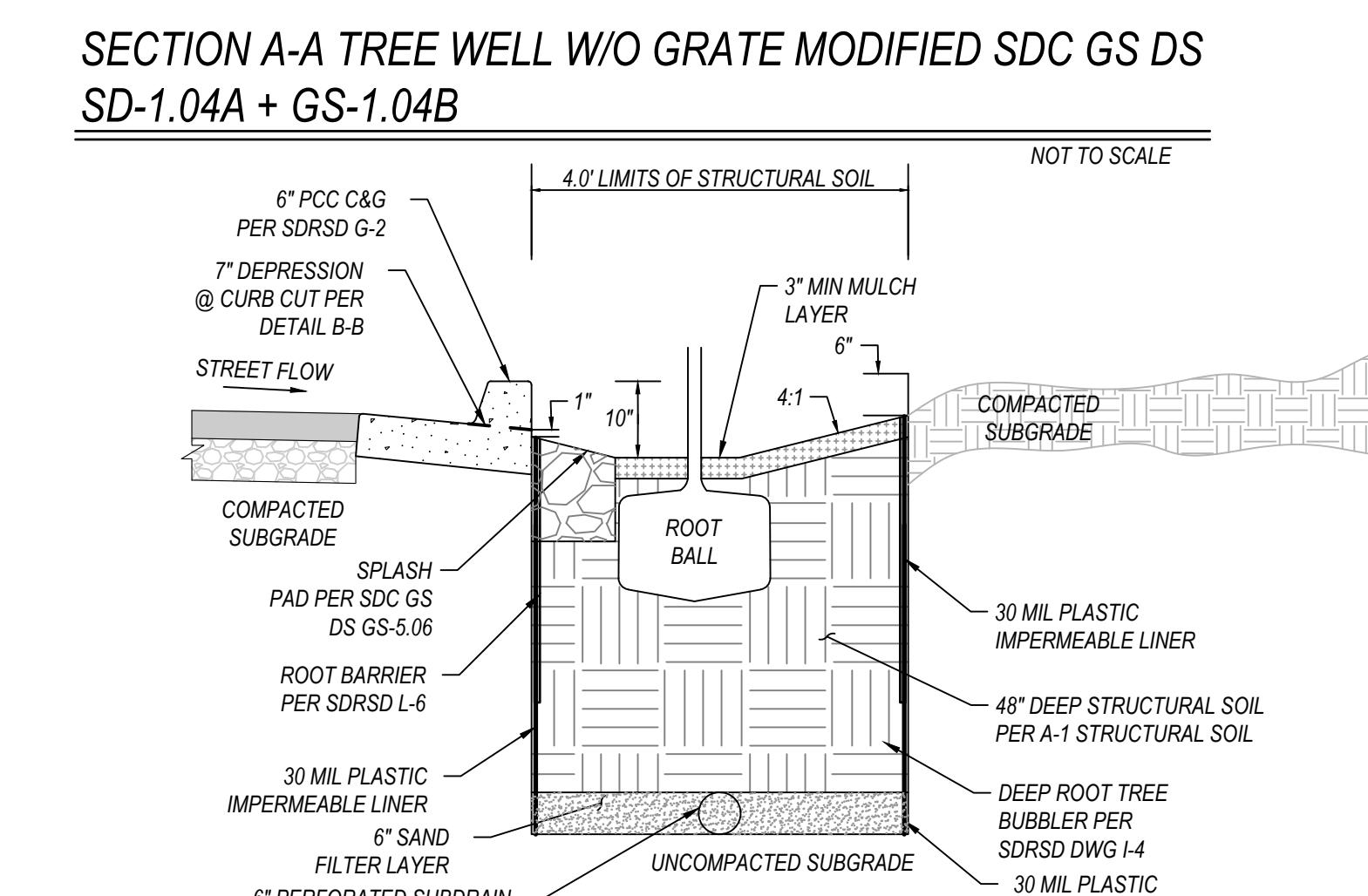
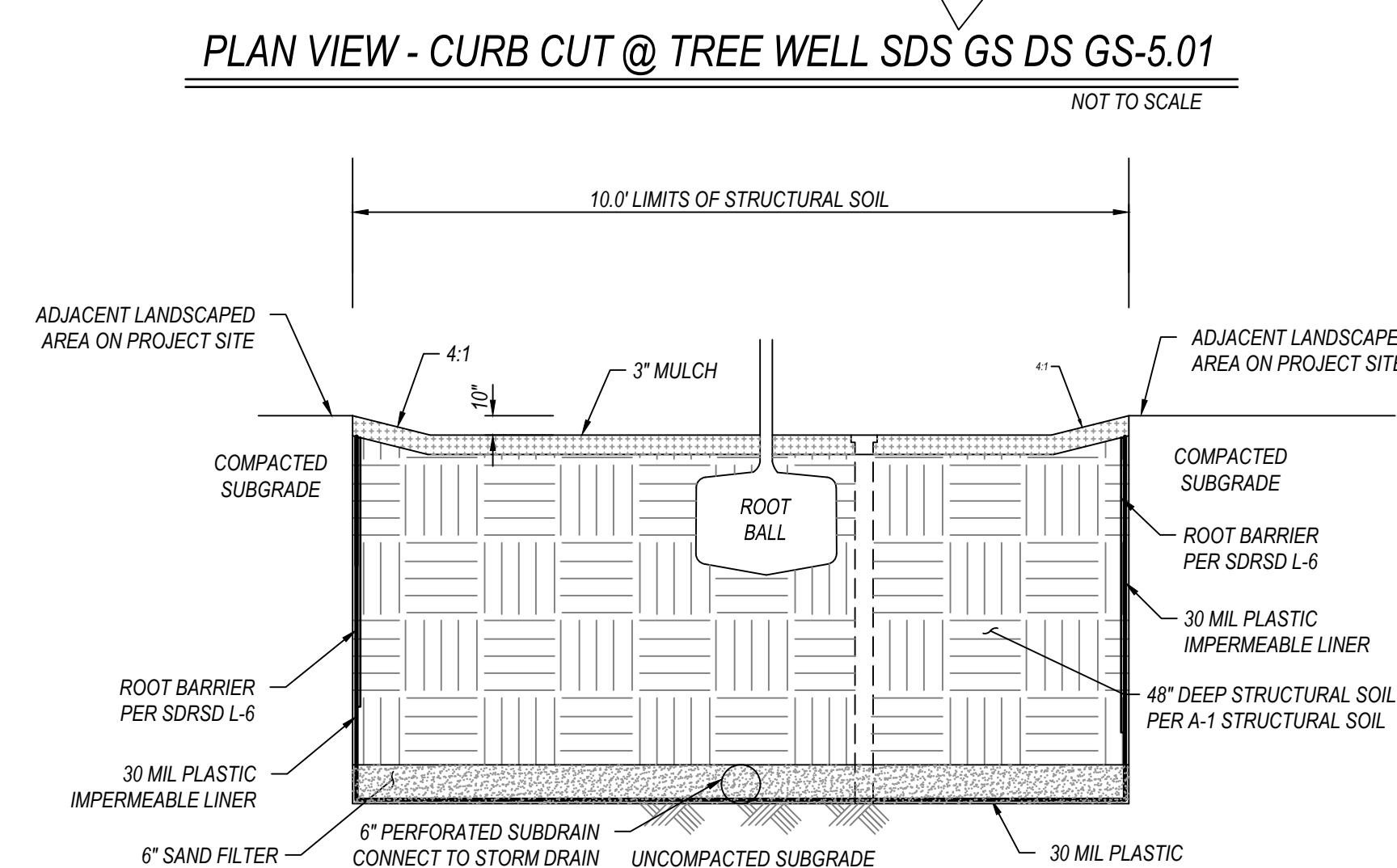
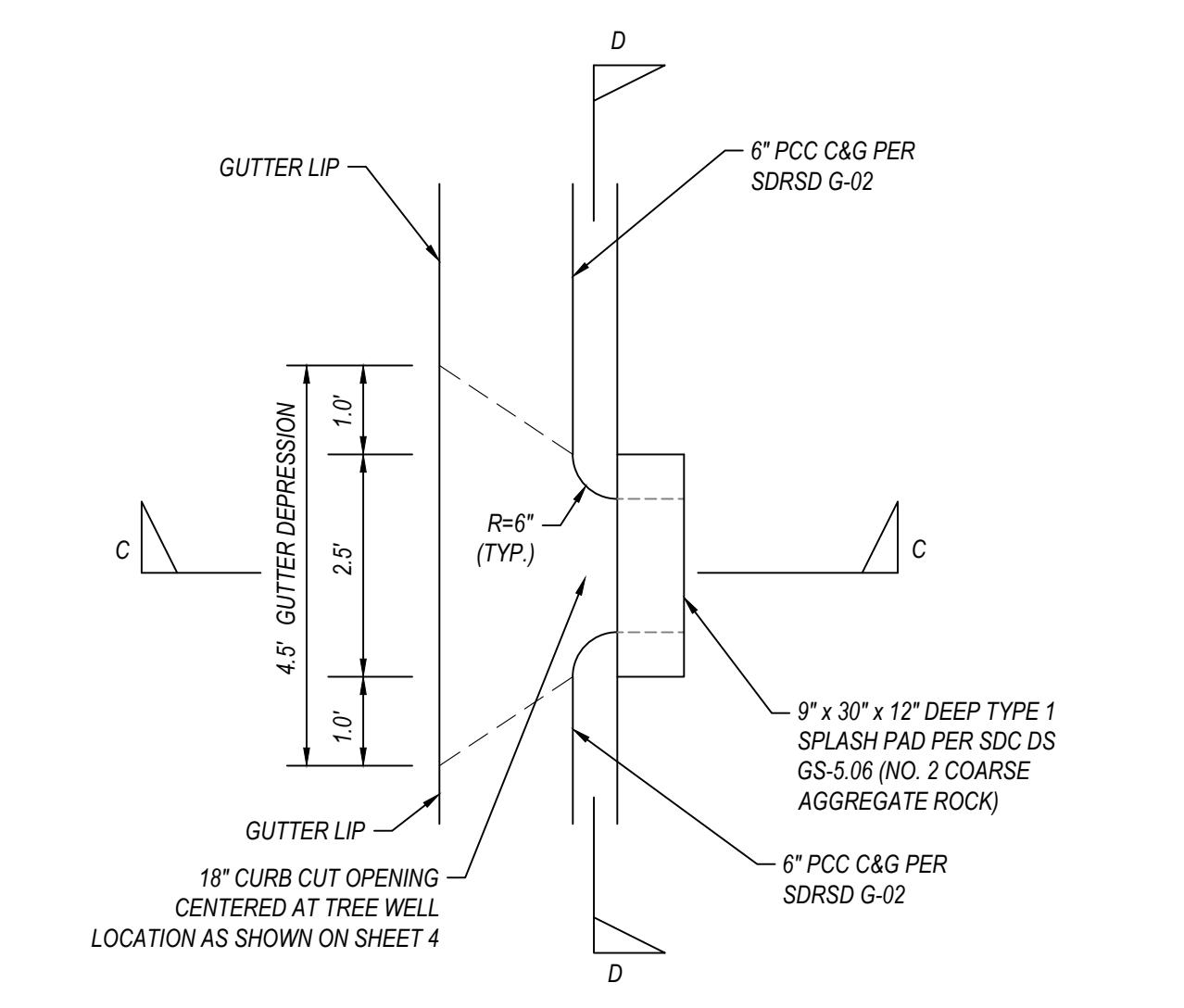
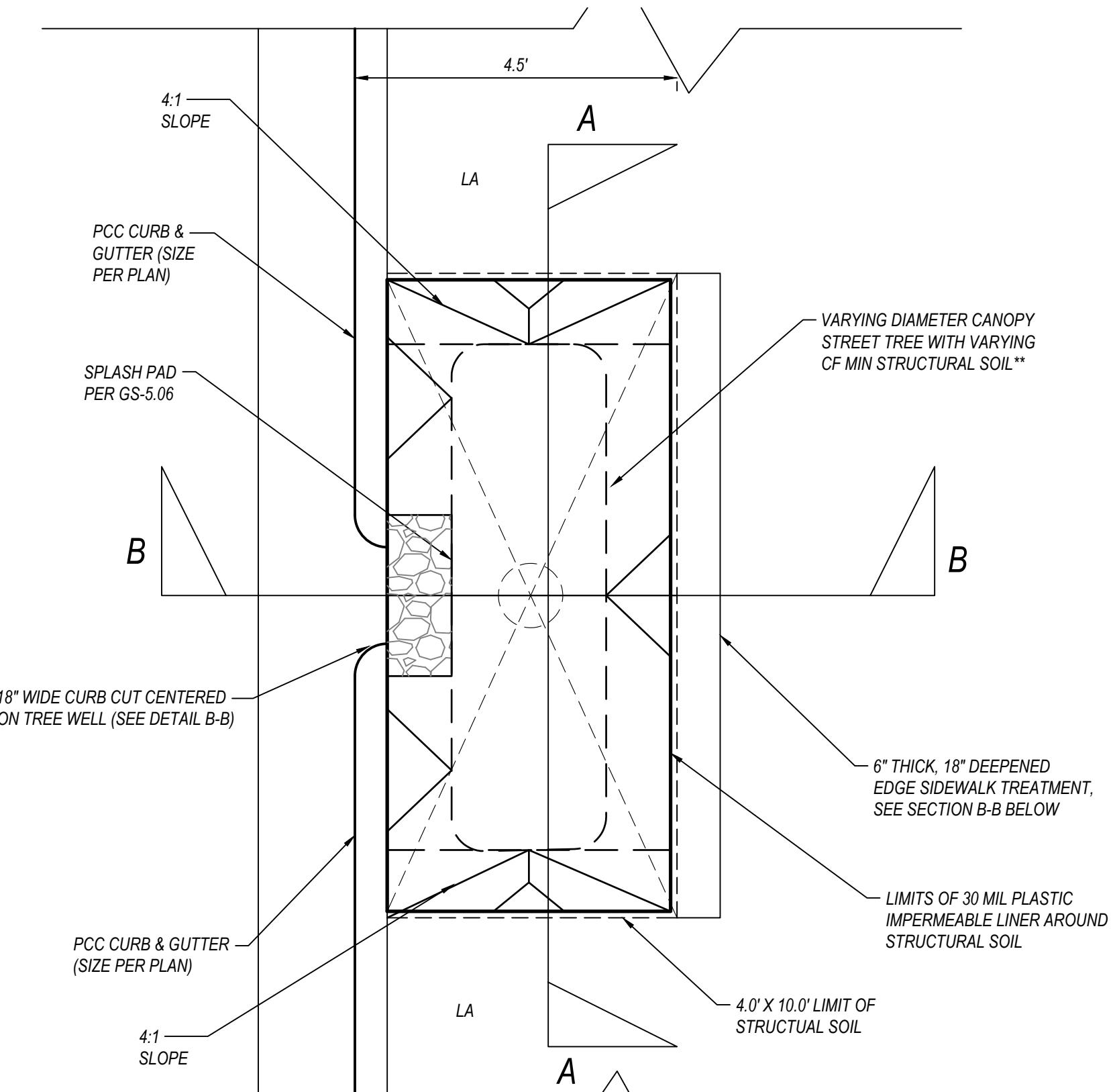
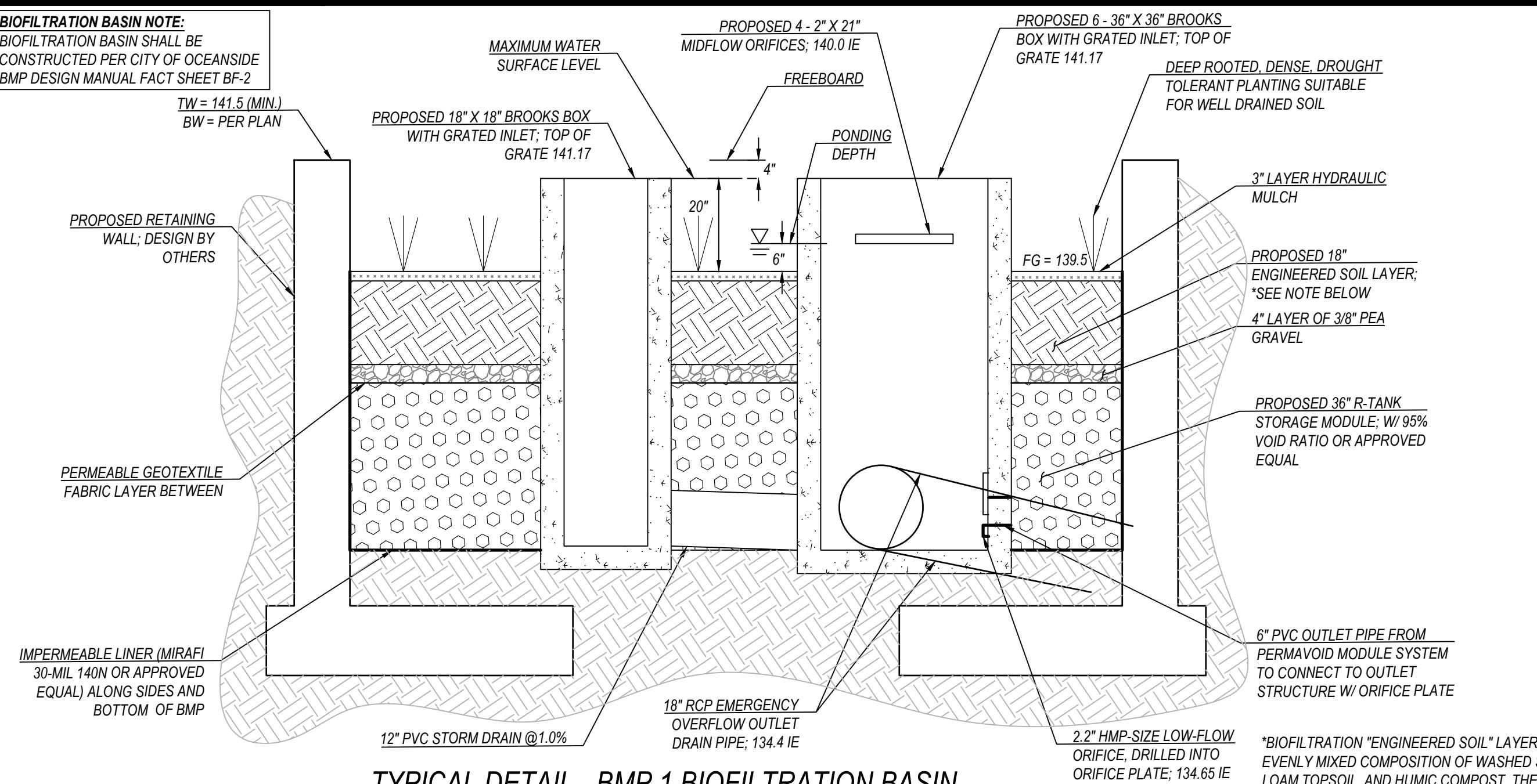
PROPOSED 8" PUBLIC PVC WATER MAIN



REGISTERED PROFESSIONAL ENGINEER  
CIVIL  
No. 80356  
Exp. 12/31/24  
STATE OF CALIFORNIA

**PASCO LARET SUITER**

& ASSOCIATES  
San Diego | Solana Beach | Orange County  
Phone 858.259.8212 | www.plsaengineering.com



# GUADALAJARA LAKES

OCEANSIDE, CA



01.16.2023

- A-1 Typical Plotting
- A-2 Plan 1 (1869) - Front Elevations
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- A-4 Plan 1A (1869) - "A" Ranch Elevations
- A-5 Plan 1A (1869) - "A" Ranch Enhanced Elevations
- A-6 Plan 1B (1869) - "B" Farmhouse Elevations
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- A-23 Plan 3A (2220) - "A" Ranch Enhanced Elevations
- A-24 Plan 3B (2220) - "B" Farmhouse Elevations
- A-25 Plan 3B (2220) - "B" Farmhouse Enhanced Elevations
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- A-29 Written Color Schemes
- A-30 Color Boards - "A" Ranch
- A-31 Color Boards - "B" Farmhouse
- A-32 Color Boards - "C" Progressive Prairie



**Developer:**  
Rincon Homes, Inc.  
5315 Avenida Encinas Suite 200,  
Carlsbad, Ca 92008  
Contact: : William R. Inghram  
Email: Wingram@Therlco.com  
Business: (831) 383-8083  
www.rincon-Homes.com



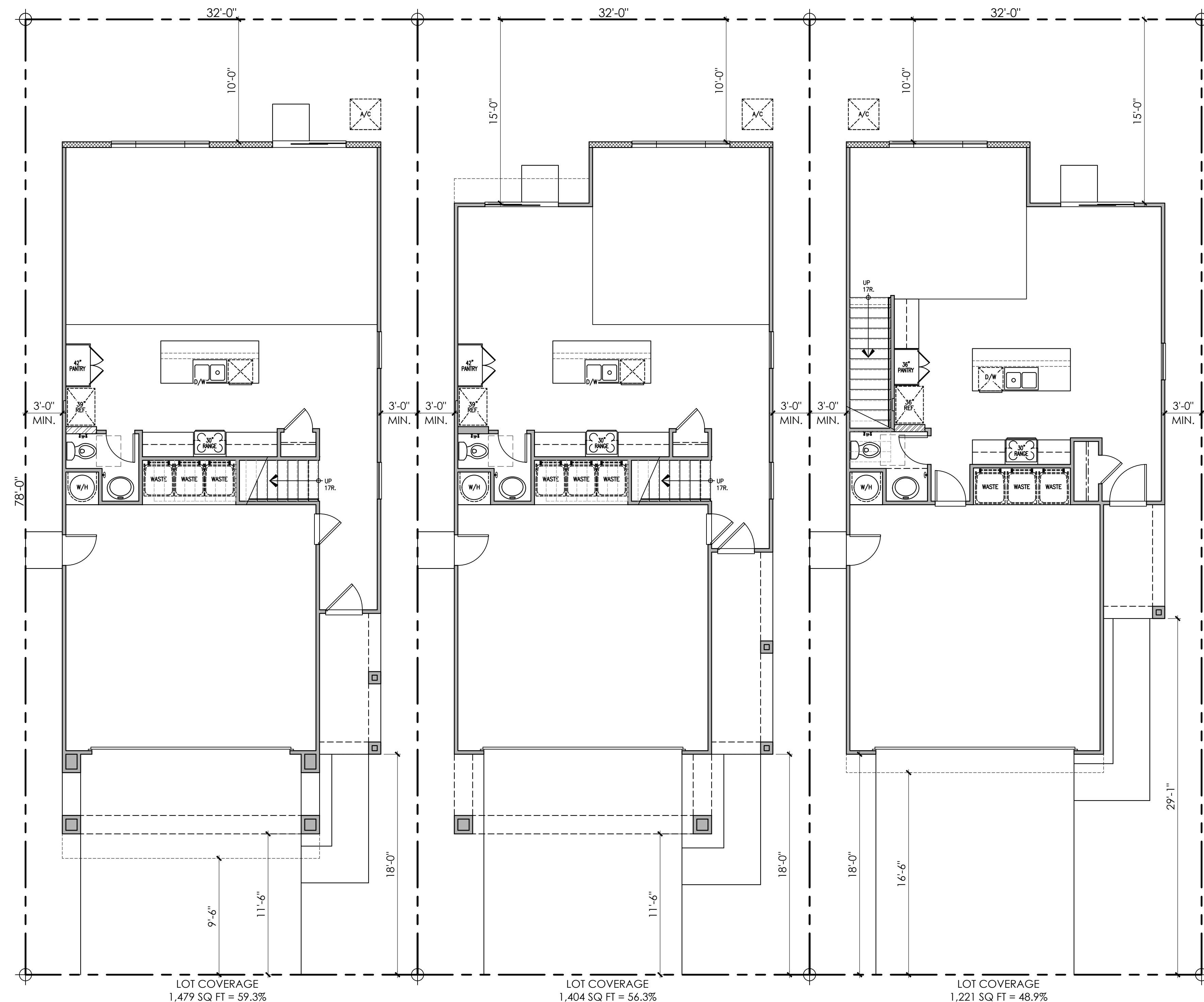
**Architect:**  
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1360 Reynolds Ave. Suite 110  
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1911 San Diego Avenue, Ste 100  
San Diego, CA 92110  
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www.plsaengineering.com



**Landscape:**  
Brightview  
8 Hughes, Suite 150  
Irvine, CA 92618  
Contact: Wade Ganes  
Email: wade.ganes@brightview.com  
Business: (949) 238-4900  
www.brightview.com





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Refer to landscape drawings for wall,  
tree, and shrub locations

PLAN 1 'A' - RANCH



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Refer to landscape drawings for wall,  
tree, and shrub locations

PLAN 1 'B' - FARMHOUSE

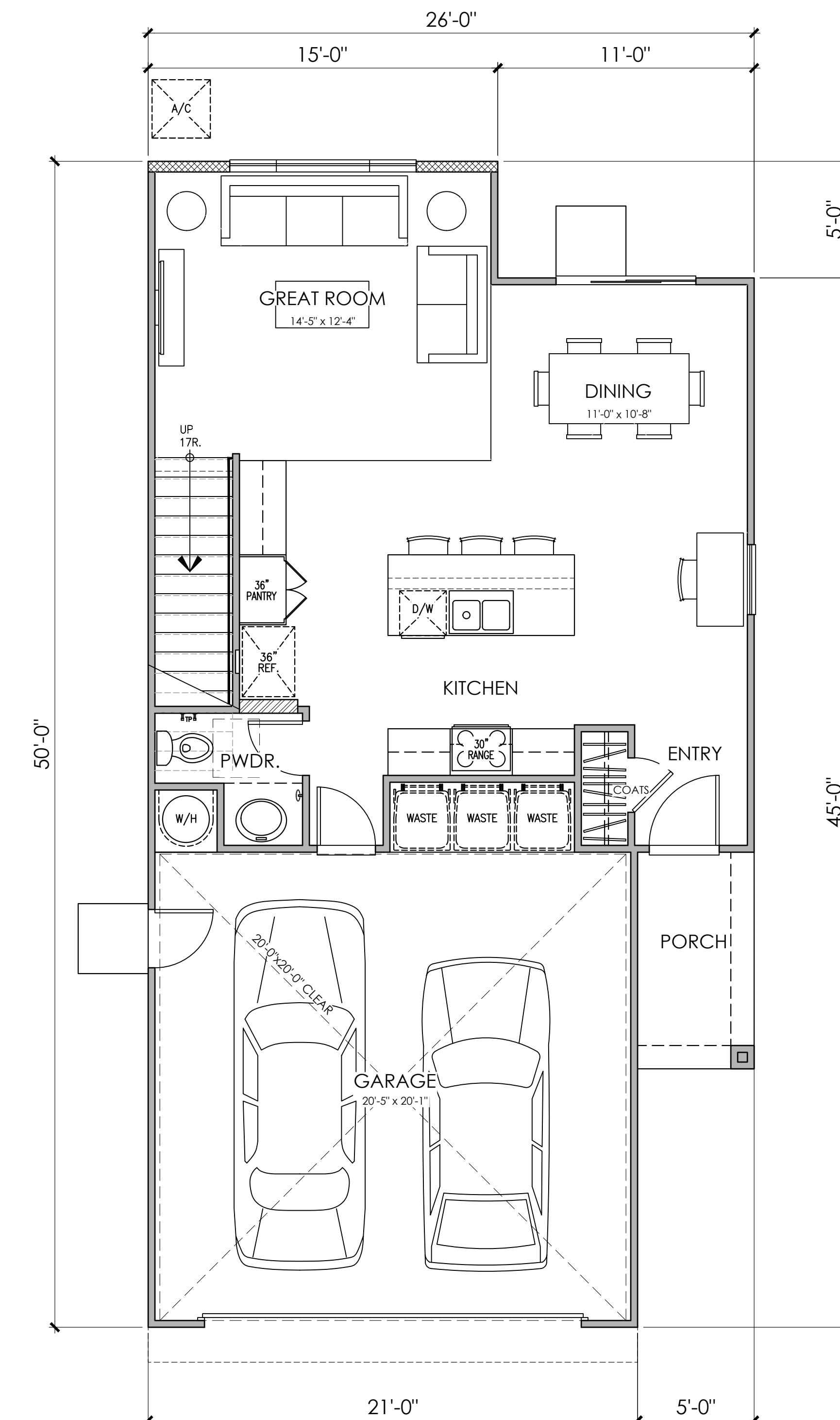
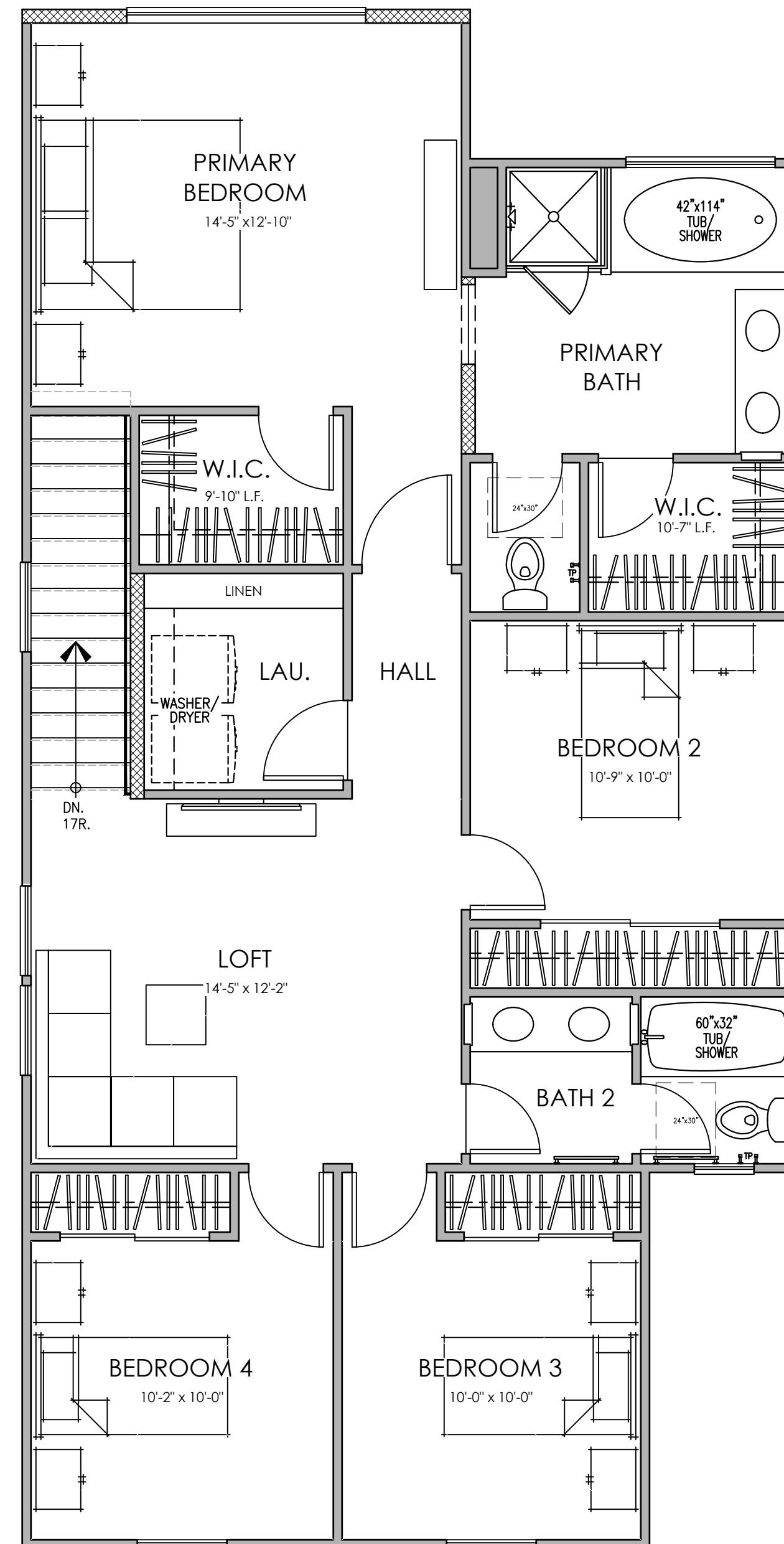


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Refer to landscape drawings for wall,  
tree, and shrub locations

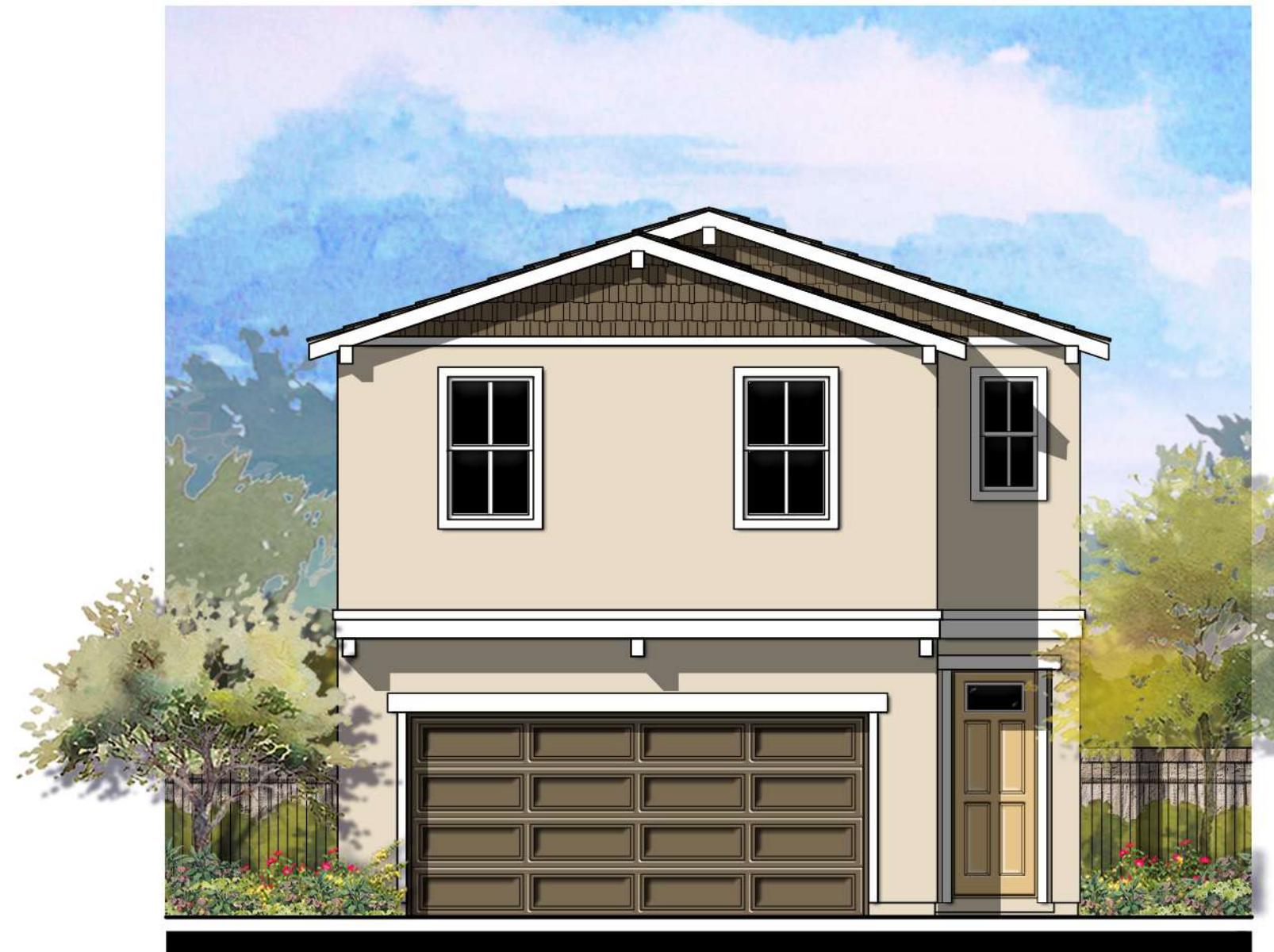
PLAN 1 'C' - PROGRESSIVE PRAIRIE

PLAN 1 (1869)  
FRONT ELEVATIONS



PLAN 1 (1869) AREA TABULATION	
CONDITIONED SPACE	
FIRST FLOOR AREA	694 SQ. FT.
SECOND FLOOR AREA	1,175 SQ. FT.
TOTAL DWELLING	1,869 SQ. FT.
UNCONDITIONED SPACE	
GARAGE	449 SQ. FT.
PORCH	46 SQ. FT.

PLAN 1A (1869)  
4 BEDROOM, 2.5 BATH, LOFT, TECH, 2-CAR GARAGE  
FLOOR PLAN



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT



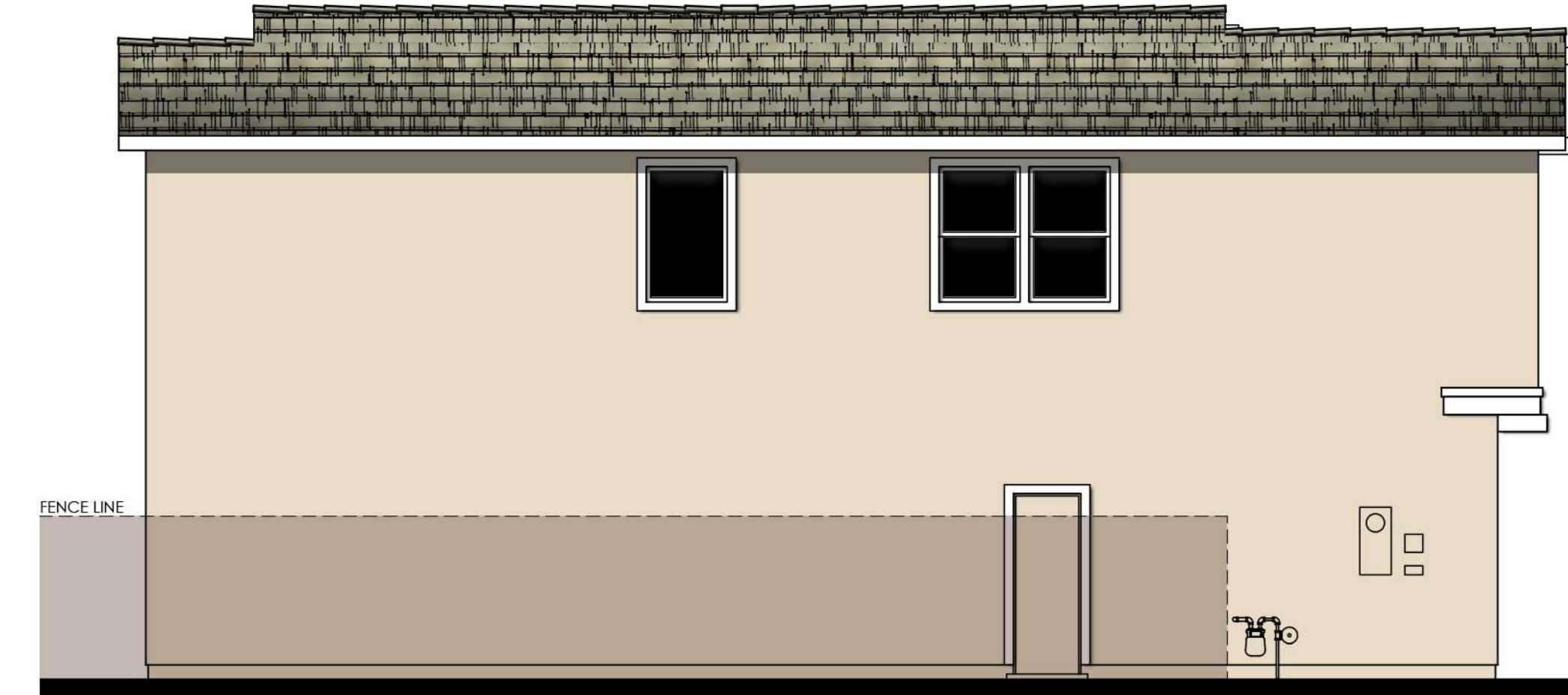
RIGHT

**MATERIALS LEGEND**  
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	CEMENTITIOUS FIBER SHINGLES
WALL:	STUCCO
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



REAR



LEFT

COLOR SCHEME 1  
PLAN 1A (1869)  
"A" RANCH ELEVATIONS



REAR

COLOR SCHEME 1  
PLAN 1A (1869)  
RANCH ENHANCED ELEVATIONS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT



RIGHT



REAR

#### MATERIALS LEGEND (WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL/ KNEE BRACE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	BOARD AND BATTEN
WALL:	STUCCO/ BOARD AND BATTEN
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



LEFT

#### COLOR SCHEME 4 PLAN 1B (1869) "B" FARMHOUSE ELEVATIONS



REAR

COLOR SCHEME 4  
PLAN 1B (1869)  
"B" FARMHOUSE ENHANCED ELEVATIONS

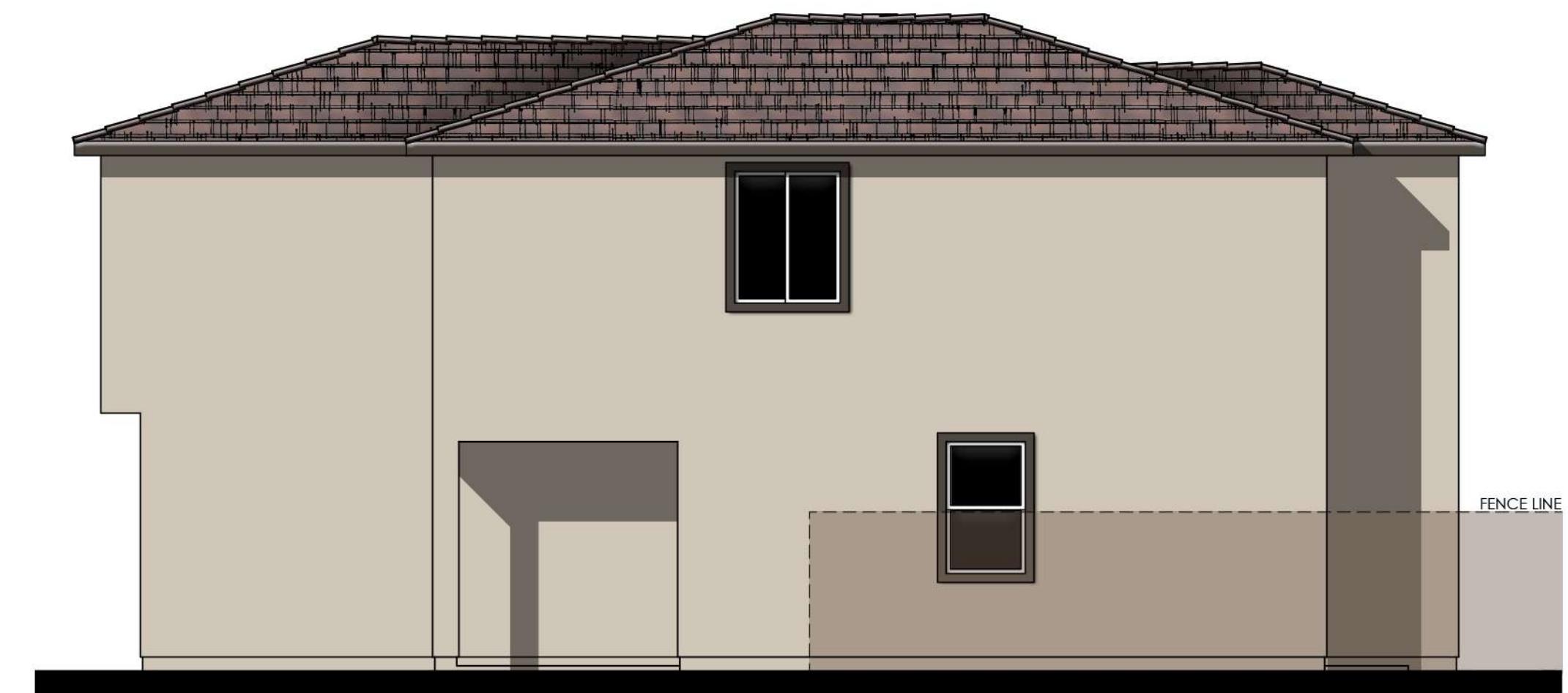
0 2 4 8 12



Kevin L. Crook Architect, Inc.

Refer to landscape drawings for wall, tree, and shrub locations

FRONT



RIGHT

**MATERIALS LEGEND**  
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
WALL:	STUCCO/ 4" LAP SIDING
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM



REAR



LEFT

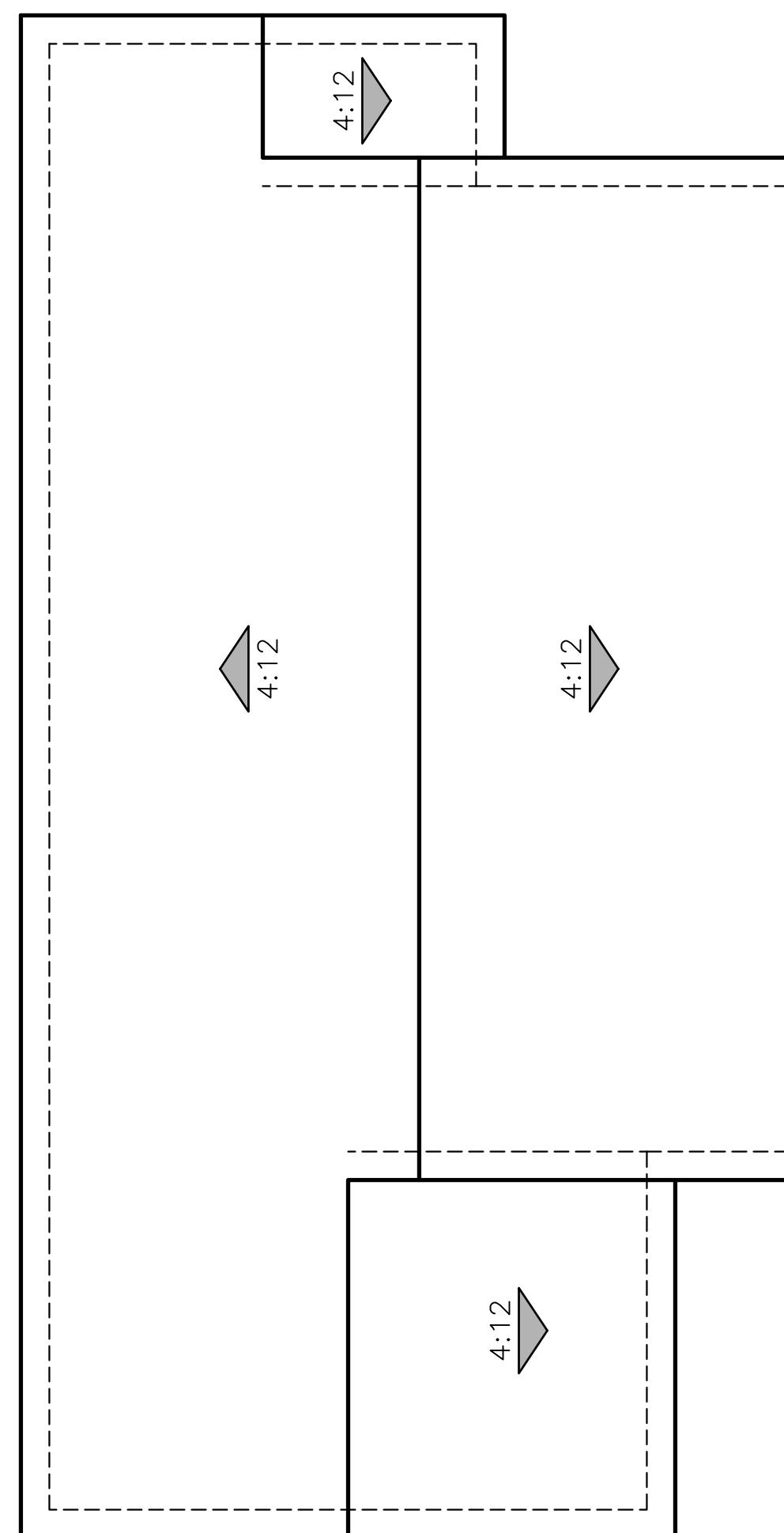
COLOR SCHEME 7  
PLAN 1C (1869)  
"C" PROGRESSIVE PRAIRIE ELEVATIONS



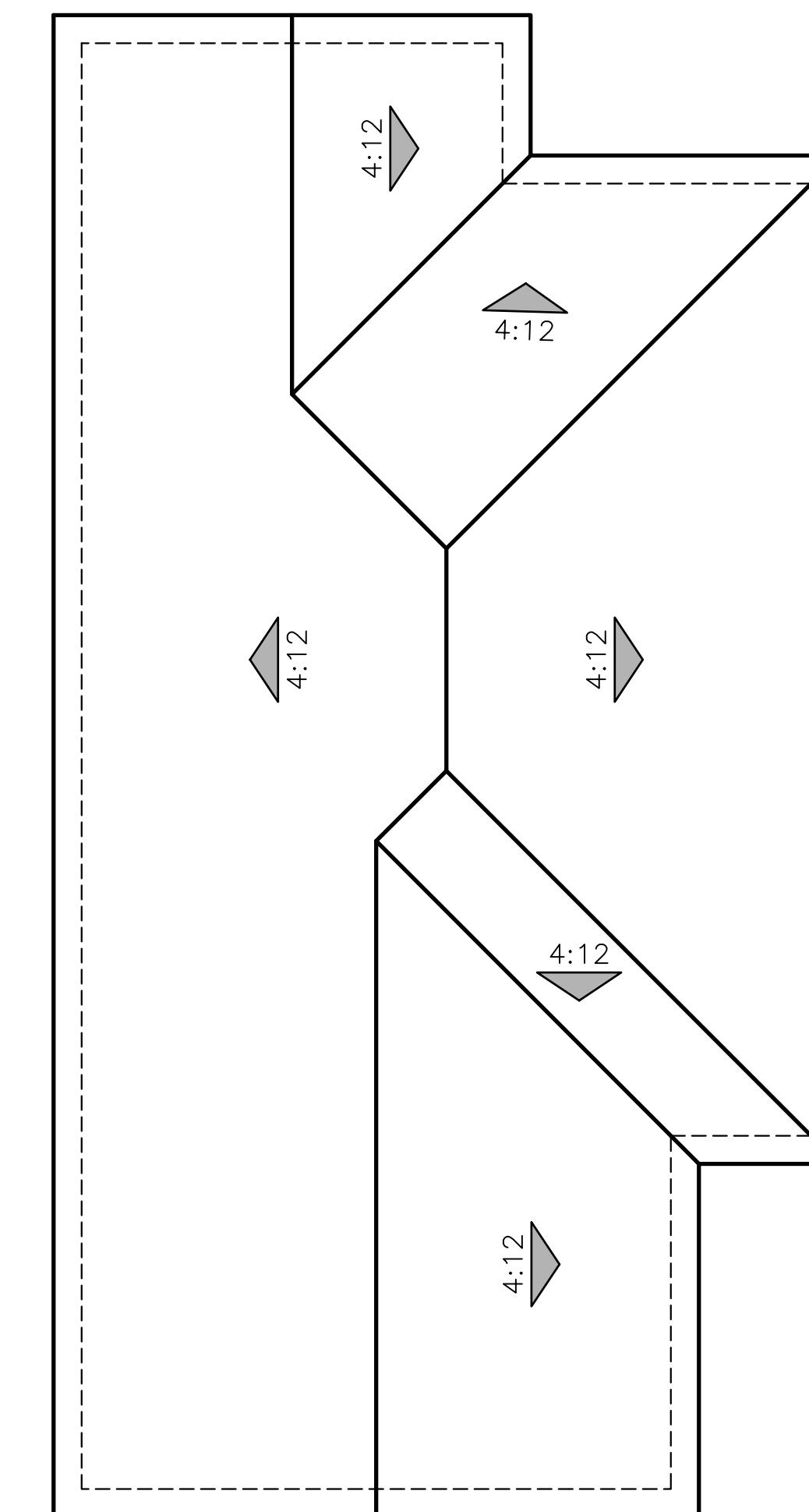
REAR

COLOR SCHEME 7  
PLAN 1C (1869)  
"C" PROGRESSIVE PRAIRIE ENHANCED ELEVATIONS

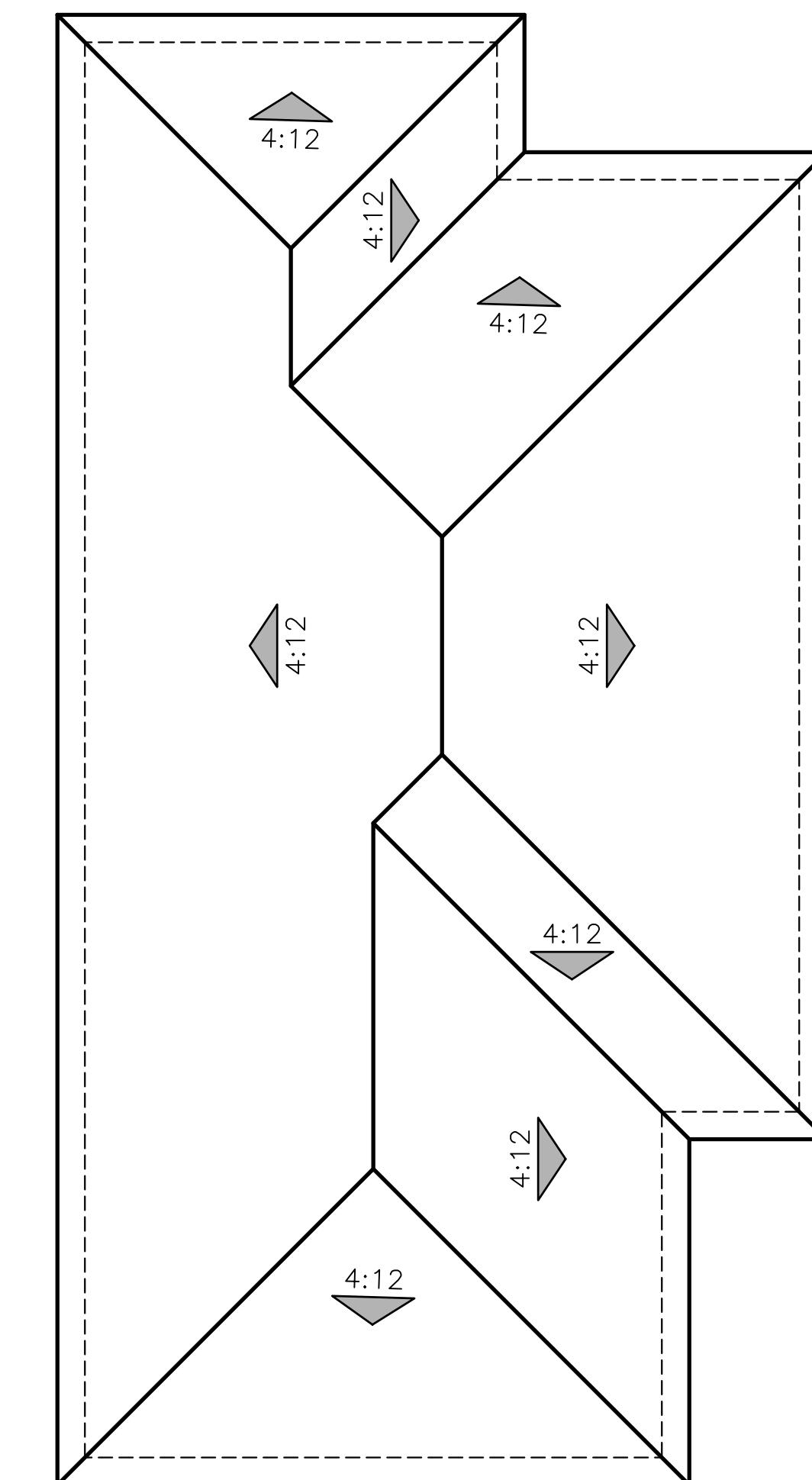
0 2 4 8 12



A - RANCH



B - FARMHOUSE



C - PROGRESSIVE PRAIRE

PLAN 1 (1869)  
ROOF PLANS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

PLAN 2 'A' - RANCH



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Refer to landscape drawings for wall, tree, and shrub locations

PLAN 2 'B' - FARMHOUSE

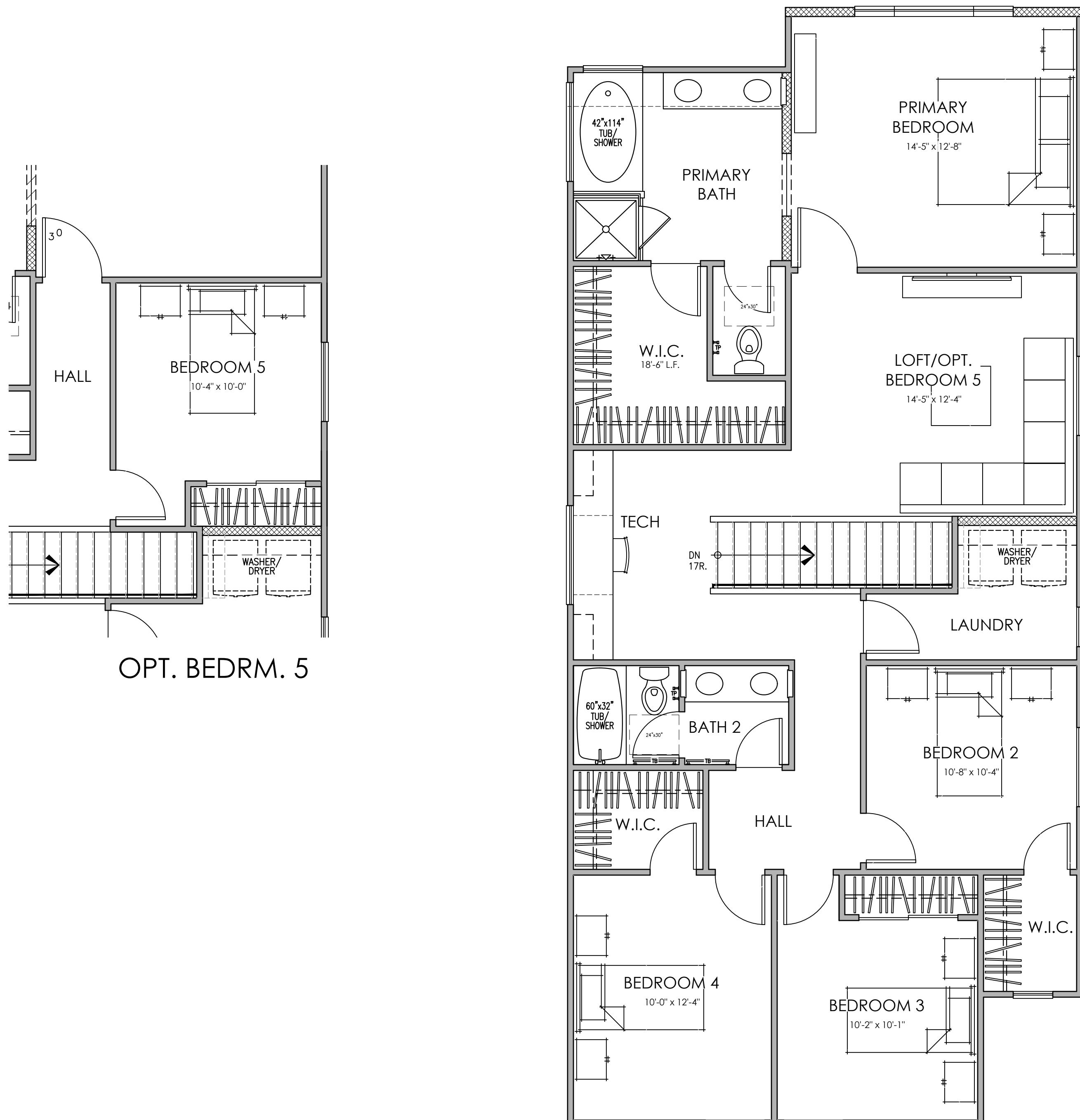


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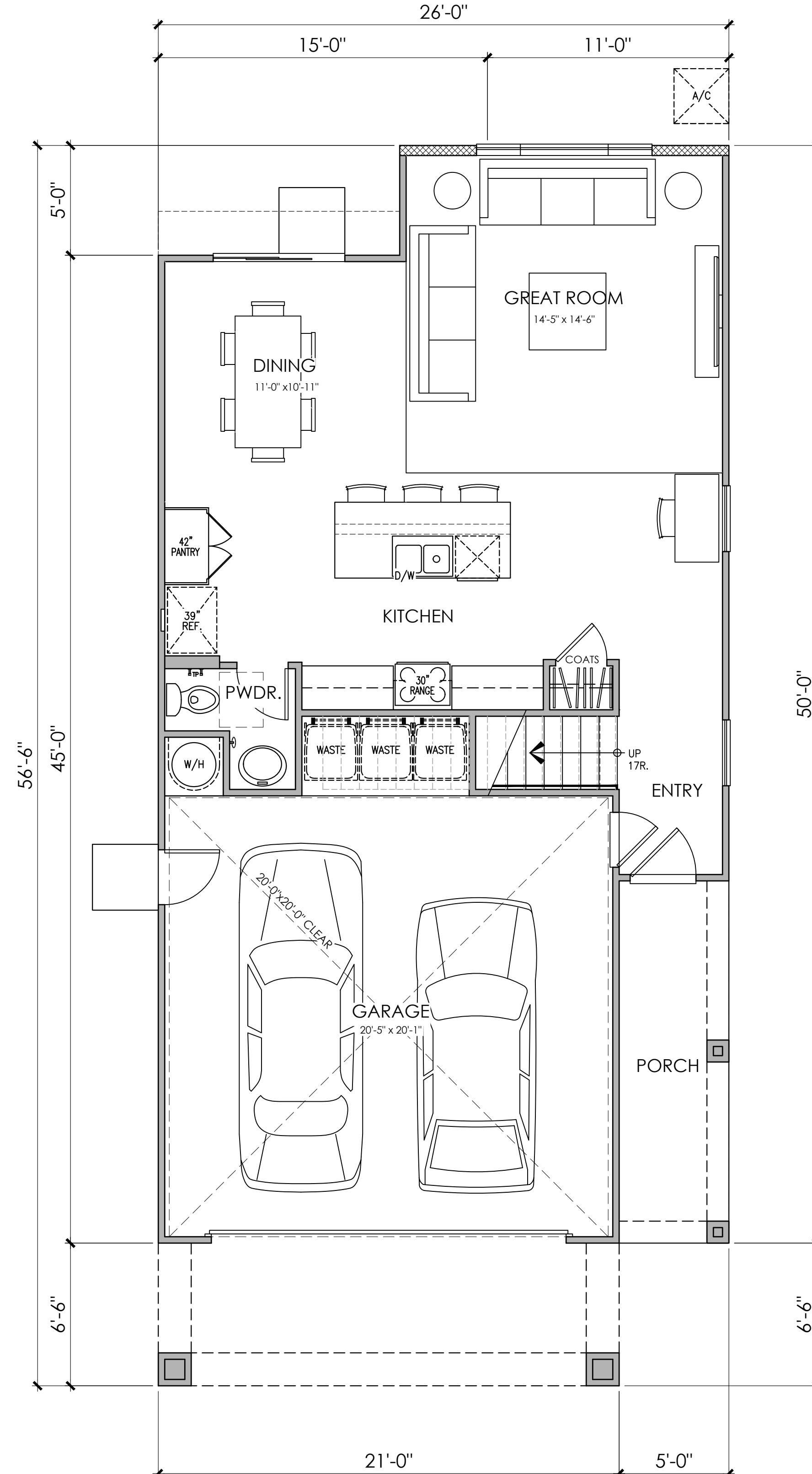
Refer to landscape drawings for wall, tree, and shrub locations

PLAN 2 'C' - PROGRESSIVE PRAIRIE

PLAN 2 (2064)  
FRONT ELEVATIONS



**PLAN 2A (2064)**  
4 BEDROOM, 2.5 BATH, LOFT/OPT. BEDROOM 5, 2-CAR GARAGE  
FLOOR PLAN



PLAN 2 (2064) AREA TABULATION	
<u>CONDITIONED SPACE</u>	
FIRST FLOOR AREA	700 SQ. FT.
SECOND FLOOR AREA	1,364 SQ. FT.
TOTAL DWELLING	2,064 SQ. FT.
<u>UNCONDITIONED SPACE</u>	
GARAGE	462 SQ. FT.
PORCH "A"	83 SQ. FT.

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT



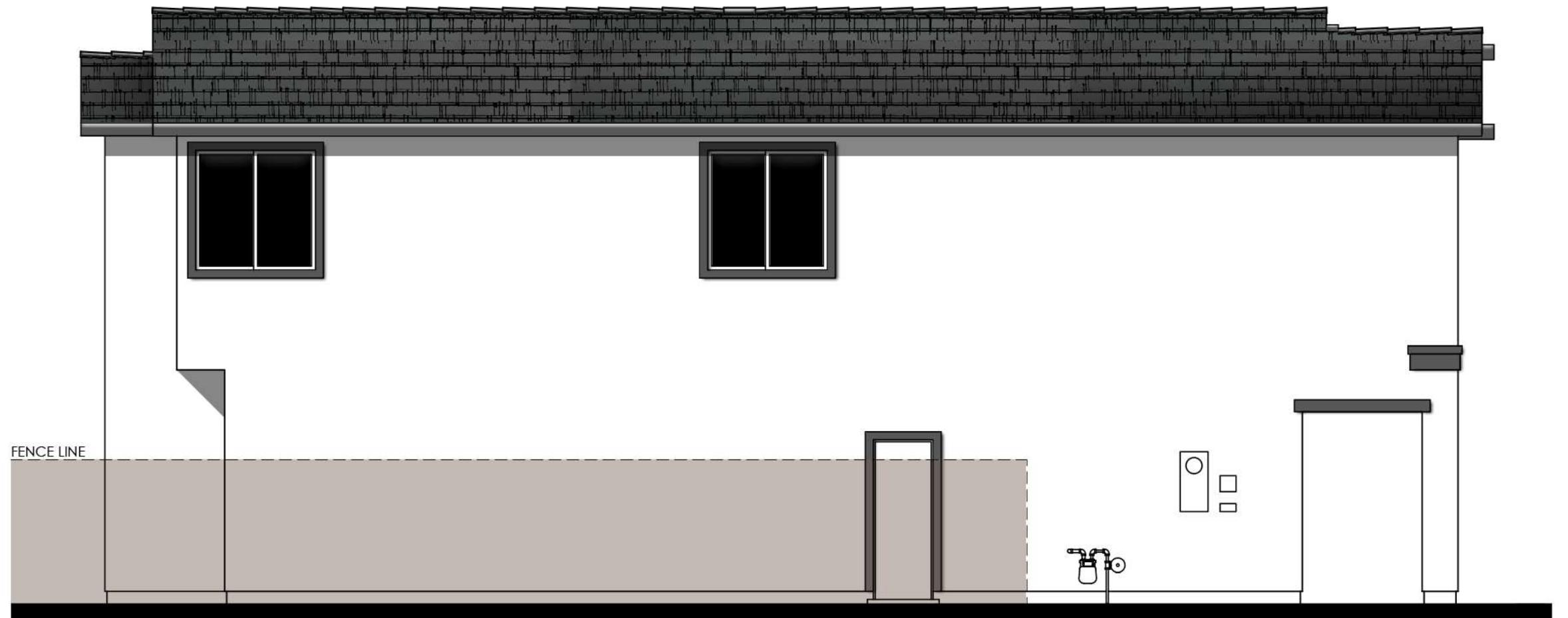
RIGHT

**MATERIALS LEGEND**  
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	CEMENTITIOUS FIBER SHINGLES
WALL:	STUCCO
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



REAR



LEFT

COLOR SCHEME 2  
PLAN 2A (2064)  
"A" RANCH ELEVATIONS



REAR

COLOR SCHEME 2  
PLAN 2A (2064)  
RANCH ENHANCED ELEVATIONS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT



RIGHT

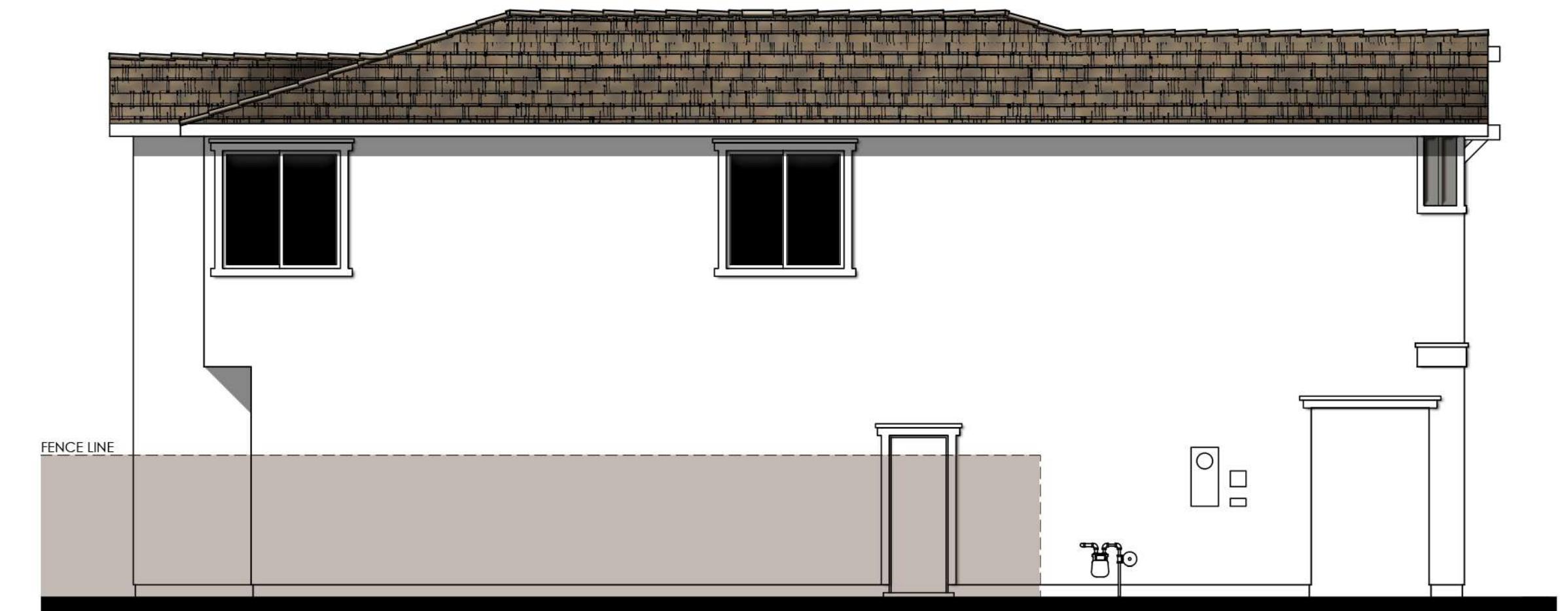


REAR

**MATERIALS LEGEND**

(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL/ KNEE BRACE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	BOARD AND BATTEN
WALL:	STUCCO/ BOARD AND BATTEN
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



LEFT

COLOR SCHEME 5  
PLAN 2B (2064)  
"B" FARMHOUSE ELEVATIONS

0 2 4 8 12



REAR

COLOR SCHEME 5  
PLAN 2B (2064)  
"B" FARMHOUSE ENHANCED ELEVATIONS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations



FRONT

**MATERIALS LEGEND**

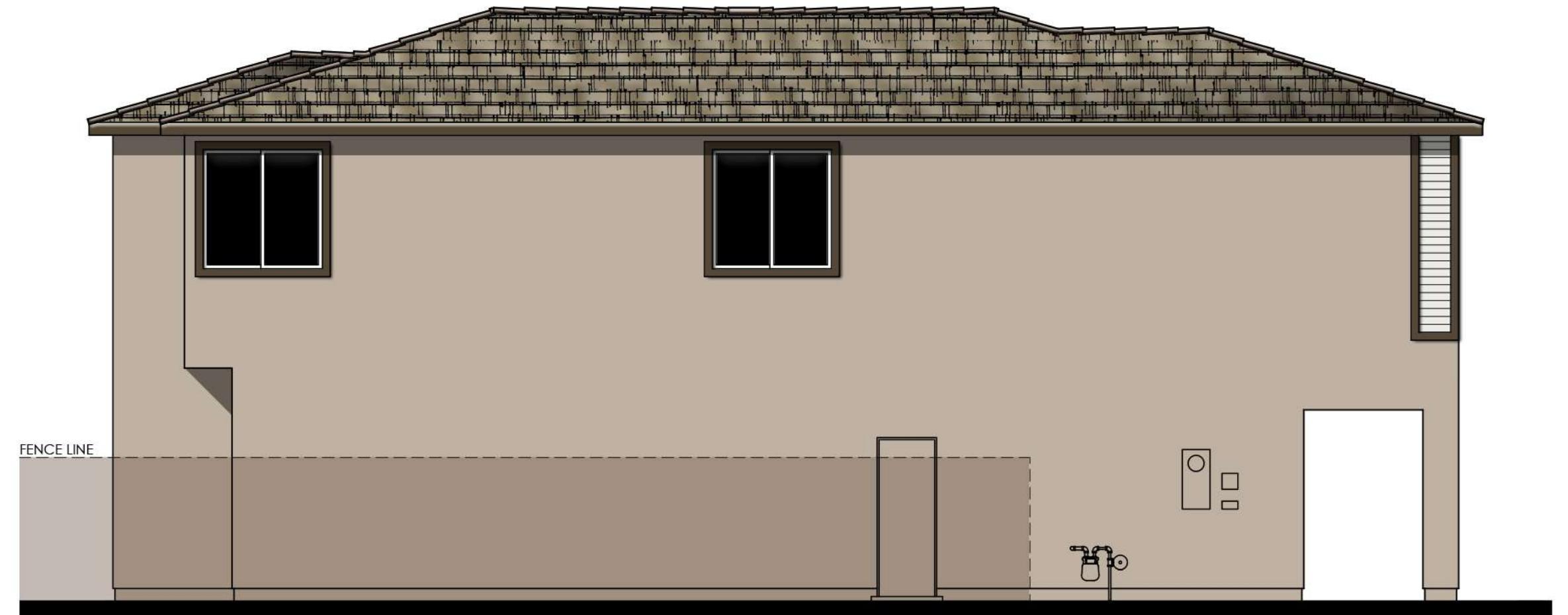
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
WALL:	STUCCO/ 4" LAP SIDING
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM

RIGHT



REAR



LEFT

COLOR SCHEME 8  
**PLAN 2C (2064)**  
 "C" PROGRESSIVE PRAIRIE ELEVATIONS

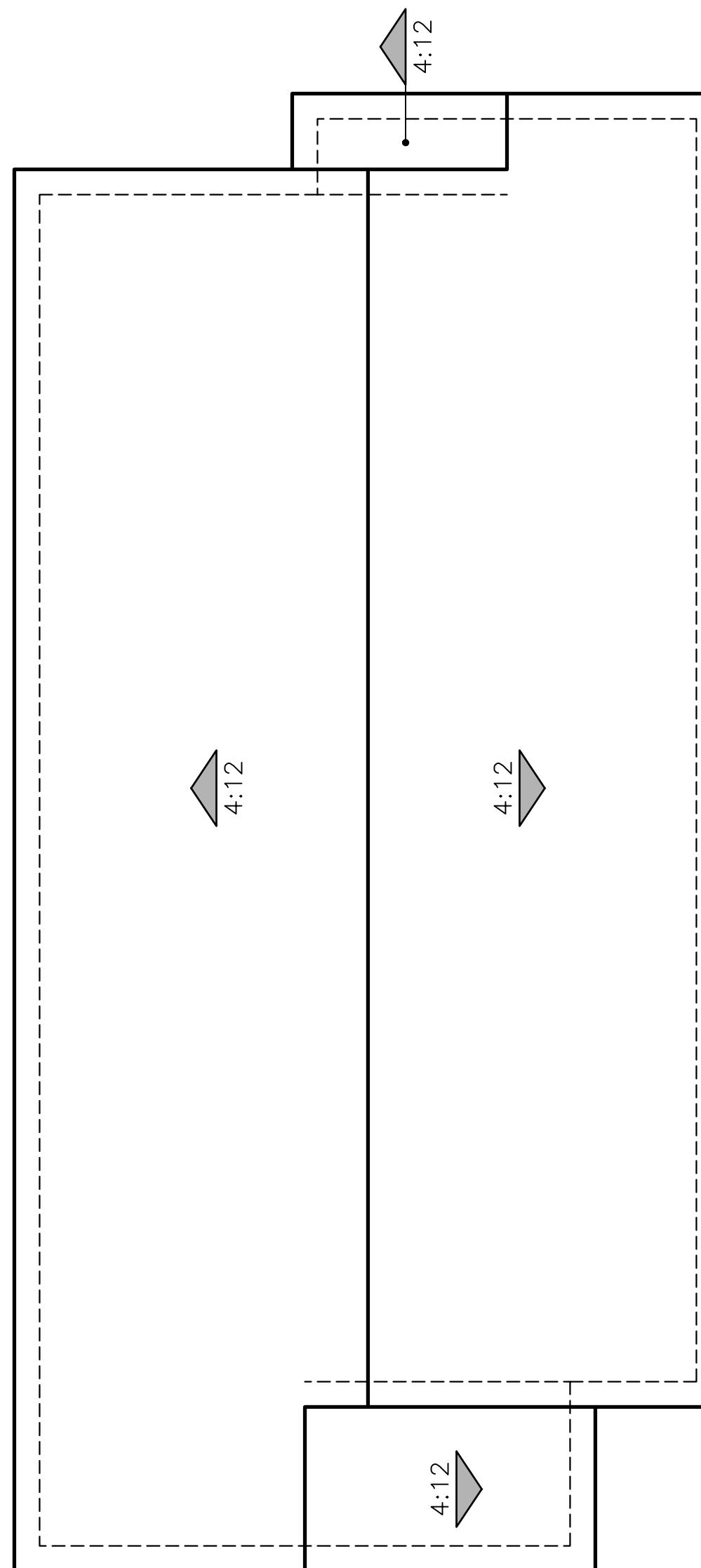
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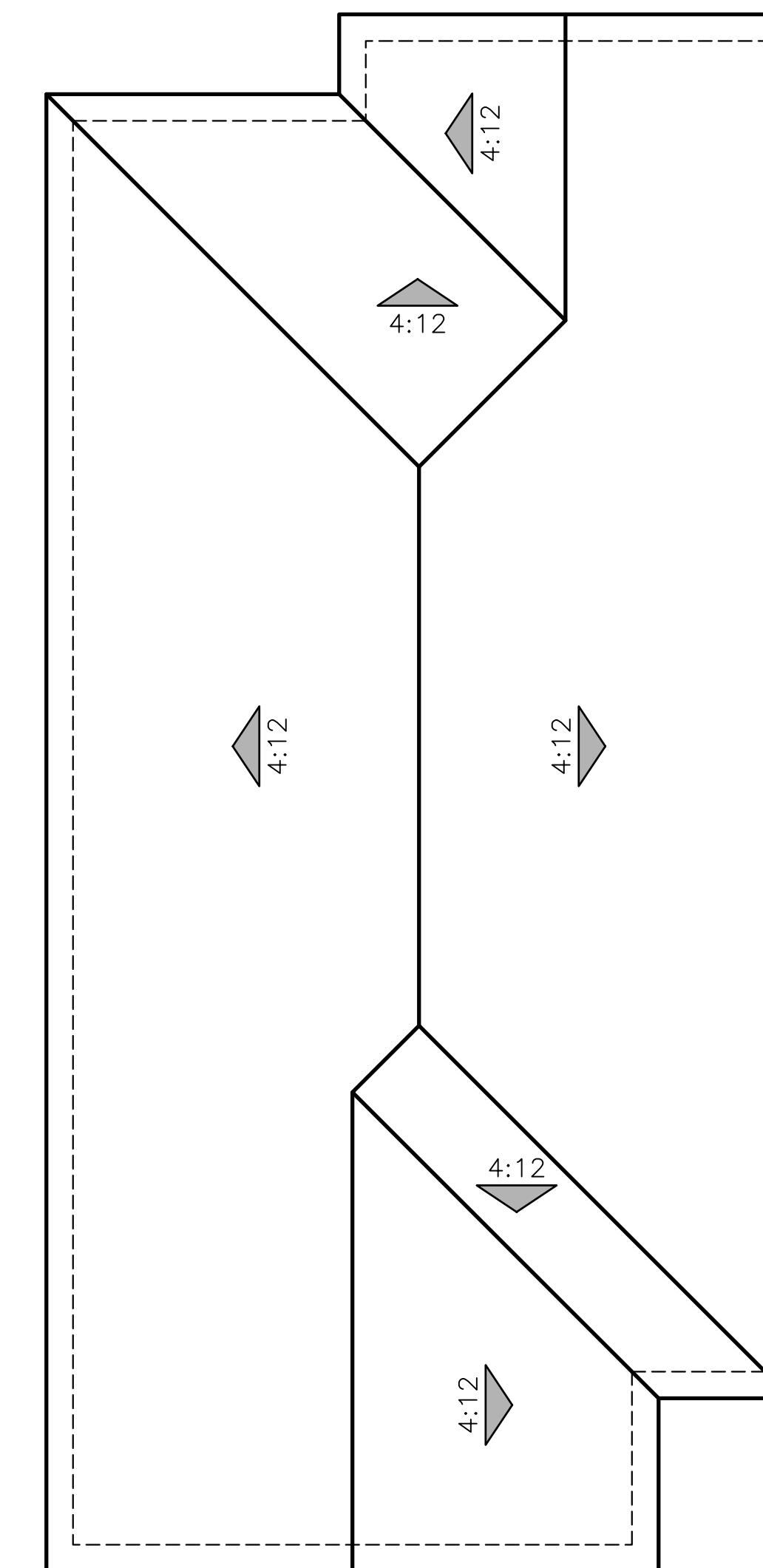
REAR

COLOR SCHEME 8  
PLAN 2C (2064)  
"C" PROGRESSIVE PRAIRIE ENHANCED ELEVATIONS

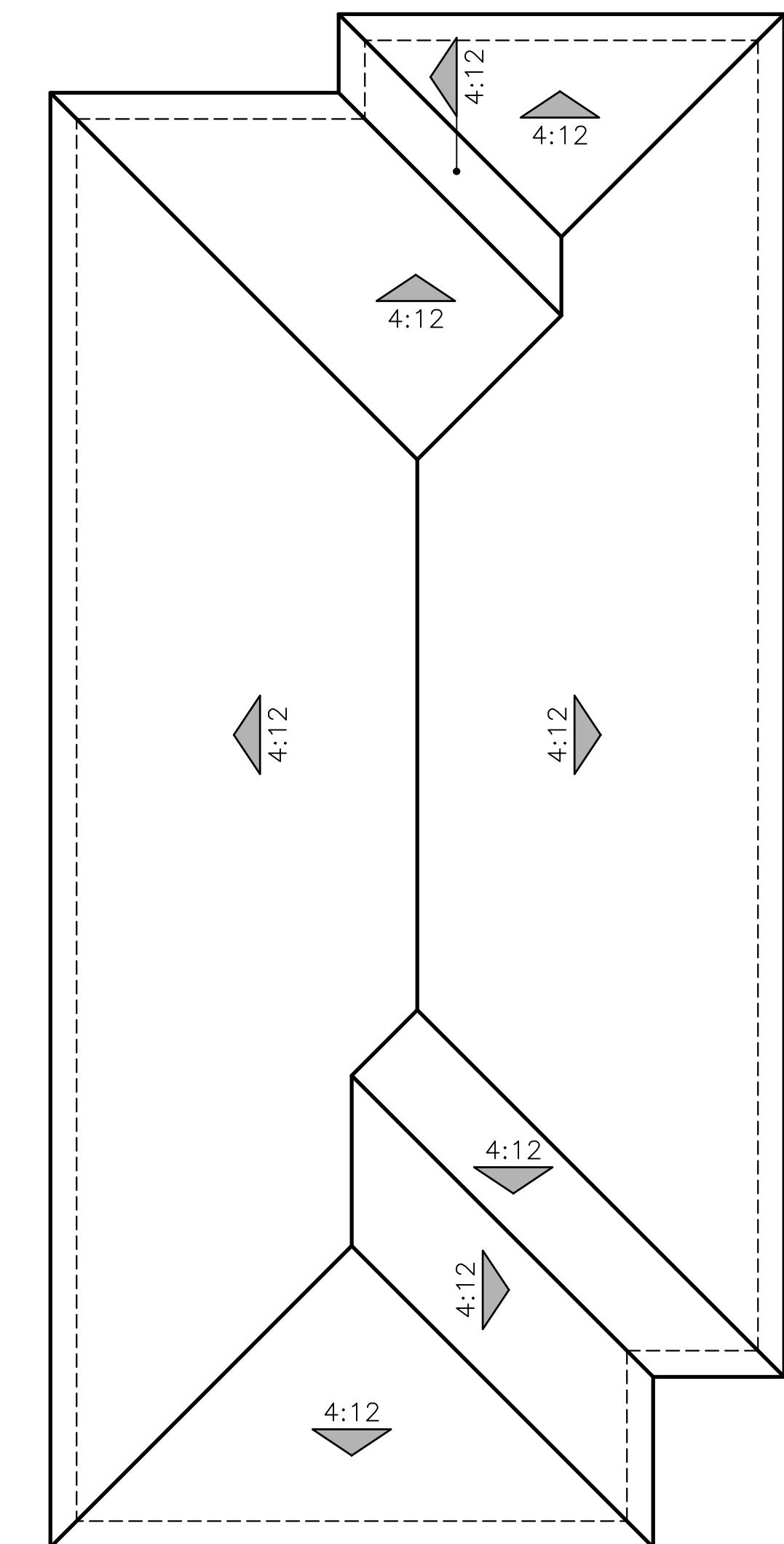
0 2 4 8 12



A - RANCH



B - FARMHOUSE



C - PROGRESSIVE PRAIRE

PLAN 2 (2064)  
ROOF PLANS



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Refer to landscape drawings for wall,  
tree, and shrub locations

PLAN 3 'A' - RANCH



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Refer to landscape drawings for wall,  
tree, and shrub locations

PLAN 3 'B' - FARMHOUSE



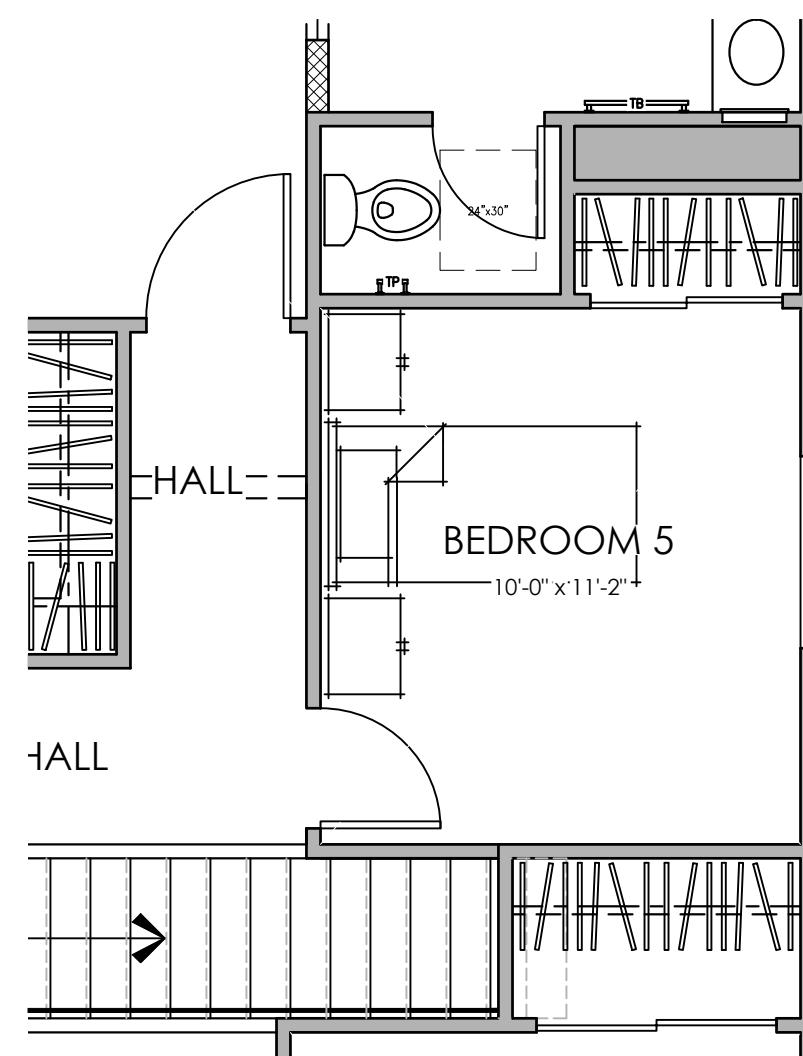
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Refer to landscape drawings for wall,  
tree, and shrub locations

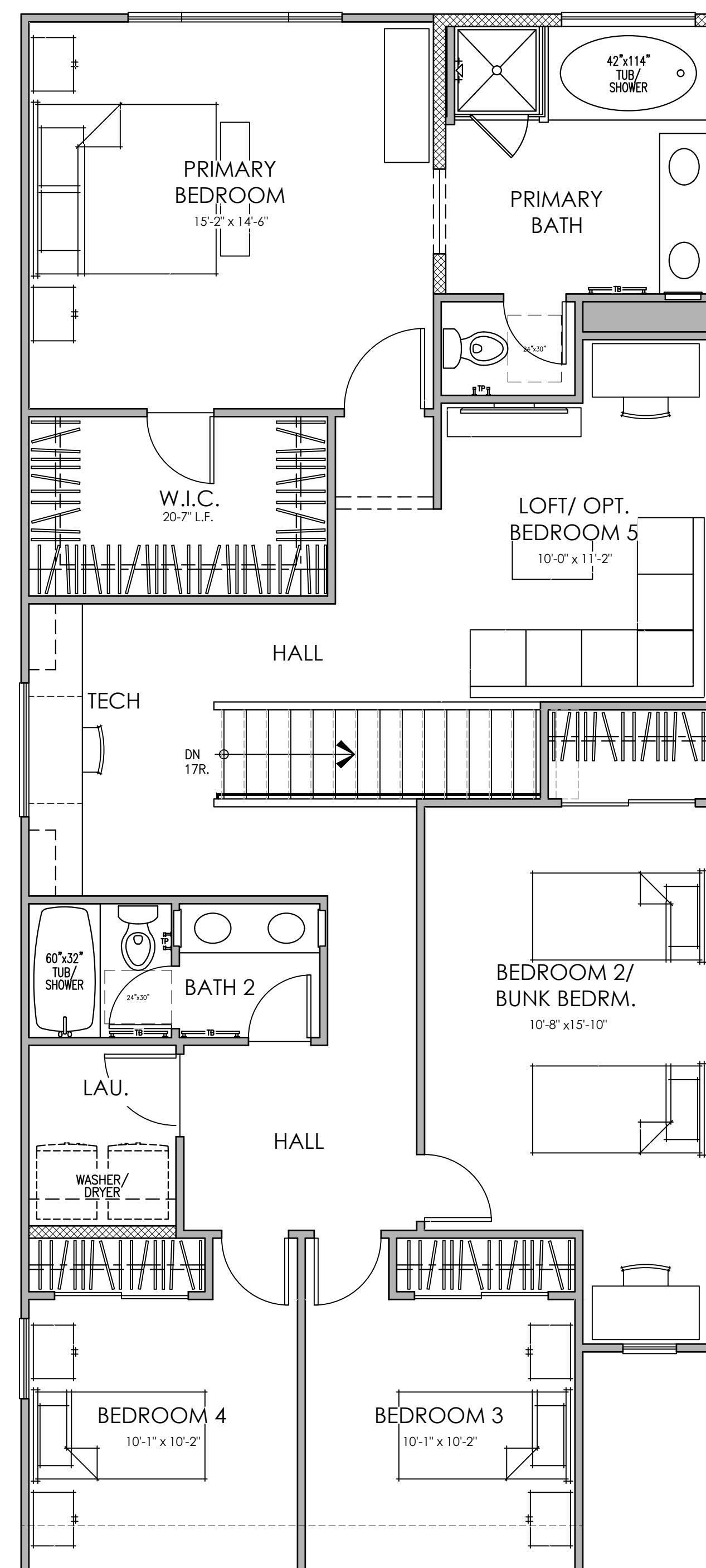
PLAN 3 'C' - PROGRESSIVE PRAIRIE

PLAN 3 (2220)  
FRONT ELEVATIONS

0 2 4 8 12

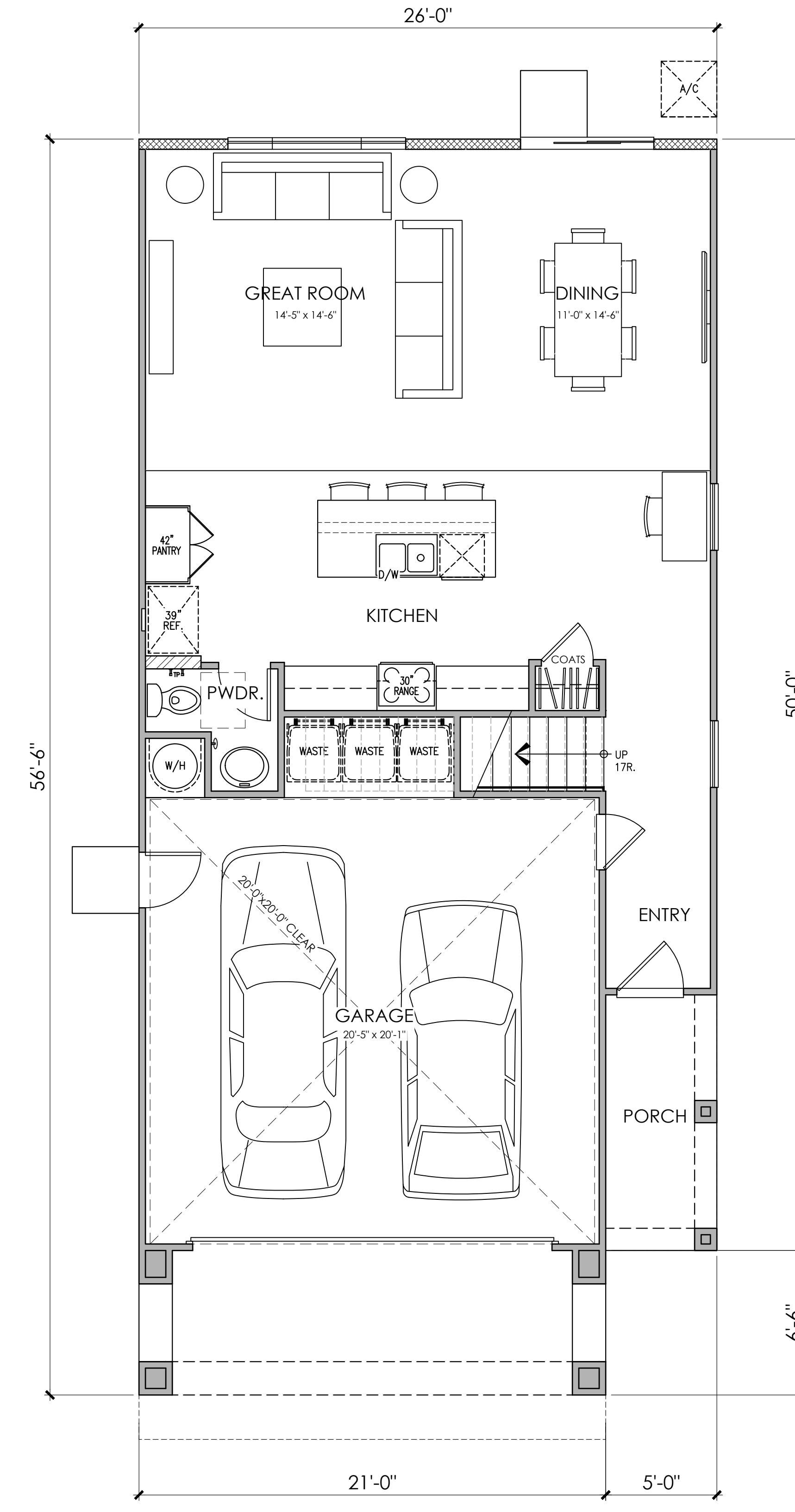


OPT. - BEDROOM 5  
SECOND FLOOR



SECOND FLOOR

PLAN 3A (2220)  
4 BEDROOM, 2.5 BATH, LOFT, TECH, OPT. BEDROOM 5, 2-CAR GARAGE  
FLOOR PLAN



FIRST FLOOR

PLAN 3 (2220) AREA TABULATION	
CONDITIONED SPACE	
FIRST FLOOR AREA	781 SQ. FT.
SECOND FLOOR AREA	1,439 SQ. FT.
TOTAL DWELLING	2,220 SQ. FT.
UNCONDITIONED SPACE	
GARAGE	462 SQ. FT.
PORCH	58 SQ. FT.

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

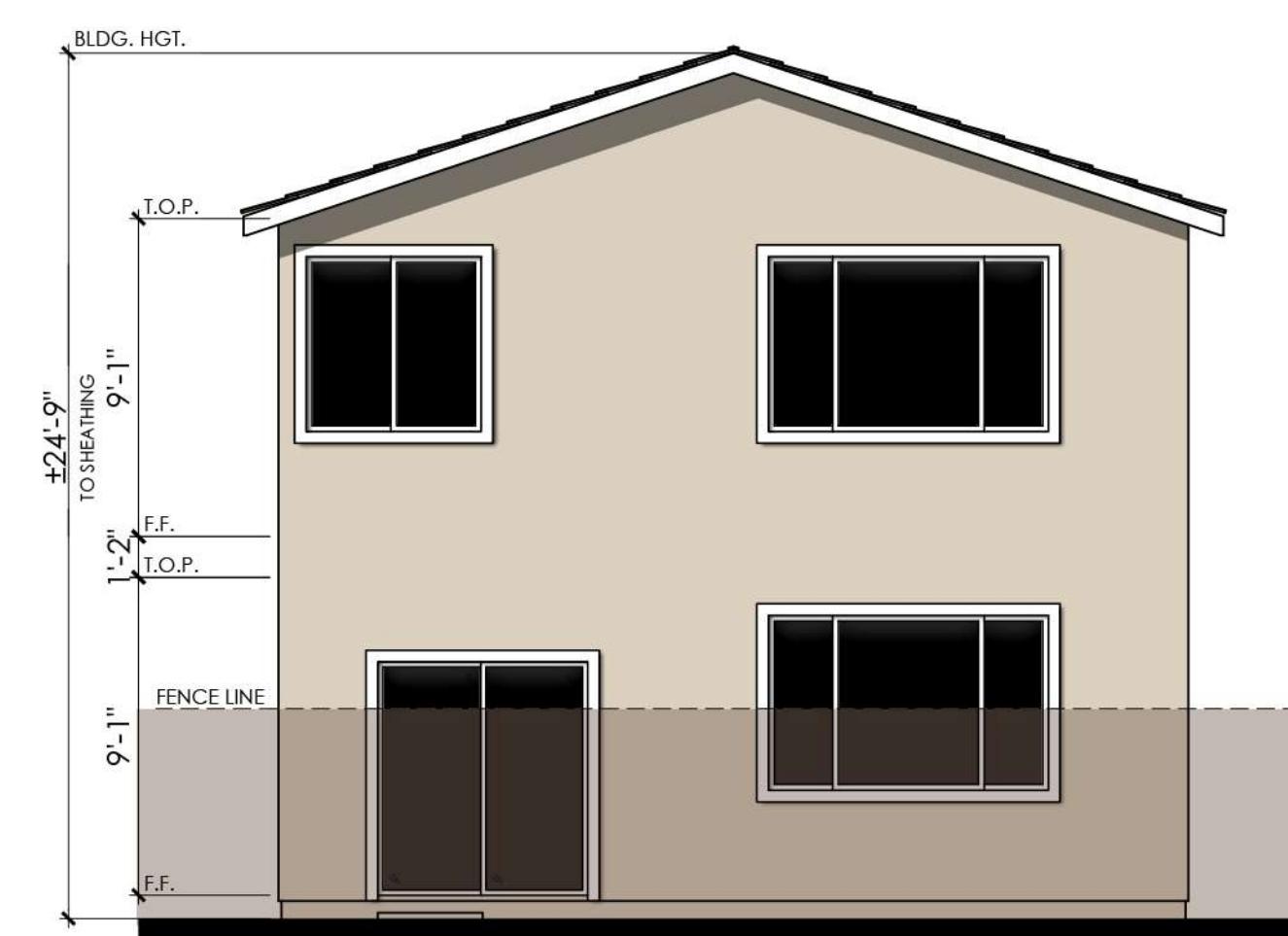
FRONT



RIGHT

**MATERIALS LEGEND**  
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	CEMENTITIOUS FIBER SHINGLES
WALL:	STUCCO
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



REAR



LEFT

COLOR SCHEME 3  
PLAN 3A (2220)  
"A" RANCH ELEVATIONS

0 2 4 8 12



REAR

COLOR SCHEME 3  
PLAN 3A (2220)  
"A" RANCH ENHANCED ELEVATIONS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT



RIGHT

**MATERIALS LEGEND**  
(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
ROOF EXTENSIONS:	WOOD CORBEL/ KNEE BRACE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
GABLE END:	BOARD AND BATTEN
WALL:	STUCCO/ BOARD AND BATTEN
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM
PORCH:	WOOD POST AND BEAM



REAR



LEFT

COLOR SCHEME 6  
PLAN 3B (2220)  
"B" FARMHOUSE ELEVATIONS

0 2 4 8 12



REAR

COLOR SCHEME 6  
PLAN 3B (2220)  
"B" FARMHOUSE ENHANCED ELEVATIONS

0 2 4 8 12



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Refer to landscape drawings for wall, tree, and shrub locations

FRONT

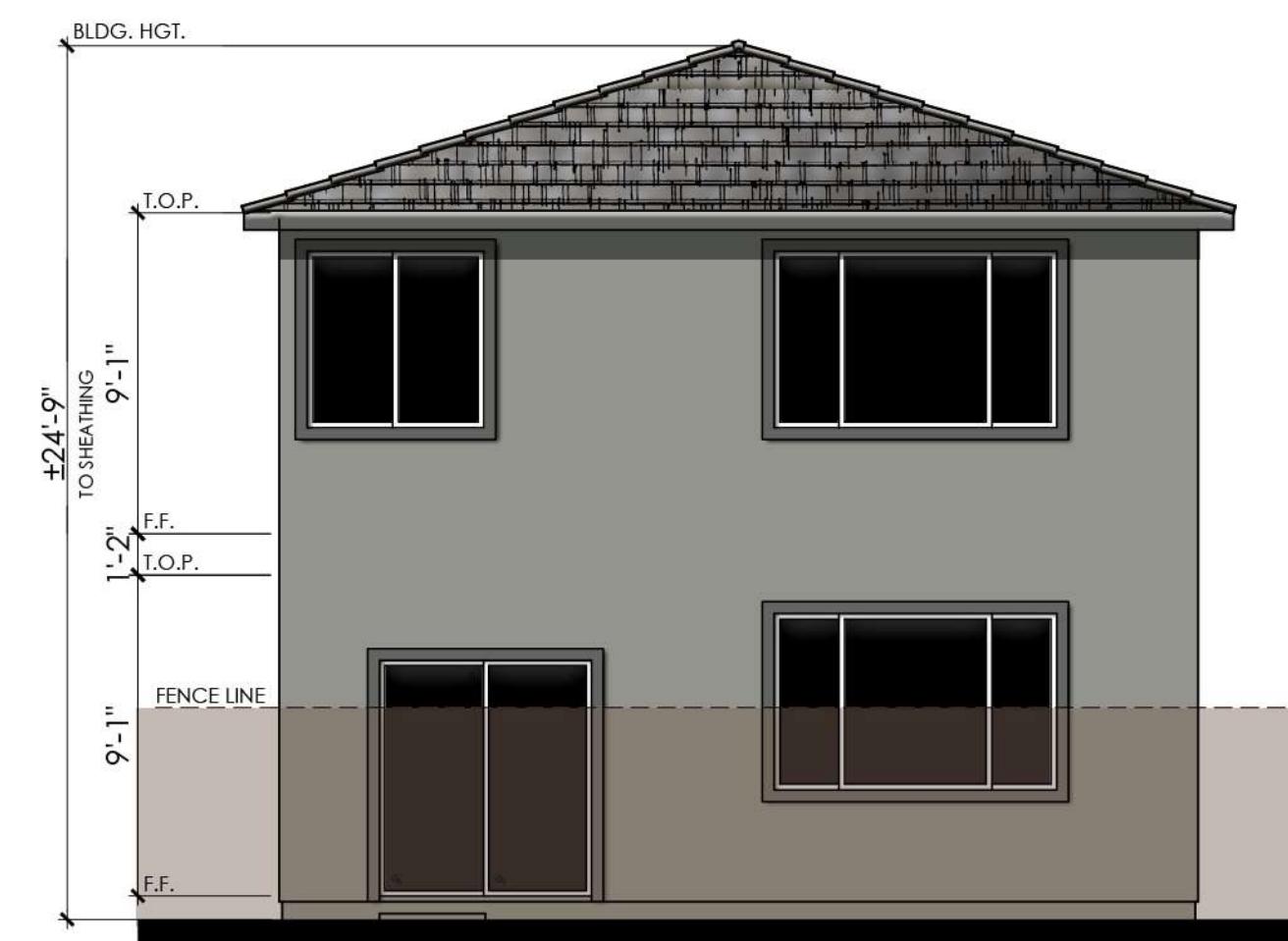


RIGHT

**MATERIALS LEGEND**

(WHERE OCCURS)

FRONT DOOR:	FIBERGLASS
GARAGE DOOR:	METAL SECTIONAL
ROOF:	CONCRETE FLAT TILE
FASCIA:	2x6 WOOD
BARGE:	2x6 WOOD
WALL:	STUCCO/ 4" LAP SIDING
WINDOWS:	VINYL W/ GRIDS
TRIM:	STUCCO OVER RIGID FOAM



REAR



LEFT

COLOR SCHEME 9  
**PLAN 3C (2220)**  
 "C" PROGRESSIVE PRAIRIE ELEVATIONS

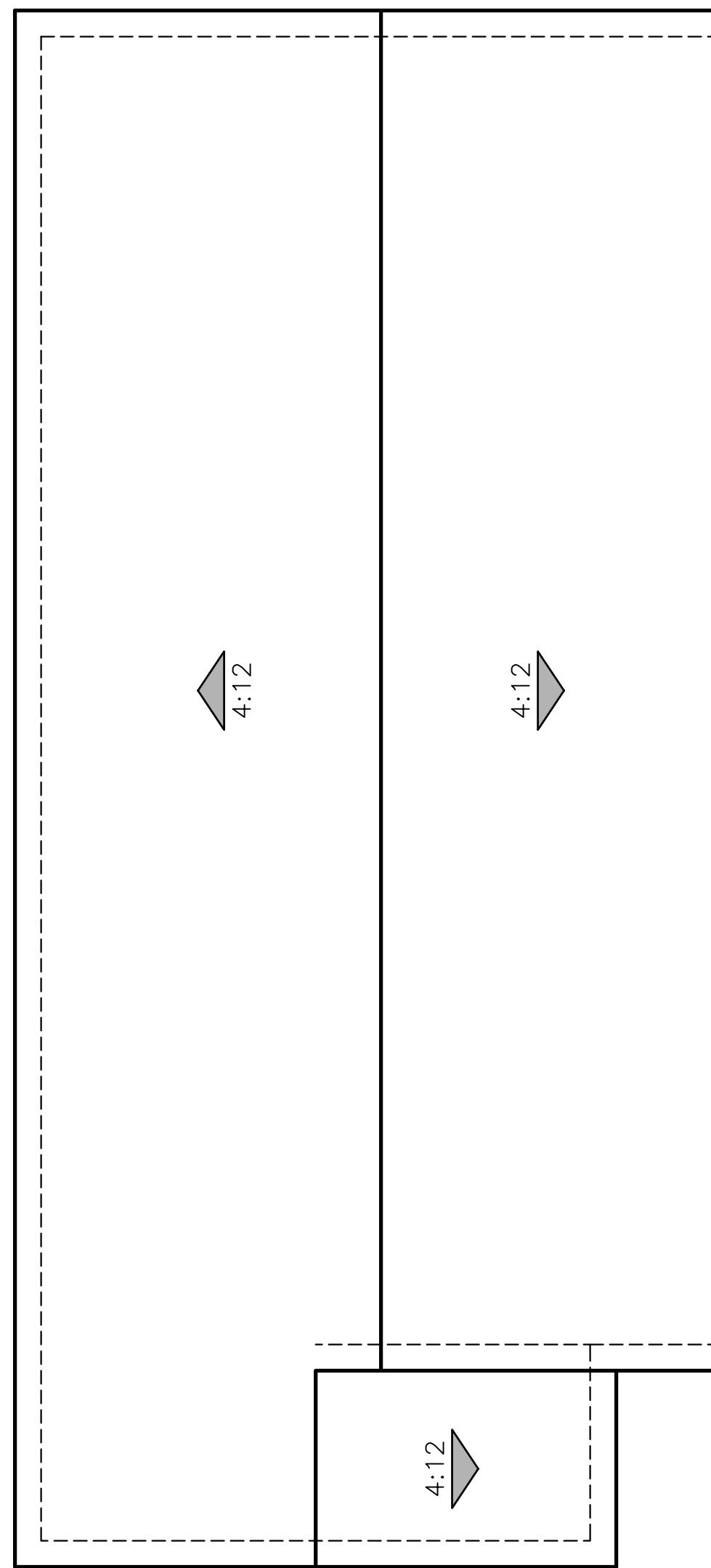
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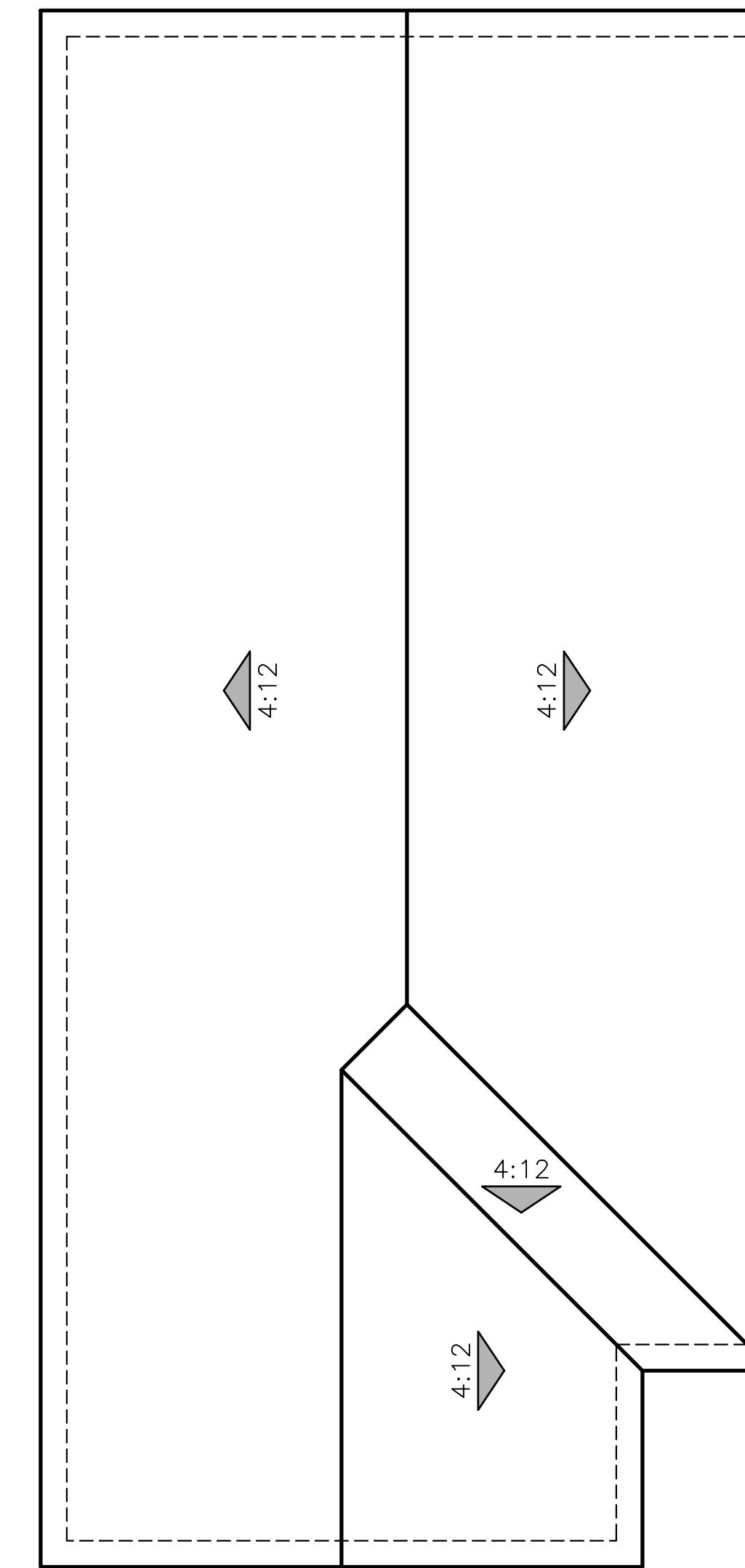
REAR

COLOR SCHEME 9  
PLAN 3C (2220)  
"C" PROGRESSIVE PRAIRIE ENHANCED ELEVATIONS

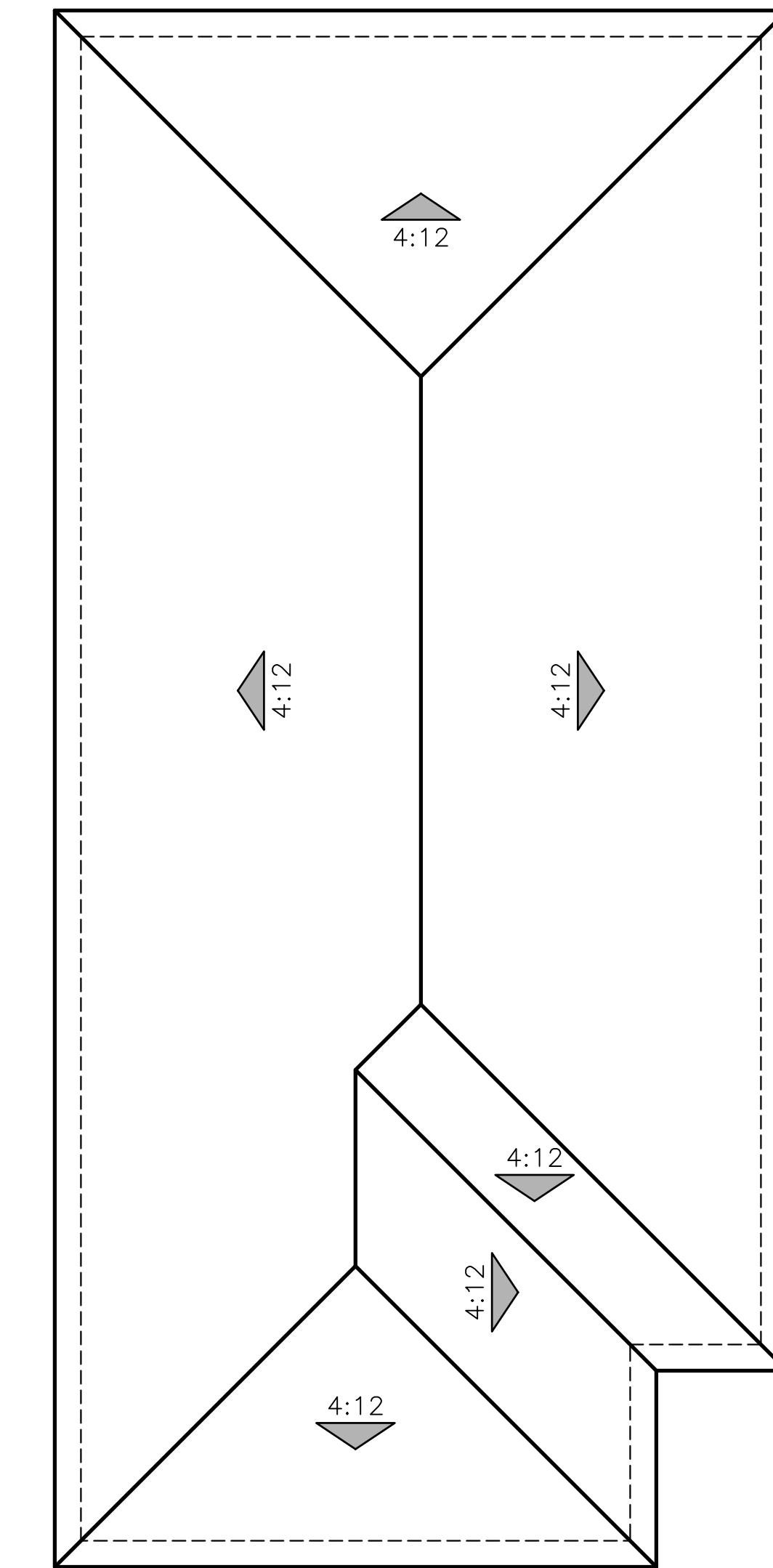
0 2 4 8 12



A - RANCH



B - FARMHOUSE



C - PROGRESSIVE PRAIRE

PLAN 3 (2220)  
ROOF PLANS

0 2 4 8 12

GUAJOME LAKE  
OCEANSIDE, CA

JOB # 22050  
CREATED 7/19/2022  
Client Revisions 9/27/2022

"A" ELEVATIONS		SCHEME 1	SCHEME 2	SCHEME 3
RANCH				
STUCCO		1590	1572	1505
STUCCO PAINT MATCH		SW6119 ANTIQUE WHITE	RGB 255/255/255	SW7531 CANVAS TAN
FASCIA / TRIM / CORNER BOARDS	Y	SW7005 PURE WHITE	SW7674 PEPPERCORN	SW7005 PURE WHITE
SHINGLE SIDING	Y	SW2843 ROYCROFT BRASS	SW7006 EXTRA WHITE	SW6256 SERIOUS GRAY
GARAGE DOOR	R	SW2843 ROYCROFT BRASS	SW7674 PEPPERCORN	SW6256 SERIOUS GRAY
FRONT DOOR	G	SW0044 HUBBARD SQUASH	SW6992 INKWELL	SW0025 ROSEDUST
ROOF:		4805	4595	4679
FLAT SLATE BEL AIR		EVERGREEN	DARK CHARCOAL	LIGHT GRAY RANGE
GUTTERS:		SATIN	TUXEDO GRAY	SATIN

"B" ELEVATIONS		SCHEME 4	SCHEME 5	SCHEME 6
FARMHOUSE				
STUCCO		1579	1572	12
STUCCO PAINT MATCH***		SW7532 URBAN PUTTY	RGB 255/255/255	SW6119 ANTIQUE WHITE
FASCIA / TRIM	Y	SW6105 DIVINE WHITE	SW7005 PURE WHITE	SW7562 ROMAN COLUMN
BOARD & BATTEN SIDING / GARAGE DOOR	Y	SW7068 GRIZZLE GRAY	SW0077 CLASSIC FRENCH GRAY	SW2846 RYCFT BRNZ GRN
FRONT DOOR	G	SW7020 BLACK FOX	SW0006 TOILE RED	SW0047 STUDIO BLUE GREEN
ROOF: FLAT SHAKE		5687	5671	5679
MONTEREY		BROWN GRAY RANGE	VILLAGE BLEND	LIGHT GRAY RANGE
GUTTERS:		SAND CASTLE	SATIN	LINEN

"C" ELEVATIONS		SCHEME 7	SCHEME 8	SCHEME 9
PROGRESSIVE PRAIRIE				
STUCCO		437	1595	1601
STUCCO PAINT MATCH		SW7036 ACCESSIBLE BEIGE	SW7037 BALANCED BEIGE	SW9164 ILLUSIVE GREEN
FASCIA / TRIM	Y	SW7020 BLACK FOX	SW7041 VAN DYKE BROWN	SW7068 GRIZZLE GRAY
LAP SIDING / GARAGE DOOR	Y	SW6075 GARRET GRAY	SW7005 PURE WHITE	SW7005 PURE WHITE
FRONT DOOR	G	SW6307 FINE WINE	SW6667 AFTERGLOW	SW0020 PEACOCK PLUME
ROOF: FLAT SLATE		4606	4804	4883
BEL AIR		VALLEJO BLEND	ROCKLIN BLEND	HILLSBOROUGH BLD
GUTTERS:		BRONZE	RUSTIC	TUXEDO GRAY

ALL FLASHING, GUTTERS, DOWNSPOUTS ETC. TO BE PAINTED TO MATCH ADJACENT SURFACE. ALL PAINT BREAKS TO BE CUT AT INSIDE CORNERS.

\*\* FOR PHOTOSHOP & RENDERING PURPOSES ONLY, DO NOT USE IN THE FIELD

TODAS LAS TAPAJUNTAS, CAÑALERAS, CAJALONES, ETC. SERÁN PINTADOS PARA IGUALAR A LA SUPERFICIE DE JUNTO

TODAS LAS RESTAURAS DE PINTURA SE CORTARÁN EN LAS ESQUINAS INTERIORES

\*\*SOLO PARA FINES DE PHOTOSHOP Y REPRESENTACIONES, NO USAR EN EL CAMPO

STUCCO:	OMEGA
PAINT:	SHERWIN WILLIAMS
ROOF:	EAGLE
GUTTERS:	RGS

 RINCON HOMES

Kevin L. Crook  
**Architect**  
Inc

PLANNING + ARCHITECTURE

GUAJOME LAKE  
OCEANSIDE, CA

MASTER NUMBER  
T22 - 00044

01.16.2023  
A-29

Kevin L. Crook  
**Architect**  
Inc

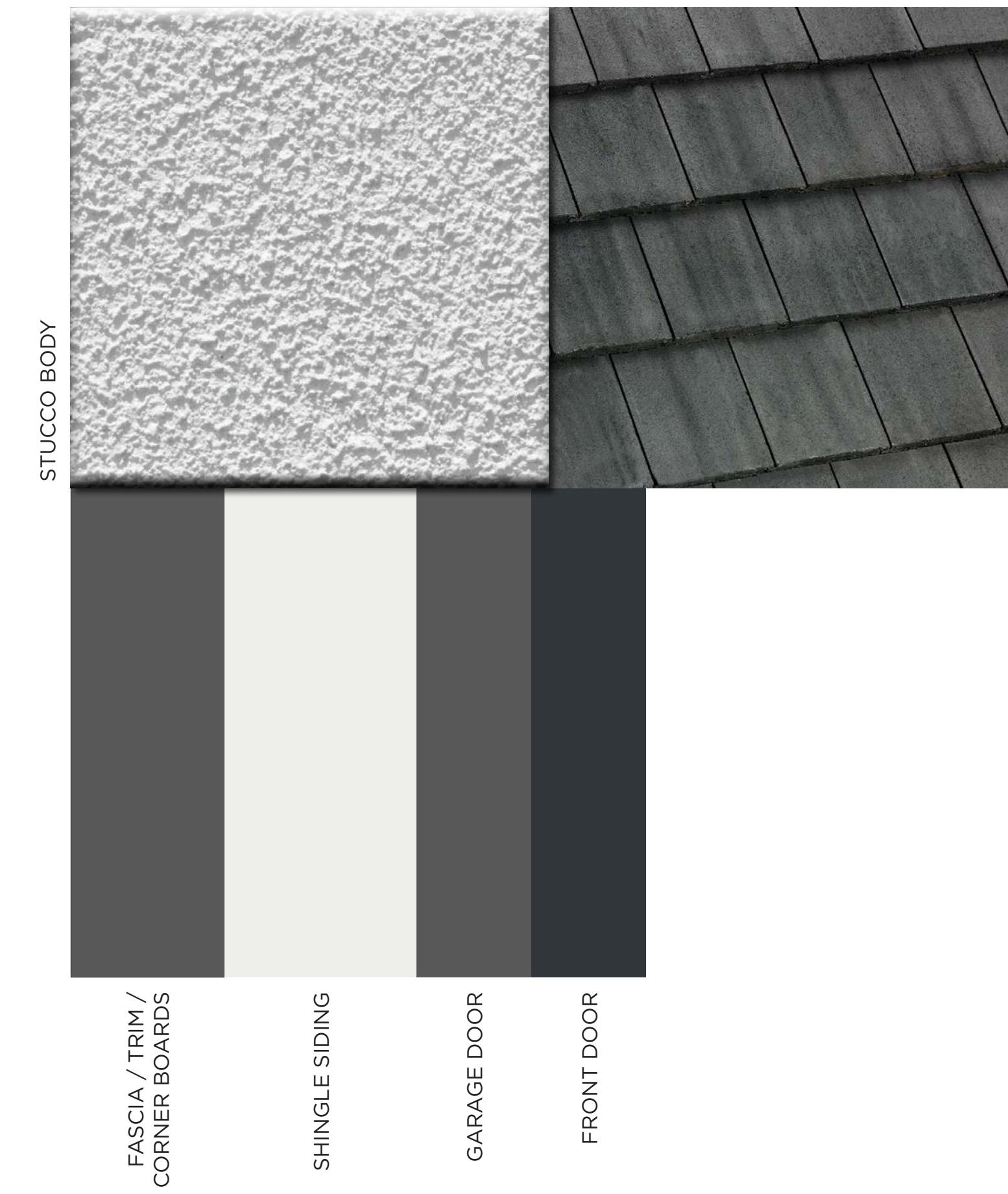
PLANNING + ARCHITECTURE

 RINCON HOMES

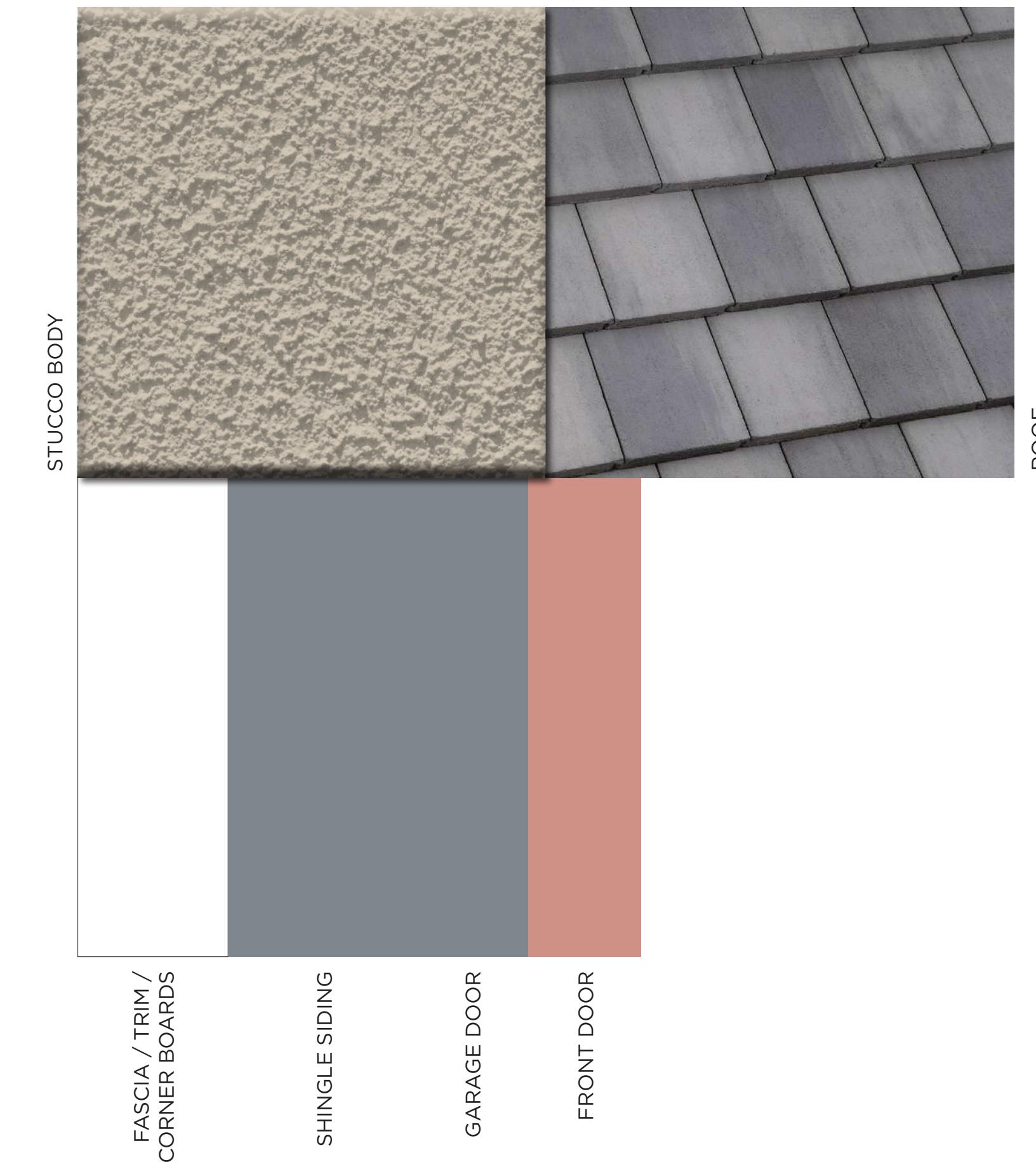
RANCH  
SCHEME #1



RANCH  
SCHEME #2



RANCH  
SCHEME #3



RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
#22050  
SEPT. 27, 2022  
PLANNING + ARCHITECTURE

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GUAJOME LAKE  
OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
#22050  
SEPT. 27, 2022  
PLANNING + ARCHITECTURE

COLOR BOARDS - "A" RANCH

\*COLORS MAY VARY DUE TO SCREEN AND PRINTER  
CALIBRATION. REFER TO PAINT CHIPS AND MATERIAL  
BOARDS FOR ACTUAL COLORS.

RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

MASTER NUMBER  
T22 - 00044

01.16.2023  
A-30

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Architect  
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FARMHOUSE  
SCHEME #4

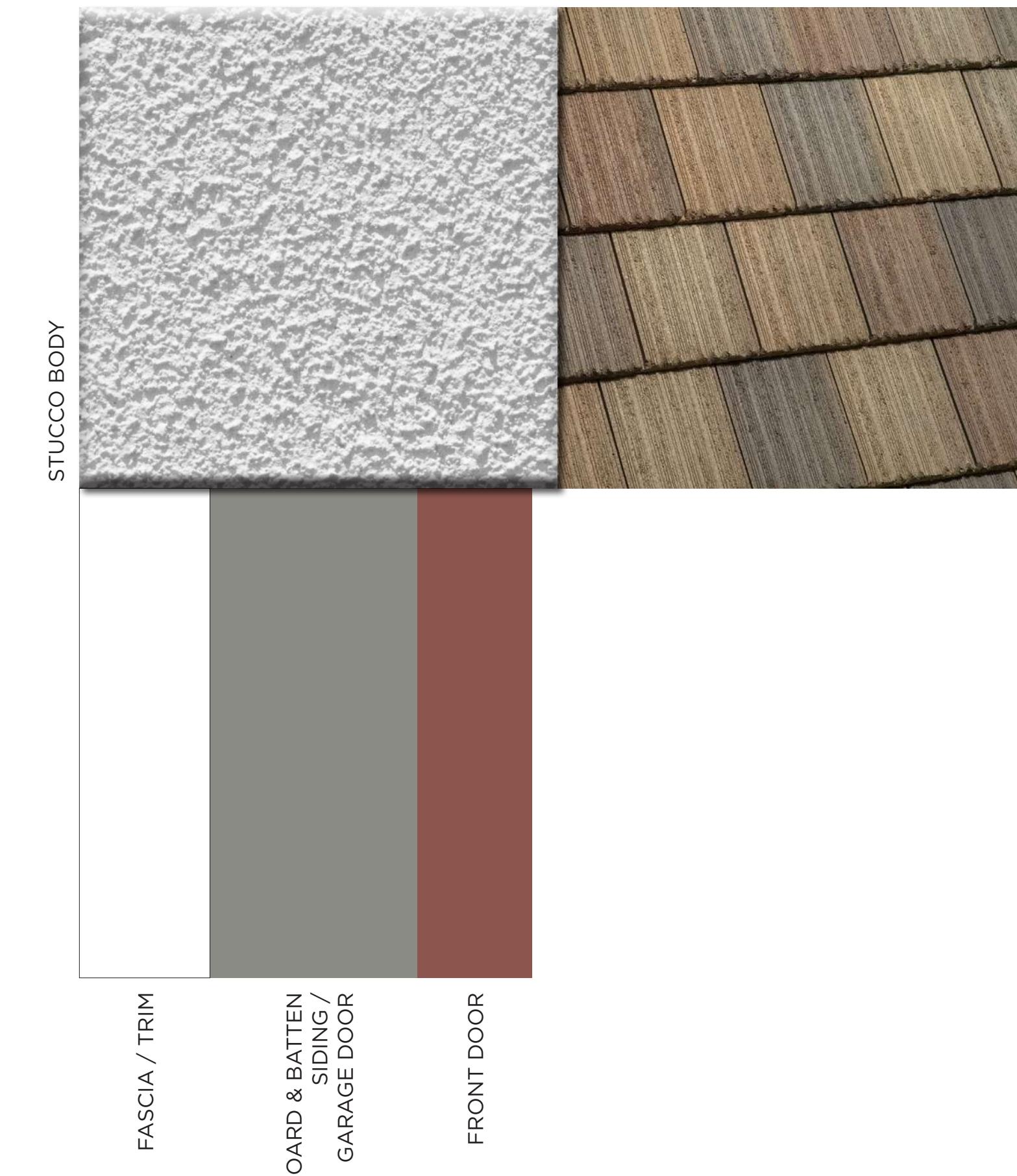


RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

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#22050  
SEPT. 27, 2022  
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FARMHOUSE  
SCHEME #5



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GUAJOME LAKE  
OCEANSIDE, CA

Kevin L. Crook  
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FARMHOUSE  
SCHEME #6



RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
#22050  
SEPT. 27, 2022  
PLANNING + ARCHITECTURE

COLOR BOARDS - "B" FARMHOUSE

\*COLORS MAY VARY DUE TO SCREEN AND PRINTER  
CALIBRATION. REFER TO PAINT CHIPS AND MATERIAL  
BOARDS FOR ACTUAL COLORS.

RINCON HOMES

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OCEANSIDE, CA

MASTER NUMBER  
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A-31

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PROGRESSIVE PRAIRIE  
SCHEME #7



RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
#22050  
SEPT. 27, 2022  
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PROGRESSIVE PRAIRIE  
SCHEME #8

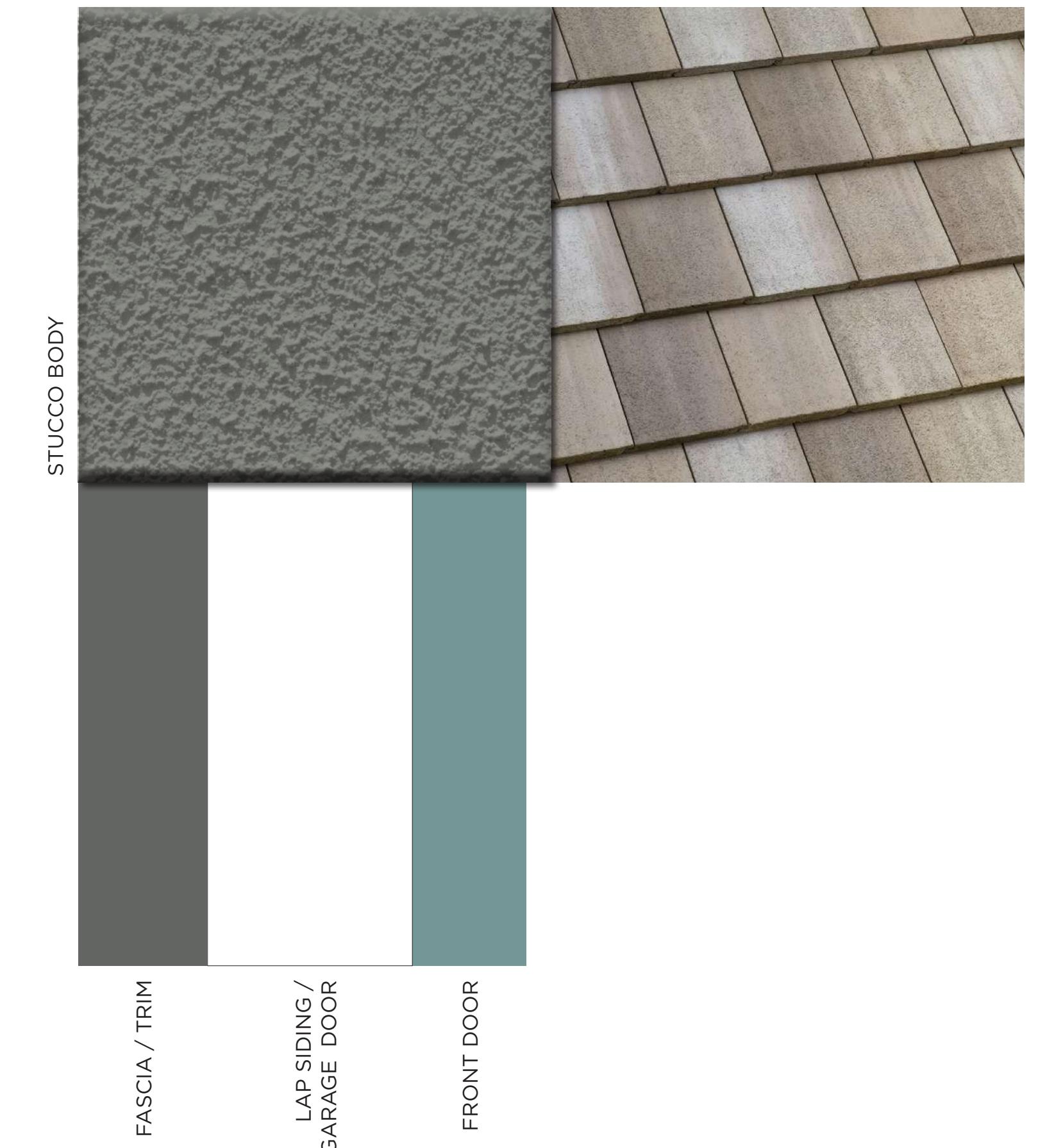


RINCON HOMES

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OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
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PROGRESSIVE PRAIRIE  
SCHEME #9



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OCEANSIDE, CA

Kevin L. Crook  
Architect  
Inc  
#22050  
SEPT. 27, 2022  
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COLOR BOARDS - "C" PROGRESSIVE PRAIRIE

\*COLORS MAY VARY DUE TO SCREEN AND PRINTER  
CALIBRATION. REFER TO PAINT CHIPS AND MATERIAL  
BOARDS FOR ACTUAL COLORS.

RINCON HOMES

GUAJOME LAKE  
OCEANSIDE, CA

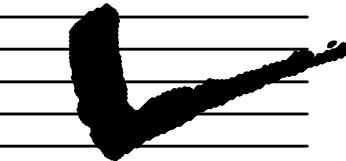
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T22 - 00044

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A-32

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# Landscape Concept Plan

## GUAJOME LAKE ROAD

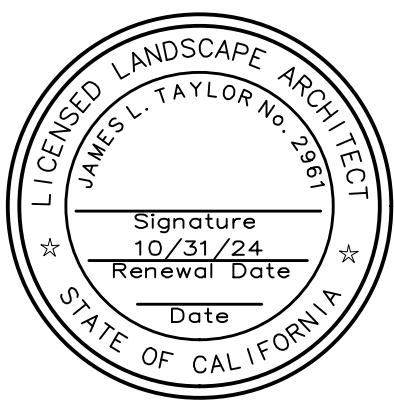


THE  
LIGHTFOOT  
PLANNING  
GROUP

PLANNING  
SITE DESIGN  
LANDSCAPE  
ARCHITECTURE

2237 FARADAY AVE, SUITE 120  
CARLSBAD, CA 92008  
(760) 692-1924

LIGHTFOOT@LIGHTFOOTPG.COM



**RINCON HOMES**  
**GUAJOME LAKE ROAD**  
OCEANSIDE, CALIFORNIA

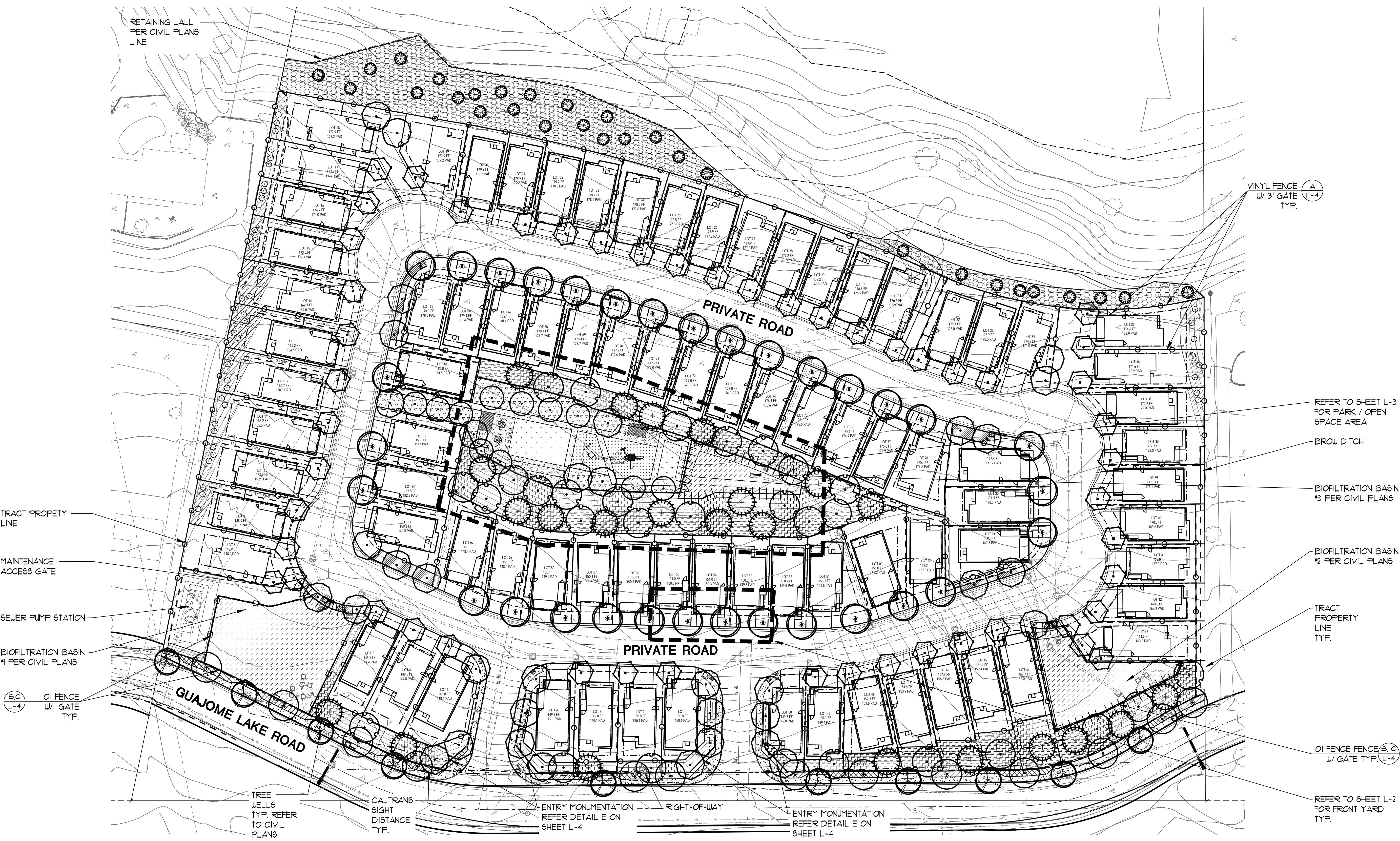
**LANDSCAPE  
CONCEPT**

Job # 1614.12.1

L-1

5

Rincon Homes 1614\Guajome Lake Rd\Landscape LCP



### CONCEPTUAL LANDSCAPE PLAN

EXISTING CONDITIONS, ACCESS ROADS, EASEMENTS AND UTILITIES.  
REFER TO CIVIL GRADING PLAN FOR LOCATION AND TYPES OF EXISTING AND PROPOSED UTILITIES, EASEMENTS AND PROPERTY LINES AND ACCESS ROADS. REFER TO CIVIL PLANS FOR LOCATION OF DRAINAGE SYSTEM, BROW DITCHES (IF ANY), RETAINING WALLS, INFORMATION ABOUT WATER QUALITY BASINS, STREET AND BMP SECTIONS, AND LIMITS OF WORK. CONCEPTUAL LANDSCAPE IS BASED ON ELECTRONIC BASE INFORMATION PROVIDED BY CIVIL ENGINEER. SEE ADDITIONAL NOTES, SHEET L-5.



SCALE 1"=40'

REFER TO SHEET L-5 FOR  
PLANTING LEGEND

APN:157-412-15-00

T22-00004, D22-00009, DB22-00005



THE  
LIGHTFOOT  
PLANNING  
GROUP

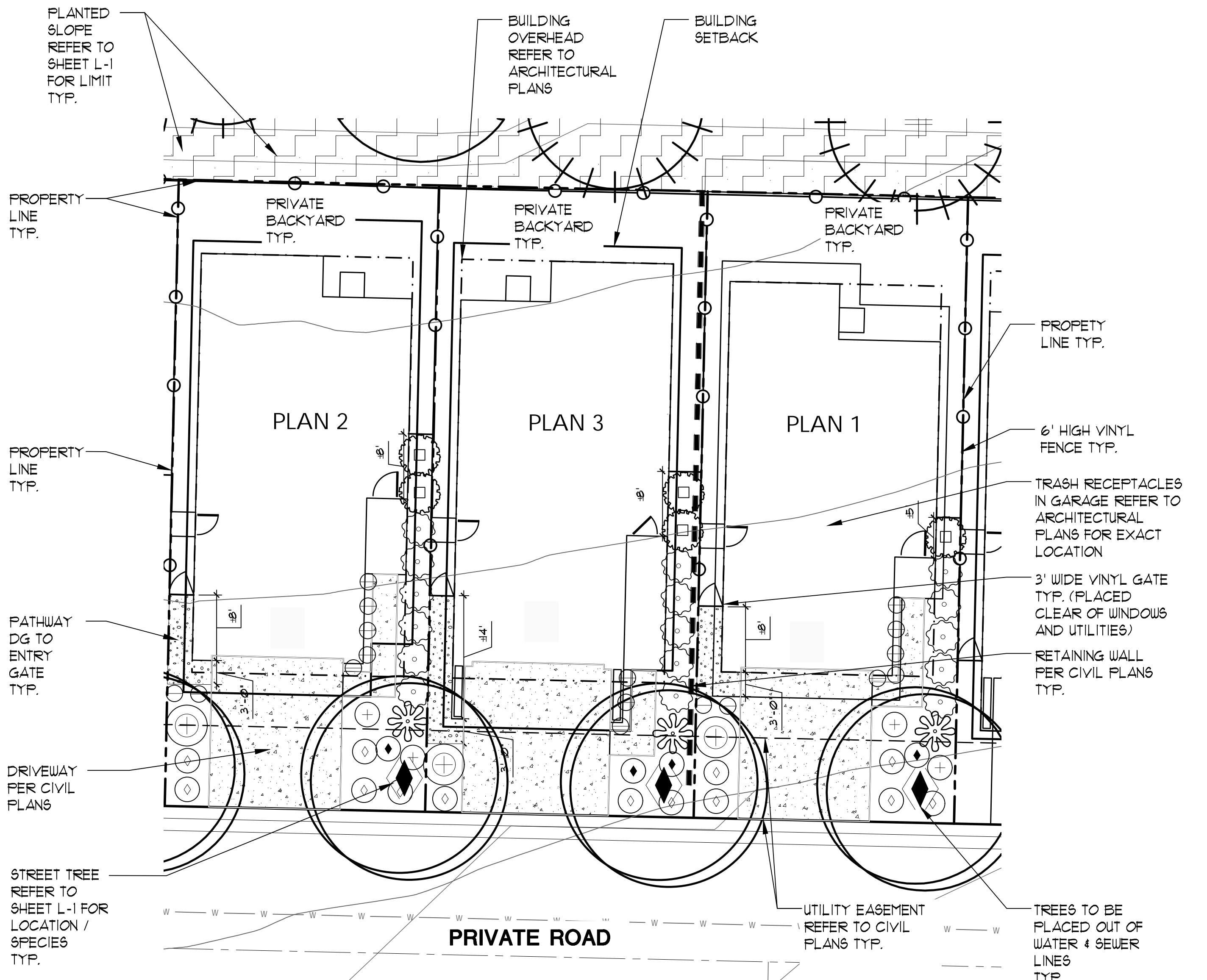
PLANNING  
SITE DESIGN  
LANDSCAPE  
ARCHITECTURE

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(760) 692-1924

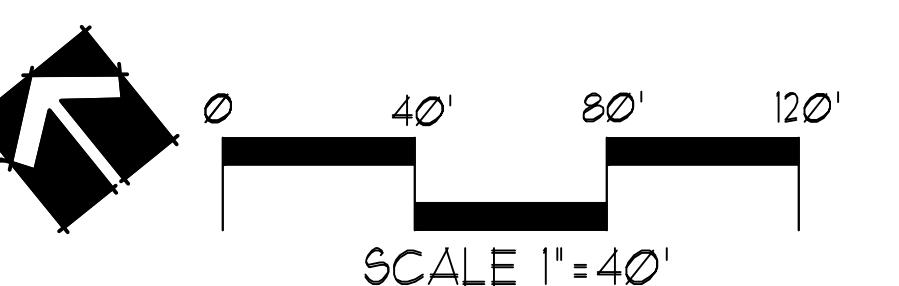
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## RINCON HOMES GUAJOME LAKE ROAD OCEANSIDE, CALIFORNIA



## FRONT-YARD TYPICAL



SCALE 1"=40'

## CONCEPT PLANT PALETTE FOR TYPICAL

SYMBOL	BOTANICAL / COMMON NAME	SIZE	WUCOLS	SPACING
	AGAVE ATTENUATA / FOXTAIL AGAVE PHORMIUM X 'YELLOW WAVE' / YELLOW WAVE NEW ZEALAND FLAX YUCCA FILAMENTOSA 'COLOR GUARD' / COLOR GUARD ADAMS NEEDLE ALOE MOONGLOW / MOONGLOW ALOE	5 GAL. 5 GAL. 5 GAL. 5 GAL.	LOW	60' O.C. 60' O.C. 60' O.C. 60' O.C.
	ALOE 'BLUE ELF' / BLUE ELF ALOE DIANELLA REVOLUTA 'BABY BLISS' / BABY BLISS FLAX LILY FESTUCA GLAUCA 'ELIJAH BLUE' / ELIJAH BLUE FESCUE ANIGOZANTHOS spp / KANGAROO PAW	1 GAL. 1 GAL. 1 GAL. 1 GAL.	LOW	36' O.C. 36' O.C. 36' O.C. 36' O.C.
	CARPENTERIA CALIFORNICA 'ELIZABETH' / ELIZABETH BUSH ANEMONE RHAMNUS CALIFORNICA 'EVE CASE' / COFFEEBERRY LEUCOPHYLLUM FRUTESCENS 'COMPACTA' / COMPACT TEXAS SAGE TECOMA 'ORANGE JUBILEE' / ORANGE JUBILEE YELLOW BELLS	5 GAL. 5 GAL. 5 GAL. 5 GAL.	LOW	60' O.C. 60' O.C. 60' O.C. 60' O.C.
	CISTUS X PURPUREUS / ORCHID ROCKROSE SALVIA GREGGII PURPLE / AUTUMN SAGE DIPLACUS AURANTIACUS 'DARK PINK' / DARK PINK MONKEYFLOWER ENCLEDIA CALIFORNICA / CALIFORNIA ENCLEDIA	5 GAL. 5 GAL. 5 GAL. 5 GAL.	LOW	60' O.C. 60' O.C. 60' O.C. 60' O.C.
	HESPERALOES PARVIFLORA 'PERPA' / BRAKELIGHTS RED YUCCA ANGIGOZANTHOS spp / KANGAROO PAW EPILOBIUM CANUM / CALIFORNIA FUCHSIA	5 GAL. 5 GAL. 5 GAL.	LOW	36' O.C. 36' O.C. 36' O.C.
	LANTANA X 'NEW GOLD' / NEW GOLD LANTANA TEUCRUM CHAMAEDRYS 'PROSTRATUM' / PROSTRATE GERMANDER VERBENA LILACINA 'DE LA MINA' / DE LA MINA LILAC VERBENA SENECIO SERPENS / BLUE CHALKSTICKS ERIGERON GLAUCUS	1 GAL. 1 GAL. 1 GAL. 1 GAL. 1 GAL.	LOW	48' O.C. 36' O.C. 36' O.C. 36' O.C. 36' O.C.
	LOMANDRA LONGIFOLIA 'BREEZE' / BREEZE MAT RUSH BOUTELOUA GRACILIS 'BLONDE AMBITION' / BLONDE AMBITION GRAMA JUNCUS PATENS / CALIFORNIA GRAY RUSH OLEA EUROPAEA 'MONTRA' / LITTLE OLIE OLIVE	5 GAL. 5 GAL. 5 GAL. 5 GAL.	LOW	48' O.C. 48' O.C. 48' O.C. 48' O.C.
	SALVIA GREGGII / AUTUMN SAGE PENSTEMON SPECTABILIS / SHOWY PENSTEMON LAVANDULA spp LUPINUS EXCELBUS / GRAPE SODA LUPINE	1 GAL. 1 GAL. 1 GAL. 1 GAL.	LOW	36' O.C. 36' O.C. 36' O.C. 36' O.C.
DECOMPOSED GRANITE - 3" DEPTH - CALIFORNIA GOLD / OR APPROVED EQUAL.				

Scale: 1"=40' (24x36)  
Date: 8/16/24  
Drawn By: EE  
Revisions:

LANDSCAPE  
CONCEPT

Job # 1614.12.1

L-2  
5

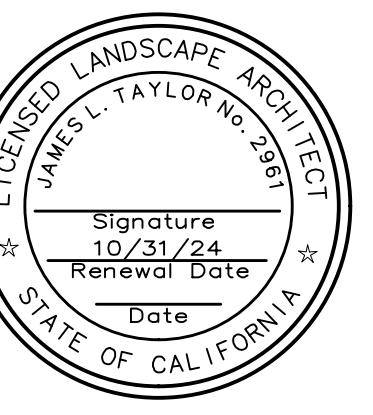


THE  
LIGHTFOOT  
PLANNING  
GROUP

PLANNING  
SITE DESIGN  
LANDSCAPE  
ARCHITECTURE

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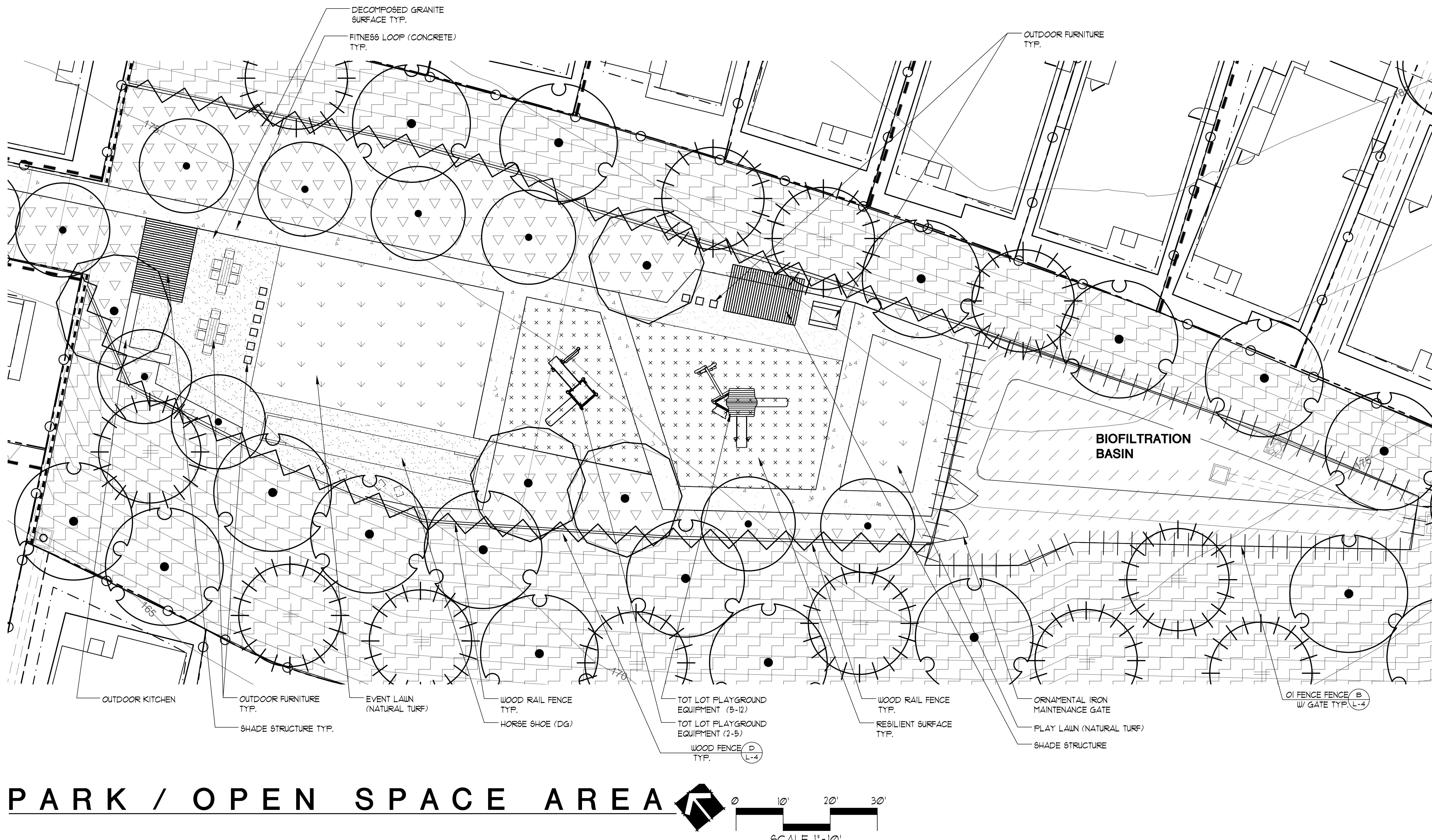


**RINCON HOMES  
GUAJOME LAKE ROAD  
OCEANSIDE, CALIFORNIA**

Scale: 1"=10' (24x36)  
Date: 8/16/24  
Drawn By: EE  
Revisions:

LANDSCAPE  
CONCEPT

Job # 1614.12.1  
L-3  
5



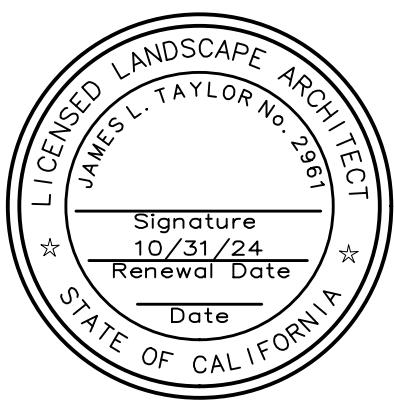


THE  
LIGHTFOOT  
PLANNING  
GROUP

PLANNING  
SITE DESIGN  
LANDSCAPE  
ARCHITECTURE

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CARLSBAD, CA 92008  
(760) 692-1924

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# RINCON HOMES GUAJOME LAKE ROAD OCEANSIDE, CALIFORNIA

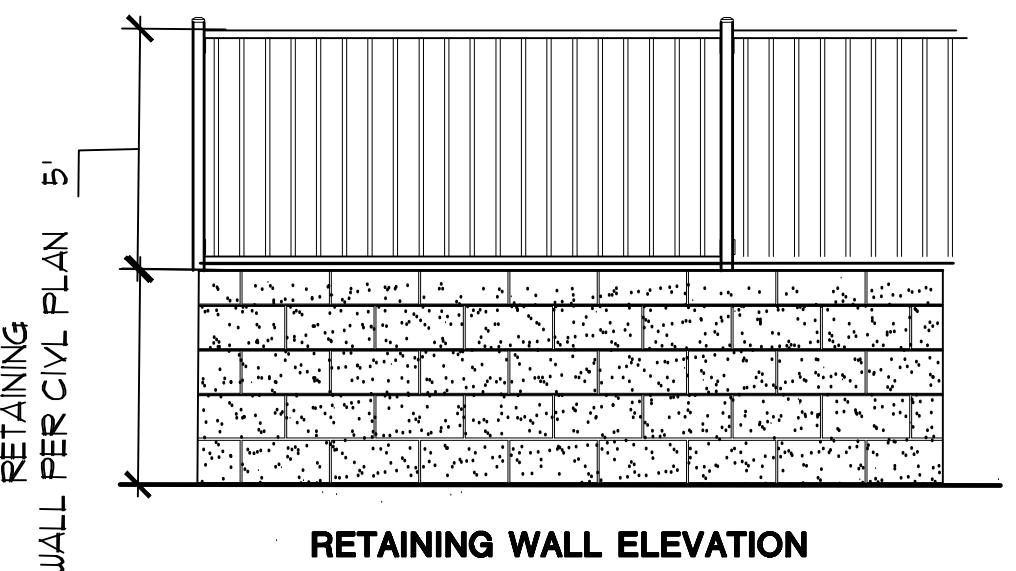
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Drawn By: EE  
Revisions:

## LANDSCAPE CONCEPT

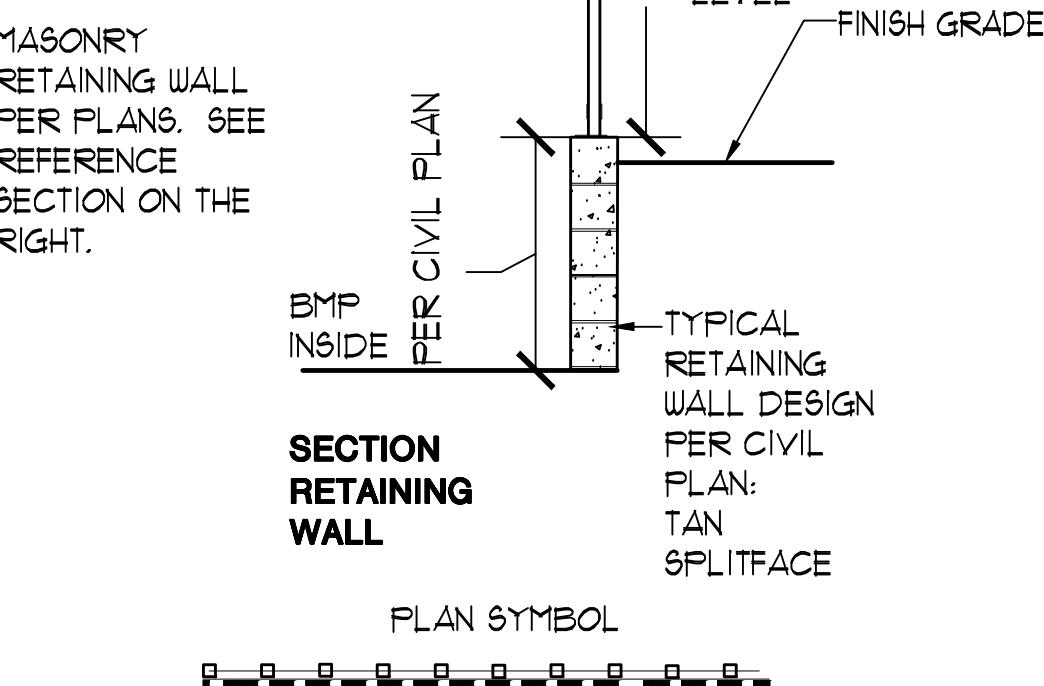
Job # 161412.1  
L-4  
5

### MASONRY RETAINING WALLS

RETAINING WALLS ARE SHOWN FOR REFERENCE ON LANDSCAPE PLAN AS TO COLOR AND FINISH. WALLS TO BE DESIGNED AND INSTALLED PER CIVIL PLANS. FINISH: TAN SPLITFACE BLOCK. SEE PLAN FOR REAR YARD AND COMMON AREA LOCATIONS.

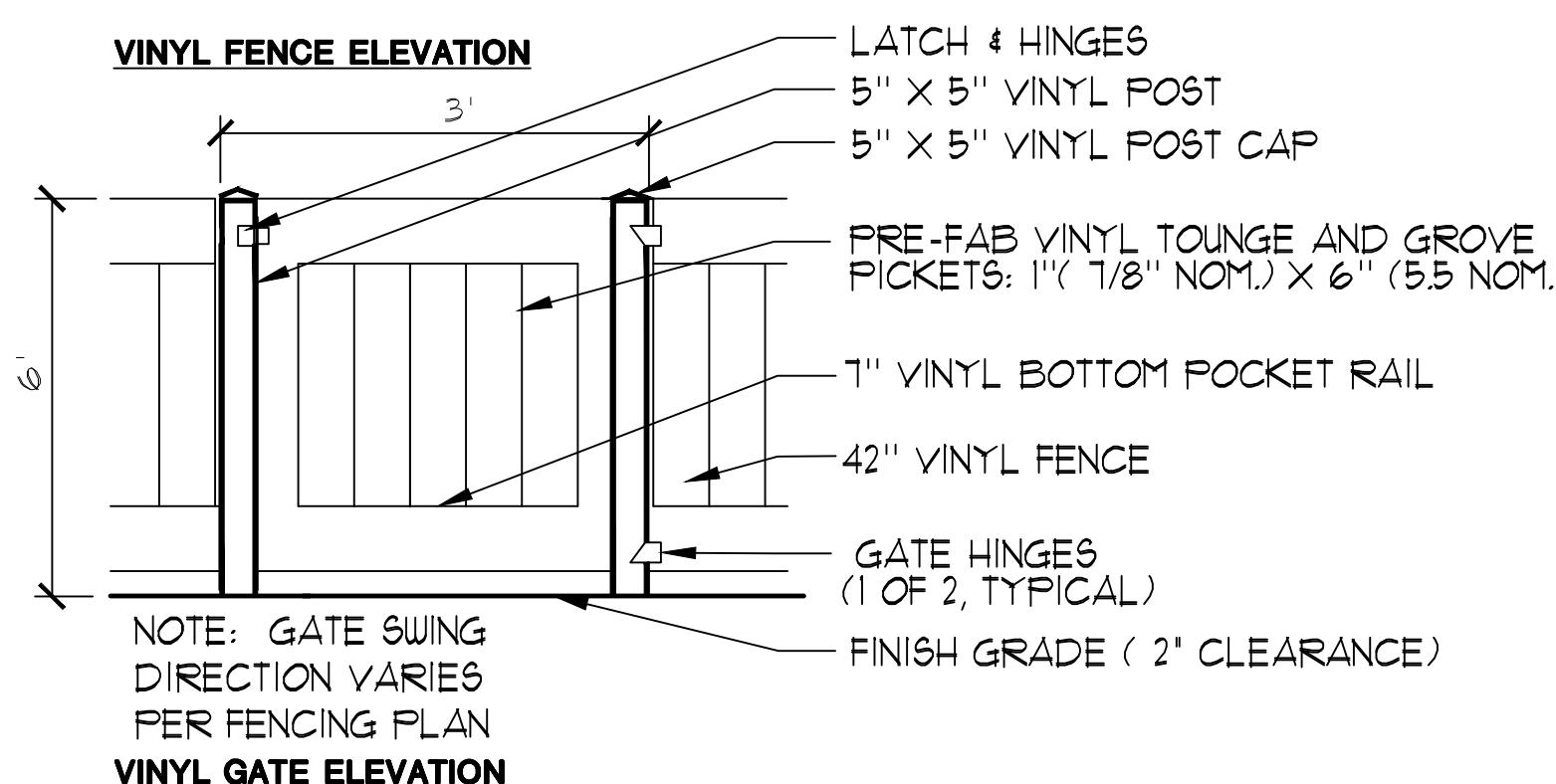
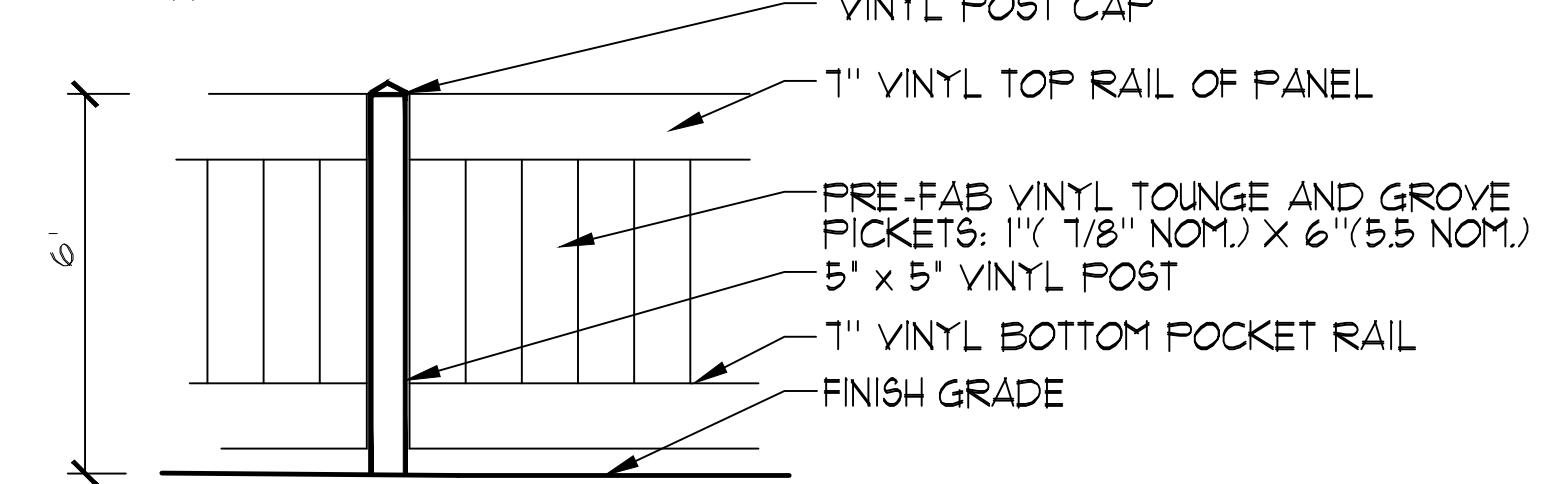


RETAINING WALL ELEVATION

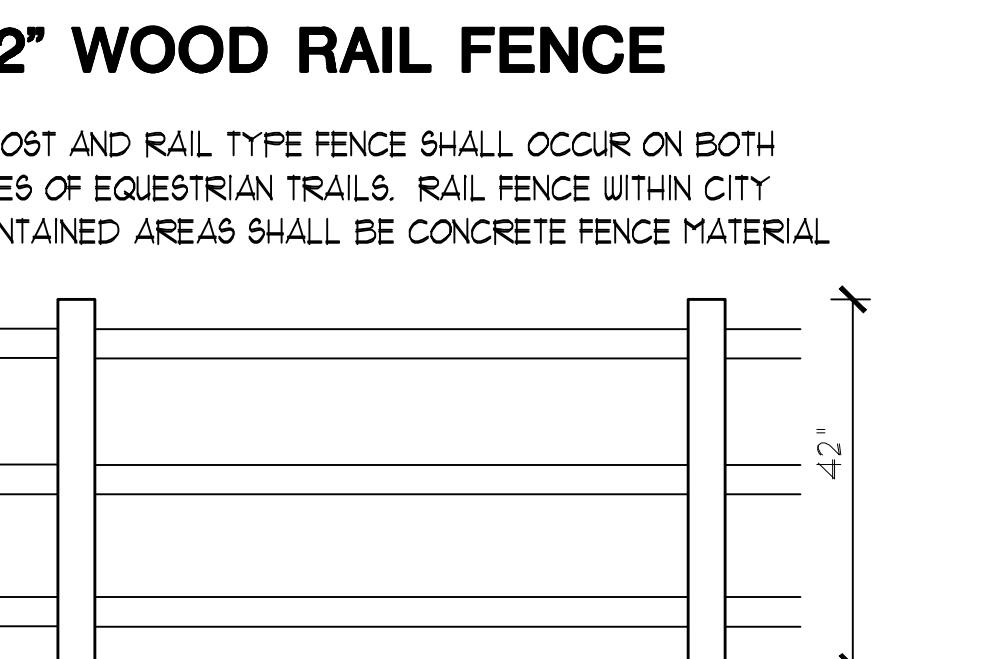


(C) CONCEPTUAL ORNAMENTAL IRON ON RETAINING WALL

6' VINYL FENCE WITH MATCHING GATES. FOR TYPICAL FRONT, SIDE YARD FENCING AND REAR FENCE, REFER TO PLAN FOR LAYOUT.  
(COLOR: WHITE OR APPROVED EQUAL-STYLE AND COLOR TO MATCH ARCHITECTURAL STYLE)



(RESIDENTIAL FENCE PLAN SYMBOL)



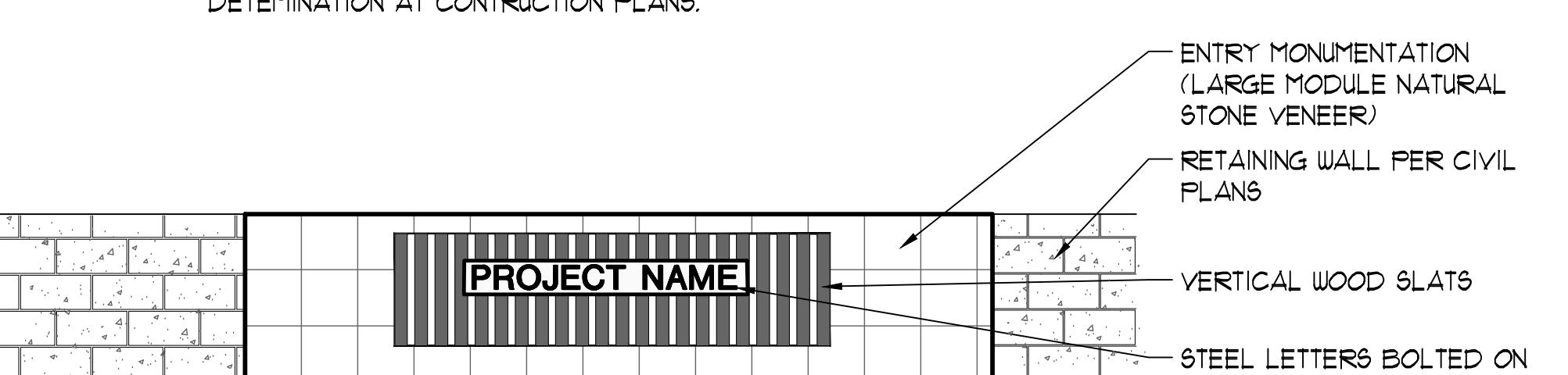
42" WOOD RAIL FENCE

A POST AND RAIL TYPE FENCE SHALL OCCUR ON BOTH SIDES OF EQUESTRIAN TRAILS. RAIL FENCE WITHIN CITY MAINTAINED AREAS SHALL BE CONCRETE FENCE MATERIAL

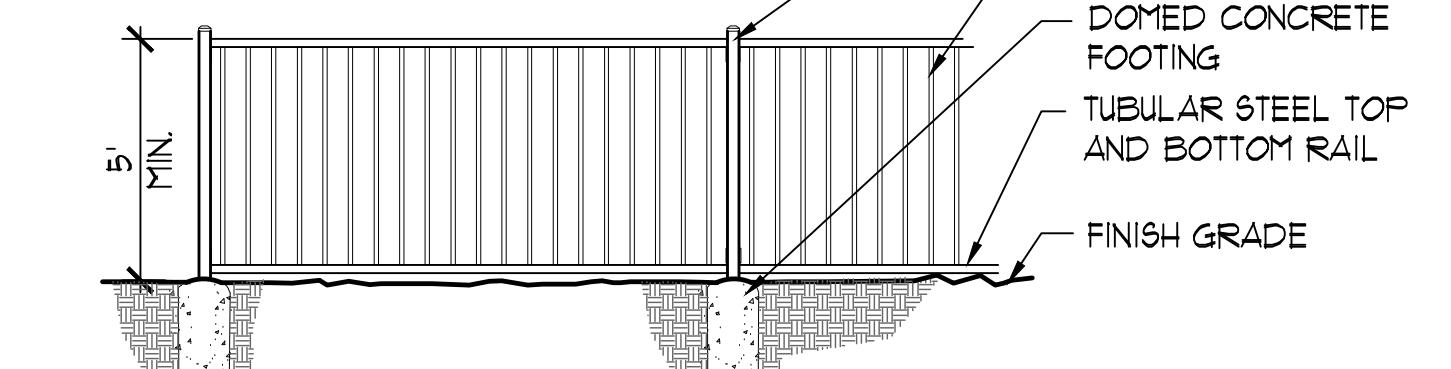
(D) CONCEPTUAL WOOD RAIL FENCE

### ENTRY MONUMENTATION

COLOR AND STYLE TO MATCH ARCHITECTURAL DESIGN- FINAL DETERMINATION AT CONSTRUCTION PLANS.

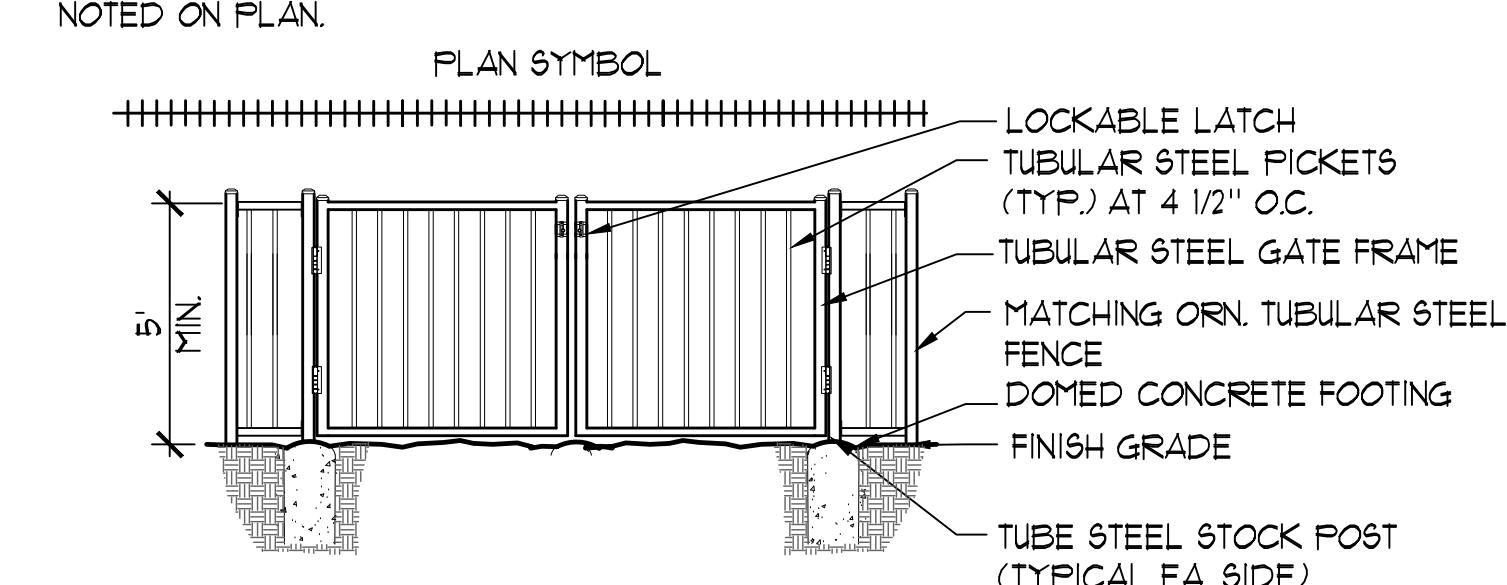


(E) CONCEPTUAL ENTRY MONUMENTATION



5' ORN. IRON MAINTENANCE GATE

BLACK TUBULAR STEEL (ORN. IRON) GATE FOR MAINTENANCE ACCESS TO WATER QUALITY TREATMENT AREA. SEE LOCATIONS AS NOTED ON PLAN.



PLAN SYMBOL

TYPICAL GATE, LOCATION AS NOTED ON PLAN

(B) CONCEPTUAL ORNAMENTAL IRON FENCE WITH MATCHING GATE

## CONCEPT PLANT PALETTE

GUAJOME LAKE STREET TREE PROJECT TREE (MIN. SIZE 24" BOX)	
CHILOPSIS LINEARIS- DESERT WILLOW	
ARBUTUS X 'MARINA' / ARBUTUS STANDARD	
ERYCBORYA DEFLEXA STD. / BROZE LOQUAT	
RAPHIOLEPIS 'MAJESTIC BEAUTY' STD. / INDIAN HOWTHORN	
LAURUS NOBILIS/ SWEET BAY	
TRISTANIA CONFERTA(LOPHOSTEMON) BRISBANE BOX	
TREE WELL STREET TREE PROJECT TREE (MIN. SIZE 24" BOX)	
ARBUTUS X 'MARINA' / ARBUTUS STANDARD	
CHILOPSIS LINEARIS- DESERT WILLOW	
ERYCBORYA DEFLEXA STD. / BROZE LOQUAT	
RAPHIOLEPIS 'MAJESTIC BEAUTY' STD. / INDIAN HOWTHORN	
LAURUS NOBILIS/ SWEET BAY	
TRISTANIA CONFERTA(LOPHOSTEMON) BRISBANE BOX	
FRONT YARD TREE (MIN. SIZE 15" GAL)	
ARBUTUS X 'MARINA' / ARBUTUS STANDARD	
ERYCBORYA DEFLEXA STD. / BROZE LOQUAT	
RAPHIOLEPIS 'MAJESTIC BEAUTY' STD. / INDIAN HOWTHORN	
CERCIS CANADENSIS/ EASTERN REDBUD	
CERCIDIDIUM FLORIDUM/ BLUE PALO VERDE	
QUERCUS AGRIFOLIA / COAST LIVE OAK	
OUTSIDE SLOPE TREE (MIN. SIZE 1.5 & 15 GAL)	
PRUNUS LUCIFOLIA / HOLLY LEAF CHERRY	
QUERCUS AGRIFOLIA / COAST LIVE OAK	
QUERCUS EUGLEMANII / ENGLEMAN OAK	
QUERCUS WISLIZENII / INTERIOR LIVE OAK	
LYONOTHAMNUS F. ASPLENIFOLIUS / CATALINA IRONWOOD	
LOPHOSTEMON CONFERTA/ BRISBANE BOXTREE	
INTERIOR TREE ACCENT CANOPY (MIN. SIZE 15" GAL)	
ARBUTUS X 'MARINA' / ARBUTUS STANDARD	
MAGNOLIA GRANDIFLORA HYBRID / SO. MAGNOLIA HYBRID	
ERYCBORYA DEFLEXA STD. / BROZE LOQUAT	
JACARANDA MIMOSIFOLIA / JACARANDA	
TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE	
PLATANUS RACEMOSA/ CALIFORNIA SYCAMORE	
GEUERA PARVIFLORA/ AUSTRALIAN WILLOW	
SLOPE TREE (MIN. SIZE 15" GAL)	
LYONOTHAMNUS F. ASPLENIFOLIUS / CATALINA IRONWOOD	
LOPHOSTEMON CONFERTA/ BRISBANE BOXTREE	
QUERCUS AGRIFOLIA/ COAST LIVE OAK	
PRIVATE SHRUB FOR SLOPE (MIN. SIZE 1 & 5 GAL)	
CISTUS PURPUREUS/ ORCHID ROCKROSE	
PHORMIUM SPP/ NEW ZEALAND FLAX	
AGAVE SPP/ AGAVE	
SALVIA SPP/ SALVIA	
LEUCOPHYLLUM SPP/ TEXAS SAGE	
PERIMETER PLANTING (MIN. SIZE FLATS, 1, 5, 15 GAL)	
CALLISTEMON VIMINALIS 'BETTER JOHN' / DWARF WEEPING BOTTLE BRUSH	
BOUGAINVILLEA X 'ROSENKA' / BOUGAINVILLEA	
HESPERALOE PARVIFLORA HYBRIDS / RED YUCCA HYBRIDS	
CEANOHTHUS GRISEUS HORIZONTALIS 'YANKEE POINT' / CALIFORNIA LILAC	
SENECIO SERPENS / BLUE CHALKSTICKS	
LANTANA HYBRIDS / LANTANA	
AGAVE DESMETTIANA VARIEGATA / VARIEGATED AGAVE	
DIANELLA CAERULEA/ CLARITY BLUE / DIANELLA	
DIANELLA TASMANICA VARIEGATA/ FLAX LILY	
CARISSA MACROCARPA/ GREEN CARPET / GREEN CARPET NATAL PLUM	
CEANOHTHUS THYSIFLORUS DIAMOND HEIGHTS / D.H CEANOHTHUS	
TRACHELOSPERMUM JASMINOIDES/ STAR JASMINE	
LEYMUS CONDENSATUS 'CANYON PRINCE' / BLUE RYE'	
PHORMIUM TENAX SPP/ NEW ZEALAND FLAX	
FRAGRARIA CHILOENSIS / ORNAMENTAL STRAWBERRY	
RIGHT OF WAY PLANTING AREA (MIN. SIZE FLATS, 1.5, GAL)	
HESPERALOE PARVIFLORA HYBRIDS / RED YUCCA HYBRIDS	
SENECIO SERPENS / BLUE CHALK FINGERS	
LANTANA SPP / LANTANA	
DIANELLA BREEZE/ LOMANDRA	
DIANELLA TASMANICA VARIEGATA/ FLAX LILY	
WESTRINIA FRUTICOSA 'WYNABIE GEM' / WYNABIE GEM COAST ROSEMARY	
PHORMIUM TENAX SPP/ NEW ZEALAND FLAX	
CARISSA MACROCARPA/ GREEN CARPET / GREEN CARPET NATAL PLUM	
BIOBASIN PLANTING (MIN. SIZE FLATS, 1.5, 15 GAL)	
JUNCUS PATENS/ CALIFORNIA RUSH	
LEYMUS C. 'CANYON PRINCE' / CANYON PRINCE GIANT WILD RYE	
LOMANDRA LONGIFOLIA BREEZE / BREEZE MAT RUSH	
MIMULUS CARDINALIS / SCARLET MONKEYFLOWER	
FESTUCA CALIFORNICA/ CALIFORNIA FESCUE	
ROSA CALIFORNICA/ CALIFORNIA WILD ROSE	
GUAJOME LAKE RD SLOPE PLANTING (MIN. SIZE FLATS, 1, 5, 15 GAL)	
AGAVE ATTENUATA/ FOXTAIL AGAVE	
AGAVE SPP/ AGAVE SPECIES	
LANTANA SPECIES/ LANTANA	
PHORMIUM TENAX SPP/ NEW ZEALAND FLAX	
CISTUS SPP / ROCKROSE	
ROMNEYA COULTERI 'WHITE CLOUD' / WHITE CLOUD MATILJA POPPY	
FURCREA VARIEGATA/ FALSE AGAVE	
BOUGAINVILLEA SPECIES / SHRUB FORM BOUGAINVILLEA	
CEANOHTHUS GRISEUS HORIZONTALIS 'YANKEE POINT' / CALIFORNIA LILAC	
BACCHARIS PILULARIS 'PIGEON POINT'/DWARF COYOTE BRUSH	
ARBUTUS UNEDO 'COMPACTA' / DWARF STRAWBERRY TREE	
PRIVATE SLOPE PLANTING (MIN. SIZE FLATS, 1, 5, 15 GAL)	
MYOPORUM PARVIFOLIUM	
BACCHARIS PILULARIS 'PIGEON POINT'/DWARF COYOTE BRUSH	
ARCTOSTAPHYLOS EDMUNDSSII 'CARMEL SUR' / CARMEL SUR MANZANITA	
CEANOHTHUS GRISEUS HORIZONTALIS 'YANKEE POINT' / CALIFORNIA LILAC	

### Minimum Tree Canopy and Permeable Surface Area Requirements

Project Site Area	Minimum Tree Canopy Area	Minimum Permeable Surface Area
1 acre or more	12%	22%
1/3 acre to 1 acre	9%	16%
Less than 1/3 acre	7%	10%

### URBAN FORESTRY PROGRAM COMPLIANCE- CODE 3049

PROPOSED TREE CANOPY CALCULATION ON GUAJOME LAKE ROAD				
DESCRIPTION	CANOPY DIA. (FT.)	CANOPY (SF.)	QUANTITY	EXTENSION (SF.)
Proposed Medium/Large	20 *	314	154	48356
Proposed Small Trees	15 *	176	51	8976
Total square footage of Proposed Tree Canopy Area			57,332	
Proposed Canopy Area Percentage of Project Site Area (8.97 ac.) @ 390,867 SF.			15%	

\* NOTE: INFORMATION ON MATURE DIMENSIONS OF TREE SPECIES WAS COLLECTED FROM UFEI SELECT TREE WEB SITE FROM CALIFORNIA POLYTECHNIC STATE UNIVERSITY. <https://selecttree.calpoly.edu/>

\*\* AREA EXCLUDES LOT E, F (OPEN SPACE LOTS) AND GUAJOME LAKE ROAD RIGHT OF WAY.

## LANDSCAPE CONCEPT DESIGN STATEMENT AND NOTES

THE LANDSCAPE EMBRACES VISUAL ENHANCEMENT FOR PROSPECTIVE RESIDENTS OF THE SITE AND AIMS TO PROVIDE CURB APPEAL FOR BOTH THE PROJECT AND THE NEIGHBORHOOD.

THE SIMPLE / DRY TOLERANT LANDSCAPE DESIGN FOR THE PROJECT EMBRACES LOW IMPACT DEVELOPMENT LANDSCAPE AND POTENTIAL LOW MAINTENANCE FOR RESIDENCE.

THE PROJECTS BIO-BASIN WILL HELP CAPTURE AND TREAT RAINWATER FROM THE STREET, PRIVATE LOTS BEFORE PUTTING IT INTO THE STORMDRAIN SYSTEM. THE BIO BASIN WILL ENHANCE THE OVERALL SITE MAKING IT VISUALLY APPEALING WITH A RHYTHMIC FASHION PLANT PALETTE, WITH CONTRASTING FORM, COLOR AND TEXTURE THAT PROVIDES VISUAL UNITY FOR THE DEVELOPMENT.

THE RIGHT-OF-WAY LANDSCAPE PROVIDES AN ATTRACTIVE PLANTING THAT IS SIMPLE MASSING OF CONTRASTING FORMS AND TEXTURES THAT FLOW INTO THE PRIVATE RESIDENCE. HELPING TO FLOW RIGHT-OF-WAY LANDSCAPE AND PRIVATE LANDSCAPE AS ONE.

EACH LOT WILL HAVE ITS OWN TRASH RECEPTACLE USE. REFER TO ARCHITECTURAL PLANS.

TREES ARE AN IMPORTANT PART OF THE LANDSCAPE FOR THE PROJECT. LOCATIONS HAVE BEEN CAREFULLY SELECTED TO PROVIDE ACCENT AND SELECTIVE SCREENING AND SCALE TO THE BUILDING. ALL TREES HAVE BEEN SELECTED FOR THEIR SALT TOLERANCES AND TREATMENT QUALIFICATIONS AND ALL PROJECT TREES SHALL RESPECT CITY-REQUIRED ROOT BARRIER REQUIREMENTS AND CLEARANCES FOR FIRE APPARATUS, UTILITIES AND EASEMENTS.

ALL PLANTINGS WILL BE GROUPED BY HYDROZONES SO THAT THEY MAY BE IRRIGATED EFFICIENTLY AND IN ACCORDANCE WITH THE CITY'S WATER CONSERVATION ORDINANCE. THE LANDSCAPE IS DESIGNED WITH LOW MAINTENANCE IN MIND, WITH NO SHEARING REQUIRED AND ONLY HAND PRUNING TO SHAPE AND REMOVE DEAD FLOWERS AND CONTROL VIEWS AND SCREENING.

### SUMMARY OF PROJECT DESIGN HIGHLIGHTS:

- THE TREE PALETTE INCLUDES TREES SELECTED FOR THEIR SIZE, FORM AND ORNAMENTAL QUALITIES RELATIVE TO THE ARCHITECTURE AND SITE LOCATION.
- TREES, SHRUBS AND VERTICAL ACCENT PLANTS WILL CONSIDER ACCENT, SCREENING AND SHADING QUALITIES (MICROCLIMATE MODIFICATION).
- TREE LOCATIONS SHALL OBSERVE ALL CLEARANCES TO UTILITIES AND EASEMENTS, AND ROOT BARRIERS SHALL BE EMPLOYED TO PROTECT HARDSCAPE AND UTILITIES PER OCEANSIDE STANDARDS.
- PROPOSED STORMWATER BMP PLANTER ARE HIGHLY ORNAMENTAL IN SPECIES COMPOSITION AND WILL BE IN CONFORMANCE WITH THE ENGINEER'S PLANS AND TREATMENT EXPECTATIONS.

### GENERAL NOTES:

THIS CONCEPTUAL LANDSCAPE PLAN DIAGRAMMATICALLY SHOWS PLACEMENT OF PROPOSED PROJECT LANDSCAPING. CONSTRUCTION LANDSCAPE PLANS SHOW PLACEMENT OF TREES, SHRUBS AND GROUND COVERS. FINAL LANDSCAPE PLANS SHALL ACCURATELY SHOW PLACEMENT OF TREES, SHRUBS AND GROUND COVERS AND REQUIRED ROOTBARRIERS FOR TREES. THE LANDSCAPE ARCHITECT IS AWARE OF ALL UTILITY, SEWER AND STORM DRAIN EASEMENTS AND THE CITY OF OCEANSIDE POLICY WHICH PROHIBITS TREES AND STRUCTURES IN UTILITY EASEMENTS. TREE LOCATIONS SHALL BE PLACED ACCORDINGLY TO MEET THE CITY OF OCEANSIDE REQUIREMENTS.

ALL PERTINENT UTILITY EASEMENTS ARE PER THE CIVIL ENGINEERING PLAN AND BASE SHEET INFORMATION. LANDSCAPE CONSTRUCTION PLANS SHALL SHOW ALL EASEMENTS THAT MAY AFFECT FINAL PLACEMENT OF PROJECT TREES AND SHRUBS, BASED ON THE EASEMENT AND UTILITY INFORMATION RECEIVED FROM THE PROJECT ENGINEER.

### EXISTING CONDITIONS, EASEMENTS, WALLS, FENCES & TRASH ENCLOSURES:

REFER TO CIVIL ENGINEERING PLAN FOR LOCATION AND TYPES OF EXISTING AND PROPOSED WALLS, UTILITIES, EASEMENTS AND PROPERTY LINES.

**IRRIGATION NOTES:** AUTOMATIC IRRIGATION SHALL BE INSTALLED AS REQUIRED TO PROVIDE COVERAGE FOR PLANTING AREAS SHOWN ON THE CONCEPTUAL PLAN. AUTOMATIC IRRIGATION SYSTEM SHALL BE INSTALLED AS REQUIRED TO PROVIDE COVERAGE FOR PLANTING AREAS SHOWN ON THE CONCEPTUAL PLAN. LOW VOLUME IRRIGATION EQUIPMENT SHALL PROVIDE SUFFICIENT WATER FOR PLANT GROWTH WITH A MINIMUM WATER LOSS DUE TO WATER RUN-OFF. IRRIGATION SYSTEMS SHALL USE HIGH QUALITY, AUTOMATIC CONTROL VALVES, TIMERS AND OTHER NECESSARY EQUIPMENT FOR PROPER COVERAGE. CONTROLLER SHALL BE 'SMART' CONTROLLER. ALL COMPONENTS SHALL BE OF NON-CORROSION MATERIAL AND ANY Drip SYSTEMS SHALL BE ADEQUATELY FILTERED AND REGULATED FOR THE MANUFACTURER GUIDELINES. CLASS 315 PRESSURE OR SCHEDULE 40 MAINLINE SHALL BE BURIED TO A MINIMUM DEPTH OF 18". PVC LATERAL LINES SHALL BE BURIED 12" MINIMUM BELOW FINISH GRADE. ALL MAINLINE SHALL BE INSTALLED PER MANUFACTURER GUIDELINES, SPECIFICATIONS, AND ADHERE TO CODES AND GUIDELINES. ALL LANDSCAPE AND IRRIGATION IMPROVEMENTS SHALL BE INSTALLED PER THE PROVISIONS OF THE CITY OF OCEANSIDE WATER CONSERVATION ORDINANCE SECTION 3019, THE CITY OF OCEANSIDE LANDSCAPE DESIGN GUIDELINES AND SHALL BE CONSISTENT WITH CURRENT STORMWATER BMP'S.

### PLANTING NOTES:

THE SELECTION OF PLANT MATERIAL IS BASED ON CULTURAL, AESTHETIC, ENVIRONMENTAL SENSITIVITY, WATER CONSERVATION AND MAINTENANCE CONSIDERATIONS. ALL PLANTING AREAS SHALL BE PREPARED WITH APPROPRIATE SOIL AMENDMENTS, FERTILIZERS, AND APPROPRIATE SUPPLEMENTS BASED UPON AN AGRICULTURAL SOILS ANALYSIS REPORT FROM SOIL SAMPLE TAKEN FROM THE SITE. GROUND COVERS OR BARK MULCH SHALL FILL IN BETWEEN THE SHRUBS TO PROTECT THE SOIL FROM EXCESSIVE SOLAR EXPOSURE, EVAPOTRANSPIRATION AND SURFACE WATER RUNOFF. ALL PLANTING AREAS SHALL BE MULCHED TO A DEPTH OF 3" TO HELP CONSERVE WATER, LOWER THE SOIL TEMPERATURE AND REDUCE WEED GROWTH. SHRUBS SHALL BE ALLOWED TO GROW INTO THEIR NATURAL FORMS WITHOUT SHEARING. ALL LANDSCAPE IMPROVEMENTS SHALL CONFORM TO THE CURRENT CITY OF OCEANSIDE GUIDELINES AS WELL AS ALL STREET TREES AND OTHER TREES SHALL SPATIAL REQUIREMENTS AND CLEARANCES.

### GENERAL MAINTENANCE AND COMPLIANCE WITH ORDINANCE CODE 3049 URBAN FORESTRY PROGRAM

THE HOA ASSOCIATION SHALL MAINTAIN PROPOSED BMP'S. THE PROJECT SHALL COMPLY WITH CODE 3049 URBAN FORESTRY PROGRAM AND PROVIDE INFORMATION AND COMPLIANCE REGARDING REGULAR SEASONAL AND EMERGENCY MAINTENANCE, TRASH ABATEMENT, IRRIGATION, TREE/PLANT CARE, TREE REPLACEMENT, INSECT AND DISEASE INFESTATION PREVENTION, INTEGRATED PEST MANAGEMENT AND APPROPRIATE RESPONSE PROCESSES. FAILURE TO COMPLY WITH MAINTENANCE IN A CONSISTENT MANNER WITH THE APPROVED (LTCMP) LANDSCAPE TREE CANOPY MANAGEMENT PLAN SHALL SUBJECT THE PROJECT TO CODE ENFORCEMENT ACTION. SEE ADDITIONAL NOTES FOR FIRE CODE COMPLIANCE.

### PROJECT SPECIFIC MAINTENANCE NOTES:

HOA SHALL BE RESPONSIBLE IN MAINTAINING STORMWATER BMP, RIGHT OF WAY AND FRONTRAGE SLOPE ALONG GUAJOME LAKE ROAD, PARK/ OPEN SPACE AREA AND PERIMETER SLOPES OUTSIDE OF RESIDENTIAL YARDS. FRONTRAGE LANDSCAPE , ALL FRONT YARD, SIDE YARDS AND REAR YARD ROW. LANDSCAPE SHALL BE PRIVATELY MAINTAINED BY EACH RESIDENTIAL OWNER. LANDSCAPE AREAS SHALL BE MAINTAINED PER THE CITY OF OCEANSIDE REQUIREMENTS.

### PROJECT STORM WATER MANAGEMENT PLAN (SUMP)

LANDSCAPING SHALL COMPLY WITH THE APPROVED STORMWATER MANAGEMENT PLAN AND MAINTENANCE SPECIFICATIONS AT CONSTRUCTION DRAWINGS, WITH ALL PLANTING SHOWN.

### TREE PLACEMENT SHALL BE DETERMINED DURING THE CONSTRUCTION PHASE, BASED ON FINAL LOCATIONS OF UTILITIES, STREET LIGHTS AND EASEMENTS. STREET

# Guajome Lake Road Single-Family Residential Development

Tentative Map & Development Plan with Density Bonus

T22-00004, D22-00009, DB22-00005

## Description & Justification

September 2024 (rev.)

### Introduction

This application presents a Development Plan for a single-family residential project which will develop 83 new homes on this property utilizing density bonus provisions for the project per State Density Bonus Law (SDBL). The proposal also includes a Tentative Map to create individual lots for the single-family homes plus additional lots for streets, a recreational amenity area, bio-filtration basins, and preserved open space. The project will provide additional single-family housing options for the community and designate four homes as affordable at the very low-income level.

The proposed site consists of a mostly vacant parcel (APN 157-412-15-00) and includes approximately 16.78 acres located in the Guajome Neighborhood Area of the City of Oceanside. The project site is located along the north side of Guajome Lake Road southeast of Albright Street in the east-central portion of the city. The City of Vista municipal boundary is located approximately 0.1 miles east of the project site. The project site is located approximately 0.5 miles south of State Route (SR) 76 and approximately 3.4 miles north of SR 78. The project site is surrounded by residential development and open space.

The General Plan designation for the project site is Single Family Detached Residential (SFD-R) with a zoning designation of Single-Family Residential - Scenic Park Overlay and Equestrian Overlay (RS-SP-EQ).

The project proposes development of approximately 9.86 acres of the overall 16.78-acre project site. The remaining 6.92 acres will be designated as open space to preserve sensitive habitat and riparian areas. The overall development impact area for the project is approximately 10.54 acres which includes right-of-way and slope grading features located outside the designated development plan area. The proposed development plan and single-family residential architecture are carefully designed to complement the site surroundings, provide safe access, and respect neighborhood character.

### Development Plan

The development is situated along the north side of Guajome Lake Road with two separate access drives connecting to the project site. A private loop road extends around the interior of the project site with all homes designed to front along this internal road. A 35,151 square foot recreational amenity area is positioned at the center of the community featuring common open space with lawn areas, a play area, and culinary lounge.

The proposed lots would range in size from approximately 5,390 to 2,464 square feet with the average lot size of approximately 3,200 square feet. Homes ranging in size from 1,869 to 2,220 square feet. As the primary access to the project site, Guajome Lake Road would be improved along the length of the property frontage, connecting to Albright Street. Road improvements along the project frontage would include 34-foot curb to curb improvements within a 40-foot-wide existing easement. An additional 10-foot wide dedication area to include a 5.0-foot parkway and a 5.0-foot sidewalk is also provided along the project frontage. The internal private road would vary from 28 – 32 feet wide allowing for guest parking at designated locations along the 32-foot-wide sections. Each proposed home would include a 2-car garage, and a private driveway that would allow for parking of two additional cars.

All homes would be developed on the southern portion of the project site which has been previously disturbed and graded. The project would avoid the northernmost portion of the project site along the riparian corridor, preserving approximately 6.92 acres of the 16.78-acre project site as open space. In existing conditions, the project site is mostly vacant and previously disturbed, with one existing residential house in the northern portion of the property which will be designated as open space.

The existing residence is a modest single-family structure (approximately 45 years old) and will be removed with implementation of the proposed development. A study prepared to analyze the home site has determined no buildings on the site qualify as historical resources under CEQA.

Two fuel modification zones (FMZs) would extend across the Project site. This defensible space consists of a combination of an irrigated, well-maintained landscape that consists of fire-resistant plants within 30 feet of the building (Zone 1) and a thinned landscape in the areas between 30 and 100 feet (Zone 2) from the structures (where applicable). The FMZs proposed for portions of this project are not standard OFD widths as some areas include reduced Zone 1 and/or Zone 2 areas and are less than 100 total feet within the property borders. These reductions are related to grading extents, portions of the fuel modifications zones extending in riparian forest protected areas, residential lot lines, or property boundaries that restrict Zone 1 and/or Zone 2.

### **Architecture**

The proposed single-family homes are designed as two-story residences with attached two-car garages. The residential designs feature multiple elevation & color scheme options to create diverse street scenes for the project. The homes would feature a variety of contemporary architectural designs in one of three styles, referred to as “ranch,” “farmhouse,” and “progressive prairie.” The architectural styles would be reinforced through massing and materials. A variety of roof forms would be included to shape the massing ranging from all gable, combination of hip and gable, and all hip. Style specific window grids and window and door trim along with front door and garage door styles help reinforce the architectural character. The homes would be predominantly stucco with either shingle, board and batten, or lap siding accents. Primary proposed building material finishes would include white, grey, or beige stucco exterior walls. Enhanced elevations would be included based on the elevation exposure to public edges. These plans each have 3 elevation styles, with each style having 3 distinct color schemes resulting in 27 combinations.

A summary of the floor plan and elevation mix for each of the residential lots is provided in Table 1 below:

**Table 1 - Residential Plan Summary**

PLAN TYPE	UNIT MIX	SQ. FT.	BDs / BAs
Plan 1	21	1,869	4 / 2.5
Plan 2	22	2,064	4 - 5 / 2.5 (loft option)
Plan 3	40	2,220	4 - 5 / 2.5 (loft option)
Elevation Styles: Ranch; Farmhouse; Progressive Prairie			

All outdoor lighting would meet Chapter 39 of the City Municipal Code (light pollution ordinance) and would be shielded appropriately. Street lighting featured throughout the site would be appropriately shielded to reduce lighting impacts to the surrounding open space areas and improve dark sky regulation compliance.

### **Sustainability**

In addition to the project's infill location, the project would include several sustainability design features to reduce potential energy and water usage and reduce potential greenhouse gas emissions. Proposed primary sustainability features include:

1. Photo-voltaic system installation
2. Drought-tolerant landscaping and a water efficient irrigation system

### **Landscape Design**

The landscape design embraces visual enhancement for prospective residents of the site and strives to provide a distinct visual character for the project and the neighborhood. The simple drought tolerant landscape design for the project incorporates low-impact development landscape and potential low-maintenance for the residential lots.

The planting layout for the project was designed with a conscious effort to provide an enhanced perimeter landscape that will be compatible with the visual character of Guajome Regional Park. In the core open space, a variety of tree species would help accentuate the social and community gathering spaces. Drought tolerant and low water use plants would be incorporated. Soft vegetation plantings would provide a layered and textured ground plane.

A variety of vegetation would be featured along the boundaries of the project site deriving influence from Guajome Regional Park and creating a welcoming environment along the street frontage. Landscape along the Guajome Lake Road frontage would provide an attractive street

tree edge which will effectively transition into the private residential areas of the site. The BMP basins feature suitable plantings to provide attractive edge conditions in these areas.

Trees are an important part of the landscape for the project with locations carefully selected to provide accent and selective screening and complementary scale to adjacent residences. The tree palette includes species selected for their size, form and ornamental qualities relative to the residential architecture and site location. Side and rear-yard slopes in each lot will be planted with appropriate ground covers and shrubs to present a softened edge condition and to provide erosion control.

The proposed project would be required to comply with Article 3049, Urban Forestry Program, of the City's Zoning Ordinance. The Urban Forestry Program requires new development over 1 acre in size to provide a minimum tree canopy area of 12%, and a minimum permeable surface area of 22%. The project would satisfy these requirements by providing approximately 15% tree canopy area and 26% permeable surface area.

Retaining walls would be located along the project frontage, entries and BMP areas to support the required grading and storm drainage for the project site. Entry monumentation would utilize the proposed corner retaining walls with signage designed to create a gateway into the community.

### **Open Space**

The project would include approximately 35,151 square feet of private recreational and amenity area within the development. Additionally, the project would avoid the northernmost portion of the project site along the riparian corridor, preserving approximately 6.92 acres of the 16.78-acre project site as open space.

The open space and theming of the community take into consideration the project's proximity to Guajome Regional Park as well as the history and culture of the site. The design is intended to pay homage to the community's cultural assets such as the Rancho Guajome adobe. Forms and patterns found in the open space take inspiration from these historic and unique visual profiles. Amenities and materiality are influenced by the equestrian nature of its surroundings and local points of interest, creating a natural and rustic landscape for the residents. The design of the community would feature a dynamic core within the community providing flexible spaces for gathering, culinary experiences, play and recreation.

The large central park takes inspiration from the geometry of the Rancho Guajome Adobe. It emphasizes long visual connections and a somewhat formal arrangement with the park divided into three distinct areas. These three areas are comprised of a culinary component featuring BBQs with picnic areas and a large lawn for social gatherings, a multi age tot-lot with shade pavilion, and a passive lawn space. A pedestrian path links the different areas together providing a series of experiences along the way.

## Affordable Housing Density Bonus Addendum

### State Density Bonus Law Unit Calculations

The State Density Bonus Law (SDBL) was established to promote the construction of affordable housing units and requires cities to provide a density bonus and other incentives or concessions, and waivers of development standards to projects which provide affordable housing units in compliance with current density bonus regulations. The City of Oceanside zoning regulations implement the state requirements, and Table 2 summarizes the calculations under SDBL for this project with specific detail provided in Table 3. As provided under SDBL the site is permitted a maximum total of 89 dwelling units with 4 affordable units dedicated at the very low-income level; however, only 83 dwellings are proposed under this application.

Four (4) of the proposed 83 single-family homes (5 percent of the total) would be designated as deed-restricted affordable housing. The remaining 79 homes would be sold at market rate. The proposed affordable homes would be distributed evenly throughout the community and located on Lots 8, 41, 53 and 59.

In order to accommodate the project as allowed under SDBL, the project cannot physically comply with all of the development standards that apply to standard single-family residential projects. Based on the proposed design to accommodate Density Bonus units, the project application incorporates waivers of development standards, including; reduction of lot size and lot width, reduction of setbacks, increase lot depth to width ratio, increased lot coverage, retaining wall height and design, and a waiver of applicable equestrian development standards.

**Table 2 – Project Density Bonus Summary**

Density Bonus Item	Calculations	Proposed for Project
<b>Base Density Permitted by Current General Plan / Zoning</b>	74 units (per SDBL)	74 units
<b>Proposed Affordable Unit Income Level</b>	5% at Very Low-Income Level	4 units
<b>Proposed Density Bonus Percentage</b>	20.0%	15 units
<b>Total Dwelling Units Proposed on Site</b>	Maximum of 89 units allowed per SDBL	83 units proposed
<b>Resulting Density in Units / Acre</b>	83 units / 12.45 acres	<b>6.67 du/ac</b>

**Table 3 – Allowable Density Bonus Calculations\***

STEP 1:	<p>Calculate Base Allowable Density</p> <p>Under SDBL the base number of units permitted is determined by multiplying the gross site acreage by the applicable maximum density for the specific zoning range and land use element of the general plan applicable to the project area. Per City of Oceanside regulations, the gross acreage for this site is determined by considering the overall site area and subtracting riparian areas and road easement areas.</p> <p><b>(SFD-R) 12.45 acres x 5.9 dwelling units per acre = 73.46 units</b></p> <p><b>Total rounded up to 74 units as designated base amount</b></p>
STEP 2:	<p>Determine Affordability Percentage and Units</p> <p>The project proposes to provide 5% of the units as affordable to very low-income households. Per State Density Bonus Law, affordable unit percentage is calculated excluding units added by a Density Bonus.</p> <p><b>5% x 74 units (base allowable) = 3.7 units; rounds up to 4 units</b></p>
STEP 3:	<p>Calculate Density Bonus Units</p> <p>Under SDBL, the provision of 5% very low-income units allows the applicant to receive a density bonus of up to 20.0%, allowing additional market-rate units to be constructed.</p> <p><b>74 units x 0.20 (density bonus) = 14.8 units; rounds up to 15 units</b></p>
STEP 4:	<p>Calculate Total Dwelling Units</p> <p>Add the base allowable units and the density bonus units.</p> <p><b>74 units (base allowable units) + 15 units (density bonus units) = 89 total units allowed</b></p> <p><b>Only a total of <u>83</u> units are proposed for the project.</b></p> <p><b>(79 market rate + 4 affordable homes)</b> <b>-Resulting Overall Project Density on 12.45 acres = 6.67 du/ac</b></p>

\* Note: Per State Density Bonus Law, all fractional units shall be rounded up.

## **INCENTIVES / CONCESSIONS and WAIVERS**

In addition to the density bonus units and the parking requirements specified in State Density Bonus Law, the project is also entitled to certain incentives / concessions and waivers. These can include, for example, a reduction in site development standards or a modification of zoning code or architectural design requirements. By providing 5% of the base units as very low-income units, this project is entitled to receive (1) incentive / concession. The Project is not requesting any incentives/concessions at this time, but reserves the right to do so prior to action by the Planning Commission or City Council.

### **Waivers**

The granting of waivers does not reduce the number of incentives / concessions allowed for a project, and the number of waivers that may be requested and granted is not limited. To accommodate the increased density allowed under SDBL and maintain the single-family lot design and character of the underlying zone, the project cannot physically comply with all the development standards that apply to standard projects. Based on the proposed design to accommodate the affordable units, the project seeks waivers of the following development standards for the proposed housing development pursuant to SDBL as presented in detail in Table 4 below. These standards and waivers as applied to each lot are also detailed in the table provided on Sheet 2 of the updated civil plan set prepared by PLSA.

Development Plan Standards and Requested Waivers under state Density Bonus Law:

- Reduction of lot sizes
- Reduction of lot width
- Increase lot depth to width ratio
- Reduction of building setbacks
- Increase lot coverage percentage
- Increase retaining wall heights
- Equestrian development standards waived

**Table 4 – Development Standards Matrix - SDBL Waivers**

DEVELOPMENT STANDARDS	RS ZONE STANDARD	PROPOSED PROJECT	NOTES
<b>Lot Size (sf)</b> (OZO Sec 1050)	6,000 sf (min)	Lots Range From: 2,464 – 5,390 sf	<b>Waiver</b> to allow affordable units and development at density proposed.
<b>Lot Width</b> (OZO Sec 1050)	65 feet (min)	Lot Widths Range From: 32ft – 59 ft	<b>Waiver</b> to allow affordable units and development at density proposed.
<b>Setbacks</b> (OZO Sec 1050)	Front 20 ft (min)	6 - 20 ft (min)	<b>Waiver</b> to allow affordable units and development at density proposed.
	Side 7.5 ft (min)	3 – 7.5 ft (min)	
	Corner Side 10 ft (min)	6 - 10 ft (min)	
	Rear 15 ft (min)	5 - 15 ft (min)	
<b>Lot Coverage</b> (OZO Sec 1050)	45% (max)	Coverage Ranges From: 20% - 54%	<b>Waiver</b> to allow affordable units and development at density proposed.
<b>Lot Depth to Width Ratio</b> (OZO Sec 1050(F))	2.5 : 1	Various lots will exceed standard up to a ratio of 3.7:1	<b>Waiver</b> to allow affordable unit and development at density proposed.
<b>Building Height</b> (OZO Sec 1050)	36 ft (max)	Approx. 25 ft proposed	Complies with Code
<b>Parking</b> (OZO Sec 3103)	2-car garage per single-family homes $\leq$ 2,500 sf.	2-car garages provided for each home; 32'-wide street sections allow for guest parking.	Complies with Code
<b>Landscaping</b> (OZO Sec 1040(T))	Minimum 50% of yard adjoining street shall be planting or landscape. The remainder may be used for driveways or walks.	Landscaped front yard areas provided for each lot.	Complies with Code
<b>Fences and Walls</b> (OZO Sec 3040(D))	Maximum height of a fence or wall, including retaining walls shall be 6'. Retaining walls over 4' in height shall be planted and irrigated.	Proposed retaining walls are not plantable / irrigated with wall sections exceeding 6' in height: <ul style="list-style-type: none"><li>• Exterior Facing – up to 9'</li><li>• Interior Facing – up to 12.1'</li><li>• Interior SW Basin – up to 10'</li></ul> *Decorative vinyl fencing up to 6' high and ornamental iron fencing up to 5' high may be added to the top of residential lots and basin walls, respectively.	<b>Waiver</b> to allow affordable units and development at density proposed.
<b>Usable Open Space</b> (OZO Sec 1040(Q))	Total useable space shall be at least 300 sf per unit (300 * 83 = 24,900 sf)	Common Open Space (35,151 sf) and Rear Yard areas for each lot provide usable open space greatly exceeding minimum sf.	Complies with Code
<b>Urban Forestry</b> (OZO Sec 3049)	Tree Canopy minimum on sites one acre or more - 12% of site	Tree Canopy - 15% or approximately 57,332 sf	Complies with Code
	Permeable Surface Area minimum on sites one acre or more - 22%	Permeable Surface Area – 26% or approximately 103,161 sf	Complies with Code
<b>Renewable Energy</b> (OZO Sec 3047)	Residential projects with 25 or more units shall install and maintain renewable energy facilities that supply at least 50% of forecasted electricity demand	Photo-voltaic system will be installed on each home to meet 50% of forecasted electricity demand for project	Complies with Code
<b>Equestrian Overlay District</b> (OZO Article 28)	Article presents specific criteria and development regulations for the EQ Equestrian Overlay District	Project lots are not designed to meet equestrian development regulations, consistent with other residential subdivisions on Guajome Lake Road.	<b>Waiver</b> to allow affordable units and development at density proposed.

## Tentative Map & Site Engineering

### Subdivision

The project application includes a Tentative Map which will create individual lots for the single-family homes plus additional lots for streets, a recreational amenity area, bio-filtration basins, and preserved open space across the existing 16.78-acre site. The proposed subdivision is detailed as follows in Table 5.

**Table 5 - Subdivision**

<b>Lot #</b>	<b>Description</b>
<u>Lots 1 – 83</u> Single-family lots	2,464 – 5,390 sf
<u>Lot A</u> Private Road	65,359 sf
<u>Lot B</u> Bio-filtration basin	13,282 sf / 0.30 ac
<u>Lot C</u> Bio-filtration basin	9,132 sf / 0.21 ac
<u>Lot D</u> Open Space – Recreation	35,151 sf / 0.81 ac
<u>Lot E</u> Open Space - Natural	301,238 sf / 6.92 ac
<u>Lot F</u> Open Space - Natural	7,507 sf / 0.17 ac

### Circulation, Access and Parking

Both entrances to the project site are located at the project frontage along Guajome Lake Road. The proposed single-family development would be connected by a private loop road within the project site. Guajome Lake Road would be improved along the length of the property frontage, connecting to Albright Street. Road improvements would include 34-foot curb to curb improvements inside the existing 40-foot wide right-of-way easement with an additional 10-foot wide dedication area to include a 5.0-foot parkway and a 5.0-foot sidewalk. Circulation and emergency access drives have been designed in consultation with Oceanside Fire staff to provide 28-foot minimum widths with designated truck turnarounds and key staging areas throughout the project site. The internal private road would vary from 28 – 32 feet wide allowing for guest parking at designated locations along the 32-foot-wide sections. Resident and guest parking is also provided via the 2-car garages and private driveways designed with each home. No resident or guest parking is designated along Guajome Lake Road along the project frontage.

### Pedestrian Circulation and Access

Pedestrian access within the site would be provided along the internal private loop. Sidewalks would also be constructed along the Guajome Lake Road frontage as described. Immediately

adjacent to the project site is Guajome County Park which includes multiple different trails. Santa Fe Trail is located approximately 0.22 mile east of the site off Guajome Lake Road to the south.

### **Public Transit Access**

The project site is provided transit service via the North County Transit District (NCTD), which operates the Santa Fe Ave & Darwin Drive Sprinter Station located approximately 1.6 miles south of the project site. Bus stops are located along N Santa Fe Avenue, south of Guajome Regional Park.

### **Grading**

The site grading and lot pattern are designed to respect the existing topography of the property which generally descend from east to west across the proposed development area in the western portion of the parcel. The total conceptual grading quantity estimates are presented in Table 6 below.

**Table 6 – Grading**

EARTHWORK	QUANTITY
Cut	84,500 CY
Fill	17,500 CY
Export	67,000 CY

Retaining walls proposed along portions of the exterior site boundary are designed to support the overall development plan and pad design. Small sections of retaining walls are designed along interior lot lines to facilitate development of the residential building pads and allow for appropriate storm water drainage and treatment across the site. These walls are designed to respond to existing site topography and waivers are implemented for the wall design to accommodate the affordable units and project density as proposed.

The proposed residential pad elevations will range from approximately 179' near the northeast corner of the property to 148' at the northwest corner. The lot pattern will 'step down' across the site to create an enhanced physical separation between the homes, provide an appropriate for the recreational open space area at the center of the site, and contribute to the overall aesthetic of the project.

### **Project Utility Infrastructure**

The project proposes to install backbone utility infrastructure consisting of storm drain, public water main, and sewer force main and lift station to serve the new proposed residences. Various surface, grading, and utility improvements typical of this type of construction are also proposed.

### **Water Facilities**

Potable water is currently provided by the City of Oceanside. The project is situated in the central northern portion of the city in an area served by the Talone 320 Pressure Zone. The nearest existing 320 Pressure Zone public water lines in the vicinity of the project are a 10-inch and 12-

inch water line in Guajome Lake Road southwest of the project and an 8-inch water line at the intersection of Melrose Drive and Spur Avenue to the northeast of the project. The public water system within the project site would be connected to the existing 12-inch public water line in Guajome Lake Road. Internal to the project, the water system would consist of 8-inch piping.

### **Sewer Facilities**

The City of Oceanside also provides wastewater collection, treatment, and disposal services of sewage for this area of the city. The existing public sewer system consists of 8-inch diameter sewer lines in Old Ranch Road and Hitching Post Drive. The sewer in Hitching Post Drive continues northwest to a 15-inch trunk sewer in Highway 76. The nearest existing public sewer to the project site is approximately 2,000 feet away. All onsite sewer facilities for the project are proposed to be private. Each home within the project site would have its own sewer lateral. The project would require a private sewer lift station to deliver flows to the existing 8-inch public sewer line in Old Ranch Road.

### **Site Drainage**

The project would include dedicated storm water treatment areas (BMPs) on-site. The proposed private lots would primarily drain from the rear of each property away from the building and out to the front of each lot by a combination of sheet flow methods, swale grading and private storm drain piping. All proposed hardscape within the developed area of the project would be captured and routed to these proposed BMP's. From there, an outlet pipe would then convey treated and detained runoff to the appropriate points of discharge from the property.

## **Summary**

The Guajome Lake Road Single-Family Residential Development will provide additional single-family housing options for the community at varying affordability levels – including four affordable homes designated at the very-low-income level.

The project has been thoughtfully planned to:

- Complement site surroundings and respect neighborhood character.
- Ensure visual and functional compatibility with nearby land uses.
- Provide new, market rate and affordable residential homes on an infill site.
- Present a high-quality architecture and landscape design.
- Improve the adjacent roadway and emergency access for nearby areas.
- Develop on a site that can be served by existing utilities, services and existing streets.

The proposed development meets the goals, intentions, and objectives of the General Plan, Zoning Ordinance, Density Bonus Law, and affordable housing objectives.

## PROJECT FINDINGS

### **Development Plan**

- 1. That the site plan and physical design of the project as proposed is consistent with the purposes of the Zoning Ordinance.*

The site plan and physical design of the project is consistent with the Zoning Ordinance purposes while also accomplishing the goal of providing market rate housing and affordable housing utilizing the state density bonus provisions.

- 2. That the Development Plan as proposed conforms to the General Plan of the City.*

The Development Plan as proposed conforms to the General Plan of the City because the proposed project is located near similarly zoned parcels and the project will result in the development of a single-family residential community which meets the goals and objectives designated by the applicable elements of the General Plan.

- 3. That the area covered by the Development Plan can be adequately, reasonably, and conveniently served by existing and planned public services, utilities and public services.*

The project site is located within an urbanized area served by existing public services, utilities, and public facilities.

- 4. That the project as proposed is compatible with existing and potential development on adjoining properties or in the surrounding neighborhood.*

The project is located on an in-fill site where the residential architecture and development plan are carefully designed to complement the site surroundings and respect the single-family neighborhood character.

- 5. That the site plan and physical design of the project is consistent with the policies contained within Section 1.24 and 1.25 of the Land Use Element of the General Plan, the Development Guidelines for Hillsides, and Section 3039 of this ordinance.*

The project site does not contain topographic resources or slopes that are undevelopable per Sections 1.24 and 1.25 of the Land Use Element and Section 3039 of the Zoning Ordinance.

# Community Outreach Summary Report

July 2025

## Guajome Lake Homes Single-Family Residential Development

This Community Outreach Summary Report has been prepared in accordance with Council Policy 300-14 and provides a summary of the notification and outreach efforts for the Guajome Lake Homes development, which are intended to comply with the policy goals to increase public awareness and promote public participation during review of this proposed development project. Additional details are provided below and in the appendices included with this report.

Since summer of 2022, Rincon Homes has been working on a proposal for the development of 83 single family homes on the 16.78 acre property located at 2389 Guajome Lake Road. As part of design process, two neighborhood meetings were held in September of 2022 and April of 2023 to solicit feedback from the community and respond to questions and concerns related to the proposed project. Individual meetings with area neighbors have also been held to discuss issues specific to neighboring properties.

Located in Oceanside's Guajome Neighborhood in the east-central portion of the city, the project site is situated along the north side of Guajome Lake Road southeast of Albright Street. The property is bordered on the east by residential land in the City of Oceanside, while The City of Vista municipal boundary is located approximately 0.5 miles further to the east.

The project site also is located approximately 0.5 miles south of State Route (SR) 76 and approximately 3.4 miles north of SR 78. The project proposes development of approximately 9.86 acres of the overall 16.78-acre project site. The remaining 6.92 acres will be designated as open space to preserve sensitive habitat and riparian areas.

### **Notification of Project Application**

Notification of the initial application filing in June 2022 was sent via mail by the city to all addresses included on the project radius list which is based on the most recently available information from SanGIS. The radius list was based on the entire 16.8-acre site and included owners within a 1,500-foot radius and tenants within a 100-foot radius resulting in notices being sent to over 580 mailing addresses. In addition, individuals on the city's interested party list were also sent the initial notification.

Project notification signage was also posted on the site after the original application was filed with the city. The notification signage has remained throughout the entitlement process and been replaced or repaired as needed. A "Certificate of Posting" for the signage is on file with the city. A copy of the notification radius map and pictures of the notification signage are included in Appendix A.

### **Neighborhood Meetings and Community Engagement**

Ongoing correspondence and follow-up discussions have continued with neighboring residents and previous meeting attendees as part of the project's community engagement efforts. In response to feedback from both the community and the City of Oceanside, the project design has been refined to

incorporate several key features intended to address concerns and enhance compatibility with the surrounding area - including the following elements:

- Traffic calming measures proposed on Guajome Lake Road including speed cushions & advisory signage along the project frontage and/or in the vicinity of the project site.
- Safety improvements on Guajome Lake Road including curb, gutter, and paving from Albright Street to the eastern terminus of the project. A sidewalk will also be installed along the project frontage.
- Enhanced landscaping along the project frontage
- Project lighting plan that avoids light intrusion and minimizes effects on adjacent homeowners
- Sensitivity to Guajome Lake Park
- Avoidance of sensitive habitat onsite
- Sewer improvements to address existing sewer deficiencies downstream of the project
- Onsite affordable housing for very-low-income families consistent with the City of Oceanside municipal code and state law

#### **CEQA Scoping Meeting**

In conjunction with the CEQA process, a Scoping Meeting was held on November 15, 2022 at the El Corazon Senior Center to gather additional public input regarding the proposed project. The initial 30-day public scoping period ended on December 1, 2022. A Notice of Preparation (NOP) mailer was posted and sent to the notification and interested parties list as required prior to the meeting. The NOP included a project narrative along with location and site plan exhibits.

The Scoping Meeting provided an overview of the proposed project, and discussion of potential environmental analysis topics and issues planned to be addressed as part of the project Environmental Impact Report (EIR). Comments received during the NOP public scoping period were considered as part of the preparation of the project EIR. Comments covered numerous topics, including biological habitat, site access and circulation, utility infrastructure and supply, tribal cultural resources, traffic generation and roadway improvements, air quality, greenhouse gas emissions, growth inducement, open space and recreation, and preservation of biological and cultural resources. Please see Appendix B which includes a copy of the NOP and attendee sign-in sheets from the Scoping Meeting.

#### **Community Outreach Meetings – September 2022 and April 2023**

Neighborhood meetings were held in September 2022 and April 2023 as part of the applicant's ongoing community outreach efforts. Both meetings were held at the Melba Bishop Recreation Center with invitations for each meeting being sent to all residents and property owners included on the project notification list. Each meeting featured a presentation by the applicant team outlining key aspects of the proposed development, including site plans, architectural exhibits, and details of the associated density bonus application. City staff also participated to help address technical questions and provide context on applicable review procedures.

The first meeting served as an introduction to the project and an opportunity to gather initial feedback from the community. The second meeting was held several months later following continued communication with neighbors, individuals, and organizations that had submitted comments or

expressed interest in the project. This meeting included updated plans and revisions made in direct response to community input received since the initial presentation.

Throughout both meetings, community members shared a range of comments and concerns. Common topics included traffic safety and the need for road or access improvements, potential traffic calming measures, project design and density, lighting impacts, and planned sewer system upgrades. All comments were recorded, and the applicant team and city staff provided responses where possible, with a commitment to continued dialogue as the project progressed.

#### **Community Outreach Update Letter – November 2024**

A Project Update Letter was also prepared by the applicant and sent to neighbors and stakeholders included on the project notification list on November 4, 2024. The letter provided a comprehensive update on the project's status and recent outreach efforts. It included a digital link to the City of Oceanside website where recipients could view the latest project plans. The letter also highlighted key design features that had been incorporated into the proposal in direct response to community feedback - many of which are described above.

In addition, the letter outlined the current status of the Environmental Impact Report (EIR), the anticipated public review schedule, and provided up-to-date contact information for the applicant team. Recipients were encouraged to reach out with any further comments, questions, or concerns.

Please see Appendix C which provides copies of the April 2023 Community Meeting Notice and the 2024 Outreach Update Letter.

## **Guajome Lake Homes**

Community Outreach  
Summary Report

### Appendix A

Notification Radius Map

On-site Notification Signage



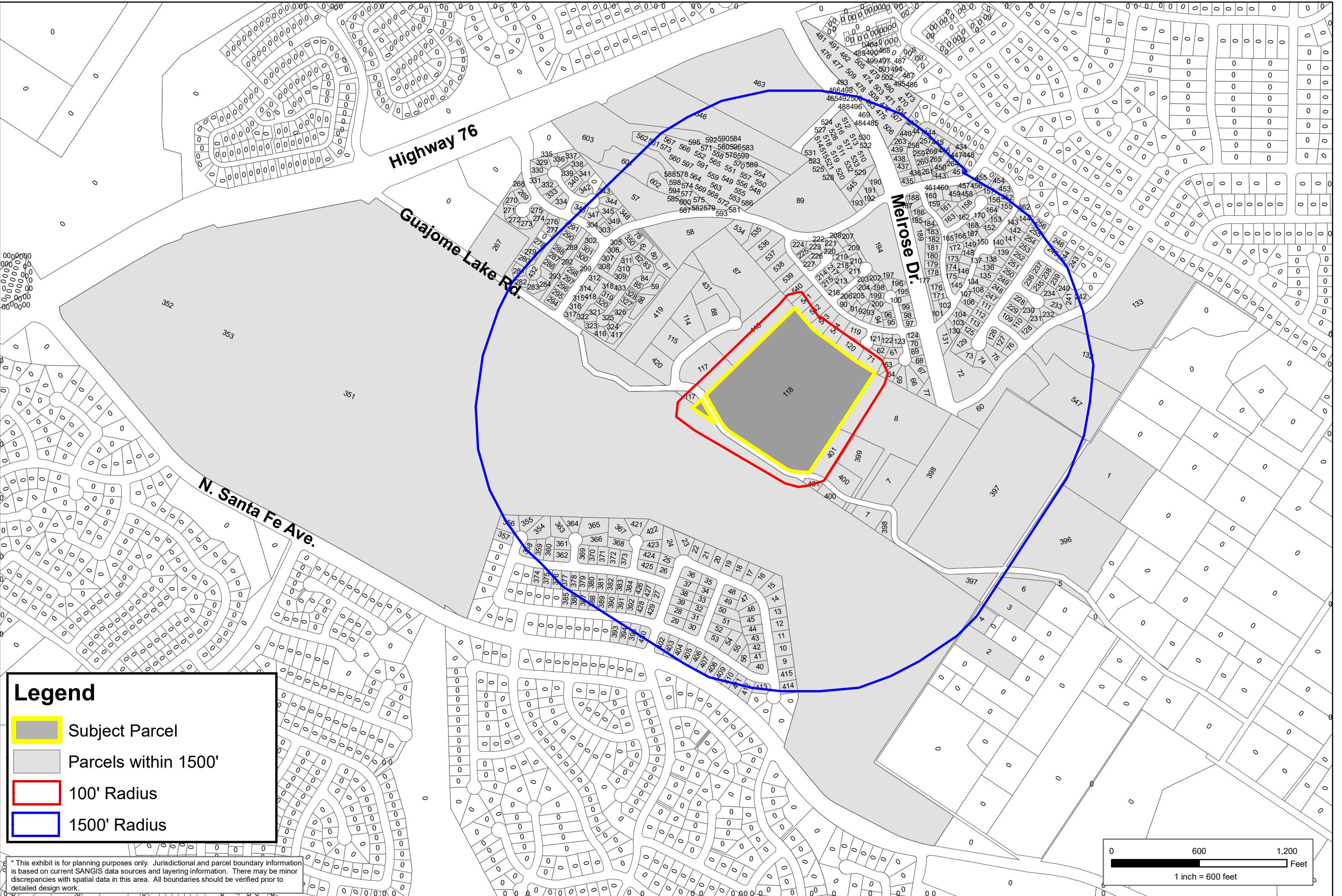
THE  
LIGHTFOOT  
PLANNING  
GROUP

GUAJOME LAKE ROAD  
APN: 157-412-15-00



RADIUS MAP  
1500' OWNERS/  
100' RESIDENTS

Source:  
SanGIS 10-2024  
nearmap imagery  
Created: 10/21/2024



## Notice Sign 1



## Notice Sign 2



# **Guajome Lake Homes**

Community Outreach  
Summary Report

## Appendix B

Notice of Preparation  
November 2, 2022

Scoping Meeting Sign-in Sheets  
November 15, 2022



## NOTICE OF PREPARATION

### City of Oceanside, California

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**To:** Office of Planning and Research  
Responsible and Trustee Agencies  
Other Interested Parties

**Subject:** Notice of Preparation of an Environmental Impact Report (EIR)

**Project:** Guajome Crest Project (APN 157-412-15-00) located at 2839 Guajome Lake Road in the City of Oceanside. Tentative Map (T22-00004), Development Plan (D22-00009), Request for Density Bonus (DB22-00005)

**Lead Agency:** City of Oceanside

**Date:** November 2, 2022

Pursuant to Section 15082(a) of the California Environmental Quality Act (1970), the City of Oceanside will be the lead agency and will require preparation of an environmental impact report for the project described below. Consistent with your agency's statutory authority, the City requests input regarding the scope and content of the EIR. The City has concluded that the project could result in potentially significant environmental impacts and therefore an EIR is required. The project description and location are included herein.

Pursuant to Section 15103 of the CEQA Guidelines, response must be sent at the earliest date and received by our agency no later than thirty (30) days after receipt of this notice. Should you have any questions regarding the project or notice of preparation, please call Rob Dmohowski, Principal Planner, at (760) 435-3563. Please mail your written response by December 1, 2022 to:

Development Services Department  
Attn: Rob Dmohowski, Principal Planner  
300 N. Coast Hwy.  
Oceanside, California 92057  
Fax: (760) 435-2958  
E-Mail: [rdmohowski@oceansideca.org](mailto:rdmohowski@oceansideca.org)

**City/County Location:** City of Oceanside, County of San Diego

**Applicant:** Rincon Homes

**Project Location:** The approximately 16.78-acre project site is located directly north of Guajome Lake Road and to the east of Albright Street, in the eastern section of the City

of Oceanside, California (APN 157-412-15) (Figure 1, Project Location). Residential development is present to the north, northwest, and east of the site. Across Guajome Lake Road to the south is Guajome Regional Park, which separates the project site from additional single-family residential development. Highway 76 is located approximately 0.5-mile north of the project site and Guajome Lake is located approximately 0.5-mile northwest of the project site within Guajome Regional Park.

**Project Description:** The proposed project would involve a request for approval of a Development Plan, Tentative Map, and Density Bonus to allow for the construction of 83 single-family homes on approximately 12.45 acres of the 16.78-acre project site (Figure 2, Site Plan). The project would also include approximately 34,391 square feet of private recreational and amenity area within the development. The project is subject to State Density Bonus Law (Government Code Section 65915) and local Density Bonus provisions (Section 3032 of the Zoning Ordinance).

The General Plan designation for the project site is Single Family Detached Residential (SFD-R) with a zoning designation of Single-Family Residential - Scenic Park Overlay zone and Equestrian Overlay zone (RS-SP-EQ).

Four (4) of the proposed 83 single-family homes (5 percent of the total) would be designated as deed-restricted affordable housing. The remaining 79 homes would be sold at market rate. The proposed affordable homes would be distributed evenly throughout the community. In order to accommodate the project as allowed under Density Bonus Law, the project cannot physically comply with all of the development standards that apply to standard single-family residential projects. Based on the proposed design to accommodate Density Bonus units, the project anticipates seeking waivers of development standards, including reduction of lot sizes, equestrian development standards removed, reduction or redistribution of setbacks, reduction of open space/landscape minimums, increased floor area ratio (FAR) per lot, and retaining wall heights.

The average proposed lot sizes would be approximately 3,200 square feet with homes ranging in size from 1,850 to 2,250 square feet. Primary access to the project site would be from Guajome Lake Road, which would be improved as part of the project. Guajome Lake Road would be improved the length of the property frontage, connecting to Albright Street. Road improvements would include 40-foot curb to curb improvements including a 5.5-foot parkway and a 4.5-foot sidewalk. The internal private road would be 28 – 32 feet wide with 5-foot sidewalks. Each proposed home would include a 2-car garage, and a private driveway that would allow for additional parking of 2 more cars.

All homes would be developed on the southern portion of the project site which has been previously disturbed and graded. The project may be required to clear part or all of the Coastal Sage Scrub to provide a fuel modification zone for fire requirements. This will be addressed in the EIR. The project would avoid the northernmost portion of the project site along the riparian corridor, preserving approximately 3.77 acres of the 16.78-acre project site as open space. In existing conditions, the project site is mostly vacant and previously

disturbed, with one existing residential house in the northern portion of the property.

**Potential Environmental Effects:** Pursuant to CEQA Section 15060(d) of the CEQA Guidelines, the project may potentially result in significant impacts related to: Aesthetics, Air Quality, Biological Resources, Cultural Resources, Energy, Geology/Soils, Greenhouse Gas Emissions, Hydrology/Water Quality, Noise, Public Services, Transportation, Tribal Cultural Resources, Utilities/Service Systems, and Wildfire. An EIR will be prepared to evaluate the proposed project's potential impacts on the environment, outline mitigation measures, and analyze potential project alternatives.

## PUBLIC SCOPING MEETING

The City of Oceanside will hold a public scoping meeting to obtain information regarding the content and scope of the Draft Environmental Impact Report (DEIR). This scoping meeting will take place at **6:00 p.m. on Tuesday November 15, 2022, at the El Corazon Senior Center, located at 3302 Senior Center Drive, Oceanside, CA 92056**. The scoping meeting format will consist of a brief project presentation, followed by a public comment period, and open forum with city staff and applicant representatives. All public agencies, organizations and interested parties are encouraged to attend and participate in this meeting.

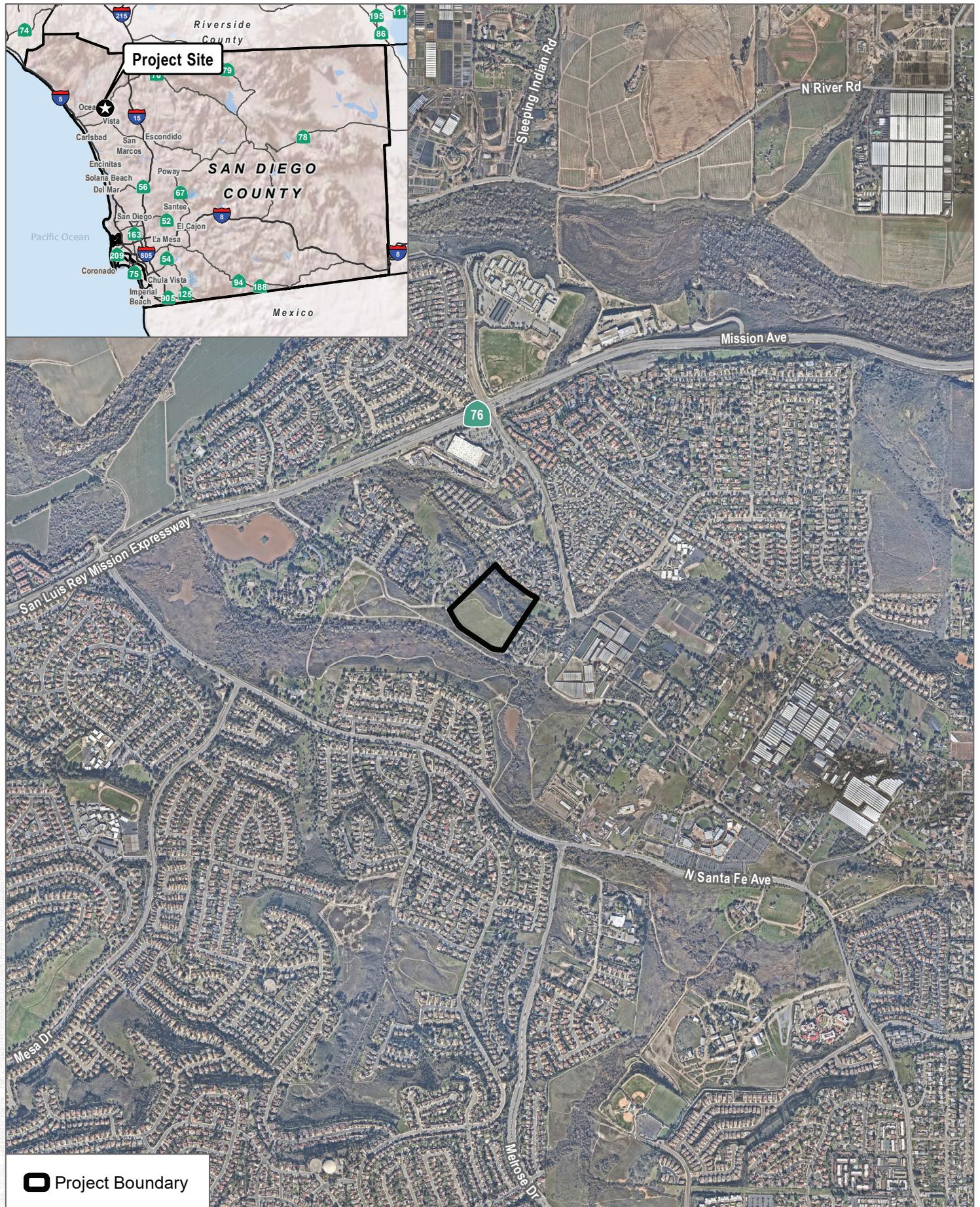
Entitlement application materials for this project have been submitted to the City and are currently being reviewed by staff and are available for public review either at the City or on the City's eTRAKIT website (<https://crw.cityfoceanside.com/etrakit3/>) under project number **T22-00004**.

**Signature:**  Robert Dmohowski  
Rob Dmohowski, Principal Planner

Digitally signed by Robert Dmohowski  
DN: E=RDmohowski@oceansideca.org,  
CN=Robert Dmohowski, OU=Planning,  
OU=Development Services, DC=oceanside-nt,  
DC=ocean, DC=local  
Date: 2022.11.01 17:10:22-07'00'

**Date:** November 2, 2022

**Attachments:** Figure 1, Project Location  
Figure 2, Site Plan



SOURCE: SANGIS 2019, Open Street Maps 2019

**DUDEK**



0 800 1,600 Feet

**FIGURE 1**

**Project Location**  
Guajome Crest Project



SOURCE: SANGIS 2019, Open Streets Map 2019

**DUDEK**



0 75 150 Feet

**FIGURE 2**

**Site Plan**

Guajome Crest Project

## SCOPING MEETING – GUAJOME CREST PROJECT

6:00 p.m. on November 15, 2022

El Corazon Senior Center, located at 3302 Senior Center Drive in the City of Oceanside

Name (please print)	Mailing Address (optional)	E-mail (optional)
Heather Morris	8431 Stirrup Way 92057	dhmorris09@att.net
Walt Hamblen	5405 Old Ranch Rd 92057	whamblly@cox.net
Rosie Garcia Bassi	5720 Damnona Cir 92057	jakeselectric@yahoo.com
Randy Marrow	5408 St. Rapp Way 92057	randall.marrow@gmail.com
Joe P. Marsteller	2701 Guajome Cr 92057	
Margaret Ogjela	2837 Guajome Lk Rd 92057	margiela@outlook.com
Ferry Kraw	5340 Erie Ct, 92056	jerry_kjx@hotmail.com
Bibi Fernande	2833 Guajome Lake Rd.	
John Twyman	54135 Sisterburst Way	jtwyman@cox.net
Lee Kelly-Gillen	340 Hidden Lake Ln Vista 92084	lkg@cox.net

SCOPING MEETING – GUAJOME CREST PROJECT

6:00 p.m. on November 15, 2022

El Corazon Senior Center, located at 3302 Senior Center Drive in the City of Oceanside

Name (please print)	Mailing Address (optional)	E-mail (optional)
Tim & Diane Rizelby	5407 Locking Horse Ct Ocean Shores, WA 98561 5497 + 5430 Albright St. Oak Harbor, WA 98277	timlynnishelan@gmail.com mouseajun@cox.net
Alison Machen		

## **Guajome Lake Homes**

Community Outreach  
Summary Report

### Appendix C

Community Outreach Meeting Notice - April 2023

Community Outreach Update Letter – November 2024



March 20, 2023

Re: Tentative Tract Map T22-00004 – Guajome Lake Road Development

Dear Guajome Regional Park Neighbors,

On behalf of Rincon Homes, I am pleased to invite you to participate in our community meeting to discuss our proposed development. The meeting will be held at El Corazon Senior Center, 3302 Senior Center Dr., Oceanside, CA 92056 on April 3, 2023, from 6:30 pm to 8:00 pm. We plan to share our latest Development Plans (including Sewer Infrastructure), Technical Reports and hold a Q&A session.

Food and drinks to be provided, please RSVP no later than April 1, 2023, by emailing [guajomelakeroad@gmail.com](mailto:guajomelakeroad@gmail.com)

We look forward to having you join with other community partners to take a close look at our progress and share your feedback.

With warm regards,

*William Inghram*

William R. Inghram  
President  
Rockwell Land Company



RINCON | HOMES

November 4, 2024

TO: Neighbors and Interested Stakeholders

FROM: Rincon Homes

*Re: Guajome Lake Homes Project Update (T22-00004)*

Dear Guajome Lake Neighbors:

Since summer of 2022, Rincon Homes has been working on a proposal for the development of 83 single family homes on that 16.8 acre property located at 2389 Guajome Lake Road. As part of our design process, two neighborhood meetings were held in September of 2022 and April of 2023 to solicit feedback from the community and respond to questions and concerns related to the proposed project. Four individual meetings with area neighbors have also been held to discuss issues specific to neighboring properties.

Since completing this outreach in 2022 and 2023, our team has worked closely with the City of Oceanside to address comments received related to traffic, utilities, landscape, lighting, stormwater, availability of affordable housing, and other important environmental issues as well as ensure compliance with all applicable City of Oceanside regulations for new development. The property is designated for single family residential development by the City of Oceanside general plan and zoning ordinance.

In response to feedback from the community and City of Oceanside, the project includes the following:

- Safety improvements to Guajome Lake Road including curb, gutter, and paving from Albright Street to the eastern terminus of the project. A sidewalk will be installed along the project frontage.

- Enhanced landscaping along the project frontage
- Lighting plan that avoids light intrusion and minimizes impacts on adjacent homeowners
- Sensitivity to Guajome Lake park
- Avoiding sensitive habitat onsite
- Sewer improvements to address existing sewer deficiencies downstream of the project
- Onsite affordable housing for very-low income families consistent with the City of Oceanside municipal code and state law

You can view the latest project plans by visiting the City's website below and searching for the project using the project number T22-00004.

<https://crw.cityfoceanside.com/etrakit3/Search/project.aspx>

### **What's next?**

The City of Oceanside will be publishing an environmental impact report ("EIR") that comprehensively analyzes the impacts of the proposed project. Members of the public will receive a notice from the City when this document is available for review. Any member of the public can comment on the EIR and all comments will receive a response in the official record. At this time, we anticipate that the project EIR will be available within the next 90 days.

After all comments are received and responses are prepared, a decision on the project will be made by the City of Oceanside Planning Commission at a duly noticed public hearing. You will also receive notice of this meeting.

In the meantime, please do not hesitate to reach out to our team with additional comments, questions, or concerns. I can be reached at [jfrankel@rincongrp.com](mailto:jfrankel@rincongrp.com) or 925-708-3638.

Sincerely,

Jonathan Frankel  
Rincon Homes

## Robert Dmohowski

---

**From:** Bill Frisbie <wrfrisbie@gmail.com>  
**Sent:** Thursday, September 1, 2022 12:19 PM  
**To:** Robert Dmohowski; Planning Web; wingham@therico.com  
**Subject:** Project numbers T22-00004, D22-00009, DB22-00005 Guajome Lake Road  
  
**Categories:** Red Category

### Warning: External Source

Dear Mr. Dmohowski and Staff,

I have received a notice of application for the above cited projects which are being proposed near my residence at 3112 Guajome Lake Road, Oceanside.

I tried to find the project application using the link shown on the application:

<http://www.ci.oceanside.ca.us/gov/dev/planning/search.asp> using project number T22-00004, but the search returns no results.

I have several issues with this project as it is described on the application. First, the sheer number of houses proposed for such a small property seems very excessive. This is an area of larger properties with a rural feel to it. The density seems way out of proportion to the character of this area. We're talking 50'x50' lots here.

Second, adding 84 new houses will vastly increase the traffic on Guajome Lake Road. That section of the road is currently unpaved, and the extra cars traversing it will add hugely to the already serious dust problem unless it is paved. The road is also twisty and dangerous already and the increase of traffic will lead to more accidents. Already, cars frequently run off the road into Guajome Lake Park and have to be hauled out. It is already quite difficult to exit onto the main part of Guajome Lake Road from my house, and the increased traffic will greatly worsen this problem.

Third, there is no sewer connection on that section of Guajome Lake Road. If septic systems are added, it will lead to pollution of the water in Guajome Lake.

Fourth, where is the water going to come from for all these new houses? Did I miss something, or are we suffering from severe drought?

Please inform me in plenty of time when you are planning to hold a hearing on this application, as I would very much like to attend. I can be notified by this email address, or by postal mail.

Thank you for your attention,

William R. Frisbie  
3112 Guajome Lake Road  
Oceanside, CA  
[wrfrisbie@gmail.com](mailto:wrfrisbie@gmail.com)  
(631) 804-0146

## Robert Dmohowski

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**From:** Charles Plummer <charleswplummer@gmail.com>  
**Sent:** Tuesday, July 26, 2022 2:58 PM  
**To:** Robert Dmohowski  
**Subject:** No on project # T22-00004

**Categories:** Red Category

### Warning: External Source

Dear Rob Dmohoswki,

I am an Oceanside resident (5403 Stirrup Way, Oceanside, Ca 92057) living across the street from Guajome Regional Park. I'm writing to express my concerns regarding the Guajome Lake Road Track Home Development Proposal (**Project Number T22-00004**). If this development of 84 track homes is approved traffic will increase along an already congested road. Guajome Lake Road currently turns into a dirt road and if this development is approved the road will be paved creating a major thoroughfare off the 76 highway. This will exacerbate the already dangerous situation of traffic speeding down Guajome Lake Road where visitors park along the street and unload kids and pets. Just last year a teenager riding a bike was hit by a car on this road.

The Guajome Regional Park area is rural and quiet. If this development (**Project Number T22-00004**) is approved there will be an increase in traffic, safety issues and noise. Please vote NO on this development!

Sincerely,  
Charles Plummer  
[charleswplummer@gmail.com](mailto:charleswplummer@gmail.com)

May 25, 2022

City of Oceanside Mayor Esther Sanchez

City Council Members

Dear Madame Mayor and City Council Members,

I am writing you this letter as a homeowner who shares an adjacent property line on the proposed land development for 2389 Guajome Lake Road, Oceanside 92057 for 80 units. I own two homes on Albright Street adjacent to the 16.6 proposed acres to be developed.

My current understanding is that this project is falling under the Density Bonus Law – allowing the developer an incentive-based method to increase maximum density, which I am not in support of.

I hope that public hearings and outreach were not intentionally avoided, because to date I have not received any notice or communication from either the city or developer of this proposed project. I want to be a good neighbor, but I am concerned about how this geographic area was zoned to accommodate such a large development. I know profit margins are the top consideration, and as a mother of two young adults I also understand the need for housing, but as a third-generation landowner – I hope this is still recognized and developed as originally intended as a rural area.

A few quick concerns:

1. What changes were made to the initial land use zoning to accommodate the developer to utilize the Density Bonus Law.
2. Will the Low-Income housing be built to look the same as the market rate homes?
3. What will be my line of site and are there any plans for a barrier for sight and sound?
4. Will they have to do an EIR? If not, why not?
5. What is planned around the creek area?

Since I am unable to see who to contact directly to set up a meeting, please send this letter to appropriate contact person who may reach me at the number listed below.

Cordially,

Diane M. Rielly

5497 Albright Street

Oceanside, CA 92057

760-533-0024

Dear Mr. Dmohowski,

Thank you for responding to my questions regarding the proposed project at Guajome Lake Road.

I am concerned that the developer has not reached out to me as a shared property line owner. The information that you provided me regarding the proposed project has now gone from 80 units to 90 units is very concerning. If my math is correct with the information you provided – 84 single family homes with 9 low income- we are now up to 93 homes! Originally this project started out many years ago as ½ acre lots to keep with the equestrian dynamics of this beautiful rural area. As property owners we were told any project that went in, would not take away the rural area and maintain equestrian availability.

One of my main goals is to ensure privacy, have dialogue with both the city and the developer to address my concerns and see how we can come to a livable agreement for both parties. I am aware with the Density Bonus to developers they may have control, but I am hopeful they are also the type who want to make the project the best it can be for everyone, rather than a project forced upon us. I am also appreciative that you as a Project Manager for the Planning Department want to have your city residents' best interest as well.

The above is why I do feel an initial meeting would be a great opportunity for you to understand my concerns from the very start of this process. I would even be happy to meet you at the actual site location so you can personally get a feel for the layout of the land and current dynamics that I do not want to lose with this proposed development project.

I do want to be involved at every chance I can to have a say to the development of the surrounding land to my families' forever home. Thank you for the opportunity to express my concerns – I look forward to meeting with you, and any City Council Members who may also want to be a part of this meeting.

I can be reached via email, or at the number listed below.

Diane Rielly - 760-533-0024

## Robert Dmohowski

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**From:** Francois L <francois.labat17@gmail.com>  
**Sent:** Sunday, August 28, 2022 7:51 PM  
**To:** Robert Dmohowski  
**Subject:** Fwd: Guajome Campground Housing

**Categories:** Red Category

### Warning: External Source

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Hi Rob

I see the proposed development near Guajome Campground. I don't mind new builds but am so concerned with traffic on the 76. It's almost at grid lock in that area during AM PM.

What do we have planned about this?

There are so many new developments , where can we reference what will be done to the 76 to accommodate ?

Thanks  
Francois

## Robert Dmohowski

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**From:** Dane Thompson  
**Sent:** Tuesday, July 26, 2022 7:39 AM  
**To:** Robert Dmohowski  
**Subject:** FW: Vote NO on Project Number T22-00004

**Categories:** Red Category

fyi

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**From:** Jacqueline Wright <jwright365@gmail.com>  
**Sent:** Monday, July 25, 2022 4:15 PM  
**To:** Planning-Planning Commission <PlanningCommission@oceansideca.org>  
**Subject:** Vote NO on Project Number T22-00004

### **Warning: External Source**

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Dear Planning Commission,

I am an Oceanside resident (5403 Stirrup Way, Oceanside, Ca 92057) living across the street from Guajome Regional Park. I'm writing to express my concerns regarding the Guajome Lake Road Track Home Development Proposal (**Project Number T22-00004**). If this development of 84 track homes is approved traffic will increase along an already congested road. Guajome Lake Road currently turns into a dirt road and if this development is approved the road will be paved creating a major thoroughfare off the 76 highway. This will exacerbate the already dangerous situation of traffic speeding down Guajome Lake Road where visitors park along the street and unload kids and pets. Just last year a teenager riding a bike was hit by a car on this road.

The Guajome Regional Park area is rural and quiet. If this development (**Project Number T22-00004**) is approved there will be an increase in traffic, safety issues and noise. Please vote NO on this development!

Sincerely,  
Jacqueline Wright  
[jwright365@gmail.com](mailto:jwright365@gmail.com)

## Robert Dmohowski

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**From:** Jacqueline Wright <jwright365@gmail.com>  
**Sent:** Monday, July 25, 2022 4:21 PM  
**To:** Robert Dmohowski  
**Subject:** Project Number T22-00004

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

### Warning: External Source

Dear Rob Dmohoswski,

I am an Oceanside resident (5403 Stirrup Way, Oceanside, Ca 92057) living across the street from Guajome Regional Park. I'm writing to express my concerns regarding the Guajome Lake Road Track Home Development Proposal (**Project Number T22-00004**). If this development of 84 track homes is approved traffic will increase along an already congested road. Guajome Lake Road currently turns into a dirt road and if this development is approved the road will be paved creating a major thoroughfare off the 76 highway. This will exacerbate the already dangerous situation of traffic speeding down Guajome Lake Road where visitors park along the street and unload kids and pets. Just last year a teenager riding a bike was hit by a car on this road.

The Guajome Regional Park area is rural and quiet. If this development (**Project Number T22-00004**) is approved there will be an increase in traffic, safety issues and noise. I strongly advocate against this development!

Sincerely,  
Jacqueline Wright  
[jwright365@gmail.com](mailto:jwright365@gmail.com)

## **Robert Dmohowski**

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**From:** Lee Kelly-Gillen <lkg46@outlook.com>  
**Sent:** Tuesday, August 30, 2022 2:50 PM  
**To:** Robert Dmohowski  
**Subject:** Guajome Lake Project

**Categories:** Red Category

Warning: External Source

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How do I get more information about this project and dates the project comes before Planning Commission? T22-00004, D2200009, DB22-00005 Thanks Lee Kelly-Gillen lkg@cox.net  
760-613-3015

Sent from my iPad

## Robert Dmohowski

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**Subject:** Guajome Lake Tract Home Development

**From:** Mark Potter <[mstpotter@yahoo.com](mailto:mstpotter@yahoo.com)>

**Sent:** Tuesday, July 19, 2022 7:33 PM

**To:** Christopher Rodriguez <[CRodriguez@oceansideca.org](mailto:CRodriguez@oceansideca.org)>; [council@oceanside.org](mailto:council@oceanside.org); Planning-Planning Commission <[PlanningCommission@oceansideca.org](mailto:PlanningCommission@oceansideca.org)>

**Subject:** Guajome Lake Tract Home Development

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**Warning: External Source**

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Hello,

I'm writing to express my concerns about the proposed new 84 lot tract home development on Guajome Lake Rd that the council is currently considering for approval. I live very near this proposed development and am concerned about the significant additional level of traffic that this large amount of homes will put on the Guajome Lake Rd out to the 76 Fwy.

I've lived in the area for over ten years and up until about a few months ago, when I would go out to the 76 Fwy and line up at the stop light, if I was more than 5 cars behind I would miss the light because it was so short. Then I had to wait another 3-4 minutes for the light to change again. The green light time has since been increased, but I'm concerned that with 84 more families using the road out to the 76 Fwy that it will cause incredibly longer backups way back to the park entrance which could potentially be catastrophic if we have an emergency like a fire and everyone tries to leave quickly.

The problem is exacerbated by the fact that Guajome Lake Rd in the direction opposite to the 76 Fwy is a dirt road in poor condition with horrible ruts that make you think your car is being damaged every time you ride on it (and over time it probably is) and which if not paved all the way out this way to where the pavement resumes it incentivizes traffic to go to the 76 Fwy and overload traffic in that direction.

In conclusion, I believe the addition of 84 homes to this location will result in too much traffic on Guajome Lake Rd in the direction to the 76 Fwy and potentially create dangerous conditions, particularly if the road is not paved all the way in the other direction resulting in cars intentionally avoiding that direction and overloading in the direction of the 76 Fwy. So I request that you don't approve this project at this location.

Sincerely,

Mark Potter



## Robert Dmohowski

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**From:** Rachel Britts <rachelbritts@gmail.com>  
**Sent:** Friday, March 31, 2023 11:53 AM  
**To:** Robert Dmohowski  
**Subject:** Concerns about Guajome Lake dirt road and the new development

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**Categories:** Red Category

Warning: External Source

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Good morning. I don't know if you are still the person to contact for this. We are planning to come to the meeting on Monday, but that seems to be hosted by the developers not the city.

I would like to invite you to drive the dirt portion of Guajome Lake Road ASAP, so that you can see for yourself what horrendous shape it is in. It is by far currently the worse road I have ever driven, far worse than the remote villages in Kenya and India and other 3rd world areas we have done work in. It is currently so bad that we are driving out of the way down North Santa Fe and around to Osborne just to get to our home at 2827 Guajome Lake Road, so that we can be on the miserable dirt portion for less time.

This is a SERIOUS accident waiting to happen. Tons of cars cut through Guajome Lake Road all the time, and currently they weave in and out of the pot holes trying to find the path that is least jarring. The road in front of the 16 acres to be developed is relatively wide and straight, and doesn't pose as much of a problem (beyond the miniature Guajome Lake that keeps forming with the rains). But what is more serious is the stretch of road to the south, heading toward the Vista/County line, because it becomes curvy and narrow, and I can't believe there hasn't been a head-on collision there yet, especially in the current state of the road.

The development is going to add so much traffic to this cut through that is definitely not secret any more, and the developer MUST pay to widen and pave the ENTIRE stretch of Guajome Lake Road. If it doesn't, we will pounce on the first tragedy to hold the city of Oceanside legally responsible, because you have been amply warned.

Again, please travel down the road TODAY to see how we residents on the road take our lives and the health of our vehicles into our hands EVERY SINGLE DAY. We pay our property taxes the same as everyone else but we face conditions that are usually only seen in impoverished countries or super off-grid locations. This is ridiculous and MUST be fixed.

Thank you.

Rachel Britts  
760-583-8438

## Robert Dmohowski

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**From:** Rachel Britts <rachelbritts@gmail.com>  
**Sent:** Wednesday, April 12, 2023 7:02 AM  
**To:** Robert Dmohowski  
**Subject:** Guajome Lake Road development ?

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**Categories:** Red Category

Warning: External Source

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Good morning.

I have been mulling over last week's meeting, really frustrated by our lack of plausible options with the road. I still can't understand how the city of Oceanside approved this development without making the paving of the entire road a stipulation for development. There is no plan in place to fix what already is a huge problem—except to get a developer to do it. When we moved in, that is what we had been told by neighbors, that it won't get paved until someone develops those 16 acres. And yet now the 16 acres are going to get OVER developed (exceeding all the neighbors' worst nightmares of what was possible!)...and the road will remain untouched at its most dangerous point.

It is so absurd that I just can't wrap my mind around it.

I paid very close attention to the North River Farms debate a few years ago, and the only good thing about that development was that they were going to pay to widen College Blvd/the bridge/etc. A no-brainer given the impact on that area.

So HOW can this developer get away with not developing the entire affected road??? Oceanside has been so great at developing with a plan and foresight across our beautiful city! HOW did you guys let this one slip??

In all my pondering and mulling this past week, I could only think of 2 plausible reasons.

Either,

1. Oceanside Planning department had a sudden lapse of good judgement and made a VERY grave mistake. (In which case, is it too late to go back and deny these plans? Or force them to fix the road???) OR 2. Oceanside planning department has been bullied into this travesty by the smooth-talking guy who knew how to manipulate the rules and state mandates to ram an obscene amount of houses into this horse-overlay/park-overlay master plan property without conscience?

Is that true? Are your hands tied on this? Is there anything we can do to push back? (I'm already hoping some artifacts or ancient bones will show up if they ever start digging!)

You were very silent at the meeting on Monday night, and I'm trying to give you the benefit of the doubt, that maybe you hate this project as much as we do? Please help me to understand.

Guajome Lake Road is already tragedy waiting to happen, and I dread the day my only response will be "I told you so."

Please don't let that happen on your watch.

Please help me to understand why Oceanside is not being wiser in this situation.

—Rachel Britts  
750-583-8438

Sent from my iPhone

## Robert Dmohowski

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**From:** Rachel Britts <rachelbritts@gmail.com>  
**Sent:** Monday, May 1, 2023 8:43 AM  
**To:** Robert Dmohowski  
**Subject:** Near head on collision on Guajome lake road

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

**Categories:** Red Category

Warning: External Source

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I know you likely find all my emails exhausting, but I am hoping they will be placed in a file for the city to review regarding the proposed development.

Once again, I was almost hit head on on Guajome Lake Road this past week, rounding a blind curve outside of my driveway. I have submitted 3 unanswered requests for road maintenance this past month due to the debilitating bumps in the dirt road. The City has neglected to respond. My car has developed expensive alignment issues, so I drive very slowly on the bumps. Passing traffic does not appreciate this caution, and so a couple days ago, a car passed me just as another car tried to pass an oncoming slow vehicle near one of the narrow curves. Somehow an accident was averted, fortunately, but once again I was reminded of how stressful it is to drive to and from my home.

This road is a disaster waiting to happen.

—Rachel Britts

Sent from my iPhone

## Robert Dmohowski

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**From:** Randall Morrow <randalljmorrow@gmail.com>  
**Sent:** Saturday, July 23, 2022 10:54 AM  
**To:** City Council; Planning-Planning Commission; Robert Dmohowski; Christopher Rodriguez  
**Subject:** Concerns regarding Project Number T22-00004

**Follow Up Flag:** Follow up  
**Flag Status:** Completed

### Warning: External Source

Hello,

Thank you for the opportunity to share my concerns with you regarding the proposed 84-lot plan, Project Number T22-00004.

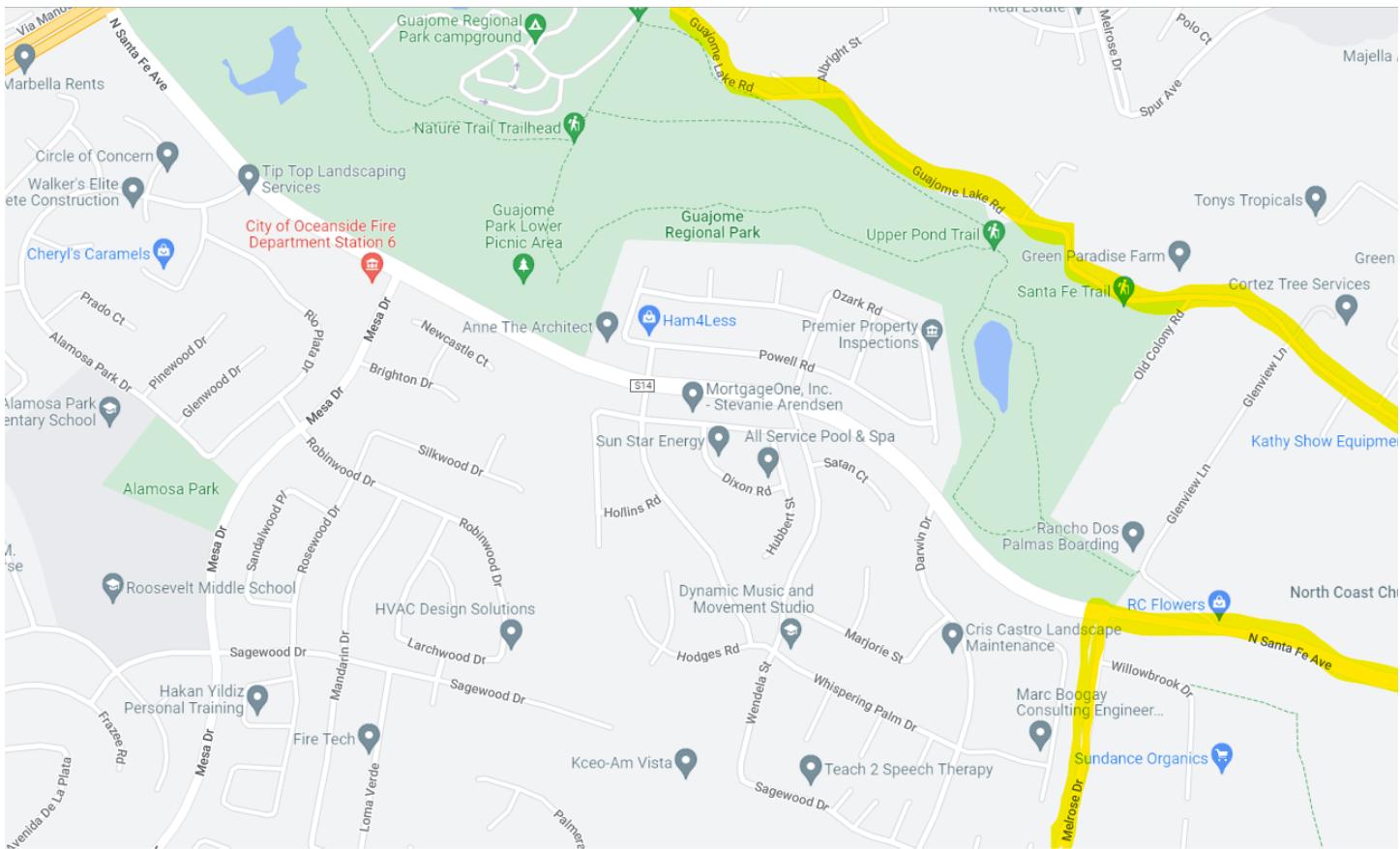
I live in a home that is part of the Rancho San Geronimo Homeowners Association, off of Old Ranch Road, which connects to Guajome Lake Road. We picked this community specifically for the quietness of the area and its proximity to Guajome Lake Park. We love this area and can appreciate why other people would like to live nearby.

We've raised three sons, and several dogs, in our home over the last 20 years, and continue to spend time in the park on a daily basis. To access Guajome Lake Park, we have to cross over Guajome Lake Road and so are very familiar with how busy this area gets with park visitors, and I am extremely concerned with our safety if the proposed building plan goes through with more serious attention to the road improvements that are included in the plan.

As you know, Guajome Lake Road becomes an unimproved dirt road right about where the new homes will be built. I believe the plan is to improve this section of the road, including the paving and realigning of the road along the project frontage. I've read the Draft Local Transportation Assessment, prepared by Justin Rasas with:LOS Engineering, Inc., and was appalled to learn that in his opinion, Mr. Rasas thinks that "The project will not result in traffic impacts as defined in the Traffic Guidelines; therefore, no off-site improvements are recommended."

My family and I stopped using Guajome Lake Road to go south due to the fact that people drive this unpaved section of the road in a very unsafe manner. We've narrowly avoided being hit by out of control vehicles so many times, we no longer drive that road going south from Old Ranch Road. However, we know that unsafe driving still occurs because we see vehicles on this road whenever we walk our dog, which is almost every day.

Many people who do not live in this area use Guajome Lake Road as a shortcut from Melrose Dr., North Santa Fe Ave. and Osborn St. to Hwy. 76:

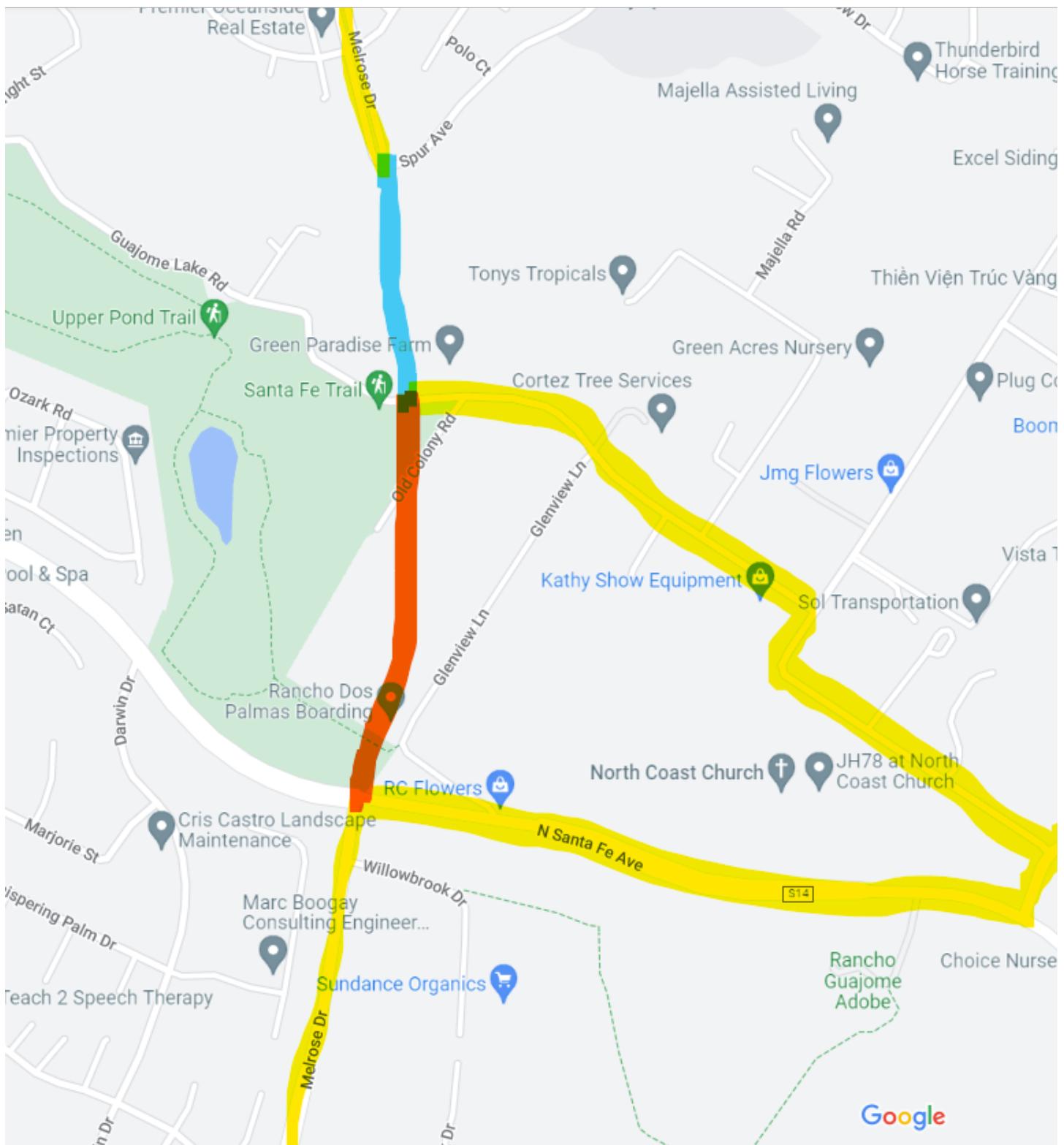


And these are the people that are usually in a hurry and drive extremely fast. As they approach the intersection of Guajome Lake Road and Old Ranch Road, they have to crest a hill and have very little time to react to children, pets and adults crossing the road to the park. In order to avoid paying the park's Day Use fee, a lot of people park along the side of the road in this area, which makes it even harder for people to see people until they are in the path of the vehicles.

I've seen people fly through this area as they use the shortcut, driving at an estimated 50 mph or more. In my opinion, there is already a safety hazard in this area, and I fear that if this building project goes through, with the improvements made to Guajome Lake Road, there will be a huge increase in traffic, with a related increased threat of there being a serious accident at this intersection. Improving the roadway itself might decrease some of the recklessness from driving on a dirt road, but it will cause an increase in the number of people using the shortcut once it is no longer a dirt road.

I can't believe Mr. Rasas would come to the conclusion that "The project will not result in traffic impacts" unless the Traffic Guidelines he refers to are worthless. Anyone with eyes, who takes the time to see how drivers use this road on a consistent basis, would see the issues I've pointed out. Therefore, I strongly object to Project Number T22-00004 as it is currently written. The only way I could agree with a plan to build those homes would include off-site improvements to reduce the increased safety hazard.

One way I think this could be established is if in addition to the paving and realigning of Guajome Lake Road, that a new piece of roadway be built to connect Guajome Lake Road to Melrose Dr. to the north, or even another section of new road that connects Melrose Dr. to the north to Guajome Lake Road AND Melrose Dr. to the south, as depicted in the image below:



It seems to me that the area that would be developed as part of proposed Project Number T22-00004 borders the area where an extension of Melrose Dr. could be built (blue line in the image above) that would connect to Guajome Lake Road. This could even provide a secondary exit to the development in case of fire or other disaster.

I know that building an extension of Melrose Dr. that would either cut through, or border the Eastern edge of Guajome Lake Park (red line in the image above) would be a very significant project. However, I believe that

there were discussions about just such a project not too many years ago, but it seems to have fizzled out. Maybe it's time to reconsider this Melrose Dr-to-Melrose Dr. project in order to divert the increased traffic destined to go on Guajome Lake Road if Project Number T22-00004 goes through, and hopefully, avoid any potential fatal accidents that could occur due to poor planning.

I would be more than happy to present this viewpoint at any public meeting. Please let me know if I can be of any help.

Thank you.

Randall Morrow  
m: 760-505-1752

## Robert Dmohowski

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**From:** Steven Moehling <sandiegan760@gmail.com>  
**Sent:** Wednesday, July 27, 2022 9:00 AM  
**To:** Robert Dmohowski  
**Subject:** Project number T22-00004, D22-00009, DB-00005

**Follow Up Flag:** Follow up  
**Flag Status:** Flagged

### Warning: External Source

Dear Mr. Dmohowski,

Please be aware that, in advance of any public hearing for the above referenced projects, residents in the adjacent areas will seek legal remedy to stop it.

#### Environmental Impact

84 homes on 10.13 acres (16.7 overall) is a project you would find in a heavily populated area, not in an ecologically sensitive and equestrian area such as Guajome. My initial search on this Oceanside project identified on your notice nets zero results. Because of this, I respectfully request a copy of the environmental Impact statement. The city should also be aware of this property being adjacent to a previous project being canceled due to environmental concerns.

#### Congestion And No Feasible Relief

The average new home square footage in San Diego County is 2,580. The LOT SIZES, not homes, LOT SIZES of each proposed home is only 2,496 exposing the fact that this project is designed to only maximize units built. In addition, the average home in San Diego County has 2.1 vehicles. This will add nearly 200 new vehicles to an area that is already overly congested with limited access. As you are aware, the Oceanside City Council voted 4-1 against the Melrose Drive expansion project. It is well known that this project was designed to alleviate congestion. This reason alone should eliminate any desire by the city to pursue such an irresponsible project that only adds to a problem the city already acknowledges.

The area east of North Santa Fe includes Guajome Regional Park that is only growing in popularity. The city of Oceanside has already limited parking at Guajome Park and the affect of this has spilled into adjacent neighborhoods.

This is the wrong project in the wrong place. The fact that it is even being considered based upon the facts above only embolden the residents of this area to protect its interests and property values.

Respectfully,

Steven Moehling

## Robert Dmohowski

---

**From:** W HAMBLY <whambly@cox.net>  
**Sent:** Monday, December 26, 2022 11:40 AM  
**To:** Planning-Planning Commission  
**Cc:** City Council; Robert Dmohowski; Walt Hambly  
**Subject:** Project Number T22-00004 GUAJOME CREST PROJECT (APN 157-412-15-00)

**Categories:** Red Category

### Warning: External Source

Dear members of the Planning Commission,

The Board Members for San Geronimo HOA are asking that you please note their concerns regarding the following project:

#### GUAJOME CREST PROJECT (APN 157-412-15-00)

This high density 84 lot track home proposal on 2839 Guajome Lake Road which is still under review.

#### Transportation concerns:

- The development will greatly increase traffic and impact congestion during peak commute and school times. Guajome Lake Road will need to be widened at the Highway 76 light in order to prevent a bottleneck. Widening the road at the light will allow for proper traffic flow enabling cars to turn left while also enabling through traffic and cars turning right. Currently, the left turn lane blocks all moving traffic when more than 3 cars are turning left due to how narrow the current single-lane roadway is.
- A large section of Guajome Lake Road needs to be paved and widened to safely allow a two-way road and enable proper traffic flow. It is currently too narrow to comfortably allow cars to flow both ways.
- Guajome Regional Park will need to give up part of the park's land and property in order to accommodate the expansion of Guajome Lake Road.

#### Safety concerns:

- Increased traffic and congestion would greatly impact the fire evacuation route for neighboring residents.
- Increased traffic means increased pedestrian safety concerns. Over the years, there have been quite a few speeding cars that have driven right over the edge of the road and into Guajome Regional Park due to the windy curves of the road. The windy section at the top of the hill that parallels the walking path located directly below the street is a major concern for those walking the trails. Guard rails, speed bumps, or dips should be used to slow down speeding traffic.
- Once the road is paved, there will be more speed concerns. Cars already fly down the road at 50 mph which is dangerous for those who park along the dirt and along the sidewalk on Guajome Lake Road.
- Increased speed and traffic is a concern for the Vista farmers who regularly let their livestock (goats, horses, etc) cross the road on the south side of Guajome Lake Road near Osborne Street.

#### Environmental concerns:

- Guajome Regional Park is a protected, sensitive habitat and the housing development would negatively impact the protected species due to water runoff, increased noise, increased traffic, and road expansion.
- The increased traffic will result in an increase in noise levels to the surrounding neighborhoods.

Thank you for your time, Rancho San Geronimo HOA Board

## Robert Dmohowski

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**From:** Jonathan Borrego  
**Sent:** Thursday, December 15, 2022 8:16 PM  
**To:** Robert Dmohowski  
**Subject:** Fwd: Guajome Lake Project

Sent from my iPhone

Begin forwarded message:

**From:** W HAMBLY <whambly@cox.net>  
**Date:** December 15, 2022 at 8:08:08 PM PST  
**To:** City Council <Council@oceansideca.org>  
**Subject: Guajome Lake Project**  
**Reply-To:** W HAMBLY <whambly@cox.net>

**Warning: External Source**

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Dear Council Members,

This is letter regards the Guajome Lake project on Guajome Lake Road. The following are my concerns and those of our community, the Rancho San Geronimo HOA.

### Transportation concerns:

- The development will greatly increase traffic and impact congestion during peak commute and school times. Guajome Lake Road will need to be widened at the Highway 76 light in order to prevent a bottleneck. Widening the road at the light will allow for proper traffic flow enabling cars to turn left while also enabling through traffic and cars turning right. Currently, the left turn lane blocks all moving traffic when more than 3 cars are turning left due to how narrow the current single-lane roadway is.
- A large section of Guajome Lake Road needs to be paved and widened to safely allow a two-way road and enable proper traffic flow. It is currently too narrow to comfortably allow cars to flow both ways.
- Guajome Regional Park will need to give up part of the park's land and property in order to accommodate the expansion of Guajome Lake Road.

### Safety concerns:

- Increased traffic and congestion would greatly impact the fire evacuation route for neighboring residents.
- Increased traffic means increased pedestrian safety concerns. Over the years, there have been quite a few speeding cars that have driven right over the edge of the road and into Guajome Regional Park due to the windy curves of the road. The windy section at the top

of the hill that parallels the walking path located directly below the street is a major concern for those walking the trails. Guard rails, speed bumps, or dips should be used to slow down speeding traffic.

- Once the road is paved, there will be more speed concerns. Cars already fly down the road at 50 mph which is dangerous for those who park along the dirt and along the sidewalk on Guajome Lake Road.
- Increased speed and traffic is a concern for the Vista farmers who regularly let their livestock (goats, horses, etc) cross the road on the south side of Guajome Lake Road near Osborne Street.

**Environmental concerns:**

- Guajome Regional Park is a protected, sensitive habitat and the housing development would negatively impact the protected species due to water runoff, increased noise, increased traffic, and road expansion.
- The increased traffic will result in an increase in noise levels to the surrounding neighborhoods.

Thank you for your time and consideration when reading the letter.

Walt Hamblly

## Merisue S. Repik

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**From:** Dan Silver <dsilverla@me.com>  
**Sent:** Tuesday, August 5, 2025 9:22 AM  
**To:** Planning-Planning Commission  
**Subject:** Guajome Lake Road Subdivision, Planning Commission, Aug. 11, 2025

### Warning: External Source

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Dear Chair and Members of the Commission:

Endangered Habitats League (EHL) appreciates the opportunity to comments. For your reference, EHL is a regional conservation group that served on the Multiple Habitat Conservation Program Advisory Committee. The City of Oceanside was part of that effort but has not yet adopted its Subarea Plan.

EHL recognizes the important biological values on site and the need for housing, and respects the appropriate use of state density bonus law. *For these reasons, we urge adoption of the Townhome Alternative or the Reduced Footprint Alternative.* These alternatives would avoid impacts to sensitive coastal sage scrub, which is nesting habitat for the federally threatened California gnatcatcher. The coastal sage scrub is a part of a larger, biological viable stretch of open space, adjacent to riparian habitat, and should be protected on-site. The proposed project would not do so, impacting virtually all of the coastal sage scrub with vegetation removal. These impacts would result in costly and time-consuming federal permitting and off-site mitigation.

All lands set aside for biological purposes should be permanently protected via easement and managed.

We note that the Townhome Alternative satisfies all project and housing goals and is more affordable than detached single family. If California is to meet housing goals, cities must permit well-designed and aesthetically pleasing attached housing adjacent to single family areas. Many high quality and desirable neighborhoods embody this type of land use.

Please enact housing solutions and protect the environment at the same time.

Yours truly,  
Dan Silver

### NOTE NEW MAILING ADDRESS

Dan Silver, Executive Director  
Endangered Habitats League  
505 S Flower St #71001  
PO Box 71001  
Los Angeles CA 90071

213-804-2750  
dsilverla@me.com  
<https://ehleague.org>

## Merisue S. Repik

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**From:** Dr. Michael Tenhover <tenhover@protonmail.com>  
**Sent:** Wednesday, August 6, 2025 10:22 AM  
**To:** Merisue S. Repik  
**Subject:** Comments on Guajome Lake Project FEIR  
**Attachments:** Fire Safety Comments on FEIR Guajome Lake Project - Tenhover.pdf

### Warning: External Source

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Merisue Repik  
Program Specialist  
City of Oceanside

Hi Merisue,

Please find attached my Fire Safety Comments on the Guajome Lake Project FEIR.

This is a follow-up on my previous comments on the DEIR and addresses issues that were not adequately resolved in the FEIR.

Please forward these to the members of the Planning Commission and other interested parties. I plan to speak on these issues at the meeting on Monday August 11th.

Thanks,

Regards,  
Mike Tenhover

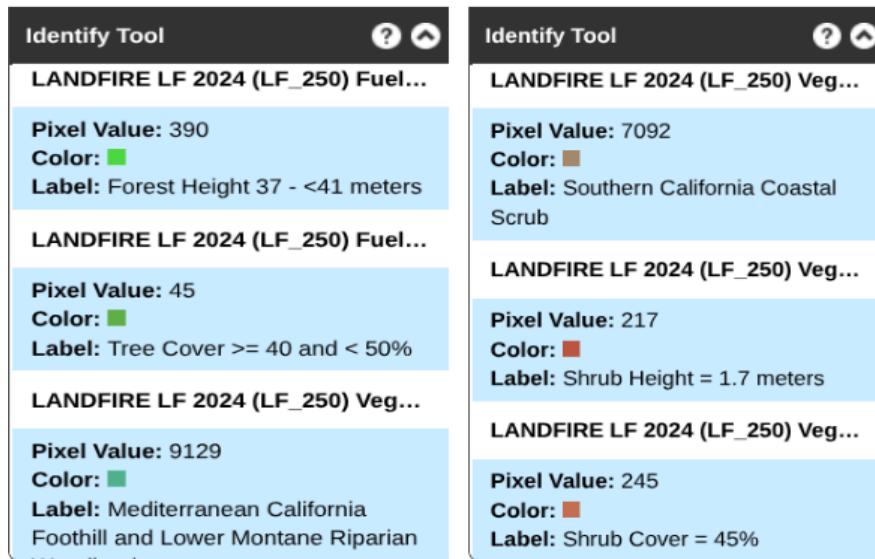
Sent with [Proton Mail](#) secure email.

# Fire Safety Concerns with the proposed Guajome Lake Homes Project.

## Specific Concerns with the FEIR:

1. Zone 0- needs to comply with CBC Chapter 7A
2. Zone 2- failure to achieve 100 foot fuel modification zone
3. Fire Modeling- failure to model the case of a Santa Ana Condition
4. Fire Modeling- failure to model the important role of embers
5. Mitigation/FMZ analysis- improperly referenced and not relevant. Based on 1990's understanding of how fires spread.
6. Potential for Restrictions of this Project– Fire Safety

**Wildland-Urban Interface.** From [LANDFIRE.GOV](https://landfire.gov). The project is adjacent to a 26.54 acre Wildland. Consisting of South Willow Scrub (up to 111 feet tall) , Southern California Coastal Sage Scrub (5 feet tall) , Riparian Woodland (up 121 feet tall) (2024 data).



There is no minimum acreage or size requirement for a wildland to be considered part of a WUI. The definition is based on the risk and potential for fire spread between vegetation and structures, which can occur with wildlands of any size. California has comprehensive Wildland-Urban Interface (WUI) building and defensible space standards designed to protect homes and communities from wildfires. These are outlined in state codes: Chapter 7A of the California Building Code (CBC), Chapter 49 of the California Fire Code and Section R337 of the California Residential Code. The dual fuel environment (wildland and structures) makes fire behavior exceptionally challenging to predict and manage, as fire spread mechanisms differ significantly between natural vegetation and built structures.

**CAL FIRE Zones: Zone 0, Zone 1, and Zone 2.** This is a proven system that leads to successful Fire Safe communities. It is unbelievable that a project in Southern California in 2025 in a WUI would be proposed without rigorously following this.

### **Specific Concerns:**

**1. Zone 0.** No combustible materials within 5 feet of a structure. The concern here is that the FEIR discusses at times Chapter 7 (OCN) and at other times Chapter 7A (CBC). It is important that guidance should come from Chapter 7A in a WUI. A specific omission in the FEIR appears to be a clear statement that the project will follow the restrictions below:

- **Decks, Balconies, and Exterior Stairs:** Exposed structural members should be non-combustible (e.g., concrete or metal) or fire-retardant treated wood.
- **Fences and Other Attachments:** Any portion of a fence or other structure within five feet of a building must be constructed of non-combustible material, pressure-treated exterior fire-retardant wood, or meet the same fire-resistive standards as the exterior walls.

**2. Zone 2 failure to achieve fuel modification zone.** Many of the residential units are proposed to have less than the required fuel modification zone. The project describes the nearby areas as being maintained by some other entity, or that “improved vegetation” will be utilized in the less than 100 feet Zone 2 parts of the project. **This is not a stable or safe situation** and is an unacceptable risk for the community.

### **3. Fire Modeling- failure to model the case of a Santa Ana Condition**

The FEIR mentions that the most serious Fire threats in San Diego County are the Santa Ana winds. It also states (page 9 Appendix O): “the Santa Ana winds of Southern California. Sustained wind speeds recorded during recent major fires in San Diego County **exceeded 30 mph and may exceed 50 mph** during extreme conditions<sup>2</sup>. The Santa Ana wind conditions are a reversal of the prevailing southwesterly winds that usually occur on a region-wide basis during late summer and early fall”

However, the four fire behavior models they present do not cover Santa Ana conditions. In all four cases the inputs to their BehavePlus program used **wind speeds of 16 mph**. Needless to say, the sustained high winds of Santa Ana conditions (30 to 50 mph) will have major impacts on the fire behavior results.

### **4. Fire Modeling- failure to model the important role of embers.**

The FEIR uses the Behaveplus program for describing fire behavior. It is a poor choice when it comes to understanding the effect of embers (firebrands). BehavePlus does not provide detailed predictions on the number, mass, shape, heat content or burning duration of embers. It gives a “maxspot” distance for embers. No accounting of the types of ember sources (Live Oak versus Willow, versus...) have been examined. In the last 15 years the crucial role of embers in fire spread has become apparent. It's **estimated that 90% of homes are destroyed indirectly by wind-borne embers** that are carried ahead of the fire perimeter. When the heat generated by an intense wildfire is combined with wind, small burning embers can travel several miles away from the fire perimeter. (ref 1-3)

Programs such as **Flammap** or **FARSITE** would be better choices (including the Wind Ninja module) for this project. Even these are somewhat limited when it comes to extreme wind conditions such as Santa Ana. Other choices (which I have not worked with) would be the procedure SWUIFT (4) or the programs WildEST (Wildfire Exposure Simulation Tool) and FIRETEC (5) . FIRETEC seems to do the best job when it comes to including the essential physics and assessing the impact on the nearby residential units. Without this understanding, it is difficult to judge the effectiveness of the other fire mitigation proposed efforts. A detailed model to understand embers for this project is shown in Appendix I.

*Also note: the ember simulations need to be done with Santa Ana Wind conditions (30-50 mph) not the 16 mph used in the FEIR.*

## **5. Mitigation/FMZ analysis- improperly referenced and not relevant. Based on 1990's understanding of how Fires spread.**

The FEIR attempts to make the case that the various mitigation efforts will compensate with the failure to achieve the CAL FIRE Zones (page 39 Appendix O). Here is an excerpt from the FEIR:

*As indicated in this report, the FMZs and additional fire protection measures proposed for the Project provide equivalent wildfire buffer but are not standard zones. Rather, they are based on a variety of analysis criteria including predicted flame length, fire intensity (Btu), site topography and vegetation, extreme and typical weather, the position of structures on pads, position of roadways, adjacent fuels, fire history, current vs. proposed land use, neighboring communities relative to the proposed Project, and type of construction. The fire intensity research conducted by Cohen (1995), Cohen and Butler (1996), and Cohen and Saveland (1997), and Tran et al. (1992) supports the fuel modification alternatives proposed for this Project.*

I know Cohen's work and the publications date back to the 1990's. Emphasis is on preventing fire spread due to flame encroachment and radiant heat effects, **not Santa Ana condition fires**. We know these are fundamentally different from the traditional fires Cohen's work covered. Crowning/embers/firebrands understanding in the 1990's was difficult to model/simulate and data was lacking. This has changed in recent times. Better references to gauge the effectiveness of various mitigation measures taking into detailed account of ember production and transport can be found in reference (6). These should be the basis of deciding whether the proposed mitigation steps will be effective.

FEIR References: this excerpt from the FEIR mentions two references - Cohen and Saveland and Tran et al that **are not found** in the Reference List

in the FEIR. I can only assume that these were not considered when the FEIR was submitted (?)

## **6. Potential for Restrictions of this Project– Fire Safety**

A local agency can argue that a high-density bonus project in a WUI area would create a “specific adverse impact upon public safety” due to increased wildfire risk. In this case with the failure to achieve defensible space standards (Zone 2) and incomplete accounting of risk factors (Santa Ana Winds, embers, outdated references), a modification or a request for additional information of the project can be considered.

## **References**

1. *Framework for Addressing the National Wildland Urban Interface Fire Problem—Determining Fire and Ember Exposure Zones Using a WUI Hazard Scale* (NISTTN 1748)
2. *Ignitability of Structural Wood Products Exposed to Embers During Wildland Fires: A Review of Literature* (NISTTN 2153)
3. *Enabling Exposure Comparison for Firebrand Showers Generated During Wildland-Urban Interface (WUI) Fires*
4. *Streamlined wildland-urban interface fire tracing (SWUIFT): : Modeling wildfire spread in communities* Nima Masoudvaziri *Environmental Modelling & Software* Volume 143, Issue C Sept 2021
5. Furman, James H.; Linn, Rodman R. 2018. *What is FIRETEC (and why should I care)?* *Fire Management Today* 76(3):33-36.
6. *WUI Structure/Parcel/Community Fire Hazard Mitigation Methodology* NIST Technical Note 2205.

Michael Tenhover (Tenhover@protonmail.com)  
873 Wala Drive  
OCN, CA 92058

## APPENDIX I. Ember Production, Transport, Ignition for a WUI:

- Ember Production Module: This component would focus on determining the initial characteristics of embers generated during a wildfire.
  - Input: Fuel types (wildland and structural), heating rate, radiant heat flux, moisture content.
  - Processing: Simulate thermal decomposition and combustion properties to estimate ember production rate, mass, shape, and dimensions.
- Ember Transport Module: This module would simulate the trajectory and travel distance of embers based on environmental conditions and ember properties.
  - Input: Ember mass, shape, dimensions, wind speed, wind direction, lofting height (influenced by canopy cover and Crown-Fire-Base), burn time, and terrain.
  - Processing:
    - Calculate initial ember lofting height and velocity based on fire intensity and plume dynamics.
    - Model ember trajectories considering drag forces, gravity, and wind conditions.
    - Determine ember landing locations and travel distances, noting that ember density tends to peak near the source and decrease with distance.
    - Incorporate factors like ember burning duration and intensity based on ember properties and environmental conditions.
    - Account for intermittency in ember generation and strong turbulent updrafts, which can lead to longer-distance transport of larger embers.
    - Consider the impact of fire-induced turbulence, particularly

over complex terrain, on ember transport.

- Ember Ignition Module: This component would assess the probability of ignition when embers land in a fuel bed.
  - Input: Ember characteristics (mass, temperature, burning state), fuel bed properties (type, moisture content, arrangement), and environmental conditions (radiant heat flux, wind).
  - Processing: Determine the likelihood of a new fire starting based on the energy transferred by the ember and the ignitability of the fuel.