

RESOLUTION NO.

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE UPHOLDING PLANNING COMMISSION RESOLUTION NO. 2025-P05 CERTIFYING A FINAL ENVIRONMENTAL IMPACT REPORT AND ASSOCIATED MITIGATION MONITORING AND REPORTING PROGRAM AND UPHOLDING PLANNING COMMISSION RESOLUTION NO. 2025-P04 APPROVING A DEVELOPMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001) AND VARIANCE (V22-00001) TO ALLOW THE CONSTRUCTION OF A WAREHOUSE, MANUFACTURING, AND OFFICE FACILITY CONSISTING OF FOUR SEPARATE BUILDINGS RANGING IN SIZE FROM 109,660 SQUARE FEET TO 134,015 SQUARE FEET WITH A CUMULATIVE TOTAL OF 497,822 SQUARE FEET AND 34 TRUCK BAYS DISTRIBUTED AMONG THE FOUR BUILDINGS ON A 31.79-ACRE SITE LOCATED AT 250 EDDIE JONES WAY (APNs: 145-021-29, 30, & 32)

**(RPG Oceanside Eddy Jones Way Owner LLC – Applicant)
(Gretchen Gary on behalf of Oceanside Speaks Out -Appellant)**

WHEREAS, applicant RPG Oceanside Eddy Jones Way Owner LLC filed a verified petition on the forms prescribed by the Planning Commission for approval of a Development Plan, Conditional Use Permit, and Variance to allow the construction of a warehouse, manufacturing, and office facility consisting of four separate buildings with a cumulative total of 497,822 square feet and 56 truck bays known as the Multi-Building and Truck Bay Reduction Alternative (MBTRA) on a 31.79-acre site; and

WHEREAS, an Environmental Impact Report (EIR) was prepared and circulated for this project in accordance with the California Environmental Quality Act (CEQA) (SCH No: 2022070365) and described the potential impacts of the MBTRA; and

WHEREAS, the Planning Commission, after giving the required notice, did on the 10th day of February, 2025 conduct a duly advertised public hearing as prescribed by law to consider the application; and

WHEREAS, following consideration of all applicable testimony and evidence and deliberation, the Planning Commission, by a 7-0 vote, adopted Resolution No. 2025-P05 certifying the Final EIR (FEIR) and adopting the Findings of Fact and Mitigation Monitoring and Reporting Program (“MMRP”); and adopted Resolution No. 2025-P04 approving Development Plan (D22-

1 00001), Conditional Use Permit (CUP22-00001), and Variance (V22-00001) with an amendment
2 to Condition 1b and Condition 11 to reduce the maximum number of truck bays from 56 to 34 bays
3 with a caveat that truck bays can be allocated between any of the four buildings at the discretion of
4 the applicant; and

5 WHEREAS, on February 20, 2025, a timely appeal of the Planning Commission’s approval
6 of said project was filed with the City Clerk; and

7 WHEREAS, on May 2, 2025, the City Council of the City of Oceanside held a duly noticed
8 public hearing and heard and considered evidence and testimony by all interested parties concerning
9 the Planning Commission’s certification of the FEIR and approval of the Development Plan (D22-
10 00001), Conditional Use Permit (CUP22-00001), and Variance (V22-00001); and

11 WHEREAS, based on such evidence, testimony, and staff reports, this Council makes the
12 findings of fact as set forth in Planning Commission Resolution Nos. 2025-P05 and 2025-P04 as
13 attached hereto as Exhibit “A” and Exhibit “B”, and incorporates them by reference as if fully set
14 forth herein;

15 NOW, THEREFORE, the City Council of the City of Oceanside does resolve as follows:

16 1. The City Council affirms the Planning Commission’s actions of February 10, 2025 and upholds
17 the certification of the FEIR and adoption of CEQA Findings of Fact and MMRP, and upholds the
18 approval of Development Plan (D22-00001), Conditional Use Permit (CUP22-00001), and Variance
19 (V22-00001). The appeal of Planning Commission Resolution Nos. 2025- P05 and 2025-P04 is denied.

20 2. Notice is hereby given that the time within which judicial review must be sought on this
21 decision is governed by Code of Civil Procedure Section 1094.6(b) as set forth in Oceanside City
22 Code Section 1.10 and Public Resources Code Section 21167.

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4 PASSED and ADOPTED by the City Council of the City of Oceanside, California this
5 ____ day of _____, by the following vote:

6
7 AYES:

8 NAYS:

9 ABSENT:

10
11 ABSTAIN:

12 _____
13 Mayor of the City of Oceanside

14 ATTEST:

15 APPROVED AS TO FORM:
16 OFFICE OF THE CITY ATTORNEY

17 _____
18 City Clerk

19 
20 _____
21 City Attorney

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4.02 ATTACHMENT 2

PLANNING COMMISSION
RESOLUTION NO. 2025-P05

A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA CERTIFYING THE FINAL ENVIRONMENTAL IMPACT REPORT AND ADOPTING THE FINDINGS OF FACT AND MITIGATION MONITORING AND REPORTING PROGRAM FOR THE EDDIE JONES WAREHOUSE, MANUFACTURING AND DISTRIBUTION FACILITY PROJECT (STATE CLEARINGHOUSE NO. 2022070365)

APPLICATION NO: D22-00001, CUP22-00001 & V22-00001
APPLICANT: RPG OCEANSIDE EDDY JONES WAY OWNER, LLC
LOCATION: 250 EDDIE JONES WAY (APN: 145-021-29, 30 & 32)

THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES RESOLVE AS FOLLOWS:

WHEREAS, on January 31, 2022, an application was filed by RPG Oceanside Eddie Jones Way Owner LLC for the construction of a new 566,905 square-foot warehouse and distribution facility on a 31.79-acre site at 250 Eddie Jones Way (“proposed project”);

WHEREAS, the Project application was submitted to, and processed by, the City of Oceanside Planning Division as a concurrent application for a Development Plan (D22-00001), Conditional Use Permit (CUP22-00001), and Variance (V22-00001); and

WHEREAS, in conjunction with consideration of the Project application described above, and in accordance with the California Environmental Quality Act (CEQA), an Environmental Impact Report (SCH No.: 2022070365) was prepared for the project;

WHEREAS, the Draft Environmental Impact Report (DEIR) was circulated for a 45-day public and agency review from October 26, 2023 to December 9, 2023 and proper notification was given in accordance with CEQA; and

WHEREAS, following the close of the public comment period on the DEIR, a Final Environmental Impact Report (FEIR) was prepared for the Project; and

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1 WHEREAS, Chapter 8, Alternatives, of the Final EIR describes the potential
2 impacts of the Multi-Building and Truck Bay Reduction Alternative (“MBTRA”), this
3 alternative has reduced or similar less than significant impacts, with mitigation, to the
4 proposed project.

5 WHEREAS, the Planning Commission did on the 10th day of February 2025 conduct
6 a duly advertised public hearing on the content of the FEIR, Findings of Fact, and Mitigation
7 Monitoring and Reporting Program (MMRP) for the MBTRA project; and

8 WHEREAS, the Findings of Fact and MMRP are appended to this resolution as
9 Exhibit A and B, respectively; and

10 WHEREAS, studies and investigations made by this Commission and on its behalf
11 reveal the following facts:

12 For the Final Environmental Impact Report:

- 13 1. The FEIR, Findings of Fact, and MMRP for the MBTRA project were completed in
14 compliance with the provisions of CEQA.
- 15 2. There are certain significant environmental effects detailed in the FEIR and MMRP
16 which have been avoided or substantially lessened by the establishment of measures
17 which are detailed in Exhibit “B” Mitigation and Monitoring and Reporting Program.
- 18 3. The FEIR, Findings of Fact, and MMRP for the MBTRA project were presented to
19 the Planning Commission, and the Planning Commission reviewed and considered
20 the information contained in these documents prior to making a decision on the
21 project.
- 22 4. The FEIR, Findings of Fact, and MMRP prepared for the MBTRA project have been
23 determined to be accurate and adequate documents, which reflect the independent
24 judgment of the City.

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1 NOW, THEREFORE, BE IT RESOLVED as follows:

- 2 1. The Planning Commission hereby certifies the Final Environmental Impact Report
3 (SCH No. 2022070365) for the MBTRA project.
- 4 2. Pursuant to Public Resources Code Section 21081, the Planning Commission hereby
5 adopts the Findings of Fact (Exhibit A) for the MBTRA project.
- 6 3. Pursuant to Public Resources Code Section 21081.6, the Planning Commission
7 hereby adopts the MMRP (Exhibit B) and finds that the MMRP meets the
8 requirements of Public Resources Code section 21081.6 by providing for the
9 implementation and monitoring of measures intended to mitigate potentially
10 significant effects of the MBTRA project and designed to ensure compliance with the
11 mitigation measures throughout the implementation of the MBTRA project.

12 PASSED AND ADOPTED Resolution No. 2025-P05 on February 10, 2025 by the
13 following vote, to wit:

14 AYES:

15 NAYS:

16 ABSENT:

17 ABSTAIN:

18 _____
19 Tom Morrisey, Chairperson
20 Oceanside Planning Commission

21 ATTEST:

22 _____
23 Sergio Madera, Secretary

24 I, SERGIO MADERA, Secretary of the Oceanside Planning Commission, hereby certify
that this is a true and correct copy of Resolution No. 2025-P05.

Dated: February 10, 2025

**FINDINGS OF FACT REGARDING SIGNIFICANT EFFECTS
PURSUANT TO STATE CEQA GUIDELINES
SECTIONS 15090 AND 15091**

Eddie Jones Project

SCH No. 2022070365

February 2025

Final EIR SCH No. 2022070365

D22-00001, V22-00001, CUP22-00001

I. INTRODUCTION

The City of Oceanside (“City”) Planning Commission hereby certifies that the Planning Commission has reviewed and considered the information contained in the Final Environmental Impact Report (“Final EIR”), identified below, for the Eddie Jones Warehouse, Manufacturing and Distribution Facility Project. The Planning Commission further certifies that the Final EIR has been completed in compliance with the California Environmental Quality Act (“CEQA”), Public Resources Code §§21000 et seq., the State CEQA Guidelines, California Code of Regulations, Title 14, §§15000 et seq. (“CEQA Guidelines”), and City requirements, and that the Final EIR reflects the independent judgment of the Planning Commission (Pub. Resources Code § 21082.1(c)(3)). In certifying the Final EIR as adequate under CEQA, the Planning Commission hereby adopts these CEQA Findings.

These findings are made pursuant to CEQA, specifically Public Resources Code sections 21081, 21081.5, and 21081.6; and the State CEQA Guidelines (Cal. Code Regs., tit. 14, §15000 et seq.), specifically section 15091. The potentially significant effects of the project were identified in both the Eddie Jones Warehouse, Manufacturing and Distribution Facility Project Draft EIR (October 2023) and Final EIR (November 2024). Public Resources Code section 21081 and State CEQA Guidelines section 15091 require that the lead agency, in this case the City of Oceanside, prepare written findings for identified significant impacts, accompanied by a brief explanation of the rationale for each finding. Specifically, State CEQA Guidelines section 15091 states, in part, that:

- (a) No public agency shall approve or carry out a project for which an EIR has been certified which identifies one or more significant environmental effects of the project unless the public agency makes one or more written findings for each of those significant effects accompanied by a brief explanation of the rationale for each finding. The possible findings are:
 - (1) Changes or alterations have been required in, or incorporated into, the project which avoid or substantially lessen the significant environmental effects as identified in the Final EIR.
 - (2) Such changes or alterations are within the responsibility and jurisdiction of another public agency and not the agency making the finding. Such changes have been adopted by such other agency or can and should be adopted by such other agency.
 - (3) Specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible the mitigation measures or project alternatives identified in the Final EIR.

If significant impacts cannot be mitigated to less than significant levels, the decision-making agency is required to balance, as applicable, the benefits of the proposed project against its significant unavoidable environmental impacts when determining whether to approve the project. (Pub. Resources Code, § 21081, CEQA Guidelines § 15093.) If the benefits of a proposed project outweigh the significant unavoidable adverse environmental impacts, the adverse effects may be considered “acceptable.”

Chapter 4, Environmental Analysis, of the Final EIR describes the potential environmental impacts of the proposed project, a development of an approximately 566,905 square-foot warehouse and distribution facility on the approximately 31.79-acre project site located north of Eddie Jones Way and the Oceanside Municipal Airport, east of Benet Road, south of the San Luis Rey River and a recreational trail, and west of vacant light industrial land (“project” or “proposed project”). Chapter 8, Alternatives, of the Final EIR describes the potential impacts of the Multi-Building and Truck Bay Reduction Alternative (“MBTRA”), this alternative has reduced or similar less than significant impacts, with mitigation, to the proposed project.

Therefore, in accordance with CEQA, Pub. Resources Code, § 21081, and the CEQA Guidelines, section 15091, the Planning Commission certifies the Final EIR for the MBTRA, adopts these findings, and the Mitigation Monitoring and Reporting Plan (“MMRP”), and approves the MBTRA project. In adopting the MMRP, the Planning Commission finds that the MMRP meets the requirements of Public Resources Code section 21081.6 by providing for the implementation and monitoring of measures intended to mitigate potentially significant effects of the MBTRA. Therefore, these findings concern the MBTRA and make the CEQA required determinations as it relates to that alternative to the proposed project.

The Planning Commission further, based on separate findings, adopts the following related project approvals to facilitate implementation and development of the MBTRA: (i) Development Plan; (ii) Conditional Use Permit; and (iii) Variance.

II. ORGANIZATION/FORMAT/NATURE OF FINDINGS

In compliance with the statutory requirements, these findings are organized as follows:

1. Introduction to the CEQA Findings and Facts in Support of Findings for Final Eddie Jones Warehouse, Manufacturing and Distribution Facility Project EIR.
2. Description of the MBTRA, including an overview of the discretionary actions required for the MBTRA approval and a statement of the Project Objectives.
3. Findings regarding the environmental impacts that were determined as a result of the Initial Study, Notice of Preparation (“NOP”), and consideration of comments received during the NOP comment period, that were assessed as having no impact.

4. Findings regarding potentially significant or significant effects identified in the Final EIR which the City has determined, after application of applicable development requirements or feasible mitigation measures identified in the Final EIR, are less than significant.
5. Findings regarding project alternatives.

Each category that discusses the MBTRA's environmental impacts identifies the significance of the effects; development requirements and mitigation measures relevant to the specific effects being considered; and the findings and facts in support of those findings.

Any finding made by the City shall be deemed made, regardless of where it appears in this document or elsewhere in the record of proceedings. All of the language included in this document constitutes findings by the City, whether or not any particular sentence or clause includes a statement to that effect. The City intends that these findings be considered as an integrated whole and, whether or not any part of these findings fail to cross-reference or incorporate by reference any other part of these findings, that any finding required or committed to be made by the City with respect to any particular subject matter of the Final EIR, shall be deemed to be made if it appears in any portion of these findings.

III. RECORD OF PROCEEDINGS

For purposes of CEQA and these Findings, the Record of Proceedings for the MBTRA consists of the following documents and other evidence, at a minimum:

- The NOP and all other public notices issued by the City in conjunction with the EIR and the City approval process.
- The Draft EIR including all technical appendices.
- All written comments submitted by agencies or members of the public during the public review comment period (or otherwise) on the Draft EIR.
- The Responses to Comments received on the Draft EIR.
- The Mitigation Monitoring and Reporting Program.
- The Staff Report and related materials, written correspondence submitted to the City regarding the MBTRA, Draft EIR or Final EIR, prepared resolutions adopted by the City of Oceanside in connection with the MBTRA and other project approval documents including conditions of approval.
- Matters of common knowledge to the City, including but not limited to federal, State, and local laws and regulations.
- Any documents expressly cited in these Findings, the Draft EIR or Final EIR.

- Any other relevant materials required to be in the record of proceedings by Section 21167.6(e) of the California Public Resources Code.
- The Final EIR.

IV. LOCATION AND CUSTODIAN OF DOCUMENTS

The official custodian of the documents and other materials that constitute the record of proceedings upon which the City's decision is based is identified as follows:

City of Oceanside Development Services Department
300 North Coast Highway
Oceanside, California 92054

This information is provided in compliance with Section 21081.6(a)(2) of the *California Public Resources Code* and with the *California Code of Regulations*, Title 14, Chapter 3, Section 15091(e).

V. ENVIRONMENTAL REVIEW AND PUBLIC PARTICIPATION

A. Notice of Preparation and Scoping Meeting

CEQA establishes mechanisms to inform the public and decision makers about the nature of a proposed project and the extent and types of impacts that the proposed project and alternatives would have on the environment should the proposed project or an alternative be implemented. Pursuant to CEQA Guidelines Section 15082, the City circulated a Notice of Preparation (NOP), published July 20, 2022, to interested agencies, organizations, and parties. The NOP was also sent to the State Clearinghouse at the California Office of Planning and Research. The State Clearinghouse assigned a state identification number (SCH No. 2022070365) to the EIR.

The NOP is intended to encourage interagency communication regarding the proposed action so that agencies, organizations, and individuals are afforded an opportunity to respond with specific comments and/or questions regarding the scope and content of the EIR. A public scoping meeting was held on August 3, 2022, at 6:00 p.m. at the Civic Center Library Community Room, located at 300 North Coast Highway in the City of Oceanside to gather additional public input. The 30-day public scoping period ended on August 18, 2022.

Comments received during the NOP public scoping period were considered as part of the preparation of the EIR. The NOP and written comments are included in Appendix A to the Final EIR. Comments covered numerous topics, including site access and circulation, utility infrastructure and supply, traffic generation and roadway improvements, air quality, greenhouse gas emissions, noise generation, aesthetics and project design, safety, project hazards, community benefits, local hiring, construction work practices, and preservation of biological and cultural resources. Public scoping

comments regarding the project’s potential impact on the environment were evaluated as part of the preparation of the Draft EIR.

B. Draft EIR and Public Review

The Draft EIR was prepared under the direction and supervision of the City. Public review of the Draft EIR was intended to focus “on the sufficiency of the document in identifying and analyzing the possible impacts on the environment and ways in which the significant effects of the project might be avoided or mitigated” (14 CCR 15204). The Notice of Completion of the Draft EIR was filed with the State Clearinghouse as required by CEQA Guidelines Section 15085. In addition, the Notice of Availability of the Draft EIR was distributed pursuant to CEQA Guidelines Section 15087. Interested parties could provide comments on the Draft EIR in written form. The Draft EIR and related technical appendices were available for review during the extended 60-day public review period, from October 26 to December 29, 2023, at the following locations:

City of Oceanside Development Services Department
300 North Coast Highway
Oceanside, California 92054

City of Oceanside Public Library – Civic Center
330 North Coast Highway
Oceanside, California 92054

City of Oceanside Public Library – Mission Branch
3861-B Mission Avenue
Oceanside, California 92508

City of Oceanside website:
<https://www.ci.oceanside.ca.us/gov/dev/planning/ceqa/default.asp>

Interested agencies and members of the public submitted written comments on the adequacy of the Draft EIR to the City’s Development Services Department.

VI. RESPONSES TO COMMENTS

During the public review period, the City of Oceanside received a total of 80 comment letters from State and local agencies, organizations, and individuals on the Draft EIR. Written responses have been prepared to all comments received during the comment period (Final EIR Appendix P).

In response to comments received, the MBTRA was included as an additional project alternative in addition to those included in the Draft EIR and various clarifications, minor modifications and additions of amplifying information have been made to the text, tables, and exhibits of the Draft EIR, as set forth in the Final EIR. These revisions do not qualify as significant new information.

Rather, the changes address comments made regarding the Draft EIR, correct typographical errors, expand upon information presented in the Draft EIR, explain or enhance the evidentiary basis for the determinations made in the Draft and Final EIR, update information and otherwise make clarifications, amplifications or other useful revisions to the Draft EIR.

The City finds that the modifications made in the Final EIR do not constitute significant new information requiring recirculation but rather, the information merely clarifies, amplifies, or makes insignificant modifications in an adequate EIR. Specifically, the City finds that the additional information (including the changes described above), does not show that any of the following would occur:

- (1) A new significant environmental impact would result from implementation of the Preferred Project or from a new mitigation measure proposed to be implemented.
- (2) A substantial increase in the severity of an environmental impact would result unless mitigation measures are adopted that reduce the impact to a level of insignificance.
- (3) A feasible project alternative or mitigation measure considerably different from others previously analyzed would clearly lessen the significant environmental impacts of the Project, but the Project's proponents decline to adopt it.
- (4) The Draft EIR was so fundamentally and basically inadequate and conclusory in nature that meaningful public review and comment were precluded.

In summary, the City hereby finds that no significant new information has been added to the Final EIR since public notice was given of the availability of the Draft EIR that would require recirculation of the EIR.

In compliance with Section 15088(b) of Title 14 of the California Code of Regulations (CEQA Guidelines), the City has met its obligation to provide written Responses to Comments to public agencies at least ten days prior to certifying the Final EIR.

The Final EIR was released on January 10th, 2025, and posted on the City's website.

VII. PROJECT LOCATION, DESCRIPTION, OBJECTIVES, AND APPROVALS

A. MBTRA Location

The approximately 31.79-acre site is located north of Eddie Jones Way and the Oceanside Municipal Airport, east of Benet Road, south of the San Luis Rey River and a recreational trail, and west of vacant light industrial land within the City of Oceanside (City), in the northwestern portion of San Diego County (County). The property is approximately 650 to 900 feet north of State Route 76, and approximately 140 feet north of the Oceanside Municipal Airport runway. The property is also

connected to the terminus of Alex Road in the northeast corner. The site is composed of Assessor's Parcel Numbers 145-021-29-00, 145-021-030-00, and 145-021-032-00.

The property is a vacant, disturbed site that was previously occupied by four primary buildings and five ancillary building that were formerly used for industrial purposes. Decommissioning of those operations started in 2016, and operations completely ceased in 2020. The industrial buildings were vacated in summer 2021 and demolished in 2022.

The property is zoned Limited Industrial (IL), corresponding with the City of Oceanside's General Plan designation of Light Industrial (LI). Areas surrounding the property are zoned Limited Industrial (to the south, east, and west), Open Space (OS) (San Luis Rey River corridor adjacent north of the project site) and residential zones, including RS (Single-Family Residential District) and RM-A (Medium Density A District), located north of the off-site San Luis Rey River.

B. MBTRA Project Description

The Final EIR includes the MBTRA as one of the reasonable range of alternatives analyzed in the CEQA document. The City decided to approve that alternative as the Preferred Project based on the evaluation in the Final EIR and in light of the comments received during the public review of the Draft EIR and other relevant information considered by the City's decision makers.

With a building footprint of 491,582 SF, the MBTRA's buildings are smaller than the single project building. The MBTRA would develop four (4) separate buildings on-site, instead of one building as proposed under the project. The total building square footage of this alternative would be 497,822 SF (inclusive of mezzanine areas), including 40,651 SF of office (ancillary) use, 334,275 SF of warehouse uses, and 122,896 SF of manufacturing uses compared to the project's 566,905 SF. The total building area for building 1 would be 109,660 SF, the total building area for building 2 would be 132,600 SF, the total building area for building 3 would be 121,547 SF, and the total building area for building 4 would be 134,015 SF. This MBTRA would include 56 dock-high doors, and 593 parking stalls which include 22 ADA stalls and 90 EV stalls. In response to public comments, in addition to the reduction in the number of truck bays compared to the project, the design of the MBTRA places the truck bays on the east/west sides of the buildings as opposed to the north side with the project.

Similar to the proposed project, access for the MBTRA would be maintained and improved as necessary, with existing access points from Alex Road at the northeast corner and Benet Road at the southwest corner. The Alex Road access would be limited to passenger vehicles. Heavy truck traffic would not use Alex Road and would be limited to the Benet Road access point. The Benet Road entry has also been redesigned to incorporate a dedicated right-turn lane into the site to better serve truck traffic in a lane separate from the north-bound travel lane of Benet Road.

Similar to the proposed project, the MBTRA would include associated landscaping and stormwater features. Like the project, the MBTRA would maintain a 100-foot buffer from the edge of the San Luis Rey River consistent with the City of Oceanside draft Subarea Plan (SAP). Although the San Luis Rey River Trail and embankment run through the buffer area forming a hard boundary between the property and the river habitat areas, like the project, the MBTRA structures and parking/circulation areas have been designed and located to specifically avoid the biological and planning buffers. The portion of the 100-foot-wide buffer area located on site would be replanted with native coastal plant species.

Additionally, the MBTRA would incorporate required building setbacks and airspace height limits established by, and otherwise be consistent with, the Oceanside Municipal Airport Land Use Compatibility Plan (OMALUCP). The southernmost portions of each of the four (4) MBTRA's proposed buildings have reduced clearance heights to conform to the OMALUCP. The MBTRA's buildings, parking and circulation areas are designed to avoid the Runway Protection Zone (RPZ), which extends across the southwest corner of the property.

Similar to the proposed project, the MBTRA proposes a Development Plan to authorize the complete redevelopment of the property with the uses described in these findings. The MBTRA requires approval of a Conditional Use Permit, just like the project, because it proposes a wholesaling, warehouse and distribution facility that exceeds 50,000 square feet in floor area. Like the project, the MBTRA also requires a Conditional Use Permit because it proposes more than six heavy trucks on the premises at one time. The MBTRA, like the project, requires a variance to allow small height increases for portions of the flood wall that will surround development on the property.

The property is currently served by the existing network of nearby roads, including Alex Road, Eddie Jones Way, Benet Road, Foussat Road, and Highway 76. Primary access to the site is currently provided via Alex Road on the east side, with a secondary access point to Benet Road on the west. Like the project, the MBTRA would improve those access points to full commercial driveway standards. Tractor/trailer/truck ingress/egress would be designated for and limited to the Benet Road access drive. Benet Road connects directly to Highway 76, located approximately 1,000 feet southwest of the site. Alex Road connects the project site to Highway 76 via Foussat Street, located southeast of the site. Highway 76 provides a direct route to Interstate 5 located approximately 1.7 miles to the west.

Similar to the project, internal circulation for the MBTRA would consist of a system of vehicular drives and pedestrian walkways providing access around the buildings and serving parking areas throughout the site. Each of the buildings would be adjacent to at least one drive aisle designed at a 35-foot minimum width to provide for required fire department access adjacent to the buildings that will be 45-feet-high or less.

As proposed for the project, the MBTRA would connect to the existing sidewalk system in the area and improve pedestrian connections to surrounding properties. A sidewalk is proposed from the

access on Alex Road north to connect with the San Luis Rey River Trail right-of-way (a distance of approximately 50 feet). Like the project, the MBTRA would also propose to construct a sidewalk along the project frontage on Benet Road from Eddie Jones Way, north to the San Luis Rey River access path (a distance of approximately 600 feet).

Water and sewer facilities are connected to the site already because of the previous industrial use and the MBTRA will connect to and utilize those existing facilities. Similar to the project, the MBTRA site design includes a new storm water conveyance system on-site, which would consist of ribbon gutters, curb and gutter, and a detention vault system. The vault system incorporates modular wetlands for treatment and a force main pump to convey storm water to the existing storm drain located in Benet Road and into an existing storm water structure to the northwest side of the site which drains to the San Luis Rey River Basin. The MBTRA would connect to existing dry utilities serving the property. Electricity and natural gas would be provided by San Diego Gas and Electric (SDG&E). The MBTRA, like the project, would connect to existing electrical lines and natural gas pipeline within existing roadways adjacent to the property.

Like the proposed project, the MBTRA would generally maintain the existing grades and landform of the property. The San Luis Rey levee embankment and Benet Road right-of-way are elevated approximately 8-12 feet above the MBTRA grades and building pad elevations maintaining a berm effect around the northern and western edges of the property. Approximately 60,000 cubic yards of raw cut and 40,000 cubic yards of raw fill would be required for the site development, resulting in a net export amount of 20,000 cubic yards.

Similar to the proposed project, the MBTRA proposes to construct a flood wall around the perimeter of the site, wrapping the parking area, to flood-proof the property. The flood wall will be designed as a solid decorative masonry block wall system, to be constructed around the perimeter of the site's graded pad area. An existing base flood elevation (BFE) of 34.0' is shown for the site and the flood wall design provides a consistent top of wall elevation (TW) of 35.5'. Exterior facing elevations for the flood wall along the majority of the site perimeter will range in height from approximately 7.9' to 9.9' above the exterior grade. Interior facing flood wall elevations will extend up to approximately 9.5' in height. With the approval of the requested Variance, the proposed flood wall heights are permitted.

The MBTRA would implement, same as the project, both construction-related and operational project design features (PDFs) that help avoid or reduce the potential for significant impacts. Those PDFs, which are incorporated by reference into the MMRP and the conditions of approval, include:

DF-AQ-1: Require the cargo handling equipment including forklifts (forklifts and pallet jacks) and yard tractors for facility operation to be electric powered operation.

PDF-AQ-1: Standard construction practices that would be employed to reduce fugitive dust emissions include watering of the active sites two times per day, depending on weather

conditions. Construction of Project components would be subject to SDAPCD Rule 55 – Fugitive Dust Control. Compliance with Rule 55 would limit fugitive dust that may be generated during grading and construction activities.

PDF-AQ-3: The applicant will incorporate the following applicable California Department of Justice Warehouse Project Best Practices measures as part of project construction and operation:

- Prohibiting grading on days with an Air Quality Index forecast of greater than 100 for particulates or ozone for the project area.
- Forbidding idling of heavy equipment for more than 3 minutes.
- Keeping on site and furnishing to the lead agency or other regulators upon request, all equipment maintenance records and data sheets, including design specifications and emission control tier classifications.
- Conducting an on-site inspection to verify compliance with construction mitigation and to identify other opportunities to further reduce construction impacts.
- Using paints, architectural coatings, and industrial maintenance coatings that have volatile organic compound levels of less than 10 grams per liter.
- Providing information on transit and ridesharing programs and services to construction employees.
- Forbidding trucks from idling for more than 3 minutes and requiring operators to turn off engines when not in use.
- Posting both interior- and exterior-facing signs, including signs directed at all dock and delivery areas, identifying idling restrictions and contact information to report violations to the California Air Resources Board (CARB), the local air district, and the building manager.
- Designing all project building roofs to accommodate the maximum future coverage of solar panels and installing the maximum solar power generation capacity feasible.
- Running conduit to designated locations for future electric truck charging stations.
- Unless the owner of the facility records a covenant on the title of the underlying property ensuring that the property cannot be used to provide refrigerated warehouse space, constructing electric plugs for electric transport refrigeration units at every dock door and requiring truck operators with transport refrigeration units to use the electric plugs when at loading docks.
- Oversizing electrical rooms by 25% or providing a secondary electrical room to accommodate future expansion of electric vehicle (EV) charging capability.
- Requiring facility operators to train managers and employees on efficient scheduling and load management to eliminate unnecessary queuing and idling of trucks.

- Posting signs at every truck exit driveway providing directional information to the truck route.
- Requiring that every tenant train its staff in charge of keeping vehicle records in diesel technologies and compliance with CARB regulations, by attending CARB-approved courses. Also requiring facility operators to maintain records on-site demonstrating compliance and make records available for inspection by the local jurisdiction, air district, and state upon request.
- Requiring tenants to enroll in the U.S. Environmental Protection Agency's SmartWay program, and requiring tenants who own, operate, or hire trucking carriers with more than 100 trucks to use carriers that are SmartWay carriers.
- Providing tenants with information on incentive programs, such as the Carl Moyer Program and Voucher Incentive Program, to upgrade their fleets.

PDF-GHG-1: Photo-voltaic (PV) systems will be installed on the building to meet 50% of forecasted electricity demand, consistent with the City of Oceanside Climate Action Plan (CAP).

PDF-GHG-2: The applicant will participate in one of SDG&E's services for non-residential development such as the Comprehensive Audit Program or the Facility Assessment Service Program, no sooner than 1 year and no later than 2 years after initial building occupancy.

C. Project Objectives

Section 15124(b) of the CEQA Guidelines requires that an EIR include a statement of the project objectives that "include[s] the underlying purpose of the project and may discuss the project benefits." The Draft EIR included the following objectives:

1. Redevelop an existing industrial land use that is already served by existing utilities, services, and street access, and within close proximity to existing transportation infrastructure.
2. Develop an employment-generating project that is consistent with the existing Light Industrial (LI) General Plan land use designation and Limited Industrial (IL) zoning designation for the property.
3. Maximize the allowable use of an existing industrial zoned site that is compatible with the adjacent light industrial zoned sites and Oceanside Municipal Airport.
4. Create a project that takes advantage of and enhances existing infrastructure, including the proximity to major regional transportation infrastructure such as State Route 76 and the Oceanside Municipal Airport.
5. Fulfill a demand for industrial and manufacturing uses in the City.

6. Ensure that siting and design of development adjacent to the San Luis Rey River corridor does not encroach upon the natural river habitat and considers floodplain management.
7. Develop the property in a manner that complies with the development, intensity, noise, use and other restrictions imposed by the Oceanside Municipal Airport Land Use Compatibility Plan.

VII. ENVIRONMENTAL IMPACTS FOUND TO BE NOT SIGNIFICANT

Pursuant to CEQA and the State CEQA Guidelines, the City as the lead agency under CEQA is responsible for certification of the EIR and, as reflected in this document and the record of proceedings, has made the following findings:

1. The Planning Commission has reviewed and considered the information in the Final EIR, which has been completed in compliance with CEQA;
2. The Final EIR reflects the City’s, as lead agency, independent judgment and analysis; and,
3. The Planning Commission adopts the Mitigation Monitoring and Reporting Program (Attachment A) to reduce or avoid the significant and mitigable impacts of the project.

Section I Environmental Effects For Which the MBTRA Would Have No Impact

State CEQA Guidelines Section 15091 does not require specific findings to address environmental effects that an EIR evaluates and identifies as “no impact”. The City finds that, based upon substantial evidence in the record, the following impacts associated with the MBTRA would have no impact without the implementation of mitigation measures in the following resource areas, pursuant to Public Resources Code Section 21081(a) and CEQA Guidelines Section 15091(a).

<i>Environmental Resource Category</i>	<i>Not Significant Environmental Impact</i>
<i>Aesthetics</i> <i>(EIR Section 4.1)</i>	<ul style="list-style-type: none"> • No impact. The MBTRA would not substantially damage scenic resources, including, but not limited to trees, rock outcroppings, and historic buildings within a state scenic highway. • No impact. The MBTRA would not cause cumulatively considerable impacts on aesthetics.
<i>Agricultural and Forest Resources</i> <i>(EIR Chapter 5.1)</i>	<ul style="list-style-type: none"> • No impact. The MBTRA would not convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance (Farmland), as shown on the maps prepared

<i>Environmental Resource Category</i>	<i>Not Significant Environmental Impact</i>
	<p>pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural uses.</p> <ul style="list-style-type: none"> • No impact. The MBTRA would not conflict with existing zoning for agricultural use, or a Williamson Act contract. • No impact. The MBTRA would not conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code section 12220(g)), timberland (as defined by Public Resources Code section 4526), or timberland zoned Timberland Production (as defined by Government Code section 51104(g)) • No impact. The MBTRA would not result in the loss of forest land or conversion of forest land to non-forest use. • No impact. The MBTRA would not involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland, to non-agricultural use or conversion of forest land to non-forest use. • No impact. The MBTRA would not cause cumulatively considerable impacts on agricultural resources.
<i>Cultural Resources</i> <i>(EIR Section 4.4)</i>	<ul style="list-style-type: none"> • No Impact. The MBTRA would not cause a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5.
<i>Geology and Soils</i> <i>(EIR Section 4.6)</i>	<ul style="list-style-type: none"> • No Impact. The MBTRA does not include or require the use of septic tanks or alternative wastewater disposal systems. Therefore, the MBTRA would have no impact related to the use of septic tanks or alternative wastewater.
<i>Mineral Resources</i>	<ul style="list-style-type: none"> • No impact. The MBTRA would not result in the loss of availability of a known

<i>Environmental Resource Category</i>	<i>Not Significant Environmental Impact</i>
<i>(EIR Chapter 5.2)</i>	<p>mineral resource that will be of value to the region and the residents of the state.</p> <ul style="list-style-type: none"> • No impact. The MBTRA would not result in the loss of availability of a locally-important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan. • No impact. The MBTRA would not cause cumulatively considerable impact to mineral resources.
<i>Recreation (Section 5.3)</i>	<ul style="list-style-type: none"> • No Impact. The MBTRA would not increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facility would occur or be accelerated. • No Impact. The MBTRA would not include recreational facilities or require the construction or expansion of recreational facilities, which might have an adverse physical effect on the environment.

Section II Less Than Significant Environmental Effects Without Mitigation

This section makes findings regarding the potential effects of the MBTRA that were determined to be less than significant under both a project-level and cumulative impacts evaluation. The thresholds identified in the discussions below are the thresholds of significance identified in the CEQA Guidelines and used in Final EIR. Though no mitigation measures are required, the findings below of less than significant impacts rely on the nature of the project, PDFs, compliance with laws or other requirements incorporated into or applicable to the MBTRA. For the reasons described in more detail below, the City hereby finds that the MBTRA would have less than significant impacts without the implementation of mitigation measures in the following resource areas: Aesthetics, Air Quality, Biological Resources, Cultural Resources, Energy, Geology and Soils, Greenhouse Gas Emissions, Hazards and Hazardous Materials, Hydrology and Water Quality, Land Use and Planning, Noise, Population and Housing, Public Services, Traffic and Circulation, Utilities and Service Systems, and Wildfire. Analysis of the individual findings is set forth below and the record of proceedings includes the substantial evidence supporting the findings.

AESTHETICS

Threshold of Significance:

- Would the project have a substantial adverse effect on a scenic vista?

The MBTRA, like the project, is located on a property adjacent to the Oceanside Municipal Airport and near the SR-76 and other urban and industrial uses. The property is within the public viewshed of the San Luis Rey River and its associated recreational trail, which is the primary visual open space resource adjacent to the site. The property, while not identified on the City's list of visual open space or natural aesthetic resources, lies in proximity to other scenic resources in the area. The San Luis Rey River corridor is largely obscured by heavy vegetation along its banks, which prevents direct views of the river from the property. The proposed multi-building layout, which includes four separate buildings rather than a single large structure, would not have a substantial adverse effect on views from public vantage points of any qualifying scenic vistas north of the property such as the Prince of Peace Abbey (St. Charles Priory) or the river. Similar to the project, the MBTRA would not interfere with existing public access to the river trail or obstruct any designated public views of the river or other designated scenic vistas.

The visual impact of the MBTRA is anticipated to be similar to the project, as it would maintain the same 100-foot buffer from the San Luis Rey River riparian habitat consistent with the draft Subarea Plan, ensuring that no development occurs within this area. The design of the MBTRA, with buildings situated along the eastern and western sides of the site, would be consistent with the surrounding land uses. Furthermore, the proposed landscaping and stormwater features would help avoid any potential visual impacts by introducing additional greenery and enhancing the overall site appearance. Therefore, impacts would be less than significant as the MBTRA would not have a substantial adverse effect on a designated scenic vista.

Threshold of Significance:

- In non-urbanized areas, would the project substantially degrade the existing visual character or quality of public views of the site and its surroundings? (Public views are those that are experienced from publicly accessible vantage point.) If the project is in an urbanized area, would the project conflict with applicable zoning and other regulations governing scenic quality?

Like the project, the MBTRA would be located within the urbanized area of the City of Oceanside, which, as defined by California Public Resources Code Section 21071, as an area with a population exceeding 100,000. The site is zoned Limited Industrial (IL), consistent with the City's General Plan designation of Light Industrial (LI). The proposed MBTRA would comply with the City's zoning regulations, including those governing scenic quality, and would be subject to the City's approval of necessary entitlements, such as a Conditional Use Permit and a Development Plan.

The project site is bordered by the Oceanside Municipal Airport to the south, undeveloped industrially designated land to the east, Benet Road to the west, and the San Luis Rey River Trail and river corridor to the north, with residential uses beyond the river. The site is disturbed, but currently vacant, due to the prior industrial use. The MBTRA would include four separate buildings rather than a single structure, resulting in a total building area of 497,822 square feet. As outlined in Table 4.10-2 in Section 4.10, Land Use, of the Final EIR, the project would be in conformance with all General Plan policies related to visual impacts and site design. Consistent with General Plan Land Use Objectives 1.2. and 2.3, and their implementing policies, and Section 3003 of the Municipal Code regulating the scenic quality of exterior materials, the MBTRA would incorporate similar architectural elements as the project, including modern industrial design features such as concrete tilt-up panels, horizontal and vertical reveals, and large window elements to add visual interest to the building facades. These design elements would serve to break up the mass of the buildings and reduce the visual bulk of the structures as viewed from surrounding areas. Color variations, vertical undulations, and canopy elements would further contribute to reducing the appearance of the building's scale and enhance the visual aesthetic.

Consistent with City regulations, native and drought tolerant landscaping would be incorporated to complement the design of the buildings and soften the overall visual impact of the development. Tree and shrub plantings would be strategically placed to enhance the visual quality of the site and screen the perimeter edges. Landscaping along the Benet Road and Alex Road frontages would provide upgraded streetscapes and improve the MBTRA's visual integration with the surrounding area. In addition, a 100-foot-wide biological buffer from the San Luis Rey River, consistent with the City of Oceanside's Draft Subarea Plan, would be maintained and replanted with native coastal species. This buffer would serve as a natural visual boundary between the MBTRA and the river corridor.

The MBTRA would adhere to all other relevant zoning and scenic quality regulations, and its architectural design, landscaping, lighting and integration with the surrounding environment would ensure that visual impacts remain less than significant. For example, the MBTRA must comply with Chapters 31, including Section 3117, and 39 of the City's Municipal Code and Building Code requirements that require all outdoor lighting installed as part of the MBTRA be energy efficient, fully shielded, and directed downward to minimize light trespass onto surrounding properties. Therefore, the MBTRA would not conflict with applicable zoning and other regulations governing scenic quality and impacts would be less than significant.

Threshold of Significance:

- Would the project create a new source of substantial light or glare which would adversely affect day or nighttime views in the area?

The MBTRA would introduce similar lighting and glare characteristics to the project. The surrounding area is already affected by lighting from nearby industrial, commercial, and residential

developments. The MBTRA would re-introduce lighting for security, landscaping, building façades, and internal circulation to an urbanized area with existing sources of day and nighttime lighting, including the Oceanside Municipal Airport.

The lighting for the MBTRA would comply with all applicable regulations, including Chapter 39 of the City Municipal Code and Title 24, Part 11 of the 2022 California Green Building Standards Code (CALGreen), to ensure energy efficiency and proper shielding to minimize light trespass and glare. The lighting design would ensure that all fixtures are directed downward and shielded to prevent light spill onto adjacent properties. Consistent with the project, lighting would be turned off during daylight hours to further reduce unnecessary light emissions. Additionally, a perimeter wall and landscape buffer along the northern edge of the site would reduce light intrusion into sensitive areas, such as the San Luis Rey River and nearby residential communities. Based on the MBTRA as proposed, and required compliance with the City's Municipal Code and CALGreen, would not create a source of substantial light that would adversely affect day or nighttime views.

Regarding potential glare, the MBTRA would follow similar design principles as the project. The MBTRA would not include large expanses of glass or highly reflective materials that could generate significant glare during daytime hours. The façade design would incorporate color variations, vertical undulations, and materials that minimize glare. Glass used in the building would be tempered and designed to comply with the relevant glazing standards, which would further reduce the potential for substantial glare. The landscape plan would include tree plantings that help minimize glare effects and provide further screening. Additionally, the MBTRA would include the use of photovoltaic (solar) panels, similar to the project. Solar panels, by design, absorb light, not reflect it, and would be positioned to minimize glare at acute angles. Given that fact, along with the MBTRA's required compliance with the ALUCP, the MBTRA would not create a new source of substantial glare that would adversely affect day or nighttime views in the area. Therefore, the MBTRA would not create a new source of substantial light or glare which would adversely affect day or nighttime views in the area and impacts would be less than significant.

Cumulative Impact:

Cumulative aesthetic impacts are assessed based on the viewshed, or area from which the development and surrounding developments are visible. The MBTRA would alter the visual character of the 31.79-acre site by developing four smaller buildings totaling 497,822 square feet, compared to the project's single large building. Visual changes would be most noticeable to nearby residents, trail users, motorists, and from the Oceanside Municipal Airport, but the MBTRA would remain consistent with surrounding industrial and urban development. The MBTRA, like the project, is not located within the public viewshed of any scenic open space areas, except the San Luis Rey River corridor, where existing views are limited by vegetation. The MBTRA would not obstruct these views. The Ocean Kamp project, located just east of the site, would introduce additional visual changes to the area, but both projects would fit within the surrounding industrial, commercial, and

residential landscape. Like the project, other cumulative projects and the MBTRA's smaller buildings and reduced truck bays would not conflict with applicable zoning and other regulations governing scenic quality. Similar to the project, the MBTRA has undergone the City's design review process to ensure compatibility with surrounding land uses, including landscaping and building design. The MBTRA would introduce some light and glare, but like all cumulative projects, would avoid the potential for significant impacts through compliance with applicable laws including the City's light pollution regulations. The MBTRA would not contribute to cumulatively considerable aesthetic impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

AIR QUALITY

Threshold of Significance:

- Would the project conflict with or obstruct implementation of the applicable air quality plan?

Like the project, the MBTRA would be consistent with the applicable air quality plans and would not conflict with or obstruct their implementation. Like the project, the MBTRA is located within the City of Oceanside's Industrial land use designation, which allows for a variety of industrial uses, including warehouses and manufacturing facilities. This zoning is consistent with regional growth projections and air quality planning documents, including the San Diego Air Pollution Control District (SDAPCD) and the San Diego Association of Governments (SANDAG) regional plans.

The air quality plans for the region, including the State Implementation Plan (SIP) and the Regional Air Quality Strategy (RAQS), account for the growth and development outlined in the City's General Plan and SANDAG's projections for the region. These plans rely on assumptions about land use and development patterns, including industrial growth, which are reflected in local and regional planning efforts.

The MBTRA would not result in development that exceeds what is anticipated in these planning documents. The property is designated for industrial uses, and the proposed square footage and intensity of the development under the MBTRA fall within the anticipated range of industrial development in the area. As such, the air quality impacts associated with this alternative are consistent with those projected in the RAQS and the SIP. The vehicle trips generated by the MBTRA would also fall within the range projected in SANDAG's growth forecasts, and the emissions from these trips have already been accounted for in the regional air quality planning process. Consequently, the MBTRA would not conflict with or obstruct their implementation. As a result, impacts would be less than significant.

Threshold of Significance:

- Would the project result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?

For the reasons specified below, the MBTRA would have a less than significant impact as the MBTRA would not result in other emissions adversely affecting a substantive number of people.

Construction

The MBTRA would be subject to the same construction-related emissions regulations as the project, including those outlined in the State of California Health and Safety Code, SDAPCD Rule 51, and the City's Municipal Code. These regulations prohibit emissions that cause injury, nuisance, or damage to public health or property. During construction, potential odors could result from vehicle and equipment exhaust emissions, as well as from the use of architectural coatings and other materials. These odors would be temporary in nature and, for the type of construction required for the MBTRA, would not reach concentrations that would adversely affect a substantial number of people.

The property is located in an industrial area with no sensitive receptors immediately adjacent to the site. The closest sensitive receptors, such as residences, are located approximately 0.15 mile north across the San Luis Rey River, which further reduces the likelihood of significant odor impacts during construction. Additionally, construction activities would be subject to the City's regulations prohibiting evening and night-time construction, which would minimize the duration and frequency of odor-generating activities during sensitive hours. Therefore, the construction activities for the MBTRA would not result in other emissions such as odors that would adversely affect a substantial number of people, and impacts during construction would be less than significant.

Operational

Similar to the project, the operational phase of the MBTRA would not be expected to generate significant odors that would affect a substantial number of people. The MBTRA includes industrial uses such as warehouses and manufacturing, which are not typically associated with objectionable odors. Potential sources of odor in the operational phase could include vehicle exhaust from trucks, occasional dust, or minimal odors from the building's activities; however, none of these would be expected to reach levels that would adversely affect a substantial number of people.

The MBTRA does not propose uses typically associated with strong, persistent odors, such as food processing facilities, chemical plants, wastewater treatment plants, or landfills. Additionally, as aligned with the project, the MBTRA would be subject to compliance with SDAPCD Rule 51, which prohibits the emission of materials that create a nuisance to a considerable number of persons or endanger the health and safety of the public. This rule applies to all operational activities.

Further, the location of the MBTRA, with its proximity to existing industrial land uses and distance from sensitive receptors, minimizes the potential for potentially significant operational impacts. Therefore, the operational phase of the MBTRA would not result in other emissions such as odors

that would adversely affect a substantial number of people, and impacts during operation would be less than significant.

Cumulative Impact:

Cumulative air quality impacts for the MBTRA are evaluated within the context of the San Diego Air Basin, which is designated as a federal nonattainment area for ozone and a state nonattainment area for both ozone and particulate matter (PM10 and PM2.5). These nonattainment designations result from cumulative emissions from past and present development activities. Like the project, the MBTRA's construction emissions would remain below significance thresholds, with short-term, temporary impacts typical of industrial projects, and would be mitigated by measures such as the use of low-VOC paints (MM-AQ-1). Once construction is complete, associated emissions would cease. Operational emissions from the MBTRA are not anticipated to exceed thresholds for any criteria pollutants. Moreover, the MBTRA is consistent with regional air quality planning documents, including the SIP and the RAQS, which rely on growth projections from SANDAG. As the MBTRA aligns with these projections, it would be consistent with the SIP and RAQS, thus not contributing to a significant cumulative impact from operational emissions. Given the same location and that the MBTRA has a reduced development intensity compared to the project, like the project and as disclosed in Section 4.2, Air Quality, CO concentrations with the MBTRA would not result in CO hotspots; would not result in TAC exposure that would exceed thresholds during construction or operation; would not expose sensitive receptors to substantial pollutant concentrations; and would not create objectionable odors. Therefore, based also on the location of the MBTRA relative to other cumulative projects, the MBTRA would not contribute to cumulative considerable increases in other emissions adversely affecting a substantive number of people. Therefore, the MBTRA would not result in a cumulatively considerable contribution to air quality impacts as disclosed in the Final EIR and elsewhere in the record of proceedings.

BIOLOGICAL RESOURCES

Threshold of Significance:

- Would the project have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department and Game of U.S. Fish and Wildlife Service?

The MBTRA would not have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department and Game of U.S. Fish and Wildlife Service. The biological study area for the MBTRA has been assessed, and no qualifying riparian or other sensitive natural community exists within the property to be developed by the MBTRA. The property is located near the San Luis Rey River, and in accordance with the City of Oceanside's Draft Subarea Plan (2010), a 100-foot biological buffer would be maintained around the upland habitats to protect sensitive natural communities. The

MBTRA's footprint is designed to avoid encroaching into these sensitive areas, ensuring that there is no direct disturbance of riparian habitats or other qualifying sensitive natural community.

Like the project, development of the MBTRA would occur on previously disturbed land and would maintain the 100-foot biological buffer from the San Luis Rey River. Approximately 0.85 acres of the buffer area is located within the project boundary. The remaining buffer area including riprap along the levee, the San Luis Rey River Trail, and disturbed land along the levee's south slope, would not be disturbed by the MBTRA.

Therefore, the MBTRA would not have a substantial adverse effect on any riparian habitat or other sensitive natural communities identified in local or regional plans, policies, regulations or by the California Department and Game of U.S. Fish and Wildlife Service and impacts would be less than significant.

Threshold of Significance:

- Would the project have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?

The MBTRA would not have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.). The biological study area for the MBTRA has been assessed, and no state or federally protected wetlands were identified within or near the property. As confirmed in the biological assessment, no direct removal, filling, hydrological interruption, or other impacts to jurisdictional wetland resources would result from the MBTRA. Therefore, impacts to state or federally protected wetlands are determined to be less than significant.

Cumulative Impact:

The cumulative biological study area for the MBTRA is consistent with the area covered by the Oceanside Subarea Plan (2010). The MBTRA design would not result in direct impacts to special-status plant or wildlife species, as the development has been planned to avoid sensitive biological areas. The proposed 100-foot biological buffer from the San Luis Rey River will further protect adjacent riparian habitats or other qualifying sensitive communities. Indirect impacts to biological resources would be mitigated to a less-than-significant level through the implementation of MM-BIO-1 through MM-BIO-4, including best management practices to protect wildlife species and habitats. Additionally, the MBTRA would be required to comply with the California Fish and Game Code and the Migratory Bird Treaty Act to prevent impacts to nesting birds. The MBTRA, as with all other cumulative projects, would be required to comply with all applicable federal, state and local regulations regarding the protection of sensitive vegetation communities, special-status plants, special-status wildlife species, jurisdictional resources, including, without limitations, wetlands, and wildlife movement or corridors/habitat linkages. Further, the Ocean Kamp project, which is the

closest cumulative project to the property, would be required to mitigate for its own impacts to biological resources, which would reduce the potential for cumulative impacts to occur. Given the design features and mitigations required for the MBTRA, it would not contribute to significant cumulative impacts on regional biological resources, as outlined in the Final EIR and the record of proceedings.

CULTURAL RESOURCES

Threshold of Significance:

- Would the project cause a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5?

The MBTRA would not cause a substantial adverse change in the significance of a historical resource as defined in CEQA Guidelines Section 15064.5. As described in the Final EIR, no buildings exist on the property. As outlined in the project's historical resources assessment, the buildings that previously occupied the site were not found to meet the criteria for eligibility for listing in the National Register of Historic Places (NRHP), California Register of Historical Resources (CRHR), or as a City of Oceanside Designated Historic Resource. These structures lacked the necessary significance for protection under CEQA. Additionally, no indirect impacts to historical resources were identified, as the MBTRA, like the project, would not affect any historical resources outside the property. Therefore, the MBTRA would not result in any substantial adverse effects on historical resources as defined in CEQA Guidelines Section 15064.5, and impacts would be less than significant.

Threshold of Significance

- Would the project disturb any human remains, including those interred outside of formal cemeteries?

Same as the project, the MBTRA would not disturb any human remains, including those interred outside of formal cemeteries. The project site is not identified as a cemetery, and no evidence of human remains is known to exist within the project area. Additionally, no human remains were discovered during field surveys conducted of the property. Further, as a matter of law, the MBTRA must comply with Section 7050.5 of the California Health and Safety Code (HSC), if human remains are discovered during excavation or grading activities. In the event of such a find as required by law, the county coroner would be immediately notified, excavation or disturbance of the site in the vicinity of the find would be halted until the appropriate treatment and disposition of the remains are determined. If the remains are identified as Native American, the coroner would contact the Native American Heritage Commission (NAHC) to identify the Most Likely Descendant (MLD), who would be consulted to make recommendations regarding the respectful treatment and disposition of the remains. Although no mitigation is required, the MBTRA would adhere to the

measures outlined in MM-CUL-9, which reflect the state law requirements under HSC section 7050.5. Therefore, like the project, the MBTRA would not disturb any human remains, including those interred outside of formal cemeteries, and the potential impacts would be less than significant.

Cumulative Impact:

The cumulative study area includes other projects in the vicinity that may affect cultural resources. Cultural resources studies will be conducted for all cumulative projects in the area to assess potential impacts just as was done with the project and the MBTRA. These studies will help ensure that cultural resources are properly identified, and that any potentially significant impacts are avoided or mitigated in accordance with local and state laws. If any significant cultural resources are discovered, appropriate mitigation measures would be implemented on a project-by-project basis. Given the absence of identified cultural resources at the MBTRA project site, and with the incorporation of standard cultural resource mitigation measures (MM-CUL-1 through MM-CUL-9) and compliance with HSC section 7050.5, the MBTRA would not result in significant cultural resource impacts. Therefore, the MBTRA would not contribute to cumulatively considerable cultural resource impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

ENERGY

Threshold of Significance:

- Would the project result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?

Construction and operation of the MBTRA would have less than significant impacts as the MBTRA would not result in any potentially significant environmental impacts due to wasteful, inefficient, or unnecessary consumption of energy resources.

Construction Use:

Under the MBTRA, the construction process would be similar to that of the project, involving roughly the same phases such as site preparation, grading, building construction, and paving. The primary energy consumption would still come from diesel-powered construction equipment and trucks and construction workers using gasoline. Electricity demand during construction would remain minimal, typically used for hand tools and trailers. However, since the MBTRA reduces the size of the building, some limited reduction in energy usage would occur compared to the project. Like the project, the MBTRA has no unusual characteristics or construction processes that would use more energy intensive equipment than comparable projects and all equipment used would have to conform to applicable energy efficiency standards. Thus, the MBTRA construction would not

result in potentially significant environmental impacts due to wasteful, inefficient, or unnecessary consumption of energy resources.

Operational Use:

Electricity

The MBTRA would lead to a slight reduction in operational energy use compared to the project, as fewer truck bays and a smaller overall building size would reduce electricity needs for lighting, HVAC, or other building systems. Like the project, the MBTRA would not represent a significant demand on electricity supplies that would require additional capacity. The MBTRA's use is not inherently energy intensive, and the MBTRA electricity demands in total would be comparable to other projects of similar scale and configuration. Due to evolving energy efficiency requirements, the MBTRA would be more energy efficient than other industrial developments built over prior years because of increasingly more stringent Title 24 standards and City Municipal Code requirements regarding renewable energy usage and drought-tolerant landscaping (reduces energy use associated with water supply). Therefore, like the project, the MBTRA electricity consumption during operations would have less than significant impacts as the MBTRA would not result in wasteful, inefficient, or unnecessary consumption of energy resources.

Natural Gas

Similar to electricity, the MBTRA would slightly reduce the project's natural gas demand compared to the project. Natural gas consumption estimates would likely remain within a small margin of what was identified for the project. The MBTRA is not inherently energy intensive, and natural gas usage would be comparable or less than other projects of similar scale and configuration. Additionally, the MBTRA is subject to statewide mandatory energy standards established by Title 24, Part 6, compliance with the City's CAP and Municipal Code requirements relative to renewable energy usage. Therefore, like the project, the MBTRA natural gas consumption during operations would have less than significant impacts as the MBTRA would not result in wasteful, inefficient, or unnecessary consumption of energy resources.

Petroleum

The MBTRA operations would be similar to the project such that estimates for the consumption of energy would be similar. Like the project, the MBTRA must comply with City Code requirements relative to the provision of EV charging/parking and bicycle facilities that reduce demand for petroleum usage. The MBTRA has no unusual characteristics or operational processes that would result in the use of more petroleum than comparable projects and all vehicles used in operation would have to comply with any increase in fuel efficiency standards. Therefore, petroleum usage during the MBTRA operations would not result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources.

Overall, the MBTRA energy usage would be minimal in the context of overall energy consumption at the regional and state levels, the MBTRA would comply with energy efficiency standards and the MBTRA would not result in wasteful, inefficient, or unnecessary consumption of resources. Therefore, the MBTRA would not result in potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation.

Threshold of Significance:

- Would the project conflict with or obstruct a state or local plan for renewable energy or energy efficiency?

Like the project, the MBTRA would comply with California's energy efficiency and renewable energy standards, ensuring it does not conflict with or obstruct state or local energy plans for energy or energy efficiency. Like the project, the MBTRA must adhere to Title 24 energy efficiency standards, which regulate energy use in residential and nonresidential buildings. Specifically, Title 20 and Title 24 addresses energy impacts of lighting, heating, cooling, and water heating systems, as well as the building envelope and appliances. Additionally, the MBTRA would incorporate solar PV to meet the City of Oceanside's Zoning Ordinance.

The MBTRA is also consistent with the Energy Climate Action Element (ECAE) of the City's General Plan that promotes energy efficiency and the use of renewable energy. Like the project, the MBTRA would still include features such as commercial PV systems, energy-efficient lighting and appliances, EV charging stations, and drought-tolerant landscaping. Therefore, the MBTRA would not conflict with or obstruct state or local plans for renewable energy or energy efficiency and impacts would be less than significant.

Cumulative Impact:

The MBTRA would not result in direct, significant energy impacts as the MBTRA will not result in wasteful, inefficient, or unnecessary use of energy during construction or operations, nor would it conflict with an applicable plan for renewable energy or energy efficiency. The MBTRA has been designed with energy-saving measures such as the installation of EV charging stations, solar panels on buildings, reduced landscaping water use, and the planting of trees. Cumulative projects would be required to meet Title 24 building standards, which further minimize energy inefficiency, and would be subject to state and federal regulations like the Low Carbon Fuel Standard, Pavley Clean Car Standards, and Low Emission Vehicle Program, which would reduce transportation-related fuel demand. Like the MBTRA, those cumulative projects would be required to demonstrate that they would not result in wasteful, inefficient, or unnecessary use of energy during construction or operations or conflicts with an applicable plan for renewable energy or energy efficiency. Therefore, the MBTRA, which would be less intense than the project in energy usage based on the reduced development footprint, slightly smaller building square footage and reduced number of truck bays,

would not contribute to cumulatively considerable energy impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

GEOLOGY AND SOILS

Threshold of Significance:

- Would the project directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving: (a) rupture of a known earthquake fault, as delineated on the most recent Alquist–Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area based on other substantial evidence of as known fault (Refer to Division of Mines and Geology Special Publication 42); (b) strong seismic ground shaking; (c) seismic-related ground failure, including liquefaction; or (d) landslides?

(a) Rupture of a Known Earthquake Fault

The MBTRA will be located in Southern California, a seismically active region. However, there are no known active or potentially active faults directly on or near the property, and the site is not located within an Alquist-Priolo Earthquake Fault Zone. The closest active fault is the Oceanside section of the Newport-Inglewood-Rose Canyon Fault Zone, which is approximately 6.8 miles southwest of the site. Given that there are no known active faults within close proximity and the MBTRA would adhere to recommendations from the geotechnical investigation and meet seismic design standards outlined in the California Building Code (CBC), like the project, the MBTRA would not directly or indirectly cause potential substantial adverse effects including the risk of loss, injury, or death involving: (a) rupture of a known earthquake fault, as delineated on the most recent Alquist–Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area based on other substantial evidence of as known fault (Refer to Division of Mines and Geology Special Publication 42) and would be less than significant under the MBTRA.

(b) Strong Seismic Ground Shaking

The MBTRA will exist in a region where strong seismic ground shaking could occur due to its proximity to active fault zones such as the Newport-Inglewood-Rose Canyon Fault Zone. Although strong ground shaking is a common occurrence in San Diego County and Southern California, the MBTRA would be designed to withstand seismic events. The MBTRA's design would comply with performance standards set by the International Building Code (IBC) and the CBC.

Additionally, the MBTRA would incorporate the recommendations from the geotechnical investigation to ensure that the buildings are structurally sound during seismic activity. In sum, the MBTRA would not directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving strong seismic ground shaking; impacts would be less than significant..

(c) Seismic-Related Ground Failure (Liquefaction)

According to the geotechnical investigation, the site is underlain by relatively deep, saturated alluvial deposits, which could be susceptible to liquefaction during a significant seismic event. However, like the project, conditions of approval will require the MBTRA to implement the necessary ground improvement techniques identified in the geotechnical investigation, such as rammed aggregate piers, to reduce liquefaction hazards. These improvements would densify the soil and reduce the potential for ground failure due to liquefaction. The geotechnical report requires post-treatment testing to verify that the soil has been sufficiently densified to prevent liquefaction. With these techniques in place, the MBTRA would not directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving seismic-related ground failure, including liquefaction and impacts would be less than significant.

(d) Landslides

The geotechnical investigation identifies no evidence of landslides or geotechnical instability at the property. The site is relatively flat, and the local geologic structure is conducive to stable construction. The lack of significant slopes or geological conditions conducive to landslides further reduces the potential for landslide risks. Therefore, the MBTRA would not directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving landslide and impacts would be less than significant.

In summary, the MBTRA, as designed and conditioned, would not cause significant adverse effects related to earthquake fault rupture, seismic ground shaking, liquefaction, or landslides. With adherence to geotechnical recommendations and compliance with seismic performance standards outlined in the CBC, the project would not directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving (a) the rupture of a known earthquake fault as delineated in the most recent Alquist–Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault; (b) strong seismic ground shaking; (c) seismic-related ground failure, including liquefaction; or (d) landslides. Therefore, the impacts associated with the MBTRA would be less than significant.

Threshold of Significance:

- Would the project result in substantial soil erosion or the loss of topsoil?

The MBTRA would not result in substantial soil erosion or loss of topsoil due to the implementation of effective erosion control measures during construction and operation. Potentially significant impacts due to erosion during construction would be avoided through adherence to the City's Grading Ordinance and the use of best management practices required by law such as silt fencing, soil binders, hydroseeding, and stormwater management plans (SWQMP and SWPPP) to control runoff and stabilize soils. Landscaping features incorporated throughout the site would further

reduce erosion risk by stabilizing the soils, particularly along undeveloped areas. These measures would ensure that the MBTRA does not result in substantial soil erosion or topsoil loss, and the impact would be less than significant.

Threshold of Significance:

- Would the project be located on a geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction or collapse?

Final EIR section 4.6 discloses that the property is not located on unstable geologic units or soils that would become unstable as a result of development, nor would it potentially cause on- or off-site landslides, lateral spreading, subsidence, liquefaction, or collapse. The site is relatively level, with no evidence of landslides or slope instabilities, and it is not located in an area of known subsidence. While the site is underlain by potentially liquefiable alluvial deposits, the MBTRA must comply with the geotechnical investigation's recommendations for ground improvements, such as rammed aggregate piers, to mitigate the risk of liquefaction and lateral spreading. Following these recommendations and adhering to performance standards set by the IBC and CBC, like the project, the MBTRA, would not be located on a geologic unit or soil that is unstable or that would become unstable as a result of the project and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse; impacts would be less than significant.

Threshold of Significance:

- Would the project be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property?

The MBTRA would not be located on expansive soil, as defined in the Uniform Building Code (1994). The geotechnical investigation included with the Final EIR indicates that the alluvial deposits underlying the project site have very low expansion potential and are suitable for construction. Additionally, as required by the conditions of approval and geotechnical investigation, to reduce any potential risks from expansive heave, the top 2 feet of material beneath building footings, concrete slabs-on-grade, hardscape, and site retaining wall footings would meet specific expansion index requirements of 50 or less. Therefore the MBTRA would not be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code (1994), creating substantial risks to life or property,, and impacts would be less than significant.

Threshold of Significance:

- Would the project directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?

The MBTRA would not directly or indirectly destroy a unique paleontological resource or site or unique geologic feature. The property is covered by Qya floodplain deposits, which have a low to very low sensitivity for paleontological resources, as noted in the geotechnical report included with the Final EIR. While ground-disturbing activities could potentially unearth previously unidentified resources, the preliminary evaluation did not identify any unique geologic features on the site. Therefore, the MBTRA would not directly or indirectly destroy a unique paleontological resource or site or unique geologic feature, and impacts would be less than significant.

Cumulative Impact:

Due to the localized nature of geology and soils, and the distance of the cumulative projects from the MBTRA, cumulatively considerable geology and soil impacts would not occur. Each cumulative project would be required to assess individual and site-specific geologic conditions, which would inform the construction and development of each project. Those projects would be subject to similar requirements and regulations as those imposed for the MBTRA, ensuring adherence to applicable standards and procedures. The MBTRA would not result in significant impacts to paleontological resources or from earthquakes, seismic-related ground shaking, liquefaction, landslides, erosion, expansive soils, or other geologic hazards. As the MBTRA would not result in any significant impacts to geology or soils and all cumulative projects would be required to analyze site-specific conditions and implement necessary recommendations or mitigation, the MBTRA would not contribute to cumulatively considerable geology and soil impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

GREENHOUSE GASES

Threshold of Significance:

- Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?

The City of Oceanside's CAP targets a reduction in GHG emissions to 4 MT CO₂e per capita by 2030 and 2 MT CO₂e per capita by 2050. For a development like the MBTRA that would emit more than 900 MTCO₂e of GHG, the CAP and City guidance provide for the evaluation of the significance of a project's GHG emissions based on an assessment of a project's consistency with the CAP and, where applicable, the CAP Consistency Checklist. As these findings, the Final EIR and the record of proceedings demonstrate, the MBTRA would not generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment. Similar to developments of comparable size and operational characteristics, the MBTRA would generate GHG emissions similar to those of the project during both construction and operation. As disclosed in the Final EIR, the project and the MBTRA would generate annual GHG emissions of approximately 7,172 MT CO₂e. Per the CAP and related City guidance, as the MBTRA would emit more than 900 MT CO₂e annually, the MBTRA could have a considerable contribution to cumulative climate

change impacts if it is unable to demonstrate consistency with the CAP Consistency Checklist. Only developments that meet one or more of the CAP established threshold criteria are eligible to rely on the CAP Consistency Checklist to demonstrate less than significant GHG impacts. The MBTRA satisfies the third criteria as an industrial development on a property designated for that use that conforms to the current land use and zoning designation. Thus, the MBTRA is eligible to use the CAP Consistency Checklist. Relevant to that checklist, the MBTRA and the project are the same. Therefore, as the Final EIR demonstrates for the project, the MBTRA conforms to the applicable CAP Checklist items including, without limitation, compliance with Municipal Code Section 3047 requirements related to renewable energy generation, provision of EV charging stations and parking and providing permeable areas and urban tree canopies at percentages consistent with Municipal Code section 3049. Accordingly, the MBTRA would not generate GHG emissions, either directly or indirectly, that may have a significant impact on the environment and impacts would be less than significant.

Threshold of Significance:

- Would the project generate conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?

As the Final EIR, record of proceedings and preceding finding demonstrate, the MBTRA, similar to the project, is consistent with the City of Oceanside's CAP and implementing plans and regulations. At the regional level, SANDAG's 2021 updated Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS or Regional Plan) was adopted consistent with State law for the purpose of reducing GHG emissions attributable to passenger vehicles in the San Diego region. The RTP/SCS is not directly applicable to the MBTRA because the RTP/SCS's underlying purpose is to provide direction and guidance on future regional growth (i.e., the location of new residential and nonresidential land uses) and transportation patterns throughout the City and greater San Diego County as contemplated by Senate Bill 375. CARB has recognized that the approved RTP/SCS is consistent with Senate Bill 375. As the growth projections and GHG emissions that underlie the Regional Plan are generally consistent with the local government plans such as the City's General Plan, and the MBTRA proposes industrial development consistent with the intensity allowed by the General Plan and zoning, the MBTRA would not result in growth or GHG emissions that would conflict with the Regional Plan. Therefore, impacts are less than significant as the MBTRA will not conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of GHG.

Cumulative Impact:

GHG impacts, by nature, are cumulative. As described above, the MBTRA would not generate GHG emissions, either directly or indirectly, that may have a significant impact on the environment nor would the MBTRA conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases. The MBTRA conforms to the applicable CAP Checklist

items, and at the regional level, the MBTRA proposes industrial development at an intensity consistent with applicable City plans and the RTP/SCS. Like the project, the MBTRA would not contribute to cumulatively considerable GHG impact as disclosed in Final EIR and elsewhere in the record of proceedings.

HAZARDS AND HAZARDOUS MATERIALS

Threshold of Significance:

- Would the project create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?

As the Final EIR, record of proceedings and preceding finding demonstrate, the MBTRA would involve similar construction and operational activities as the project, including the transport, use, and disposal of hazardous materials such as diesel fuel, gasoline, equipment fluids, solvents, and adhesives during construction, as well as industrial chemicals and cleaning products during operation. However, the MBTRA would adhere to the same rigorous federal, state, and local regulations that govern the handling, transport, and disposal of hazardous materials. These regulations include the Federal Chemical Accident Prevention Provisions, California's Hazardous Waste Control Law, and specific guidelines for the transportation of hazardous materials, all of which would ensure that any hazardous materials are managed safely and in compliance with industry standards. Additionally, operational activities, such as the use of industrial chemicals and fertilizers, would be conducted in accordance with the manufacturer's recommendations and stored properly to minimize risks. Therefore, through adherence to these safety standards and mandatory regulatory compliance, the MBTRA would not create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials, and potential impacts would be less than significant.

Threshold of Significance:

- Would the project create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?

The MBTRA, similar to the project, would be subject to the same regulations and safety protocols designed to prevent hazardous material releases during construction and operation. Due to the historical industrial use of the site, as described in Final EIR Table 4.8.1 and Section 4.8, there are recognized environmental conditions (RECs) related to the presence of hazardous materials such as petroleum and volatile organic hydrocarbons (VOCs). However, these potential concerns would be managed in accordance with federal, state, and local regulations including those related to the removal of contaminated soils and clean-up of the property pursuant to approvals to be granted by CEQA Responsible Agency the Department of Toxic Substances Control (DTSC). During

construction, hazardous materials like diesel fuel, gasoline, and cleaning solvents would be used, but spill containment measures enforced by the Oceanside Fire Department (OFD), along with mandatory compliance with hazardous materials regulations, would minimize the risk of any accidental release. Similarly, during operations, industrial chemicals and other hazardous materials would be stored and handled according to the manufacturer's guidelines and applicable regulations, with permitted and licensed service providers managing their transport, removal, and disposal. As a result, with adherence to stringent safety protocols and other measures required by law, the MBTRA would not create a significant hazard to the public or the environment through reasonably foreseeable upset or accident conditions involving the release of hazardous materials into the environment, and impacts would be less than significant.

Threshold of Significance:

- Would the project emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?

The MBTRA would not emit hazardous emissions or handle hazardous materials within one-quarter mile of an existing or proposed school. The existing Teri Learning Academy is located approximately 0.21 miles from the property. The site has been identified as containing RECs from its historical industrial use. Soil removal and remediation would be completed in compliance with applicable law. That work is necessary independent of implementation of the MBTRA. Additionally, the school is located across State Route 76, beyond the Oceanside Municipal Airport and other industrial uses, providing physical separation and further reducing the potential for a significant impact. To the extent applicable, the MBTRA would also adhere to strict local, state, and federal regulations regarding the transport, use, storage, and disposal of hazardous materials including those enforced by the DTSC. With compliance with all necessary environmental safeguards required by law, including the Phase I Environmental Site Assessment (ESA) and site remediation efforts, the MBTRA would not emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school.

Threshold of Significance:

- Would the project be located on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?

The MBTRA would be developed on a property with historical environmental concerns due to past industrial activities, which involved the use of hazardous materials such as petroleum hydrocarbons, volatile organic compounds, metals, and per- and polyfluoroalkyl substances. The property is included on hazardous materials lists under Government Code Section 65962.5, reflecting its prior industrial uses, including metal plating and hazardous waste treatment. A Response Plan addressing the hazardous substance information disclosed in the Final EIR would be reviewed and approved by

DTSC prior to construction of the MBTRA. The Response Plan must satisfy the regulatory requirements of DTSC under the state regulatory process known as California Land Reuse and Revitalization Act. The Response Plan sets forth remedial action objectives (RAOs) for the site that are based on the future planned industrial use. The Response Plan contemplates site remediation activities including, without limitation, the removal of certain contaminated soils from the property. As with the project, those site remediation activities, including the remedial grading and disposal of contaminated soils, are within the scope of the MBTRA construction and grading operations described and analyzed throughout the Final EIR. The Response Plan and RAOs must comply with the CLRA and DTSC regulatory scheme specifically adopted to protect, and avoid significant hazards to, public health and the environment. After completion of remedial action and approval of the completion report by DTSC, the property would be deemed suitably remediated and it may be released for industrial/commercial usage. Additionally, the MBTRA would comply with other applicable local, state, and federal regulations described in the Final EIR and record of proceedings, ensuring that the site is adequately prepared for industrial use. As such, although on a site that is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5, the MBTRA would not create a significant hazard to the public or environment, and impacts would be less than significant.

Threshold of Significance:

- For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?

The MBTRA property is located adjacent to the Oceanside Municipal Airport and falls within several safety zones outlined in the Airport Land Use Compatibility Plan (ALUCP). Specifically, portions of the site fall within safety zones 1, 2, 3, 5, and 6. However, the MBTRA would not develop any habitable structures within Safety Zone 1, which prohibits such uses, and the ALUCP allows warehouse and distribution facilities in Safety Zones 2 through 6. As such, the MBTRA is consistent with the land use compatibility criteria for these zones.

Regarding noise exposure, the ALUCP specifies that the majority of the site falls within a 60 dB noise contour, with a small portion in the 65 dB contour. Both noise levels are compatible with warehouse and distribution uses according to the ALUCP and City noise thresholds for such uses. Additionally, as analyzed in Final EIR section 4.11, like the project, the MBTRA will not exceed the City's noise standards during construction or operations.

The site is also located within Review Area 1 of the ALUCP, which may have limitations due to noise and safety concerns. The Airport Land Use Commission issued a Consistency Determination for the larger project. The MBTRA proposes the same types of uses and less overall intensity and building square footage compared to the project. Further, like the project, the MBTRA would comply with all necessary regulatory requirements, including recordation of an overflight

notification, to ensure consistency with the ALUCP. Based on the MBTRA's design, compliance with ALUCP safety, noise and other requirements, the MBTRA would not result in a safety hazard or excessive noise for people residing or working in the project area relative to the Oceanside Municipal Airport and ALUCP; impacts would be less than significant.

Threshold of Significance:

- Would the project impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?

The MBTRA would not impair implementation or physically interfere with any adopted emergency response plan or emergency evacuation plans. Compared to the project, the MBTRA proposes fewer truck bays, the same type of uses and a slight reduction in the intensity of development. Thus, the potential for the MBTRA to have significant impacts relative to an adopted emergency response plan or emergency evacuation plan would be the same as the project. Like the project, as analyzed in Final EIR section 4.8, the MBTRA's General Plan consistent industrial use would not impair implementation or physically interfere with existing emergency plans such as the San Diego County Multi-Jurisdictional Hazard Mitigation Plan, the San Diego County Emergency Operations Plan (EOP), and the City of Oceanside EOP. The MBTRA is located outside of the tsunami evacuation area, as identified in the City's tsunami evacuation map, and would not interfere with any designated evacuation routes. The MBTRA would provide two access points for emergency responders and would not require closure of public or private streets during construction or operations. Additionally, the MBTRA would meet all emergency access requirements set by the OFD. The Wildfire Evacuation Study included with the Final EIR provides further support for the determination regarding consistency with relevant emergency evacuation plans and emergency response plans. That study discloses the prevention and minimization regulations and measures applicable to the MBTRA, and documents evacuation times for the existing and post-project conditions. The study describes additional emergency preparedness information and practices related to efficient evacuation of the property and the surrounding area in the event of an emergency. In sum, the MBTRA would not impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan.

Threshold of Significance:

- Would the project expose people or structures, either directly or indirectly, to a significant risk of loss, injury, or death involving wildland fires?

The MBTRA site is located within a Local Responsibility Area Very High Fire Hazard Severity Zone (VHFHSZ). For the reasons described in the Final EIR, including Sections 4.8, 4.17 and Appendix N, including the site's existing development, relatively flat topography, and the obligation under law that the MBTRA adhere to updated building standards, like the project, the MBTRA is not expected to increase wildfire risks. By way of example, the San Luis Rey River Trail provides a

physical break between the site and adjacent wildland areas, and the MBTRA would include a 100-foot buffer from the San Luis Rey River corridor that will be planted on-site with native plant species with lower wildfire risk potential

As disclosed in the Final EIR, the Wildfire Evacuation Study recognizes that the property is in a designated VHFHSZ, but not immediately adjacent to wildland areas. Development to the south, east, and west of the site includes various infrastructure and urban developments, while the northern boundary is separated from the San Luis Rey River by the bike trail/levee. Like the project, the MBTRA would include ignition-resistant construction, native landscaping, and 100 feet of fuel modification, as required in high-risk fire zones. Additionally, the study's modeling takes into consideration nearby residential communities, proximity to open space areas, the capacity of applicable roadways, EOPs, and protocols utilized by the authorities responsible for issuing evacuation orders and warnings, and features of the proposed development that help lessen wildfire risks associated with the construction and operation. In sum, the analysis demonstrates that, similar to the project, the MBTRA's evacuation scenarios and considers emergency response plans, confirming that the MBTRA would not expose people or structures, either directly or indirectly, to a significant risk of loss, injury, or death involving wildland fires and impacts would be less than significant.

Cumulative Impact:

Past, present, and reasonably foreseeable projects will result in the use and transport of oils, greases, and petroleum products for operational purposes. While there is potential for accidental spills, these events are random and unrelated to one another. Provided that individual projects, including the MBTRA, comply with regulations governing the storage, transportation, and handling of hazardous materials, the cumulative effect on human health and safety would not be significant. All such projects would be required to assess existing hazardous materials on site and follow regulations for their management, use, and disposal. The MBTRA's construction phase may involve hazardous materials from debris, landscaping, and commercial products, but the MBTRA will adhere to federal, state, and local laws such as the California Health and Safety Code, Hazardous Waste Control Act, and OSHA requirements, all of which regulate hazardous materials management and aim to minimize public health risks. The MBTRA's construction and operation, even though on property identified on the Cortese list, would not cumulatively contribute to significant impacts relative to hazardous emissions, materials or substances. Cumulative projects within the vicinity of the Oceanside Municipal Airport and subject to ALUCP, similar to the MBTRA, would also have to comply with applicable standards and requirements to avoid safety hazard or excessive noise for people residing or working in the applicable area. Like the MBTRA, cumulative projects within a Fire Hazard Severity Zone, must also meet fire fuel modification and clearing requirements, as well as fire code standards, which would be reviewed and approved by the fire marshal for each individual development, to avoid significant impacts. Thus, the MBTRA would not contribute to cumulatively

considerable hazards and hazardous materials impacts as disclosed in Final EIR and elsewhere in the record of proceedings

HYDROLOGY AND WATER QUALITY

Threshold of Significance:

- Would the project violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?

The MBTRA would not violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality. The MBTRA involves modifications to the project that would reduce the overall building square footage and the number of truck bays compared to the original design. The Final EIR includes a drainage study and storm water quality management plan for the MBTRA. Like the project, the MBTRA would exceed City Municipal Code requirements for the tree canopy and pervious surface area requirements. During construction, the MBTRA must comply with the NPDES State Water Resources Control Board Construction General Permit Order No. 2009-0009-DWQ. The Order regulates stormwater discharges and general construction activities and incorporates standard BMPs such as regular cleaning or sweeping of construction areas and impervious areas, and runoff controls. In compliance with the Order, and applicable law, the MBTRA must prepare and implement a development specific Stormwater Pollution Prevention Plan (SWPPP) that specifies BMPs that the MBTRA must implement during construction to minimize impacts to water quality. As required by applicable laws, construction and operation of the MBTRA must implement the City approved final Storm Water Quality Management Plan (SWQMP) that substantially conforms to the plan included in the Final EIR. That final plan will require the MBTRA (including the off-site improvements in Benet Road) to include a combination of structural BMPs, site design BMPs, and source control BMPs that provide post-construction pollutant controls, reducing potential operational impacts related to water quality standards or waste discharge. Similar to the project, the MBTRA site design includes a new storm water conveyance system on-site, which would consist of ribbon gutters, curb and gutter, and a detention vault system. The vault system incorporates modular wetlands for treatment and a force main pump to convey storm water to the existing storm drain located in Benet Road and into an existing storm water structure to the northwest side of the site which drains to the San Luis Rey River Basin. Therefore, through the MBTRA's compliance with applicable laws, conditions of approval and plans, the MBTRA would not violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality; impacts would be less than significant.

Threshold of Significance:

- Would the project substantially decrease groundwater supplies or interfere substantially with groundwater discharge such that the project may impede sustainable groundwater management of the basin?

The MBTRA would not substantially decrease groundwater supplies or interfere substantially with groundwater discharge such that the MBTRA may impede sustainable groundwater management of the basin. The MBTRA would not use groundwater during construction or operations. Like the project, the MBTRA would have less impervious surface area than the maximum percentage established by the City Municipal Code. Although the MBTRA has a smaller building footprint, like the project the MBTRA will be located within the boundaries of the Lower San Luis Rey Valley Groundwater Basin, and the groundwater table is relatively shallow (around 7 to 7.5 feet below ground surface). Like the project, the MBTRA would implement a combination of structural BMPs, site design BMPs, and source control BMPs to provide post-construction pollutant control according to requirements for Priority Development Projects (PDPs) identified in the City of Oceanside BMP Design Manual. These systems would reduce pollutants that could potentially degrade groundwater quality prior to recharge. Thus, like the project, the MBTRA design and operations would not substantially decrease groundwater supplies or interfere substantially with groundwater recharge in a way that may impede sustainable groundwater management of the basin.

Threshold of Significance:

- Would the project substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner which would (i) result in substantial erosion or siltation on or off site; (ii) substantially increase the rate or amount of surface runoff in a manner which would result in flooding on or off site; (iii) create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or (iv) impede or redirect flood flows?

(i) Similar to the project, the MBTRA would implement BMPs during construction, as required by the City's regulations and the NPDES Construction General Permit. Construction BMPs described in the SWPPP include, but are not limited to, measures minimizing exposed soils, silt fencing, soil binders, street sweeping, hydroseeding soils, and using sandbags, check dams, or berms during rain events to direct flows. Surface drainage during construction would be controlled through implementation of the SWQMP and SWPPP required by the NPDES regulations and provisions of the City's Grading and Erosion Control Ordinances. During operations, like the project, the MBTRA would implement a new storm water conveyance system that includes ribbon gutters, curb and gutter, and a detention vault system with modular wetlands for treatment and a force main pump to convey storm water to the existing storm drain located in Benet Road and into an existing storm water structure to the northwest side of the site which drains to the San Luis Rey River Basin. Just like the

project, the MBTRA must comply with the City's Erosion Control Ordinance and implement structural BMPs (biofiltration facilities and underground detention vault) to minimize the potential for excessive downstream erosion in receiving waters. Landscaped areas of the MBTRA would also remove sediment and particulate-bound pollutants from stormwater prior to leaving the property. Therefore, the MBTRA would not substantially alter the drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner that would result in substantial erosion or siltation on or off site; impacts would be less than significant.

(ii) Like the project, the MBTRA would incorporate a stormwater conveyance system designed to manage runoff, including biofiltration and underground storage vaults and avoid an increase in peak flow rates. The hydrology analysis for the MBTRA included in the Final EIR, like the analysis for the project, demonstrates that the MBTRA's new stormwater conveyance and detention system would control flows during the peak of a 100-year, 6-hour storm event to predevelopment conditions. Thus, the MBTRA will not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces in a manner that would result in flooding on or off site; impacts would be less than significant impact on runoff.

(iii) As with the project, the smaller footprint of the MBTRA would not exceed the capacity of the existing stormwater infrastructure, as the MBTRA's stormwater conveyance system would ensure that peak flows do not exceed those of the predevelopment condition. With installation of the MBTRA's stormwater drainage facilities along with the underground detention facilities, as the MBTRA's hydrology analysis illustrates, the MBTRA would treat flows in a manner that would not increase flows such that the capacity of existing or planned stormwater drainage systems are exceeded.

As addressed in preceding findings, during construction, the MBTRA must comply with the Construction General Permit Order, SWQMP, SWPPP, and BMPs. Compliance with the applicable regulatory requirements and MBTRA specific plans, including those that retain and treat runoff before discharge, ensures MBTRA construction would not result in substantial additional sources of polluted runoff. During operation, as required by applicable laws, the MBTRA must implement the SWQMP and operate a combination of structural BMPs, site design BMPs, and source control BMPs, as described in the Final EIR and the record of proceedings, that will achieve provide post-construction pollutant controls so that the MBTRA would not result in substantial additional sources of polluted runoff. In sum, like the project, the MBTRA would not substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner that would create or contribute runoff water that would exceed the capacity of existing or planned stormwater drainage facilities or provide substantial additional sources of polluted runoff, impacts would be less than significant.

(iv) The MBTRA would not impede or redirect flood flows. The property is within a Special Flood Hazard Area (Zone A99) and protected by levees and a perimeter flood wall around the boundary of the entire property. The site and development specific hydrology analysis included in the Final EIR confirms that the MBTRA would not increase water surface elevation during a 100-year flood event. Additionally, the MBTRA's stormwater detention and treatment systems would manage runoff such that water would not be diverted away from existing drainage patterns, and the MBTRA's peak runoff would not have an adverse effect on the downstream watershed and existing infrastructure. Therefore, like the project, the MBTRA would not alter the course of a stream or river or through the addition of impervious surfaces in a manner that would impede or redirect flood flows; impacts would be less than significant.

Threshold of Significance:

- In flood hazard, tsunami, or seiche zones, would the project risk release of pollutants due to project inundation?

The property is located in Special Flood Hazard Area Zone A99 according to FEMA, which is within a 100-year floodplain that is protected by levees along the San Luis Rey River. Although not yet certified by FEMA, the levees are already in place to provide protection, and the hydrology analysis included in the Final EIR demonstrates that water surface elevation during a 100-year flood event would remain the same for both the existing and proposed conditions. The MBTRA's perimeter wall around the property boundary provides additional flood protection. All those features, coupled with the MBTRA's stormwater conveyance system and BMPs described in previous findings, reduce to less than significance the potential risk of the release of pollutants from the site in the event of a flood. Regarding the risk of tsunami related inundation, the MBTRA would not be located within the tsunami inundation area, as per the Tsunami Inundation Map for Emergency Planning (CalEMA 2009). Because no enclosed or partially enclosed bodies of water exist in the vicinity of the property, the MBTRA is also not proposed within a seiche zone. Therefore, similar to the project and consistent with the SWQMP and Hydrology Study, the MBTRA would not risk release of pollutants due to project inundation in a flood hazard zone and impacts would be less than significant.

Threshold of Significance:

- Would the project conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan?

The MBTRA would not conflict with or obstruct the implementation of a water quality control plan or sustainable groundwater management plan, similar to the original project design.

The project site is within the San Luis Rey River Watershed Water Quality Improvement Plan (WQIP) area, which aims to protect, preserve, and enhance water quality in the watershed. Like the project with its similar uses and larger building footprint, the MBTRA is consistent with the goals

of the WQIP as it complies with the relevant regulations designed to manage water quality. Specifically, the MBTRA adheres to the San Diego Regional Water Quality Control Board's NPDES MS4 Permit and the City of Oceanside's BMP Design Manual, which includes a comprehensive SWQMP. The MBTRA incorporates appropriate BMPs to reduce water quality pollutants during both construction and operational phases, including a stormwater conveyance system that collects, filters, and treats runoff before discharging it.

Regarding groundwater management, the site is not located within the San Luis Rey Valley, which is a medium-priority basin or any other basin subject to the Sustainable Groundwater Management Act (SGMA). As a result, the MBTRA does not fall under a mandatory Groundwater Sustainability Plan. The MBTRA does not involve the use or extraction of groundwater and, as address in the Final EIR and preceding findings, does not significantly impact groundwater resources. Therefore, the MBTRA would not conflict with or obstruct implementation of any applicable sustainable groundwater management plan.

In conclusion, similar to the project design, the MBTRA would not conflict with or obstruct implementation of a water quality control plan or sustainable groundwater management plan and impacts would be less than significant.

Cumulative Impact:

The MBTRA, like the project, would have less than significant, direct hydrology and water quality impacts as the MBTRA's construction, design and operation must implement all applicable hydrology and water quality management plans, strategies, measures and regulations described in the Final EIR and these findings. Like the MBTRA, each cumulative project applicant would be required to comply with the same, including by hydrologically engineering the respective cumulative project sites to ensure that post-development surface runoff flows can be accommodated by the regional drainage system, as required by applicable laws, implement BMPs and project-specific measures outlined in the project-specific Storm Water Quality Management Plan and Drainage Report required by law and otherwise ensure compliance with federal, state and City flood hazard, water quality control and groundwater standards and plans. Therefore, like the project, the MBTRA would not contribute to cumulatively considerable hydrology and water quality impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

LAND USE AND PLANNING

Threshold of Significance:

- Would the project physically divide an established community?

The site is located within the Airport Neighborhood Area of Oceanside and is bordered by the Oceanside Municipal Airport to the south, Benet Road to the west, the San Luis Rey River and

recreational trail to the north, and vacant light industrial land to the east. The site is approximately 900 feet north of the Highway 76 corridor. The land is currently zoned Limited Industrial (IL), consistent with the Light Industrial (LI) designation in the General Plan and an industrial facility operated for decades on the property. Like the project, the MBTRA would involve the development of a warehouse and distribution facility that would be in harmony with the surrounding industrial uses and Oceanside Municipal Airport. The MBTRA does not involve the construction of any significant infrastructure (e.g., highways, railroads) that would physically divide the community or impede mobility. Furthermore, the MBTRA improvements would not obstruct or restrict access to adjacent properties or roadways.

Similar to the project, as an infill development situated in a highly developed area, the MBTRA is consistent with both the General Plan and Zoning designations and it would not create any physical barriers within the existing community. The proposed development would not hinder community connectivity or impede access to other areas. As such, the MBTRA would have not physically divide an established community. Impacts would be less than significant.

Threshold of Significance:

- Would the project cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?

The MBTRA is subject to several local and regional plans intended to avoid or mitigate environmental effects. These plans, policies, and regulations are contained within the City's General Plan, Zoning Ordinance, the draft Oceanside Subarea Plan of the North County MHCP, the ALUCP, the San Luis Rey Watershed WQIP, and SDAPCD. As the Final EIR demonstrates, and as the MBTRA and project are similar in design, use and operations as it relates to the consistency analysis related to those plans, policies and regulations, the MBTRA would not cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect.

City of Oceanside General Plan

The MBTRA proposes a reduction in the total building square footage and size and fewer truck bays compared to the project design. Like the project, as disclosed in Final EIR Table 4.10.1, the MBTRA aligns with the City's General Plan as it maintains the Light Industrial (LI) land use. The proposed warehouse and distribution facility under the MBTRA is consistent with the applicable General Plan's policies and goals, which support industrial uses in areas designated as Light Industrial. The reduction in building size and truck bays does not change the less than significant General Plan land use compatibility determinations relative to goals and policies adopted for the purpose of avoiding or mitigating an environmental effect.

City of Oceanside Zoning Ordinance

The MBTRA would be consistent with the City's Zoning Ordinance, which designates the site as Limited Industrial (IL), corresponding with the General Plan's Light Industrial (LI) designation. The Limited Industrial zone allows for warehouse, storage, and distribution facilities, including truck bays. With the approval of the Development Plan, CUPs and variance, like the larger project, the MBTRA complies with zoning regulations and would not result in a conflict with the Zoning Ordinance regulations adopted for the purpose of avoiding or mitigating an environmental effect.

Oceanside Subarea Plan of the North County MHCP

The MBTRA is proposed for a property located in an area that the draft SAP contemplates for development and is not in an area designated for conservation. Like the project, the MBTRA would provide the 100-foot biological buffer from the San Luis Rey River consistent with the goals of the draft SAP. Similar to the project, as addressed in Final EIR Sections 4.2 and 4.10, the MBTRA would adhere to the biological resource avoidance and protection requirements of the SAP.

Oceanside Municipal Airport Land Use Compatibility Plan (ALUCP)

The project site is located within Review Area 1 of the Oceanside Municipal ALUCP, which requires compliance with policies regarding noise and safety. With its smaller footprint and lower intensity compared to the project, the Final EIR's determination that the project would comply with the ALUCP's airspace height limits, setback requirements, noise compliance zones and other elements of the ALUCP also apply to the MBTRA. Thus, the MBTRA would not conflict with an ALUCP policy or requirement adopted for the purpose of avoiding or mitigating an environmental effect.

San Luis Rey Watershed Water Quality Improvement Plan (WQIP)

The MBTRA would comply with the San Luis Rey Watershed WQIP. As was the case with the project, the MBTRA's SWQMP as part of the NPDES MS4 Permit requirements, would incorporate BMPs to reduce water pollutants and control runoff. The reduction in building and truck bay area would not significantly alter the stormwater runoff pattern nor the effectiveness of the BMPs as disclosed for the project. Thus, the MBTRA complies with the WQIP's goals for water quality improvement and standards adopted for the purpose of avoiding or mitigating an environmental effect.

San Diego Air Pollution Control District (SDAPCD)

The MBTRA would be consistent with the SDAPCD regulations and regional clean air plans. Like the larger project, the MBTRA is consistent with the SIP and RAQS adopted for the purpose of avoiding or mitigating an environmental effect, and regional air quality projections account for the type and intensity of development proposed by the MBTRA.

In sum, like the project, the MBTRA would have less than significant land use planning impacts as the MBTRA would not cause a significant environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect.

Cumulative Impact:

The MBTRA, like the project, is subject to the goals and policies outlined in the City of Oceanside's General Plan and other plans, policies and regulations discussed in these findings and the Final EIR. Like the project, the MBTRA would not physically divide a community or conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect. Thus, the MBTRA will have less than significant, direct land use and planning impacts. Like the MBTRA, all other cumulative projects would be subject to the goals and policies of the General Plan and all other plans, policies and regulations adopted for the purpose of avoiding or mitigating an environmental effect, as applicable. Like the MBTRA, consistency of those cumulative projects with applicable plans, policies and regulations would ensure those projects do not result in significant land use and planning impacts. Therefore, like the project, the MBTRA would not contribute to a cumulatively considerable land use and planning impact as disclosed in Final EIR and elsewhere in the record of proceedings.

NOISE

Threshold of Significance:

- Would the project result in generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?

Short-Term Construction Noise

For both the project and 4-building MBTRA, as demonstrated in the Final EIR's appendices, the construction distance to the nearest sensitive receptors remains constant. With this similar distance, the MBTRA's construction noise levels experienced at sensitive receptors would be the same as for the project. Like the project, construction noise will be generated by equipment such as graders, backhoes, forklifts, cranes, and rollers. Noise levels would depend on the specific construction phase, with the highest noise levels predicted to occur during grading and site preparation, particularly near the northern boundary of the site.

The analysis for the project did not identify any exceedances of applicable noise standards at nearby receptors. The analysis, using predictive modeling based on Federal Highway Administration (FHWA) construction noise guidelines, predicts that the construction noise levels at the nearest sensitive receptors (residences on Tishmal Court) will not exceed 60 dBA Leq over an 8-hour period. Similar to the project, MBTRA generated construction noise would remain well below the FTA's

80 dBA Leq significance threshold over an 8-hour period. Therefore, the MBTRA construction would not result in the generation of a substantial temporary increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies, the MBTRA's construction noise impacts would be less than significant

Long-Term Operational Noise

Off-Site Traffic Noise Exposure: The MBTRA, because of its reduced square footage of each use, would generate fewer vehicles and accordingly lower traffic noise than the project.

Traffic noise modeling based on FHWA Traffic Noise Model (TNM 2.5) demonstrates that the project traffic noise levels would not exceed the applicable roadway significance threshold. As disclosed in Appendix H-1, with its reduced traffic generation, the MBTRA would have less than significant traffic noise impacts.

On-Site Project Noise Emission: As the MBTRA proposes a smaller building, fewer truck bays and most (but not all) truck bays facing other buildings as opposed to the San Luis Rey River and residential development to the north, the operational noise emissions would be reduced compared to the project. The analyzed operational noise analysis took into consideration elements such as outdoor mechanical equipment, parking lot activity and trucking operations. As demonstrated by the analysis in Final EIR Appendix H-1, MBTRA operational noise levels at the nearest residential receptors will be below 40 dBA, while at the San Luis Rey River trail north of the property, noise levels will remain below 50 dBA. As disclosed in Final EIR Section 4.11, the project would have resulted in less than significant operational noise impacts under the applicable thresholds even though the project's operational noise emissions were predicted to be higher those of the MBTRA. The Final EIR demonstrates that the project would have less than significant construction and operational noise impacts under applicable thresholds. The MBTRA would result in reduced construction-related noise and operational noise impacts compared to the project. Therefore, the MBTRA would not result in generation of a substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies.

Threshold of Significance:

- Would the project result in generation of excessive groundborne vibration or groundborne noise levels?

Construction Vibration and Noise

Construction vibration impacts associated with the project were found to be less than significant at nearby sensitive receptors. The 4-building MBTRA would generate similar construction vibration levels given the similarities in the type and amount of construction for the developments. Like the

project, construction activities under the MBTRA would involve the use of heavy machinery such as bulldozers, backhoes, and cranes.

Groundborne vibration associated with construction equipment attenuates quickly as it travels through the ground. Based on guidance from Caltrans and FTA, the typical vibration from heavy equipment like bulldozers at a reference distance of 25 feet is approximately 0.089 ips. Given the at least 600-foot distance from the northern boundary of the property to the nearest residential properties, the estimated peak particle velocity (PPV) would decrease significantly with distance. The Final EIR discloses that the project would have a predicted level of 0.003 ips of groundborne vibration, well below the significance threshold that would cause annoyance (around 0.2 ips) or damage (around 0.3 ips). Therefore, like the project, the MBTRA construction would not result in generation of excessive groundborne vibration or groundborne noise levels and impacts would be less than significant.

Operational Vibration and Noise

Given the similarities of their industrial operations, the MBTRA and project would generate similar groundborne vibration and noise. Just as disclosed in the Final EIR for the project, the MBTRA would involve activities that may generate minimal vibration or noise, including the use of mechanical systems such as HVAC units and occasional truck movements. Therefore, like the project, operation of the MBTRA would not result in generation of excessive groundborne vibration or groundborne noise levels and impacts would be less than significant.

Threshold of Significance:

- For a project located within the vicinity of a private airstrip or an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project expose people residing or working in the project area to excessive noise levels?

The project site is located in close proximity to the Oceanside Municipal Airport, which is located directly south of the property boundary. According to the ALUCP, the project site falls within the 60 dB CNEL and 65 dB CNEL noise contours (San Diego County Regional Airport Authority 2010). The CNEL metric, which accounts for noise levels over an entire day, is used to assess the potential for significant noise impacts. Using the City's threshold of significance for industrial developments, as the project proposes no residential development, a significant impact would occur in the daytime if levels exceed 70 dB CNEL or the nighttime levels exceed 65 dB CNEL. For purposes of this analysis, the project and the MBTRA would experience the same level of noise due to airport operations. Thus, as the highest noise level relative to the airport is 65 dB CNEL, impacts would be below the applicable thresholds. Further, areas within the development with offices and other enclosed spaces are located primarily within the 60 dB CNEL area. Thus, the MBTRA would not expose people residing or working in the project area to excessive noise levels relative to the

property's location within the Oceanside Municipal Airport ALUCP and impacts would be less than significant. Additionally, as this analysis of noise represents an impact of the environment on the project, there would be no cognizable impact under CEQA.

Cumulative Impact:

Regarding cumulative construction noise, groundborne vibration and groundborne noise, given the substantial distance and intervening topography and structures between the MBTRA and cumulative projects and the sensitive receptors in proximity to the same, like the project, MBTRA construction and all forms of groundborne vibration and noise, would not result in a cumulative construction impact with respect to any of the thresholds of significance addressed in the preceding direct impact findings. Regarding operational noise, as a result of that substantial distance and intervening topography and structures between the MBTRA and the cumulative projects and in accordance with the principles of sound propagation, the MBTRA's operational noise and off-site traffic noise would not result in an exceedance of the applicable noise significance threshold nor would they be cumulatively considerable with respect to any of the thresholds of significance addressed in the preceding direct impact findings. Further, similar to the MBTRA, cumulative projects would be required to comply with the applicable City noise standards to reduce any potentially significant cumulative noise impacts to a level below significance. Thus, like the project, the MBTRA would not contribute to cumulatively considerable noise impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

POPULATION AND HOUSING

Threshold of Significance:

- Would the project induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?

The project site is zoned Limited Industrial (IL) and designated Light Industrial (LI) in the General Plan, allowing industrial uses such as warehouse, storage, and distribution facilities. The MBTRA would generate additional employment, but on a smaller scale than the project. As a development consistent with the General Plan, and at significant lower intensity than what is allowed by the Zoning Ordinance, implementation of the MBTRA would result in planned growth under the 2021 Regional Plan and the General Plan. The development replaces an outdated industrial facility with a more efficient operation, supporting the City's goal to address industrial land shortages, without requiring substantial infrastructure improvements. The MBTRA would utilize existing infrastructure and does not propose major upgrades that could induce unplanned growth. Any new on-site utilities would be specific to the development and would not facilitate broader growth in the area. Overall, like the larger project, the MBTRA will create jobs but will not induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses)

or indirectly (for example, through extension of roads or other infrastructure. Therefore, impacts are less than significant.

Threshold of Significance:

- Would the project displace substantial numbers of existing people or housing?

The property is currently vacant and has never been used for residential purposes. The previous outdated industrial building was demolished and there are no residents or housing that the MBTRA would displace. Thus, like the project, the MBTRA would not displace substantial numbers of existing people or housing and impacts would be less than significant.

Cumulative Impact:

The MBTRA would not result in significant, direct population and housing impacts as the MBTRA, like the project, is consistent with the existing general plan land use designation and zoning for the property and SANDAG growth projections such that the MBTRA would not result in unplanned population growth. As stated above the MBTRA would not displace a substantial number of people or housing. The cumulative projects, like the MBTRA, would similarly have to comply with applicable population and housing thresholds of significance. Therefore, the MBTRA, like the project, would not contribute to cumulatively considerable population and housing impacts as disclosed in Final EIR and elsewhere in the record of proceedings.

PUBLIC SERVICES

Threshold of Significance:

- Would the project result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the public services?

As the following, the Final EIR and the record of proceedings demonstrate, the MBTRA would have less than significant impacts as the MBTRA would not result in substantial adverse physical impacts associated with the provision of new or physically altered governmental facilities, need for new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any of the public services.

Fire Protection:

The MBTRA is consistent with the site's industrial zoning and General Plan designation. Like the project, the MBTRA with its reduced level of intensity may slightly increase demand for fire protection compared to existing circumstances. The property is located in a developed area already served by Fire Station 7 that is 0.7 miles away. Like the analysis demonstrates for the project, for the MBTRA, Fire Station 7 could respond to an emergency anywhere on the property within 4 to 5 minutes. Fire Station 3 is located approximately 1.75 miles away and OFD's response time to development on the property would be approximately 5 minutes. Just like the project, the MBTRA would place a slight increase in demand for fire protection services in comparison to existing conditions; however, it would not result in the need for new fire personnel or equipment or require construction of a new station or expansion of existing fire facilities as the MBTRA can be adequately served by existing fire stations. Further, the project will contribute to fire service funding through taxes and impact fees, helping maintain service levels. Thus, the MBTRA would not result in substantial adverse physical impacts associated with the provision of new or physically altered fire protection facilities or the need for new or physically altered fire protection facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for fire protection services and impacts would be less than significant.

Police Protection:

Similar to fire protection, like the project would have, the MBTRA would slightly increase demand for police services compared to existing conditions. The MBTRA is in an area already served by the Oceanside Police Department. As the MBTRA is consistent with the zoning and land use designation of the property, and growth projections for the City reflected in the SANDAG 2021 Regional Plan, the MBTRA does not introduce a new service population greater than accounted for in the City's General Plan. While development of the MBTRA would place a slight increase in demand on police protection services, like the project, it is not anticipated that the MBTRA would result in the need for construction or expansion of existing police facilities to accommodate new police personnel or equipment. The project would also contribute to police funding through development fees and taxes. Thus, the MBTRA would not result in substantial adverse physical impacts associated with the provision of new or physically altered police protection facilities or the need for new or physically altered police protection facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for police protection services and impact would be less than significant.

Schools:

Although the MBTRA is not residential, like the project, using Oceanside Unified School District methodology, the MBTRA could introduce up to 6 students due to the added workforce. Even assuming all those students attend a OUSD school, which is speculative given that it is not known where employees of the MBTRA would be moving, that number is inconsequential compared to the

number of students enrolled in OUSD. The MBTRA will pay development impact fees to mitigate any potential impacts on local schools, as required by SB 50 guidelines. The impact on schools would be less than significant, as it would not require new or physically altered school facilities.

Parks:

As an industrial development, the MBTRA does not introduce residents and would not create a demand for new or expanded parks nor is the MBTRA required to provide park facilities. Thus, the MBTRA would not result in substantial adverse physical impacts associated with the provision of new or physically altered park facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios or other performance objectives for park facilities and impacts would be less than significant.

Other Public Facilities (Libraries and Services):

The MBTRA is not expected to increase the need for libraries or other public services. The MBTRA will also contribute to funding for public facilities through the payment of development impact fees. Thus, the MBTRA would not result in substantial adverse physical impacts associated with the provision of new or physically altered other public services including library facilities or the need for new or physically altered public facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios or other performance objectives and impacts would be less than significant.

In conclusion, the MBTRA would not result in substantial adverse impacts on public services, and all related impacts would be less than significant.

Cumulative Impact:

The MBTRA, like the project, would have less than significant, direct impacts to public services, as a development consistent with applicable land use designations and growth projections. The MBTRA would contribute to expected cumulative demand for public services as contemplated by the General Plan. The MBTRA and the cumulative projects would pay development impact fees intended to offset this demand and would not significantly contribute to the cumulative demand for additional facilities or facility improvements that would lead to significant physical environmental effects. The CEQA Guidelines specifically recognize that requiring a project to implement or fund its fair share of a measure designed to mitigate a cumulative impact is an effective way to address a project's contribution to the impact (14 CCR 15130[a][3]). Therefore, like the project, the MBTRA, in combination with the cumulative projects, would not result in cumulatively considerable impacts related to public services and facilities as disclosed for the in Final EIR and elsewhere in the record of proceedings.

TRAFFIC AND CIRCULATION

Threshold of Significance:

- Would the project conflict with a program, plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities?

The MBTRA would not conflict with a program, plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities. The primary source for the required consistency analysis is the General Plan Circulation Element. The Circulation Element goals and policies are aimed at incorporating complete streets throughout the Oceanside transportation network that serve all users of streets, roads and highways, regardless of their age or ability, or whether they are driving, walking, bicycling, or using transit. The Circulation Element includes an objective that the City should aim for a LOS of D or better on all Circulation Element roadways and intersections and the City's formally adopted Traffic Guidelines include implementing policies related to the same. Relevant to that objective and those policies, the MBTRA would generate fewer trips than the project, but the MBTRA would still contribute to traffic at the SR-76/Benet Road intersection, an intersection that currently operates at a failing LOS under certain conditions. Like the project, the MBTRA's contribution would not cause the intersection to fall below LOS D. To address this cumulative contribution, and achieve consistency with the Circulation Element objective and Traffic Guideline policies, the MBTRA would make a fair share contribution towards intersection improvements, including converting the eastbound and westbound right turn lanes to combined through-right lanes, resulting in three through lanes in each direction. This contribution, made prior to permit issuance, would go toward the City's Thoroughfare and Signal Account for safety and mobility improvements. As the Final EIR demonstrates for the project, which generates more trips at the relevant intersection than the MBTRA, the identified intersection improvements would eliminate the MBTRA's contribution to an exceedance of the Circulation Element objective in a manner consistent with the Traffic Guideline policies.

Regarding pedestrian facilities, the MBTRA would construct a sidewalk along Alex Road from the property access north to the San Luis Rey River Trail, as well as a sidewalk along the Benet Road frontage from Eddie Jones Way north to the San Luis Rey River access path. These improvements align with the City's goals to improve walkability, reduce automobile dependence, and provide multi-modal transportation options, as outlined in the General Plan Circulation Element and the City's Pedestrian Master Plan.

Additionally, the MBTRA is consistent with the City's Bicycle Master Plan, as it would not require any further improvements to the existing Class II bike lanes along Benet Road or the Class I bike path along the San Luis Rey River Trail. No deficiencies exist with respect to those facilities and thus, the MBTRA does not conflict with the Bicycle Master Plan. The closest NCTD bus route 303 operates approximately 4,300 feet from the site, at Foussat Road and Mission Avenue. Consistent with the Traffic Guidelines, the MBTRA does not trigger the need for any improvements to transit infrastructure, it would not conflict with transit-related policies of the Circulation Element.

As for construction traffic, like the project, construction vehicles working on the MBTRA would access the site via Benet Road and Alex Road, with haul trucks using only Benet Road. Construction would comply with City regulations and construction traffic management policies, including two-way traffic maintenance for any activities within the public right-of-way. The temporary construction traffic would not cause lasting impacts, and the amount of construction traffic is materially lower than MBTRA operation related traffic especially during peak periods. Therefore, like the project, the MBTRA would not conflict with a program, plan, ordinance, or policy addressing traffic from construction activities.

In summary, the MBTRA is in alignment with the City's General Plan Circulation Element and related policies for roadway, pedestrian, bicycle, and transit facilities. With improvements to pedestrian infrastructure, contributions to intersection upgrades, and adherence to construction traffic management protocols, like the project, the MBTRA would not conflict with a program, plan, ordinance, or policy addressing the circulation system, including transit, roadway, bicycle, and pedestrian facilities and impacts are less than significant.

Threshold of Significance:

- Would the project substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment)?

The MBTRA would not substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses (e.g., farm equipment). Similar to the project, the MBTRA has been designed to prioritize safety, with access points from Alex Road and Benet Road that meet commercial driveway standards. Large vehicle movements, such as tractor/trailer ingress and egress during operations, will be confined to Benet Road to minimize the potential for hazards. Internal circulation will accommodate both vehicles and pedestrians with designated lanes and walkways, reducing the potential for conflicts between transportation modes. The design does not include any sharp curves, dangerous intersections, or incompatible uses, such as farm equipment, that could substantially increase hazards. All access and internal circulation will comply with City standards, and final plans will be reviewed to ensure safe roadway and emergency access. Therefore, the MBTRA would not substantially increase hazards due to a geometric design feature or incompatible uses, and impacts are less than significant.

Threshold of Significance:

- Would the project result in inadequate emergency access?

The MBTRA would not result in inadequate emergency access. Like the project, the MBTRA has been designed to provide safe and adequate emergency access throughout the property. Primary access would be from Alex Road, with secondary access via Benet Road. Tractor/trailer/truck ingress/egress would be designated for and limited to the Benet Road access drive. Internal

circulation includes 28-foot-wide drives for truck turnarounds and staging, along with a 35-foot-wide fire lane to meet OFD requirements. The design has been developed in consultation with OFD staff to ensure compliance with all relevant standards. No public or private streets would need to be fully closed during construction or operations, and emergency vehicles would have continuous, unimpeded access to the site and surrounding areas.

Additionally, during sidewalk improvements along Alex and Benet Roads, a traffic control plan would maintain access and ensure emergency vehicles can reach the site. The City Traffic Engineer would review and approve this plan. The MBTRA would not conflict with regional or City emergency response plans and will ensure compliance with emergency access standards. Final site plans would be reviewed by OFD and City staff. Further, as Final EIR Section 4.8 and the Wildfire Evacuation Study evaluate and demonstrate, like the project, the less intense MBTRA would not result in inadequate emergency access. Impacts would be less than significant.

Cumulative Impact:

The MBTRA, as conditioned to make the fair share contribution to the intersection improvements, like the project, would not conflict with a program, plan, ordinance, or policy addressing the circulation system. With mitigation, the MBTRA would not conflict with or be inconsistent with CEQA Guidelines Section 15064.3(b). Just as with the project, and as further addressed in Final EIR Section 4.8 and the Emergency Evacuation Study, the MBTRA complies with applicable public safety standards such that it would not result in inadequate emergency access. As all circulation improvements comply with applicable regulatory standards, and the MBTRA's use is consistent with the General Plan and zoning, the MBTRA would not substantially increase hazards due to a geometric design feature or incompatible use. All cumulative projects would be required to prepare similar transportation studies and VMT analyses to determine potential impacts, provide mitigation if necessary, and pay fair-share fees towards the circulation system if necessary. Cumulative projects would require similar analysis and compliance with applicable standards. Overall, like the more project with its greater intensity, the MBTRA would not contribute to a cumulatively considerable transportation impact as disclosed in Final EIR and elsewhere in the record of proceedings.

UTILITIES AND SERVICE SYSTEMS

Threshold of Significance:

- Would the project require or result in the relocation or construction of new or expanded water, wastewater treatment, or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects?

As the following finding demonstrates, MBTRA impacts would be less than significant as the MBTRA would not require or result in the relocation of new or expanded water, wastewater

treatment, or storm water drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant environmental effects.

Water

Like the project, the MBTRA would be in compliance with the General Plan and zoning code, and therefore the identified water demand for the MBTRA's industrial use on the property has been considered in City and regional water supply documents, which are based on the buildout of the City consistent with the General Plan. The MBTRA would utilize existing water infrastructure with no major changes or expansions needed. Water facilities within the area surrounding the property are adequate. Like the project, the MBTRA would connect to available existing public water mains with new laterals on site to serve the MBTRA. The new lateral on site for domestic water service would require 4-inch pipeline, irrigation would require a 2-inch pipeline, and fire service would require an 8-inch pipeline. The proposed connections to existing water facilities would be designed and constructed in accordance with the guidelines, standards, and approved materials of the City.

As discussed in Final EIR Section 4.16, Utilities and Service Systems, the City has reviewed project plans and issued conditions of approval for the project. The MBTRA is subject to corresponding conditions of approval. With the exception of new on-site laterals and connection to the existing public water main, no relocation or construction of new water facilities would be required to provide adequate service to the MBTRA. Based on the analysis and the required conditions of approval, like the project, the less intense MBTRA would not have a substantial adverse effect on water facilities or result in an increase in demand that would require the relocation or construction of new or expanded facilities, the construction or relocation of which could cause significant environmental effects. Impacts related to water demand and service would be less than significant.

Wastewater

Like the project, the MBTRA would not require any off-site sewer pipeline upgrades or wastewater treatment plant improvements to accommodate the additional sewer flows resulting from the less intensive MBTRA. As analyzed in the Final EIR, sufficient wastewater and wastewater treatment capacity exists to service the MBTRA. The MBTRA, like the project, would connect to an existing public sewer main and construct a new 6-inch pipeline on site. The proposed sewer lines within the property would be designed and constructed in accordance with the guidelines, standards, and approved materials of the City, and no relocation or construction of new or expanded wastewater facilities would be required. Additionally, as described above, conditions of approval would be required for water and wastewater service. Therefore, the MBTRA would not have a substantial adverse effect on wastewater facilities or result in an increase in demand that would require the relocation or construction of new or expanded facilities, the construction or relocation of which could cause significant environmental effects. Impacts would be less than significant.

Storm Water Drainage

As described and analyzed in the Final EIR including Final EIR Sections 4.8 and 4.16, the MBTRA, similar to the project, would include a new stormwater conveyance system. The MBTRA new storm water conveyance system includes ribbon gutters, curb and gutter, and a detention vault system with modular wetlands for treatment and a force main pump to convey storm water to the existing storm drain located in Benet Road and into an existing storm water structure to the northwest side of the site which drains to the San Luis Rey River Basin. To treat the proposed improvements within the Benet Road right-of-way, tree wells with curb cuts are proposed in the parkway to receive surface drainage from Benet Road. Additionally, the tree wells have been designed to treat the proposed hardscape and manage pollutant control in accordance with the U.S. Environmental Protection Agency's Green Street Design Guidance which conforms with the County of San Diego Green Streets Design. Due to the drainage system and other design features, like the project, the MBTRA would not contribute runoff that would have substantial adverse effect on stormwater drainage or result in an increase in runoff that would require the relocation or construction of new or expanded facilities, the construction or relocation of which could cause significant environmental effects. Impacts would be less than significant.

Electric Power, Natural Gas, and Telecommunication Facilities

Like the project, the MBTRA would meet the Title 24 and CALGreen standards to reduce energy demand and increase energy efficiency. Like the project, implementation of the MBTRA would not result in the use of substantial amounts of local or regional energy supplies compared to existing conditions. The MBTRA's resultant increase in energy demand would not exceed the available capacity of SDG&E servicing infrastructure to the site or beyond. The property is already connected to SDG&E's electric grid and no new or additional facilities would be required to serve the MBTRA's electrical needs. Further, like the project, the MBTRA must comply with the CAP requirements relative to the provision of on-site generation of renewable energy. SDG&E would also provide natural gas service to the property. Because the property is already connected to SDG&E's natural gas services, and no new or additional facilities would be required to serve the MBTRA's needs. Like the project, the MBTRA would connect to telecommunications facilities in the surrounding area and would have the option of using a variety of different providers. Therefore, as with the project, the less intensive MBTRA would not require relocation or construction of new or expanded electric power, natural gas and telecommunication facilities which could cause significant environmental effects. Impacts would be less than significant.

Threshold of Significance:

- Would the project have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry, and multiple dry years?

Like the project, the MBTRA would rely on the City of Oceanside's existing water infrastructure, managed by the Water Division, which sources approximately 85% of its water from the San Diego County Water Authority (SDCWA) and the remainder from the Mission Basin Groundwater Purification Facility. Like the project, the MBTRA would be in compliance with the General Plan and zoning code in terms of use and intensity of development, and therefore water demand for the MBTRA's industrial use on the property has been considered in City and regional water supply documents, which are based on the buildout of the City consistent with the General Plan. According to the City's Water Master Plan, industrial land uses have a water demand factor of 2,500 gallons per day, per acre. Therefore, like the project, the MBTRA would generate a water demand of 79,475 gallons per day. According to the City's Urban Water Management Plan (UWMP), which estimates water supply based on General Plan land uses, sufficient water would be available during normal, dry, and multiple-dry years to meet demand. Consistent with the City's Water Conservation Master Plan, like the project, the MBTRA includes water-conserving landscaping and efficient irrigation design. The MBTRA must also comply with all Building Code standards relative to water conservation. Further, the SDCWA has developed a Water Shortage Contingency Plan, which identifies strategies for the region to reduce water consumption during catastrophic events and in drought years. Like the project, the MBTRA would have to comply with water conservation measures imposed by the City pursuant to that plan. In sum, sufficient water supply would be available to serve the MBTRA, and reasonably foreseeable future development, during normal, dry, and multiple dry years. Thus, MBTRA impacts related to water supply would be less than significant.

Threshold of Significance:

- Would the project result in a determination by the wastewater treatment provider, which serves or may serve the project that it has adequate capacity to serve the project's projected demand in addition to the provider's existing commitments?

As the MBTRA would be an industrial land use, according to the City's Sewer Master Plan, industrial land uses typically generate approximately 1,000 gallons per day of wastewater per acre. Nonetheless, the Final EIR conservatively assumed a wastewater demand of approximately 79,475 gallons per day based on projected water demand. The MBTRA would connect to the public sewer system and install on-site infrastructure to comply with the City's standards. Given the existing and planned capacity of the SLRWRF there exists sufficient capacity to accommodate the MBTRA's wastewater needs, in addition to its existing and projected commitments. Therefore, MBTRA impacts related to wastewater service would be less than significant.

Threshold of Significance:

- Would the project generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure, or otherwise impair the attainment of solid waste reduction goals?

The MBTRA would adhere to CALGreen Section 5.408.1, which requires a minimum of 65% of non-hazardous construction waste to be recycled or salvaged for use. Solid waste collection and disposal for the MBTRA would be provided by the City of Oceanside through its franchise agreement with Waste Management of North County. Similar to the project, solid waste generated from the MBTRA would be taken to the Palomar Transfer Station in Carlsbad, where it is sorted and transferred to landfills, including El Sobrante Landfill in Riverside County, which is the primary destination for solid waste from Oceanside.

The MBTRA is designed to have four separate buildings, totaling approximately 497,822 square feet of developed area, compared to the single 566,905 square-foot warehouse proposed by the project. This reduction in building area would result in a proportional decrease in solid waste generation compared to the project. Based on the square footage and typical solid waste generation rates for similar industrial facilities, the MBTRA's solid waste generation would align with and not exceed local and state guidelines for waste disposal.

The El Sobrante Landfill, with a daily throughput of 16,054 tons and remaining capacity of 143,977,170 tons, as well as other local landfills, are sufficient to accommodate the solid waste generated by the MBTRA. Given the City of Oceanside's robust waste management infrastructure, which is designed to handle both existing and forecasted solid waste, the MBTRA would not generate solid waste in excess of local infrastructure capacities or impede progress toward achieving solid waste reduction goals, such as those set by state and local mandates for recycling and diversion. Based on the MBTRA's required compliance with applicable state and local regulations to solid waste, waste diversion and recycling at the time of development, the El Sobrante Landfill has sufficient capacity and the MBTRA would not generate solid waste in excess of State or local standards, or in excess of the capacity of local infrastructure. Impacts would be less than significant.

Threshold of Significance:

- Would the project comply with federal, state, and local management and reduction statutes and regulations related to solid waste?

Similar to the proposed project, the MBTRA would be required to comply with required solid waste and recycling measures as provided in the City's Municipal Code, CALGreen, AB 341, and AB 1826. The MBTRA would comply with the state and City regulations, by providing enclosures with adequate space for solid waste collection, storage, and separation of all recyclable materials in full compliance with mandated regulations and City standards.

The MBTRA would implement best practices in waste management by incorporating dedicated areas for recycling and complying with all applicable state and local laws. This includes measures to reduce the amount of waste sent to landfills through recycling, diversion, and proper waste disposal. As the MBTRA would comply with federal, state, and local management and reduction statutes and regulations related to solid waste, the MBTRA impacts would be less than significant.

Cumulative Impact:

The MBTRA would increase the demand for utilities and service systems, such as water, wastewater, stormwater, solid waste, power, and telecommunications, in a way that aligns with the anticipated growth under the General Plan land use and zoning designations for the property. The Final EIR evaluates the proposed project's impact on existing and projected demand for these facilities, as well as the capacity of the relevant systems to serve the property and any cumulative developments. The analysis in the Final EIR is applicable to the MBTRA due to the similarities between it and the project as it relates to use of utilities and service systems. The Final EIR demonstrates that there is adequate supply and capacity for these utilities, and the MBTRA would not require new or expanded facilities that could lead to significant environmental effects. Like the MBTRA, cumulative projects will also need to analyze their specific impacts on utilities and service systems, including water, wastewater, stormwater, power, telecommunications, and solid waste, ensuring consistency with current and future supply, capacity, and demand. Therefore, similar to the proposed project, the MBTRA would not contribute to a cumulatively significant impact on utilities and service systems.

WILDFIRE

Threshold of Significance:

- If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project?
 - Substantially impair an adopted emergency response plan or emergency evacuation plan?

The City of Oceanside relies on its EOP and the San Diego County Operational Area Emergency Plan for disaster preparedness, response, and recovery. The MBTRA, like the project, would comply with these plans and would not physically impair or interfere with the City's EOP. The MBTRA would not disrupt the system for coordinating prevention, preparedness, or response efforts in the City. The MBTRA's development must follow all necessary safety protocols outlined in the EOP and federal National Incident Management System, ensuring that established responsibilities, emergency organization, and communication lines remain unaffected.

The County's EOP outlines comprehensive measures for handling emergencies such as natural disasters, technological incidents, and terrorism. Like the project, the MBTRA would not impair the

County's emergency response systems, mutual aid agreements, or the Emergency Management Organization's operational concepts. The property is not located near any critical emergency operation centers, further minimizing the potential for interference with these plans.

While the property is within a Local Responsibility Area VHFHSZ, as identified by CAL FIRE, like the project, the MBTRA would be designed to meet the most recent California Building Standards Code to reduce potential hazards. The design of the MBTRA would include two access points, one from Benet Road and one from Alex Road, to ensure emergency responders can access the site. Final site plans for the MBTRA would be reviewed by OFD to ensure proper emergency access and to comply with local, fire access standards.

The MBTRA would not require road closures during construction or operations that would impede emergency vehicle access to the property or surrounding areas. All emergency access for the MBTRA would comply with OFD's standards. Further, the Final EIR includes the Wildfire Evacuation Study. That study evaluated the project's, and by extension the MBTRA's because it would be the same as the project as it relates to the matters addressed in that study, consistency with relevant emergency evacuation plans and emergency response plans, discloses the prevention and minimization regulations and measures applicable to the MBTRA, and determined evacuation times for the existing and post-project conditions, as well as provided emergency preparedness information and resources to increase occupant preparedness and facilitate efficient evacuation in the event of an emergency. The Wildfire Evacuation Study is further support for the Final EIR's analysis and determination that the MBTRA would not substantially impair an adopted emergency evacuation plan or emergency response plan. Therefore, like the project, the MBTRA would not substantially impair an adopted emergency response plan or emergency evacuation plan, and impacts would be less than significant.

Threshold of Significance:

- Due to slope, prevailing winds, and other factors, would the project exacerbate wildfire risks, and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?

The project site is located within a VHFHSZ as defined by CAL FIRE. The MBTRA, like the project, would involve the redevelopment of a previously disturbed property. The MBTRA would be built in compliance with the latest California Building Standards Code, which includes provisions for fire-resistant building materials, construction methods, and features designed to minimize fire risks, specifically in VHFHSZs.

Although the property is relatively flat and located in an urban area, it is adjacent to the San Luis Rey River corridor to the north. The river corridor includes native vegetation that could potentially fuel wildfires. However, like the project, the MBTRA and surrounding area includes features that would reduce wildfire risks. These features include the San Luis Rey River Trail's paved trail that would serve as a fuel break between the property and the vegetation associated with the river corridor. Like the project, the MBTRA would incorporate a floodwall surrounding the property. This wall serves as an additional fuel break in the event of a wildfire from the river corridor. The MBTRA must also incorporate a 100-foot fuel management zone and incorporate

fire-resistant landscaping, irrigation systems, and other ignition reducing measures consistent with the CBC and the California Fire Code (CFC), as adopted by the City of Oceanside.

In summary, while the MBTRA site is in a VHFHSZ, the design and associated features would not exacerbate wildfire risks due to slope, prevailing winds, and other factors and thereby expose project occupants to pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire; impacts would be less than significant.

Threshold of Significance:

- Would the project require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines, or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?

Like the proposed project, the MBTRA would be connected to existing infrastructure that is readily available to the site, and it would not require the installation of new roads, power lines, or emergency water sources that would exacerbate fire risk. To the contrary, the MBTRA includes access improvements and road modifications, such as the redesign of the Benet Road entry to incorporate a dedicated right-turn lane, which would serve to manage truck traffic more effectively, reducing the risk of traffic-related fire hazards. In summary, like the project, the MBTRA would not require the installation or maintenance of associated infrastructure that may exacerbate fire risk or result in temporary or ongoing impacts to the environment; therefore, impacts would be less than significant.

Threshold of Significance:

- Would the project expose people or structures to significant risks, including downslope or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes?

The MBTRA would have a similar risk profile regarding flooding, landslides, and drainage changes as the project. The site itself remains relatively flat, and landslide hazards on the property would be less than significant. The MBTRA's development does not introduce new risks for downslope or downstream flooding or landslides as a result of runoff due to post-fire instability. MBTRA's design

would not significantly alter the drainage patterns compared to the existing conditions nor increase the amount of runoff as addressed in Final EIR Sections 4.9 and 4.17.

Therefore, the MBTRA, like the project, would not expose people or structures to significant risks including downslope or downstream flooding or landslides, as a result of runoff, post-fire instability, or drainage changes and impacts would be less than significant.

Cumulative Impact:

Like the MBTRA, site plans for all cumulative projects would be subject to compliance with public safety standards, regulatory requirements relative to wildfire hazards and review and approval by the OFD prior to project development. All cumulative projects, like the MBTRA, would be required to assess wildfire risk and demonstrate compliance with applicable standards, regulations and applicable emergency response plans. Like the MBTRA, cumulative projects must be in accordance with the most recent California Building Standards Code, including standards for building materials used in the exterior design and construction of structures located within fire hazard areas. Like the MBTRA, the cumulative projects would need to provide adequate emergency access, not require infrastructure that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment, and not expose people or structures to significant risks related to flooding, landslides, runoff, post-fire instability, or drainage alterations. Therefore, like the project, the MBTRA would not contribute to cumulatively considerable wildfire impacts.

Section III Environmental Effects Mitigated to Below a Level of Significance

Pursuant to Section 21081(a) of the Public Resources Code and Section 15091(a)(1) of the State CEQA Guidelines, the City finds that, for each of the following significant effects identified in the Final EIR, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the identified significant effects on the environment to less than significant levels. The significant effects and mitigation measures are stated fully in the Final EIR and each of the mitigation measures have been imposed. These findings are explained below and are supported by substantial evidence in the record of proceedings. Analysis of the individual findings is set forth below and the record of proceedings includes the substantial evidence supporting the findings.

AIR QUALITY

Threshold of Significance:

- Would the project result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard?

Mitigation Measures:

MM-AQ-1 **Require Low-Volatile Organic Compound Coatings During Construction.** The project applicant and/or their contractors shall ensure that low-VOC coatings with daily average VOC content of 45 grams per liter (g/l) or less are used during construction for interior building coatings and follow the requirements of Rule 67.0.1 for exterior building envelope coatings (50 g/l) and traffic marking coatings (100 g/l).

Finding:

The City finds that, with implementation of MM-AQ-1 changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal or state ambient air quality standard with the mitigation.

Facts in Support of Finding:

Construction Emissions:

Similar to the project, construction of the MBTRA would result in temporary emissions from on-site sources, such as off-road construction equipment, soil disturbance, and off-gassing of volatile organic compounds from materials. Off-site emissions would also be generated by vendor and haul trucks, as well as worker vehicle trips. As with the project, the MBTRA would be required to comply with SDAPCD Rule 55 – Fugitive Dust Control, which limits fugitive dust emissions (PM10 and PM2.5) during grading and construction activities. Best management practices required by this rule, such as watering exposed areas at least twice per day, would mitigate dust generation, ensuring that fugitive dust emissions remain below significant levels.

Additionally, to further reduce emissions from construction activities, MM-AQ-1 would be implemented. This measure requires the use of low-VOC coatings during construction, with a daily average VOC content of 45 grams per liter (g/l) or less for interior coatings and compliance with Rule 67.0.1 for exterior building envelope coatings (50 g/l) and traffic marking coatings (100 g/l). The use of low-VOC coatings would reduce VOC emissions during construction.

With the implementation of dust control measures like those required by SDAPCD Rule 55, and the use of low-VOC coatings per MM-AQ-1, construction-related air quality impacts for the MBTRA would be reduced to a less than significant level, similar to the project.

Operational Emissions:

Operational emissions for the MBTRA are expected to be similar to those of the proposed project in terms of criteria air pollutants, including VOC, NO_x, CO, SO_x, PM10, and PM2.5. While the MBTRA includes 56 dock-high doors and a smaller operational footprint than the project, it would

still generate emissions from mobile sources (e.g., worker vehicles and truck traffic), energy sources (natural gas and electricity consumption), area sources (consumer products and maintenance equipment), and off-road equipment (electric-powered forklifts, pallet jacks, and yard tractors).

The MBTRA features a significant reduction in truck bays, which is anticipated to result in lower emissions from truck traffic compared to the project. Additionally, the MBTRA incorporates design features, such as electric-powered cargo handling equipment, that would help reduce operational emissions. Like the project, the MBTRA would implement strategies such as the Warehouse Project Best Practices to further limit mobile emissions, particularly from the truck fleet used for distribution.

Given the CalEEMod model's operational emissions calculations for the project, and the expectation that the MBTRA will generate fewer emissions due to its reduced scale, the MBTRA is anticipated to stay within SDAPCD's operational emissions thresholds. The MBTRA's design, focused on minimizing emissions from mobile sources (e.g., electric-powered equipment and low-VOC coatings), ensures that operational emissions would remain below significant levels.

Conclusion:

In summary, the construction and operational emissions for the MBTRA would be similar to those of the project, with potential impacts from temporary construction activities and long-term operational emissions. However, through the implementation of dust control practices and mitigation measures such as the use of low-VOC coatings (MM-AQ-1), these impacts would be reduced to less than significant levels. Specifically, with the adherence to SDAPCD Rule 55 for fugitive dust control during construction, and the incorporation of low-VOC coatings during construction and operation, emissions would remain below applicable thresholds set by SDAPCD. Therefore, the MBTRA would not result in a cumulatively considerable net increase of any criteria pollutant for which the region is non-attainment under federal or state air quality standards. With the implementation of these mitigation measures, the impact would be less than significant.

Threshold of Significance:

- Would the project expose sensitive receptors to substantial pollutant concentrations?

Mitigation Measures:

MM-AQ-1 **Require Low-Volatile Organic Compound Coatings During Construction.** The project applicant and/or their contractors shall ensure that low-VOC coatings with daily average VOC content of 45 grams per liter (g/l) or less are used during construction for interior building coatings and follow the requirements of Rule 67.0.1 for exterior building envelop coatings (50 g/l) and traffic marking coatings (100 g/l).

Finding

The City finds that, with implementation of MM-AQ-1, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not expose sensitive receptors to substantial concentrations of criteria air pollutant emissions, including substantial VOC pollutant concentrations with the mitigation.

Facts in Support of Finding:

Like the project, the MBTRA could result in direct impacts to air quality as, without mitigation, construction would emit VOC's beyond the applicable threshold of significance. Implementation of mitigation measure MM-AQ-1 will reduce this impact to below a level of significance by utilizing low architectural coatings for (i) interior application that do not exceed VOC of 10 grams per liter; exterior application that do not exceed VOC content of 50 grams per liter; and (ii) parking application that do not exceed VOC content of 100 grams per liter. As the Final EIR documents, use of the reduced VOC content architectural coatings required by MM-AQ-1 will reduce the potential VOC emissions during construction below the SDAPCD's thresholds of significance for VOCs. Therefore, with incorporation of this measure, the MBTRA would not expose sensitive receptors to substantial concentrations of criteria air pollutant emissions, including substantial VOC pollutant concentrations with the mitigation. Impacts are less than significant with mitigation.

BIOLOGICAL RESOURCES

Threshold of Significance:

- Would the project have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?

Mitigation Measures:

MM-BIO-1 Nesting Bird Surveys. Construction-related ground-disturbing activities (e.g., clearing/grubbing, grading, and other intensive activities) that occur during the breeding season (typically February 1 through September 15) shall require a one-time biological survey for nesting bird species to be conducted within the limits of grading and a 500-foot buffer within 72 hours prior to construction. This survey is necessary to ensure avoidance of impacts to nesting raptors and/or birds protected by the federal Migratory Bird Treaty Act and California Fish and Game Code, Sections 3503 and 3513. If any active nests are detected, the area shall be flagged and mapped on the

construction plans or a biological resources figure, and the information provided to the construction supervisor and any personnel working near the nest buffer. Active nests will have buffers established around them (e.g., 250 feet for passerines and 500 feet for raptors) by the project biologist in the field with brightly colored flagging tape, conspicuous fencing, or other appropriate barriers or signage. The project biologist shall serve as a construction monitor during those periods when construction activities occur near active nest areas to avoid inadvertent impacts to these nests. The project biologist may adjust the 250-foot or 500-foot setback at their discretion depending on the species and the location of the nest (e.g., if the nest is well protected in an area buffered by dense vegetation). However, if needed, additional qualified monitors shall be provided in order to monitor active nests or other project activities in order to ensure all the project biologist's duties are completed. Once the nest is no longer occupied for the season, construction may proceed in the setback areas.

MM-BIO-2

Biological Monitoring. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, all grading of native habitat shall be monitored by a qualified biologist with 5 years of experience in biological resource evaluation in San Diego County. The qualified biological monitor(s) shall be familiar with the local flora/fauna and shall be contracted to perform biological monitoring during all clearing and grubbing activities.

The project biologist(s) also shall:

- A. Attend the pre-construction meeting with the contractor and other key construction personnel prior to clearing and grubbing to reduce conflict between the timing and location of construction activities with other mitigation requirements (e.g., seasonal surveys for nesting birds).
- B. During clearing and grubbing, conduct meetings with the contractor and other key construction personnel each morning prior to construction activities to go over the proposed activities for the day, and for the monitor(s) to describe the importance of restricting work to designated areas and of minimizing harm to or harassment of wildlife prior to clearing and grubbing.
- C. Review and/or designate the construction area in the field with the contractor in accordance with the final grading plan prior to clearing and grubbing.

- D. Supervise and monitor vegetation clearing and grubbing weekly to ensure against direct and indirect impacts to biological resources that are intended to be protected and preserved and to document that protective fencing is intact.
- E. Flush wildlife species (i.e., reptiles, mammals, avian, or other mobile species) from occupied habitat areas immediately prior to brush-clearing activities. This does not include disturbance of nesting birds (see MM-BIO-1).
- F. Periodically monitor the construction site to verify that the project is implementing the following stormwater pollution prevention plan best management practices: dust control, silt fencing, removal of construction debris and a clean work area, covered trash receptacles that are animal-proof and weather-proof, prohibition of pets on the construction site, and a speed limit of 15 mph during daylight.
- G. Periodically monitor the construction site after grading is completed and during the construction phase to see that artificial security light fixtures are directed away from open space and are shielded, and to document that no unauthorized impacts have occurred.
- H. Keep monitoring notes for the duration of the proposed project for submittal in a final report to substantiate the biological supervision of the vegetation clearing and grading activities and the protection of the biological resources.
- I. Prepare a monitoring report after the construction activities are completed, which describes the biological monitoring activities, including a monitoring log; photos of the site before, during, and after the grading and clearing activities; and a list of any special-status species observed.

MM-BIO-3 **Temporary Installation of Fencing.** To prevent inadvertent disturbance to areas outside the limits of grading for each phase, the contractor shall install temporary fencing, or utilize existing fencing, along the limits of grading.

MM-BIO-4 **Invasive Species Prohibition.** The final landscape plans shall be reviewed by the project biologist and a qualified botanist to confirm that there are no invasive plant species as included on the most recent version of the California Invasive Plant Council Inventory for the project region. In addition, any planting stock to be brought onto the project site for landscape or habitat

creation/restoration/ enhancement will be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to, Argentine ants (*Linepithema humile*), fire ants (*Solenopsis invicta*), and other insect pests. Any planting stock found to be infested with such pests will not be allowed on the project site or within 300 feet of natural habitats unless documentation is provided to the U.S. Fish and Wildlife Service that these pests already occur in natural areas around the project site. The stock will be quarantined, treated, or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats. All temporary irrigation will be for the shortest duration possible, and that no permanent irrigation will be used, for landscape adjacent to the on-site preserve.

Upon completion of construction, to avoid and minimize the presence of predators and brown-headed cowbirds on site, signs will be placed around the site near trash containers reminding people to pick up and throw away their trash properly. In addition, trash will be removed as required to prevent overflow of trash from closed trash receptacles. All trash cans will have secure lids to prevent scattering of litter. The dumpsters and recycling enclosures will be fitted with lids and kept closed to avoid attraction of scavenging mammals and birds including rats, opossum, raccoon, ravens, crows, gulls, and cowbirds. Spoil, trash, or any debris will be removed off site to an approved disposal facility.

Finding

The City finds, with implementation of mitigation measures MM-BIO-1 through MM-BIO-4, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service.

Facts in Support of Finding:

Like the project, the MBTRA would only directly impact disturbed habitat and developed land through vegetation clearing, grubbing, and grading activities. Surveys demonstrate that the MBTRA would not directly impact any special-status plants. Thus, the MBTRA would not directly cause habitat modification that would result in substantial adverse effects on any species identified as candidate, sensitive, or special-status according to local or regional plans, policies, regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service.

Two special-status wildlife species, the Belding's orange-throated whiptail and northern harrier, have been observed in the vicinity of the property. Two additional special-status species, San Diego tiger whiptail and Cooper's hawk, have the potential to occur within the property, but were not directly observed during surveys. Those species may occasionally use the disturbed habitat on-site such that the MBTRA could result in the loss of some foraging and/or breeding and nesting habitat for those species. A potential also exists for the MBTRA to impact bird nests on the property or take migratory birds contrary to the Migratory Bird Treaty Act and other regulations during construction. Any potentially significant impacts would be mitigated to less than significant through specific measures outlined in MM-BIO-1, MM-BIO-2, and MM-BIO-3, which include nesting bird surveys, biological monitoring, and temporary installation of fencing. Implementation of mitigation measure MM-BIO-1 would reduce potential impacts by ensuring that nesting bird surveys are conducted prior to construction activities during the breeding season. By identifying active nests and establishing buffers around them, this measure helps avoid disturbance to nesting raptors and protected bird species, thus minimizing potential impacts on their populations. Mitigation measure MM-BIO-2 would reduce impacts by implementing biological monitoring during all clearing and grubbing activities. The presence of a qualified biologist helps ensure that construction activities are conducted in accordance with regulations and mitigation measures, minimizing inadvertent disturbance to sensitive habitats and species. Mitigation measure MM-BIO-3 would reduce impacts by installing temporary fencing along the limits of grading for each construction phase. This measure helps prevent unauthorized access to areas outside the construction zone, reducing the potential for disturbance to natural habitats and wildlife.

As disclosed in the Final EIR, like the project, the MBTRA could also cause short-term or temporary indirect impacts on adjacent special-status vegetation communities and plants due to construction-related activities such as dust generation, changes in hydrology, and chemical pollutants. Further, introduction of invasive plant species and the reintroduction of human activities at the property could result in indirect impacts. Just as with the project, the MBTRA could result in short-term, construction-related, or temporary indirect impacts to special-status wildlife species that may occur adjacent to the biological study area such as the northern harrier, least Bell's vireo (*Vireo bellii pusillus*), southwestern willow flycatcher (*Empidonax traillii extimus*), coastal California gnatcatcher, Southern California rufous-crowned sparrow [*Aimophila ruficeps canescens*], Southern California legless lizard [*Anniella stebbinsi*], and orange-throated whiptail.

Through a combination of compliance with laws, PDFs and mitigation measures, MBTRA impacts would be less than significant. Compliance with regulations such as the National Pollution Discharge Elimination System and the BMPs required by the same would avoid potential erosion, sedimentation and chemical pollution from construction-related impacts. Compliance with SDAPCD Rule 55, which requires the restriction of visible emissions of fugitive dust beyond the property line, would provide protection against significant impacts related to fugitive dust. Furthermore, as an element of the design, the MBTRA aligns with the Subarea Plan by maintaining a 100-foot biological buffer (and revegetating the on-site portion with native plants) from the San

Luis Rey River and incorporating design features that minimize edge effects. As described above, implementation of MM-BIO-1 through MM-BIO-4 would contribute to the less than significant indirect impact determination. Mitigation measure MM-BIO-4 would also serve to reduce indirect impacts by ensuring the final landscape plans exclude invasive plant species and conducting inspection of planting stock for pest species. By preventing the introduction of invasive species and pests, this measure helps protect native habitats and species from potential harm and maintains ecosystem integrity.

As with the project, the MBTRA has the potential to have long-term or permanent indirect impacts to special-status vegetation and wildlife species that may occur adjacent to the property through the introduction of non-native, invasive plant and animal species and increased human activity. With the establishment of the 100-foot buffer between the MBTRA operations and the San Luis Rey River, these long-term effects would be minimized. Required compliance with City regulations requiring outdoor lighting to be directed down and away from the San Luis Rey River, would similarly avoid impacts. With the prohibition of invasive species used in the landscape plans as described in MM-BIO-4, any potential indirect impacts related to invasive plant species would be less than significant

In sum, with the incorporation of the above mitigation measures, compliance with relevant laws and regulations, conditions of approval and plans, the MBTRA would not have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service. Impacts are less than significant with mitigation.

Threshold of Significance:

- Would the project interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?

MM-BIO-1 **Nesting Bird Surveys.** Construction-related ground-disturbing activities (e.g., clearing/grubbing, grading, and other intensive activities) that occur during the breeding season (typically February 1 through September 15) shall require a one-time biological survey for nesting bird species to be conducted within the limits of grading and a 500-foot buffer within 72 hours prior to construction. This survey is necessary to ensure avoidance of impacts to nesting raptors and/or birds protected by the federal Migratory Bird Treaty Act and California Fish and Game Code, Sections 3503 and 3513. If any active nests are detected, the area shall be flagged and mapped on the construction plans or a biological resources figure, and the information provided to the construction supervisor and any personnel working near the

nest buffer. Active nests will have buffers established around them (e.g., 250 feet for passerines and 500 feet for raptors) by the project biologist in the field with brightly colored flagging tape, conspicuous fencing, or other appropriate barriers or signage. The project biologist shall serve as a construction monitor during those periods when construction activities occur near active nest areas to avoid inadvertent impacts to these nests. The project biologist may adjust the 250-foot or 500-foot setback at their discretion depending on the species and the location of the nest (e.g., if the nest is well protected in an area buffered by dense vegetation). However, if needed, additional qualified monitors shall be provided in order to monitor active nests or other project activities in order to ensure all the project biologist's duties are completed. Once the nest is no longer occupied for the season, construction may proceed in the setback areas.

MM-BIO-2

Biological Monitoring. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, all grading of native habitat shall be monitored by a qualified biologist with 5 years of experience in biological resource evaluation in San Diego County. The qualified biological monitor(s) shall be familiar with the local flora/fauna and shall be contracted to perform biological monitoring during all clearing and grubbing activities.

The project biologist(s) also shall:

- A. Attend the pre-construction meeting with the contractor and other key construction personnel prior to clearing and grubbing to reduce conflict between the timing and location of construction activities with other mitigation requirements (e.g., seasonal surveys for nesting birds).
- B. During clearing and grubbing, conduct meetings with the contractor and other key construction personnel each morning prior to construction activities to go over the proposed activities for the day, and for the monitor(s) to describe the importance of restricting work to designated areas and of minimizing harm to or harassment of wildlife prior to clearing and grubbing.
- C. Review and/or designate the construction area in the field with the contractor in accordance with the final grading plan prior to clearing and grubbing.
- D. Supervise and monitor vegetation clearing and grubbing weekly to ensure against direct and indirect impacts to biological resources that are

intended to be protected and preserved and to document that protective fencing is intact.

- E. Flush wildlife species (i.e., reptiles, mammals, avian, or other mobile species) from occupied habitat areas immediately prior to brush-clearing activities. This does not include disturbance of nesting birds (see MM-BIO-1).
- F. Periodically monitor the construction site to verify that the project is implementing the following stormwater pollution prevention plan best management practices: dust control, silt fencing, removal of construction debris and a clean work area, covered trash receptacles that are animal-proof and weather-proof, prohibition of pets on the construction site, and a speed limit of 15 mph during daylight.
- G. Periodically monitor the construction site after grading is completed and during the construction phase to see that artificial security light fixtures are directed away from open space and are shielded, and to document that no unauthorized impacts have occurred.
- H. Keep monitoring notes for the duration of the proposed project for submittal in a final report to substantiate the biological supervision of the vegetation clearing and grading activities and the protection of the biological resources.
- I. Prepare a monitoring report after the construction activities are completed, which describes the biological monitoring activities, including a monitoring log; photos of the site before, during, and after the grading and clearing activities; and a list of any special-status species observed.

MM-BIO-3

Temporary Installation of Fencing. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, the contractor shall install temporary fencing, or utilize existing fencing, along the limits of grading.

MM-BIO-4

Invasive Species Prohibition. The final landscape plans shall be reviewed by the project biologist and a qualified botanist to confirm that there are no invasive plant species as included on the most recent version of the California Invasive Plant Council Inventory for the project region. In addition, any planting stock to be brought onto the project site for landscape or habitat creation/restoration/ enhancement will be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to, Argentine ants (*Linepithema humile*), fire ants

(*Solenopsis invicta*), and other insect pests. Any planting stock found to be infested with such pests will not be allowed on the project site or within 300 feet of natural habitats unless documentation is provided to the U.S. Fish and Wildlife Service that these pests already occur in natural areas around the project site. The stock will be quarantined, treated, or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats. All temporary irrigation will be for the shortest duration possible, and that no permanent irrigation will be used, for landscape adjacent to the on-site preserve.

Upon completion of construction, to avoid and minimize the presence of predators and brown-headed cowbirds on site, signs will be placed around the site near trash containers reminding people to pick up and throw away their trash properly. In addition, trash will be removed as required to prevent overflow of trash from closed trash receptacles. All trash cans will have secure lids to prevent scattering of litter. The dumpsters and recycling enclosures will be fitted with lids and kept closed to avoid attraction of scavenging mammals and birds including rats, opossum, raccoon, ravens, crows, gulls, and cowbirds. Spoil, trash, or any debris will be removed off site to an approved disposal facility.

Finding:

The City finds, with implementation of mitigation measures MM-BIO-1 through MM-BIO-4, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites.

Facts in Support of Finding:

Although the property is identified in the draft SAP as being within the Wildlife Corridor Preserve Zone, the property does not support coastal sage scrub and has been previously developed and disturbed by to the decades of industrial use. The Final EIR's Biological Technical Report determined that the enter property contains only Urban/Developed and Disturbed Habitat that, along with the industrial use, have prevented the site from serving as a wildlife movement corridor. The site does not contain habitat for coastal California gnatcatcher, nor does it function as a steppingstone for dispersing coastal California gnatcatchers. Therefore, direct impacts would be less than significant as the MBTRA would not interfere substantially with the movement of any native resident or migratory fish or wildlife species, or with established native resident or migratory wildlife corridors, nor impede the use of native wildlife nursery sites.

Like the project, short-term indirect impacts to habitat connectivity and wildlife corridors could result from increased human activity associated with the MBTRA construction. However, project construction would occur during the daytime and would not affect wildlife species, such as mammals, that are most active in the evenings and nighttime. Wildlife species such as birds, rabbits, and lizards which are active during the day, could continue utilizing other habitats within and adjacent to the biological study area for wildlife movement. Further, the MBTRA must comply with the City noise limits that regulate maximum construction noise levels. Potential short-term indirect wildlife movement impacts relative to construction would be mitigated to less than significant through implementation of MM-BIO-1, MM-BIO-2, and MM-BIO-3, which include nesting bird surveys, biological monitoring, and temporary installation of fencing. Implementation of mitigation measure MM-BIO-1 would reduce potential impacts by ensuring that nesting bird surveys are conducted prior to construction activities during the breeding season. This would avoid disturbing nesting birds, particularly species protected under the Migratory Bird Treaty Act, by identifying active nests and establishing appropriate buffers around them. MM-BIO-2 would provide continuous biological monitoring during clearing and grubbing activities to ensure that wildlife movement is not disrupted, and sensitive species are protected from inadvertent disturbance. The temporary fencing required by MM-BIO-3 would further prevent encroachment into sensitive areas and guide wildlife away from construction zones, thus minimizing the potential for impacts to wildlife corridors.

As disclosed in the Final EIR, like the project, the MBTRA could also cause long-term indirect impacts such as increased human activity or lighting, which could deter wildlife from utilizing nearby habitats. However, similar to the project, the MBTRA would be located on a previously disturbed site that has not served as wildlife corridor. The establishment of the MBTRA's buffer between the active industrial uses of the MBTRA and off-site sensitive wildlife habitats, such as the San Luis Rey River, would help avoid long-term impacts. Furthermore, lighting would be designed to minimize light pollution and comply with local ordinances imposing directional and shading requirements, which would reduce potential disruption to nocturnal wildlife species. Compliance with the City's Municipal Code and the CALGreen Standards Code would ensure that light pollution is minimized. With the implementation of MM-BIO-4, which requires invasive species management, the MBTRA operations would not interfere substantially with the movement of native or migratory species or disrupt wildlife corridors. As a result, long-term indirect impacts to wildlife movement would be reduced to less than significant levels.

In summary, with implementing measures identified in PDF-AQ-3 to further minimize air quality, health risk and GHG impacts through measures such as limits on heavy equipment and truck idling, low VOC paints and worker education, incorporation of the above mitigation measures and compliance with relevant laws and regulations, the MBTRA would not substantially interfere with the movement of any native resident or migratory fish or wildlife species, nor with established native resident or migratory wildlife corridors, nor impede the use of native wildlife nursery sites. Impacts would be less than significant with mitigation.

Threshold of Significance:

- Would the project conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?

MM-BIO-1 **Nesting Bird Surveys.** Construction-related ground-disturbing activities (e.g., clearing/grubbing, grading, and other intensive activities) that occur during the breeding season (typically February 1 through September 15) shall require a one-time biological survey for nesting bird species to be conducted within the limits of grading and a 500-foot buffer within 72 hours prior to construction. This survey is necessary to ensure avoidance of impacts to nesting raptors and/or birds protected by the federal Migratory Bird Treaty Act and California Fish and Game Code, Sections 3503 and 3513. If any active nests are detected, the area shall be flagged and mapped on the construction plans or a biological resources figure, and the information provided to the construction supervisor and any personnel working near the nest buffer. Active nests will have buffers established around them (e.g., 250 feet for passerines and 500 feet for raptors) by the project biologist in the field with brightly colored flagging tape, conspicuous fencing, or other appropriate barriers or signage. The project biologist shall serve as a construction monitor during those periods when construction activities occur near active nest areas to avoid inadvertent impacts to these nests. The project biologist may adjust the 250-foot or 500-foot setback at their discretion depending on the species and the location of the nest (e.g., if the nest is well protected in an area buffered by dense vegetation). However, if needed, additional qualified monitors shall be provided in order to monitor active nests or other project activities in order to ensure all the project biologist's duties are completed. Once the nest is no longer occupied for the season, construction may proceed in the setback areas.

MM-BIO-2 **Biological Monitoring.** To prevent inadvertent disturbance to areas outside the limits of grading for each phase, all grading of native habitat shall be monitored by a qualified biologist with 5 years of experience in biological resource evaluation in San Diego County. The qualified biological monitor(s) shall be familiar with the local flora/fauna and shall be contracted to perform biological monitoring during all clearing and grubbing activities.

The project biologist(s) also shall:

- A. Attend the pre-construction meeting with the contractor and other key construction personnel prior to clearing and grubbing to reduce conflict

between the timing and location of construction activities with other mitigation requirements (e.g., seasonal surveys for nesting birds).

- B. During clearing and grubbing, conduct meetings with the contractor and other key construction personnel each morning prior to construction activities to go over the proposed activities for the day, and for the monitor(s) to describe the importance of restricting work to designated areas and of minimizing harm to or harassment of wildlife prior to clearing and grubbing.
- C. Review and/or designate the construction area in the field with the contractor in accordance with the final grading plan prior to clearing and grubbing.
- D. Supervise and monitor vegetation clearing and grubbing weekly to ensure against direct and indirect impacts to biological resources that are intended to be protected and preserved and to document that protective fencing is intact.
- E. Flush wildlife species (i.e., reptiles, mammals, avian, or other mobile species) from occupied habitat areas immediately prior to brush-clearing activities. This does not include disturbance of nesting birds (see MM-BIO-1).
- F. Periodically monitor the construction site to verify that the project is implementing the following stormwater pollution prevention plan best management practices: dust control, silt fencing, removal of construction debris and a clean work area, covered trash receptacles that are animal-proof and weather-proof, prohibition of pets on the construction site, and a speed limit of 15 mph during daylight.
- G. Periodically monitor the construction site after grading is completed and during the construction phase to see that artificial security light fixtures are directed away from open space and are shielded, and to document that no unauthorized impacts have occurred.
- H. Keep monitoring notes for the duration of the proposed project for submittal in a final report to substantiate the biological supervision of the vegetation clearing and grading activities and the protection of the biological resources.

- I. Prepare a monitoring report after the construction activities are completed, which describes the biological monitoring activities, including a monitoring log; photos of the site before, during, and after the grading and clearing activities; and a list of any special-status species observed.

MM-BIO-3 **Temporary Installation of Fencing.** To prevent inadvertent disturbance to areas outside the limits of grading for each phase, the contractor shall install temporary fencing, or utilize existing fencing, along the limits of grading.

MM-BIO-4 **Invasive Species Prohibition.** The final landscape plans shall be reviewed by the project biologist and a qualified botanist to confirm that there are no invasive plant species as included on the most recent version of the California Invasive Plant Council Inventory for the project region. In addition, any planting stock to be brought onto the project site for landscape or habitat creation/restoration/ enhancement will be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to, Argentine ants (*Linepithema humile*), fire ants (*Solenopsis invicta*), and other insect pests. Any planting stock found to be infested with such pests will not be allowed on the project site or within 300 feet of natural habitats unless documentation is provided to the U.S. Fish and Wildlife Service that these pests already occur in natural areas around the project site. The stock will be quarantined, treated, or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats. All temporary irrigation will be for the shortest duration possible, and that no permanent irrigation will be used, for landscape adjacent to the on-site preserve.

Upon completion of construction, to avoid and minimize the presence of predators and brown-headed cowbirds on site, signs will be placed around the site near trash containers reminding people to pick up and throw away their trash properly. In addition, trash will be removed as required to prevent overflow of trash from closed trash receptacles. All trash cans will have secure lids to prevent scattering of litter. The dumpsters and recycling enclosures will be fitted with lids and kept closed to avoid attraction of scavenging mammals and birds including rats, opossum, raccoon, ravens, crows, gulls, and cowbirds. Spoil, trash, or any debris will be removed off site to an approved disposal facility.

Findings:

The City finds, with implementation of mitigation measures MM-BIO-1 through MM-BIO-4, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or

avoid the significant effects on the environment as the MBTRA would not conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance.

Facts in Support of Findings:

The property does not include any native trees or unique vegetation or wildlife habitats. The site has been previously developed and utilized for industrial purposes and it consists of disturbed habitat and urban/developed land. As such, like the project, the MBTRA would not disturb unique vegetation or wildlife habitats, resources with significant scenic, ecological, or recreational value, nor endangered or threatened species as outlined in the City's General Plan Policies 3.11B, 3.11D, and 3.11E. Therefore, the MBTRA would not conflict with these General Plan policies.

The City of Oceanside landscape regulations require a tree survey showing all existing trees to be relocated or removed, with a 1:1 replacement ratio based on diameter at breast height for canopy trees and brown trunk height for palms. The property is heavily disturbed and does not contain native trees, a tree survey is not required under the City's regulations. The MBTRA would not result in the removal of any native trees and would be consistent with the City's landscape regulations. If ornamental trees are present and removed as part of site development, like the project, the MBTRA would comply with the City's tree replacement requirements, ensuring no net loss of urban tree canopy. The MBTRA would also exceed the minimum tree canopy requirement established by the City's Municipal Code.

The City's General Plan biological resource policies, including Policy 3.11A and 3.11C, call for the protection and preservation of biological resources, or mitigation for impacts when habitat modification is unavoidable. Like the project, the MBTRA is consistent with these policies, as it would not result in the modification of native vegetation or sensitive habitats. However, as with the project and as addressed in prior findings, the MBTRA may have the potential to cause significant indirect impacts on sensitive biological resources. In addition to the design features such as the maintenance of a 100-foot buffer from the San Luis Rey River and regulatory compliance with lighting and noise regulations and policies, the MBTRA would result in less than significant impacts through the implementation of mitigation measures MM-BIO-1 through MM-BIO-4. Mitigation measure MM-BIO-1, which requires nesting bird surveys, ensures that active nests of protected bird species are identified before construction begins, preventing disturbance to nesting birds by implementing buffer zones around active nests. MM-BIO-2 provides for biological monitoring during construction to ensure that sensitive species are not disturbed and that wildlife movement is not disrupted. MM-BIO-3 includes the installation of temporary fencing to prevent wildlife from entering construction zones, guiding them safely around the site and minimizing the risk of disruption to their movement or direct harm from construction activities. Finally, MM-BIO-4 prohibits the introduction or spread of invasive species, maintaining the integrity of native habitats

and ensuring that wildlife can continue to thrive in the area without competition from non-native species.

In summary, with the incorporation of the above mitigation measures and compliance with laws, the MBTRA would not conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance. Impacts would be less than significant with mitigation.

Threshold of Significance:

- Would the project conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?

MM-BIO-1 Nesting Bird Surveys. Construction-related ground-disturbing activities (e.g., clearing/grubbing, grading, and other intensive activities) that occur during the breeding season (typically February 1 through September 15) shall require a one-time biological survey for nesting bird species to be conducted within the limits of grading and a 500-foot buffer within 72 hours prior to construction. This survey is necessary to ensure avoidance of impacts to nesting raptors and/or birds protected by the federal Migratory Bird Treaty Act and California Fish and Game Code, Sections 3503 and 3513. If any active nests are detected, the area shall be flagged and mapped on the construction plans or a biological resources figure, and the information provided to the construction supervisor and any personnel working near the nest buffer. Active nests will have buffers established around them (e.g., 250 feet for passerines and 500 feet for raptors) by the project biologist in the field with brightly colored flagging tape, conspicuous fencing, or other appropriate barriers or signage. The project biologist shall serve as a construction monitor during those periods when construction activities occur near active nest areas to avoid inadvertent impacts to these nests. The project biologist may adjust the 250-foot or 500-foot setback at their discretion depending on the species and the location of the nest (e.g., if the nest is well protected in an area buffered by dense vegetation). However, if needed, additional qualified monitors shall be provided in order to monitor active nests or other project activities in order to ensure all the project biologist's duties are completed. Once the nest is no longer occupied for the season, construction may proceed in the setback areas.

MM-BIO-2 Biological Monitoring. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, all grading of native habitat shall be monitored by a qualified biologist with 5 years of experience in biological

resource evaluation in San Diego County. The qualified biological monitor(s) shall be familiar with the local flora/fauna and shall be contracted to perform biological monitoring during all clearing and grubbing activities.

The project biologist(s) also shall:

- A. Attend the pre-construction meeting with the contractor and other key construction personnel prior to clearing and grubbing to reduce conflict between the timing and location of construction activities with other mitigation requirements (e.g., seasonal surveys for nesting birds).
- B. During clearing and grubbing, conduct meetings with the contractor and other key construction personnel each morning prior to construction activities to go over the proposed activities for the day, and for the monitor(s) to describe the importance of restricting work to designated areas and of minimizing harm to or harassment of wildlife prior to clearing and grubbing.
- C. Review and/or designate the construction area in the field with the contractor in accordance with the final grading plan prior to clearing and grubbing.
- D. Supervise and monitor vegetation clearing and grubbing weekly to ensure against direct and indirect impacts to biological resources that are intended to be protected and preserved and to document that protective fencing is intact.
- E. Flush wildlife species (i.e., reptiles, mammals, avian, or other mobile species) from occupied habitat areas immediately prior to brush-clearing activities. This does not include disturbance of nesting birds (see MM-BIO-1).
- F. Periodically monitor the construction site to verify that the project is implementing the following stormwater pollution prevention plan best management practices: dust control, silt fencing, removal of construction debris and a clean work area, covered trash receptacles that are animal-proof and weather-proof, prohibition of pets on the construction site, and a speed limit of 15 mph during daylight.
- G. Periodically monitor the construction site after grading is completed and during the construction phase to see that artificial security light fixtures

are directed away from open space and are shielded, and to document that no unauthorized impacts have occurred.

- H. Keep monitoring notes for the duration of the proposed project for submittal in a final report to substantiate the biological supervision of the vegetation clearing and grading activities and the protection of the biological resources.
- I. Prepare a monitoring report after the construction activities are completed, which describes the biological monitoring activities, including a monitoring log; photos of the site before, during, and after the grading and clearing activities; and a list of any special-status species observed.

MM-BIO-3 **Temporary Installation of Fencing.** To prevent inadvertent disturbance to areas outside the limits of grading for each phase, the contractor shall install temporary fencing, or utilize existing fencing, along the limits of grading.

MM-BIO-4 **Invasive Species Prohibition.** The final landscape plans shall be reviewed by the project biologist and a qualified botanist to confirm that there are no invasive plant species as included on the most recent version of the California Invasive Plant Council Inventory for the project region. In addition, any planting stock to be brought onto the project site for landscape or habitat creation/restoration/ enhancement will be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to, Argentine ants (*Linepithema humile*), fire ants (*Solenopsis invicta*), and other insect pests. Any planting stock found to be infested with such pests will not be allowed on the project site or within 300 feet of natural habitats unless documentation is provided to the U.S. Fish and Wildlife Service that these pests already occur in natural areas around the project site. The stock will be quarantined, treated, or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats. All temporary irrigation will be for the shortest duration possible, and that no permanent irrigation will be used, for landscape adjacent to the on-site preserve.

Upon completion of construction, to avoid and minimize the presence of predators and brown-headed cowbirds on site, signs will be placed around the site near trash containers reminding people to pick up and throw away their trash properly. In addition, trash will be removed as required to prevent overflow of trash from closed trash receptacles. All trash cans will have secure lids to prevent scattering of litter. The dumpsters and recycling

enclosures will be fitted with lids and kept closed to avoid attraction of scavenging mammals and birds including rats, opossum, raccoon, ravens, crows, gulls, and cowbirds. Spoil, trash, or any debris will be removed off site to an approved disposal facility.

Findings:

The City finds, with implementation of mitigation measures MM-BIO-1 through MM-BIO-4, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan.

Facts in Support of Findings:

No applicable and adopted or approved Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan exist. Consistent with City policy, the MBTRA was evaluated for consistency with the draft SAP. As the MBTRA is located on the same site and within the same general footprint as the project, like the project, the MBTRA would have less than significant impacts as it relates to conflicts with the draft SAP. For example, with respect to potential edge effects and consistent with Section 5.2.4 of the draft SAP, the MBTRA would maintain a 100-foot biological buffer from the adjacent San Luis Rey River, revegetation the portion of that 100-foot buffer on the property with native vegetation, implementing the measures identified in PDF-AQ-3 to further minimize air quality, health risk and GHG impacts, and complying with applicable City noise standards. Lighting associated with the MBTRA would also be directed downward and away from the San Luis Rey River, minimizing light pollution in sensitive wildlife areas, consistent with the City's lighting regulations. These and other design features are consistent with the draft Subarea Plan and support this finding that the MBTRA would not conflict with the draft SAP.

In addition, implementation of mitigation measures MM-BIO-1 through MM-BIO-4 would further ensure that the MBTRA does not conflict with the draft SAP. MM-BIO-1 involves conducting nesting bird surveys to identify and avoid disturbing protected species during construction. This measure would mitigate potential impacts to avian species and ensure compliance with the draft SAP's goals and policies. MM-BIO-2 provides for biological monitoring during construction, ensuring that construction activities do not disrupt wildlife movement or harm sensitive species consistent with the draft SAP. MM-BIO-3 includes the installation of temporary fencing to prevent wildlife from entering construction zones, helping guide them away from areas where they may be exposed to construction activities. Finally, MM-BIO-4 addresses invasive species management consistent with the SAP by preventing non-native species from displacing native vegetation and wildlife.

In summary, with the incorporation of the above mitigation measures, the MBTRA would not conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan. Impacts would be less than significant with mitigation.

CULTURAL RESOURCES

Threshold of Significance:

- Would the project cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA Guidelines Section 15064.5?

Mitigation Measures:

MM-CUL-1 Prior to the issuance of a Grading Permit, the Applicant/Owner shall enter into a pre-excavation agreement, otherwise known as a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement with the Traditionally and Culturally Affiliated (TCA) Native American Monitor associated with a TCA Luiseño Tribe. A copy of the agreement shall be included in the Grading Plan Submittals for the Grading Permit. The purpose of this agreement shall be to formalize protocols and procedures between the Applicant/Owner and the Traditionally and Culturally Affiliated (TCA) Native American Monitor associated with a TCA Luiseño Tribe for the protection and treatment of, including but not limited to, Native American human remains, funerary objects, cultural and religious landscapes, ceremonial items, traditional gathering areas and Tribal Cultural Resources, located and/or discovered through a monitoring program in conjunction with the construction of the proposed project, including additional archaeological surveys and/or studies, excavations, geotechnical investigations, grading, and all other ground disturbing activities. Through consultation with the Tribes that consulted on the project and with their consent, certain artifacts may be made available for 3D scanning/printing, with scanned/printed materials to be curated at a local repository meeting the federal standards of 36CFR79.

MM-CUL-2 Prior to the issuance of a Grading Permit, the Applicant/Owner or Grading Contractor shall provide a written and signed letter to the City of Oceanside Planning Division stating that a Qualified Archaeologist and Luiseño Native American Monitor have been retained at the Applicant/Owner or Grading Contractor's expense to implement the monitoring program, as described in the pre-excavation agreement.

- MM-CUL-3** The Qualified Archaeologist shall maintain ongoing collaborative consultation with the Luiseño Native American Monitor during all ground disturbing activities. The requirement for the monitoring program shall be noted on all applicable construction documents, including demolition plans, grading plans, etc. The Applicant/Owner or Grading Contractor shall notify the City of Oceanside Planning Division of the start and end of all ground disturbing activities.
- MM-CUL-4** The Qualified Archaeologist and Luiseño Native American Monitor shall attend all applicable preconstruction meetings with the General Contractor and/or associated Subcontractors to present the archaeological monitoring program. The Qualified Archaeologist and Luiseño Native American monitor shall be present on-site full-time during grubbing, grading and/or other ground altering activities, including the placement of imported fill materials or fill used from other areas of the project site, to identify any evidence of potential archaeological or Tribal Cultural Resources. All fill materials shall be absent of any and all Tribal Cultural Resources.
- MM-CUL-5** In order for potentially significant archaeological artifact deposits and/or cultural resources to be readily detected during mitigation monitoring, a written “Controlled Grade Procedure” for CA-SDI- 5345 shall be prepared by a Qualified Archaeologist, in consultation with the other TCA Luiseño Tribes that have participated in the state-prescribed process for this project, and the Applicant/Owner, subject to the approval of City representatives. The Controlled Grade Procedure shall establish requirements for any ground disturbing work with machinery occurring in and around areas the Qualified Archaeologist and Luiseño Native American Monitor determine to be sensitive through the cultural resource mitigation monitoring process. The Controlled Grade Procedure shall include, but not be limited to, appropriate operating pace, increments of removal, weight and other characteristics of the earth disturbing equipment. A copy of the Controlled Grade Procedure shall be included in the Grading Plan Submittals for the Grading Permit.
- MM-CUL-6** The Qualified Archaeologist or the Luiseño Native American Monitor may halt ground disturbing activities if unknown Tribal Cultural Resources, archaeological artifact deposits or cultural features are discovered. Ground disturbing activities shall be directed away from these deposits to allow a determination of potential importance. Isolates and clearly non-significant deposits will be minimally documented in the field, and before grading proceeds these items shall be secured until they can be repatriated. If items cannot be securely stored on the project site, they may be stored in off-site

facilities located in San Diego County. If the Qualified Archaeologist and Luiseño Native American monitor determine that the unearthed tribal cultural resource, artifact deposits or cultural features are considered potentially significant TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project shall be notified and consulted regarding the respectful and dignified treatment of those resources. The avoidance and protection of the significant tribal cultural resource and/or unique archaeological resource is the preferable mitigation. If, however, it is determined by the City that avoidance of the resource is infeasible, and it is determined that a data recovery plan is necessary by the City as the lead agency under CEQA, TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project shall be notified and consulted regarding the drafting and finalization of any such recovery plan. For significant Tribal Cultural Resources, artifact deposits or cultural features that are part of a data recovery plan, an adequate artifact sample to address research avenues previously identified for sites in the area will be collected using professional archaeological collection methods. The data recovery plan shall also incorporate and reflect the tribal values of the TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project. If the Qualified Archaeologist collects such resources, the Luiseño Native American monitor must be present during any testing or cataloging of those resources. Moreover, if the Qualified Archaeologist does not collect the Tribal Cultural Resources that are unearthed during the ground disturbing activities, the Luiseño Native American monitor, may at their discretion, collect said resources and provide them to the appropriate TCA Luiseño Tribe, as determined through the appropriate process, for respectful and dignified treatment in accordance with the Tribe's cultural and spiritual traditions. Ground disturbing activities shall not resume until the Qualified Archaeologist, in consultation with the Luiseño Native American Monitor, deems the cultural resource or feature has been appropriately documented and/or protected.

MM-CUL-7

The landowner shall relinquish ownership of all Tribal Cultural Resources unearthed during the cultural resource mitigation monitoring conducted during all ground disturbing activities, and from any previous archaeological studies or excavations on the project site to the appropriate TCA Luiseño Tribe, as determined through the appropriate process, for respectful and dignified treatment and disposition, including reburial at a protected location on-site, in accordance with the Tribe's cultural and spiritual traditions. All cultural materials that are associated with burial and/or funerary goods will be repatriated to the Most Likely Descendant as determined by the Native

American Heritage Commission per California Public Resources Code Section 5097.98. No Tribal Cultural Resources shall be subject to curation.

MM-CUL-8

Prior to the release of the grading bond, a monitoring report and/or evaluation report, if appropriate, which describes the results, analysis and conclusions of the archaeological monitoring program (e.g., data recovery plan) shall be submitted by the Qualified Archaeologist, along with the Luiseño Native American monitor's notes and comments, to the City of Oceanside Planning Division for approval.

MM-CUL-9

As specified by California Health and Safety Code Section 7050.5, if human remains are found on the project site during construction or during archaeological work, the person responsible for the excavation, or his or her authorized representative, shall immediately notify the San Diego County Office of the Medical Examiner by telephone. No further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains shall occur until the Medical Examiner has made the necessary findings as to origin and disposition pursuant to Public Resources Code 5097.98. If such a discovery occurs, a temporary construction exclusion zone shall be established surrounding the area of the discovery so that the area would be protected, and consultation and treatment could occur as prescribed by law. If suspected Native American remains are discovered, the remains shall be kept in-situ, or in a secure location in close proximity to where they were found, and the analysis of the remains shall only occur on-site in the presence of a Luiseño Native American monitor. By law, the Medical Examiner will determine within two working days of being notified if the remains are subject to his or her authority. If the Medical Examiner identifies the remains to be of Native American ancestry, he or she shall contact the Native American Heritage Commission (NAHC) within 24 hours. The NAHC shall make a determination as to the Most Likely Descendant.

Finding

The City finds that, with implementation of mitigation measures MM-CUL-1 through MM-CUL-9, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA Guidelines Section 15064.5.

Facts in Support of Finding:

Despite no significant archaeological resources being identified in the studies of the property, the Final EIR recognizes the potential for development of the MBTRA to impact unknown archaeological resources. The potential for such a find necessitated the imposition of mitigation requiring implementation of monitoring by a qualified archaeologist and construction protocols to protect against potentially significant impacts. The MBTRA is required to implement the City's standard cultural mitigation measures identified in the Final EIR as MM-CUL-1 through MM-CUL-9. Although not all of those mitigation measures necessarily apply to every unique archaeological resource that may be found, MM-CUL-1 and MM-CUL-2 relate to the retention of qualified monitors of the MBTRA's ground disturbing activities for purposes of identifying potentially qualifying archaeological resources. MM-CUL-3 requires the monitoring program be identified on all construction documents and notification of the City at the start and end of all ground disturbing activities. MM-CUL-4 requires the monitors attend all pre-constructing meetings and be present full-time during grubbing, grading, placement of fill and other initial ground disturbing activities. MM-CUL-5 identifies the grading procedures that would apply if significant archeological impacts are detected. MM-CUL-6 requires specific procedures and substantive requirements that will apply if applicable resources are identified including testing and when to stop or restart grading activities. To the extent applicable to the unique archaeological resources, MM-CUL-7 requires the landowner to relinquish ownership of all resources unearthed. MM-CUL-8 requires a monitoring report and/or evaluation report to be submitted to the City prior to release of the grading bond. Together, implementation of the imposed mitigation measures would reduce to less than significant any potential for the MBTRA to cause a substantial adverse change in the significance of an archaeological resource pursuant to CEQA Guidelines Section 15064.5

TRAFFIC AND CIRCULATION

Threshold of Significance:

- Would the project conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b)?

Mitigation Measures:

MM-TRA-1 The project applicant will be required to implement a Voluntary Employer Commute Program in order to reduce trips. The program may include a carpool or vanpool system, subsidized or discount transit passes, bike amenities, commute trip reduction marketing, and/or preferential parking permit program. This mitigation measure would result in a VMT reduction of 6.2%.

Finding

The City finds that, with implementation of mitigation measure MM-TRA-1, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not conflict or be inconsistent with CEQA Guidelines Section 15064.3, subdivision (b).

Facts in Support of Finding:

Like the project, the MBTRA would re-introduce industrial uses to the same property using the same access points and access restrictions as the project. As the MBTRA is approximately 69,000 square feet smaller than the project, the MBTRA would also have a less than significant VMT impact with implementation of mitigation measure MM-TRA-1. Even conservatively assuming the MBTRA would exceed the City's formally established 85% VMT significance threshold by 2.9% like the project, implementation of mitigation measure MM-TRA-1 will reduce this potential impact to below a level of significance by requiring the implementation of a Voluntary Employer Commute Program. The SANDAG Voluntary Employer Commute Program, which is mandatory for the employers to offer, but voluntary for the employees to participate in, has been demonstrated to produce a measurable reduction in VMT. The SANDAG Mobility Management VMT Reduction Calculator Tool is utilized to determine how various mobility management strategies, when implemented, can reduce a project's VMT. That takes into consideration the voluntary choice employees have to participate in the different program elements such as carpooling, utilizing subsidized transit passes, or bicycle commuting. The Final EIR demonstrates that, with implementation of MM-TRA-1, the MBTRA would achieve at least a 6% reduction in VMT where only a 2.9% reduction is required to reduce impacts to less than significance. Thus, as mitigated, the MBTRA would have a less than significant impact as the MBTRA would not conflict or be inconsistent with CEQA Guidelines Section 15064.3.

TRIBAL CULTURAL RESOURCES

Threshold of Significance:

- Would the project cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is:
 - i) Listed or eligible for listing in the California Register of Historical Resources, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k), or

- ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1. In applying the criteria set forth in subdivision (c) of Public Resource Code Section 5024.1, the lead agency shall consider the significance of the resource to a California Native American tribe.

Mitigation Measures:

See **MM-CUL-1** through **MM-CUL-9**, discussed above under Cultural Resources.

Finding

The City finds that, with implementation of mitigation measures MM-CUL-1 through MM-CUL-9, changes or alterations have been required in, or incorporated into, the MBTRA which mitigate or avoid the significant effects on the environment as the MBTRA would not cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is: (i) Listed or eligible for listing in the CRHR, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k), or (ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1.

Facts in Support of Finding:

While considered unlikely based on the cultural resource study, the current disturbed state of the property due to the previous industrial operations, and other information received by the City, there remains the potential for construction of the MBTRA to encounter previously unknown, qualifying tribal cultural resources. To avoid or reduce potential impacts to less than significance, the MBTRA must implement MM-CUL-1 through MM-CUL-9 that require retention of a Native American monitor and construction protocols to protect against potentially significant impacts. Specifically, MM-CUL-1 and MM-CUL-2 relate to the retention of qualified monitors of the MBTRA's ground disturbing activities for purposes of identifying potentially qualifying tribal cultural resources. MM-CUL-3 requires the monitoring program be identified on all construction documents and notification of the City at the start and end of all ground disturbing activities. MM-CUL-4 requires the monitors to attend all pre-constructing meetings and be present full-time during grubbing, grading, placement of fill and other initial ground disturbing activities. MM-CUL-5 identifies the grading procedures that would apply if significant archeological impacts are detected. MM-CUL-6 requires specific procedures and substantive requirements that will apply if applicable resources are identified including noticing, testing and cataloging protocols and when to stop and restart grading activities. MM-CUL-7 requires the landowner to relinquish ownership of all tribal cultural resources

unearthed. MM-CUL-8 requires a monitoring report and/or evaluation report to be submitted to the City prior to release of the grading bond. Consistent with California Health and Safety Code Section 7050.5, MM-CUL-9 establishes the protocols that apply if suspected Native American human remains are found. Together, implementation of the imposed mitigation measures would reduce to less than significant any potential for the MBTRA to cause a substantial adverse change in the significance of a tribal cultural resource, defined in Public Resources Code Section 21074 as either a site, feature, place, cultural landscape that is geographically defined in terms of the size and scope of the landscape, sacred place, or object with cultural value to a California Native American tribe, and that is: (i) Listed or eligible for listing in the CRHR, or in a local register of historical resources as defined in Public Resources Code Section 5020.1(k), or (ii) A resource determined by the lead agency, in its discretion and supported by substantial evidence, to be significant pursuant to criteria set forth in subdivision (c) of Public Resources Code Section 5024.1.

Section IV Environmental Effects that Cannot be Mitigated to Below a Level of Significance

CEQA Guidelines Section 15126.2(b) requires that an EIR describe any significant impacts that cannot be avoided, including those impacts that can be mitigated but not reduced to a less-than-significant level. Chapter 4, Environmental Analysis, of the EIR describes the potential environmental impacts of the project, and recommends mitigation measures to reduce impacts, where feasible. Chapters 4 and 8, and the Appendices, of the Final EIR describe the potential impacts of the MBTRA and recommends the same mitigation measures and includes the applicable PDFs as the project to reduce impacts to less than significant. As discussed in the Final EIR, implementation of the MBTRA would not result in any significant impacts that cannot feasibly be mitigated below a level of significance.

Section V Findings Regarding Project Alternatives

The State CEQA Guidelines section 15126.6(a) requires the discussion of a “a reasonable range of alternatives to a project, or the location of a project, which would feasibly attain most of the basic objectives of the project but would avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives.” Section 15126.6(a) also provides that an EIR need not consider every conceivable alternative to a project. The Guidelines states that the “range of potential alternatives to the project shall include those that could feasibly accomplish most of the basic objectives of the project and could avoid or substantially lessen one or more of the significant effects” (Section 15126(c)). An alternative may be rejected if it is “infeasible” or if it fails to achieve the most basic project objectives identified within the EIR. “Feasibility” under CEQA encompasses the desirability of the project based on a reasonable balancing of relevant economic, environmental, social, or other considerations which make infeasible the project alternatives.

Accordingly, the Final EIR presented a reasonable range of four alternatives listed below and considered and rejected for further consideration two potential alternatives. The four alternatives considered are:

- No Project/No Development Alternative
- Multi-Building Alternative
- Reduced Building Footprint Alternative
- Multi-Building and Truck Bay Reduction Alternative

A. Alternatives Considered by Rejected

The Final EIR considered the following two alternatives that were considered during the EIR preparation process but not carried forward for further analysis consistent with CEQA Guidelines section 15126.6(c):

1) Alternative Location:

Development of the project on an alternative site was not carried forward for more detailed consideration. The Alternate Location Alternative was considered but rejected due to the project being consistent with the General Plan, Zoning, and other applicable land use plans and regulations. The property is also a previously disturbed and developed site, located immediately adjacent to the Oceanside Municipal Airport and in close proximity to SR-76, that was utilized for decades as an industrial operation. In accordance with CEQA Guidelines Section 15126.6(f)(2), an EIR is only required to consider an alternative location if the project's potentially significant project effects would be avoided or substantially lessened by moving the project to another site. An alternative site would have had to been within an urban area of Oceanside with the same General Plan and zoning designation where a development with the same components of the project could avoid or substantially lessen one or more of the project's potentially significant impacts to air quality, biological resources, cultural resources, transportation, and tribal cultural resources.

One could speculate that other sites of an approximately equivalent size, that were previously disturbed, with required infrastructure and utilities adjacent and such close proximity to the regional road network, could be redeveloped with a large, single-building industrial development like the project; however, the City is not aware of such an alternative site. Further, the project applicant does not own or control another site of that nature within the City. As one factor for feasibility of an alternative is "whether the proponent can reasonably acquire, control or otherwise have access to the alternative site," and it is unlikely and speculative to assume the feasibility of assembling another site similar to the project site that meets most of the project objectives and avoids or substantially lessens the project's potential significant impacts, the Alternate Location Alternative was considered but rejected due to infeasibility. As the Final EIR analyzes a reasonable range of alternatives, CEQA does not require full consideration of the Alternative Location Alternative when it is speculative whether such a feasible site exists that could accommodate the project and meet most project

objectives, the applicant could feasibly acquire such a site if one existed and the project's potentially significant impacts would be avoided or substantially lessened at such a site.

2) Buildout Under Existing Zoning

The Buildout Under Existing Zoning Alternative considered the development of the project site using the maximum Floor Area Ratio and other development standards allowed by the City Zoning Code. The zoning allows for a maximum Floor Area Ratio of 1.00, with a maximum lot coverage of 75%. Under this alternative, the industrial development could be a single building up to approximately 1,000,000 square feet in size (total building area). In comparison, this Buildout Under Existing Zoning Alternative would be approximately 433,095 square feet larger in size than the project.

A proposed industrial development of up to 1,000,000 square feet would be potentially feasible, and it would likely meet most of the project objectives. Such an alternative was considered during the EIR preparation process but not carried forward for further analysis because that intense a development would not avoid or substantially lessen any of the project's potentially significant impacts and likely have greater impacts than the project in a number of CEQA areas. Therefore, the Buildout Under Existing Zoning Alternative would not meet the CEQA definition of a project alternative and it was rejected and not considered for further evaluation.

B. Reasonable Range of Alternatives

The Final EIR evaluated a reasonable range of four alternatives for their ability to avoid or substantially lessen the impacts of the project identified in the EIR, as well as consideration of their ability to feasibly attain most of the basic objectives of the project as described in the EIR Chapter 3, Section 3.1 Project Objectives. California Public Resources Code section 21081 provides that if one or more significant impacts will not be avoided or substantially lessened by adopting mitigation measures, the environmentally superior alternatives described in the EIR must be found infeasible if they are not adopted. The City need not make findings rejecting alternatives described in an EIR if all of a project's significant impacts will be avoided or substantially lessened by mitigation measures. As the Final EIR demonstrates, the project would not have significant, unavoidable impacts so CEQA does not require findings rejecting alternatives.

In light of the analysis presented in the Final EIR, including the comments received regarding the Draft EIR and the responses to those comments, the MBTRA, not the project as identified in the Draft EIR, has been approved. As disclosed in Final EIR, Chapter 8, compared to the project, the MBTRA would result in a reduced or similar levels of potentially significant impacts in some environmental analysis areas, including air quality, biological resources, cultural resources, transportation and tribal cultural resources. All the MBTRA's potentially significant impacts would be avoided or reduced to less than significant with the mitigation and PDFs described in these findings. The MBTRA would also meet all but one of the project objectives, the objective to

maximize the allowable use of an industrial zoned site that is compatible with the adjacent light industrial zoned sites and Oceanside Municipal Airport.

The City finds, after due consideration, that the four alternatives considered in the Final EIR constitute a reasonable range of alternatives as required by CEQA. As set forth in the Final EIR and below, the City finds as follows with respect to the three alternatives other than the MBTRA that have not been approved.

1. Alternative 1: No Project/No Development Alternative

CEQA Guidelines Section 15126.6(e) requires that an EIR evaluate a “No Project” alternative to allow decision makers to compare the impacts of approving a project with the impacts of not approving that project. For a specific development not requiring a land use regulatory change like the project, the No Project alternative addresses a no development scenario. Under the No Project Alternative, the project and associated improvements would not be implemented, and the property would remain as disturbed, contaminated site. This alternative does not preclude future development of the property in accordance with the site’s industrial designations.

Finding

The No Project Alternative would not provide any development, so overall impacts would be reduced compared to the project and the MBTRA. The No Project Alternative would also not meet any of the project objectives.

Finding and Facts in Support of Finding

The potentially significant impacts to air quality, biology, cultural resources, traffic and tribal cultural resources would be substantially lessened or avoided by the No Project/No Development Alternative compared to the MBTRA and the project. However, this alternative would also not meet any of the project objectives. Specifically, the No Project/No Development Alternative would not result in an employment-generating development consistent with the industrial land use designation and zoning, fulfil a demand for industrial and manufacturing uses in the City or take advantage of and enhance existing infrastructure, including SR-76 and the Oceanside Municipal Airport, located proximate to the property. As this alternative would eliminate all of the potentially significant impacts identified for the project, it would qualify as the environmentally superior alternative. However, CEQA Guidelines Section 15126.6(e)(2) states that if the No Project Alternative is identified as the environmentally superior alternative, then an environmentally superior alternative should be identified among the other alternatives.

2. Alternative 2: Multi-Building Alternative

Under the Multi-Building Alternative, the site would be developed with industrial uses similar to the project and consistent with the General Plan land use and zoning designation for the site. Instead of

one building as proposed by the project, this alternative would develop three buildings on site. This alternative would be approximately 55,745 square feet smaller than the project's total building area and the footprint of the alternative's buildings would be approximately 88,160 square feet smaller than the project's. The Multi-Building Alternative would require substantially more employee parking spaces as a result of the increase in office-use space and office tenants, but it would have 100 dock high doors compared to the projects 114. Other design elements of the Multi-Building Alternative would largely remain the same as the project.

The Multi-Building Alternative was requested during the public Notice of Preparation comment period, and it would meet all project objectives, with the exception of objective 3 (maximize the allowable use of an existing industrial zoned site that is compatible with adjacent light industrial zoned sites and Oceanside Municipal Airport). As a smaller development compared to the project, this alternative would not maximize the allowable development on site to the extent feasible.

Finding

The Multi-Building Alternative would meet most of the project objectives. Overall impacts of the Multi-Building Alternative, with mitigation, would be reduced or similar compared to the project as it relates to biological, cultural resource and tribal cultural resources. However, this alternative would have greater potentially significant impacts as it relates to air quality and transportation.

Finding and Facts in Support of Finding

The Multi-Building Alternative would meet all the project objectives other than objective number 3 regarding maximizing development on the property. The decrease in total building area and project footprint means the Multi-Building Alternative would potentially reduce some potentially significant impacts to biological, cultural and tribal cultural resources compared to the project. With mitigation, and compliance with laws and the PDFs, like the project and the MBTRA, the Multi-Building Alternative would have less than significant impacts to biological, cultural and tribal cultural resources.

Compared to the project, the Multi-Building Alternative could have a net increase in potential air quality and transportation impacts. Even though the overall square footage of the buildings is less, this alternative would have 104,000 square feet of office space compared to the project's 39,170 square feet. As office space results in relatively more vehicle trips compared to industrial uses, a corresponding increase in trip related air quality and transportation impacts could occur under this alternative compared to the project.

3. Alternative 3: Reduced Building Footprint Alternative

Under the Reduced Building Footprint Alternative, the site would be developed with industrial uses similar to the project and consistent with the relevant use designations for the site. This alternative

would reduce the building footprint by developing a multi-story building with a 270,560 square-foot footprint compared to the project's single-level building footprint of 547,320 square feet. In addition to the smaller footprint, the Reduced Building Footprint Alternative's building would be a total of 25,785 square feet smaller. Parking would be reduced to 502 car spaces and the design would include 74 dock high doors compared to the project's 114.

Given the design, particularly the second story of the buildings, the Reduced Building Footprint Alternative would not be consistent with the OMALUCP due to the building's height in proximity to the Oceanside Municipal Airport runway. Further, the design would conflict with the OMALUCP's building setback requirements. As a result, to pursue this alternative, the City might be required to override an Airport Land Use Commission inconsistency finding. Other design elements of the Reduced Building Footprint Alternative would largely remain the same as the project.

The Reduced Building Footprint Alternative would meet most of the project objectives. However, the alternative does not meet objective 3 and objective 7. As to objective 3, the alternative does not maximize the allowable use of an existing industrial-zoned site that is compatible with the adjacent light-industrial-zoned sites and Oceanside Municipal Airport. As to objective 7, the Reduced Building Footprint Alternative does not comply with all the development and other restrictions imposed by the OMALUCP).

Finding

The Reduced Building Footprint Alternative would meet most of the project objectives. Overall impacts of the Reduced Building Footprint Alternative, with mitigation, would be reduced or similar compared to the project as it relates to air quality, biological, transportation, cultural resource and tribal cultural resources.

Finding and Facts in Support of Finding

The Reduced Building Footprint Alternative, with mitigation, would be considered the environmentally superior alternative of the possible alternatives. The smaller building footprint, in comparison to the project, would reduce indirect, potentially significant impacts to biological resources and potentially significant impacts on unknown cultural and tribal cultural resources. Like the project, the Reduced Building Footprint Alternative would require mitigation to reduce the potential impacts in those areas to less than significance. Although mitigation would still be required to achieve a less than significant impact, the reduced development footprint and slightly smaller building square footage would reduce the potential air quality impacts compared to the project. The slight reduction in overall building square footage would result in similar transportation related impacts to the project and mitigation would still be required to achieve less than significance. Because of the potential inconsistency with the OMALUCP, the Reduced Building Footprint Alternative would have potentially greater impacts than the project in the areas of hazards (airports)

and land use (OMALUCP). Additionally, this alternative would not meet project objective 3 (maximize the allowable use of an existing industrial-zoned site that is compatible with the adjacent light industrial-zoned sites) or project objective 7 (develop the property in a manner that complies with the development, intensity, noise, use, and other restrictions imposed by the OMALUCP). As the Reduced Building Footprint Alternative would not conform to the OMALUCP, the City might be required to override an Airport Land Use Compatibility inconsistency finding.

Section VI Other CEQA Considerations

A. Growth-Inducing Impacts

CEQA Guidelines section 15126.2(e) mandates a discussion of the growth-inducing nature of the project evaluated in an EIR. Growth-inducing analysis is intended to address the potential for a project to “foster economic or population growth, or the construction of additional housing, either directly or indirectly, in the surrounding environment.” Growth inducement refers to facilitating planned growth or inducing unplanned growth. CEQA Guidelines Section 15126.2(e) provides that it must not be assumed that growth in any area is necessarily beneficial, detrimental, or of little significance to the environment.

Final EIR sections 4.12 and 7.1 address the potential for growth inducement due to the project. The MBTRA would also directly facilitate growth through development of 497,822 square feet of employment generating uses albeit at a reduced intensity compared to the project’s 566,905 square feet. The Final EIR discloses in Sections 4.12 and 7.1 that project operations would generate approximately 499 jobs. The MBTRA may generate proportionately fewer operational jobs because of the reduced square footage. Construction of the MBTRA, like the project, would generate approximately 1,425 construction related jobs although no more than 50 to 100 construction workers are expected to be on-site daily. As the MBTRA’s intensity of development is substantially less than the approximately 1,000,000 square feet of development potentially allowed under the zoning regulations, as described in the City’s General Plan Economic Development Element, the City continues to provide fewer job opportunities than most other cities in the region. The MBTRA’s temporary and permanent increase in population from employment opportunities is accounted for in SANDAG’s growth projections and would assist with the City’s employment deficits. Implementation of the MBTRA would be consistent with land use and development anticipated by local plans, and thus would not lead to increases in population/housing growth beyond those contemplated by SANDAG and the City.

The MBTRA is a redevelopment of a previously developed industrial property that would not lead to indirect growth because the development would not provide additional infrastructure that would allow for unplanned growth in the area. All infrastructure necessary to serve the MBTRA exists at or in the vicinity of the property, and the MBTRA would not extend such facilities to other undeveloped or underdeveloped properties. The MBTRA would not remove obstacles to growth by extending infrastructure to new areas, nor would it result in significant adverse environmental

impacts beyond those analyzed in the Final EIR due to the expansion of infrastructure, such as water supply facilities, wastewater treatment plants, roads, or freeways. The MBTRA's utility and infrastructure improvements would only be to the property's connection points or for purposes of improvements required for the MBTRA.

B. Significant Irreversible Effects

CEQA Guidelines Section 15126.2(d) requires a discussion of any significant irreversible environmental changes associated with a project. Irreversible effects include large commitment of nonrenewable resources, secondary impacts such as highway improvement that grant access to a previously inaccessible area or irreversible damage from environmental accidents associated with a project. CEQA Guidelines Section 15127 specifies that irreversible changes only require addressing when connected with the adoption or amendment of a local plan, policy, or ordinance; adoption by a local agency formation commission of a resolution making determinations; or when the project is subject to National Environmental Policy Act and requires an environmental impact statement. Like the project, the MBTRA does not involve any of those types of actions activities, and as such this analysis is not required nor provided in these findings.

VII. Other CEQA Findings

A. Mitigation Monitoring and Reporting Program

1. General Finding

Pursuant to Section 21081.6 of the Public Resources Code, the City, in adopting these Findings, also adopts the MMRP for the MBTRA. The MMRP is designed to ensure that, during project implementation, the City and other responsible parties will comply with the mitigation measures adopted in these Findings. The City hereby binds itself to cause the various feasible mitigation measures and PDFs described in the MMRP to be implemented in accordance with the Final EIR and MMRP. The measures identified in the MMRP constitute a binding set of obligations upon the City's certification and approvals identified herein.

The City hereby finds that the MMRP, which is incorporated into the project conditions of approval, meets the requirements of Public Resources Code Section 21081.6 by providing for the implementation and monitoring of project conditions intended to mitigate potentially significant environmental effects of the MBTRA.

2) Regulatory Compliance

Federal, state, regional, and local laws contain certain regulatory compliance measures that must be adhered to in implementing the MBTRA. The Final EIR describes the regulatory setting within each chapter, which includes the details of regulatory compliance measures. Where regulatory compliance measures are required by law, the City has not separately proposed or adopted mitigation

requiring regulatory compliance (as it would be declaratory of existing law). Nonetheless, the City finds that the MBTRA must comply with all applicable regulatory compliance measures.

B. Certification of the Final Environmental Impact Report, CEQA Guidelines § 15090

The Planning Commission certifies that the Final EIR, dated November 2024, on file with the Development Services Department (SCH # 2022070365), has been completed in compliance with CEQA and the State CEQA Guidelines, that the Final EIR was presented to the Planning Commission, and that the Planning Commission reviewed and considered the information contained therein and in the record of proceedings for the project and MBTRA before approving the MBTRA, and that the Final EIR reflects the independent judgment and analysis of the Planning Commission. (State CEQA Guidelines § 15090.)

Mitigation Monitoring
and Reporting Program and Exhibit A

Eddie Jones Warehouse, Manufacturing and Distribution Facility Project

JANUARY 2025

Prepared for:

CITY OF OCEANSIDE

Community Development Department, Planning Division
300 North Coast Highway
Oceanside, California 92054
Contact: Robert Dmohowski

Prepared by:

DUDEK

605 Third Street
Encinitas, California 92024
Contact: Carey Fernandes, Project Manager

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1 Introduction

1.1 Introduction

California Public Resources Code Section 21081.6 requires that, upon certification of an EIR, “the public agency shall adopt a reporting or monitoring program for the changes made to the project or conditions of project approval, adopted in order to mitigate or avoid significant effects on the environment. The reporting or monitoring program shall be designed to ensure compliance during project implementation.” (PRC Section 21000–21177)

This Mitigation Monitoring and Reporting Program was developed in compliance with Section 21081.6 of the California Public Resources Code and Section 15097 of the CEQA Guidelines (14 CCR 15000–15387 and Appendices A–L), and includes the following information:

- A list of mitigation measures
- The timing for implementation of the mitigation measures
- The party responsible for implementing or monitoring the mitigation measures
- The date of completion of monitoring

The City of Oceanside must adopt this Mitigation Monitoring and Reporting Program, or an equally effective program, if it approves the proposed Project with the mitigation measures that were adopted or made conditions of Project approval. Exhibit A provides a list of the Project Design Features (PDFs) that are proposed for incorporation into the project to reduce or avoid certain project effects. These PDFs will also be made a Condition of Approval for the project, as adopted by the City of Oceanside with approval of the project.

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2 Mitigation Monitoring and Reporting Program Table

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
Air Quality				
MM-AQ-1: Require Low-Volatile Organic Compound Coatings During Construction. The project applicant and/or their contractors shall ensure that low-VOC coatings with a daily average VOC content of 45 grams per liter (g/l) or less are used during construction for interior building coatings and follow the requirements of Rule 67.0.1 for exterior and building envelop coatings (50 g/l) and traffic marking coatings (100 g/l).	During construction	Applicant	City of Oceanside	
Biological Resources				
MM-BIO-1: Nesting Bird Surveys. Construction-related ground-disturbing activities (e.g., clearing/grubbing, grading, and other intensive activities) that occur during the breeding season (typically February 1 through September 15) shall require a one-time biological survey for nesting bird species to be conducted within the limits of grading and a 500-foot buffer within 72 hours prior to construction. This survey is necessary to ensure avoidance of impacts to nesting raptors and/or birds protected by the federal Migratory Bird Treaty Act and California Fish and Game Code, Sections 3503 and 3513. If any active nests are detected, the area shall be flagged and mapped on the construction plans or a biological resources figure, and the information provided to the construction supervisor and any personnel working near the nest buffer. Active nests will have buffers established around them (e.g., 250 feet for passerines and 500 feet for raptors) by the project biologist in the field with brightly	Prior to start of construction during breeding season	Applicant	City of Oceanside	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>colored flagging tape, conspicuous fencing, or other appropriate barriers or signage. The project biologist shall serve as a construction monitor during those periods when construction activities occur near active nest areas to avoid inadvertent impacts to these nests. The project biologist may adjust the 250-foot or 500-foot setback at their discretion depending on the species and the location of the nest (e.g., if the nest is well protected in an area buffered by dense vegetation). However, if needed, additional qualified monitors shall be provided in order to monitor active nests or other project activities in order to ensure all the project biologist’s duties are completed. Once the nest is no longer occupied for the season, construction may proceed in the setback areas.</p>				
<p>MM-BIO-2: Biological Monitoring. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, all grading of native habitat shall be monitored by a qualified biologist with 5 years of experience in biological resource evaluation in San Diego County. The qualified biological monitor(s) shall be familiar with the local flora/fauna and shall be contracted to perform biological monitoring during all clearing and grubbing activities. The project biologist(s) also shall:</p> <ul style="list-style-type: none"> a. Attend the pre-construction meeting with the contractor and other key construction personnel prior to clearing and grubbing to reduce conflict between the timing and location of construction activities with other mitigation requirements (e.g., seasonal surveys for nesting birds). b. During clearing and grubbing, conduct meetings with the contractor and other key construction personnel each morning prior to construction activities to go over the proposed activities for the day, and for the monitor(s) to describe the importance of restricting work to designated 	<p>Prior, during, and after construction completion</p>	<p>Applicant</p>	<p>City of Oceanside</p>	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>areas and of minimizing harm to or harassment of wildlife prior to clearing and grubbing.</p> <p>c. Review and/or designate the construction area in the field with the contractor in accordance with the final grading plan prior to clearing and grubbing.</p> <p>d. Supervise and monitor vegetation clearing and grubbing weekly to ensure against direct and indirect impacts to biological resources that are intended to be protected and preserved and to document that protective fencing is intact.</p> <p>e. Flush wildlife species (i.e., reptiles, mammals, avian, or other mobile species) from occupied habitat areas immediately prior to brush-clearing activities. This does not include disturbance of nesting birds (see MM-BIO-1).</p> <p>f. Periodically monitor the construction site to verify that the project is implementing the following stormwater pollution prevention plan best management practices: dust control, silt fencing, removal of construction debris and a clean work area, covered trash receptacles that are animal proof and weather-proof, prohibition of pets on the construction site, and a speed limit of 15 mph during daylight.</p> <p>g. Periodically monitor the construction site after grading is completed and during the construction phase to see that artificial security light fixtures are directed away from open space and are shielded, and to document that no unauthorized impacts have occurred.</p> <p>h. Keep monitoring notes for the duration of the proposed project for submittal in a final report to substantiate the biological supervision of the vegetation clearing and grading activities and the protection of the biological resources.</p> <p>i. Prepare a monitoring report after the construction activities are completed, which describes the biological monitoring activities, including a monitoring log; photos of</p>				

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>the site before, during, and after the grading and clearing activities; and a list of any special-status species observed.</p>				
<p>MM-BIO-3: Temporary Installation of Fencing. To prevent inadvertent disturbance to areas outside the limits of grading for each phase, the contractor shall install temporary fencing, or utilize existing fencing, along the limits of grading.</p>	<p>Prior to construction</p>	<p>Applicant</p>	<p>City of Oceanside</p>	
<p>MM-BIO-4: Invasive Species Prohibition. The final landscape plans shall be reviewed by the project biologist and a qualified botanist to confirm that there are no invasive plant species as included on the most recent version of the California Invasive Plant Council Inventory for the project region. In addition, any planting stock to be brought onto the project site for landscape or habitat creation/restoration/enhancement will be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to, Argentine ants (<i>Linepithema humile</i>), fire ants (<i>Solenopsis invicta</i>), and other insect pests. Any planting stock found to be infested with such pests will not be allowed on the project site or within 300 feet of natural habitats unless documentation is provided to the U.S. Fish and Wildlife Service that these pests already occur in natural areas around the project site. The stock will be quarantined, treated, or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats. All temporary irrigation will be for the shortest duration possible, and that no permanent irrigation will be used, for landscape adjacent to the on-site preserve.</p>	<p>Prior to Final Grading Release/During construction</p>	<p>Applicant</p>	<p>City of Oceanside</p>	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
Cultural Resources				
<p>MM-CUL-1: Prior to the issuance of a Grading Permit, the Applicant/Owner shall enter into a pre-excavation agreement, otherwise known as a Tribal Cultural Resources Treatment and Tribal Monitoring Agreement with the Traditionally and Culturally Affiliated (TCA) Native American Monitor associated with a TCA Luiseño Tribe. A copy of the agreement shall be included in the Grading Plan Submittals for the Grading Permit. The purpose of this agreement shall be to formalize protocols and procedures between the Applicant/Owner and the Traditionally and Culturally Affiliated (TCA) Native American Monitor associated with a TCA Luiseño Tribe for the protection and treatment of, including but not limited to, Native American human remains, funerary objects, cultural and religious landscapes, ceremonial items, traditional gathering areas and Tribal Cultural Resources, located and/or discovered through a monitoring program in conjunction with the construction of the proposed project, including additional archaeological surveys and/or studies, excavations, geotechnical investigations, grading, and all other ground disturbing activities. Through consultation with the Tribes that consulted on the project and with their consent, certain artifacts may be made available for 3D scanning/printing, with scanned/printed materials to be curated at a local repository meeting the federal standards of 36CFR79</p>	Prior to issuance of a Grading Permit	Applicant	City of Oceanside	
<p>MM-CUL-2: Prior to the issuance of a Grading Permit, the Applicant/Owner or Grading Contractor shall provide a written and signed letter to the City of Oceanside Planning Division stating that a Qualified Archaeologist and Luiseño Native American Monitor have been retained at the Applicant/Owner or Grading Contractor's expense to</p>	Prior to issuance of a Grading Permit	Applicant	City of Oceanside	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
implement the monitoring program, as described in the pre-excavation agreement.				
MM-CUL-3: The Qualified Archaeologist shall maintain ongoing collaborative consultation with the Luiseño Native American Monitor during all ground disturbing activities. The requirement for the monitoring program shall be noted on all applicable construction documents, including demolition plans, grading plans, etc. The Applicant/Owner or Grading Contractor shall notify the City of Oceanside Planning Division of the start and end of all ground disturbing activities.	During construction (start/end of all ground disturbing activities)	Applicant/Qualified Archaeologist	City of Oceanside	
MM-CUL-4: The Qualified Archaeologist and Luiseño Native American Monitor shall attend all applicable preconstruction meetings with the General Contractor and/or associated Subcontractors to present the archaeological monitoring program. The Qualified Archaeologist and Luiseño Native American monitor shall be present on-site full-time during grubbing, grading and/or other ground altering activities, including the placement of imported fill materials or fill used from other areas of the project site, to identify any evidence of potential archaeological or Tribal Cultural Resources. All fill materials shall be absent of any and all Tribal Cultural Resources.	Prior to construction/During construction (start/end of all ground disturbing activities)	Applicant/Qualified Archaeologist and Luiseño Native American Monitor	City of Oceanside	
MM-CUL-5: In order for potentially significant archaeological artifact deposits and/or cultural resources to be readily detected during mitigation monitoring, a written “Controlled Grade Procedure” for CA-SDI- 5345 shall be prepared by a Qualified Archaeologist, in consultation with the other TCA Luiseño Tribes that have participated in the state-prescribed process for this project, and the Applicant/Owner, subject to the approval of City representatives. The Controlled Grade Procedure shall establish requirements for any ground	During construction (start/end of all ground disturbing activities)	Applicant/Qualified Archaeologist	City of Oceanside	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>disturbing work with machinery occurring in and around areas the Qualified Archaeologist and Luiseño Native American Monitor determine to be sensitive through the cultural resource mitigation monitoring process. The Controlled Grade Procedure shall include, but not be limited to, appropriate operating pace, increments of removal, weight and other characteristics of the earth disturbing equipment. A copy of the Controlled Grade Procedure shall be included in the Grading Plan Submittals for the Grading Permit.</p>				
<p>MM-CUL-6: The Qualified Archaeologist or the Luiseño Native American Monitor may halt ground disturbing activities if unknown Tribal Cultural Resources, archaeological artifact deposits or cultural features are discovered. Ground disturbing activities shall be directed away from these deposits to allow a determination of potential importance. Isolates and clearly non-significant deposits will be minimally documented in the field, and before grading proceeds these items shall be secured until they can be repatriated. If items cannot be securely stored on the project site, they may be stored in off-site facilities located in San Diego County. If the Qualified Archaeologist and Luiseño Native American monitor determine that the unearthed tribal cultural resource, artifact deposits or cultural features are considered potentially significant TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project shall be notified and consulted regarding the respectful and dignified treatment of those resources. The avoidance and protection of the significant tribal cultural resource and/or unique archaeological resource is the preferable mitigation. If, however, it is determined by the City that avoidance of the resource is infeasible, and it is determined that a data</p>	<p>During construction (start/end of all ground disturbing activities, if applicable)</p>	<p>Applicant/ Qualified Archaeologist or the Luiseño Native American Monitor</p>	<p>City of Oceanside</p>	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>recovery plan is necessary by the City as the lead agency under CEQA, TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project shall be notified and consulted regarding the drafting and finalization of any such recovery plan. For significant Tribal Cultural Resources, artifact deposits or cultural features that are part of a data recovery plan, an adequate artifact sample to address research avenues previously identified for sites in the area will be collected using professional archaeological collection methods. The data recovery plan shall also incorporate and reflect the tribal values of the TCA Luiseño Tribes that have participated in the state-prescribed consultation process for this project. If the Qualified Archaeologist collects such resources, the Luiseño Native American monitor must be present during any testing or cataloging of those resources. Moreover, if the Qualified Archaeologist does not collect the Tribal Cultural Resources that are unearthed during the ground disturbing activities, the Luiseño Native American monitor, may at their discretion, collect said resources and provide them to the appropriate TCA Luiseño Tribe, as determined through the appropriate process, for respectful and dignified treatment in accordance with the Tribe’s cultural and spiritual traditions. Ground disturbing activities shall not resume until the Qualified Archaeologist, in consultation with the Luiseño Native American Monitor, deems the cultural resource or feature has been appropriately documented and/or protected.</p>				
<p>MM-CUL-7: The landowner shall relinquish ownership of all Tribal Cultural Resources unearthed during the cultural resource mitigation monitoring conducted during all ground disturbing activities, and from any previous archaeological studies or excavations on the project site to the appropriate</p>	<p>During construction (start/end of all ground disturbing activities, if applicable)</p>	<p>Applicant</p>	<p>City of Oceanside</p>	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>TCA Luiseño Tribe, as determined through the appropriate process, for respectful and dignified treatment and disposition, including reburial at a protected location on-site, in accordance with the Tribe’s cultural and spiritual traditions. All cultural materials that are associated with burial and/or funerary goods will be repatriated to the Most Likely Descendant as determined by the Native American Heritage Commission per California Public Resources Code Section 5097.98. No Tribal Cultural Resources shall be subject to curation.</p>				
<p>MM-CUL-8: Prior to the release of the grading bond, a monitoring report and/or evaluation report, if appropriate, which describes the results, analysis and conclusions of the archaeological monitoring program (e.g., data recovery plan) shall be submitted by the Qualified Archaeologist, along with the Luiseño Native American monitor’s notes and comments, to the City of Oceanside Planning Division for approval.</p>	<p>Prior to the release of the grading bond</p>	<p>Applicant/Qualified Archaeologist/</p>	<p>City of Oceanside</p>	
<p>MM-CUL-9: As specified by California Health and Safety Code Section 7050.5, if human remains are found on the project site during construction or during archaeological work, the person responsible for the excavation, or his or her authorized representative, shall immediately notify the San Diego County Office of the Medical Examiner by telephone. No further excavation or disturbance of the site or any nearby area reasonably suspected to overlie adjacent remains shall occur until the Medical Examiner has made the necessary findings as to origin and disposition pursuant to Public Resources Code 5097.98. If such a discovery occurs, a temporary construction exclusion zone shall be established surrounding the area of the discovery so that the area would be protected, and consultation and</p>	<p>During construction (start/end of all ground disturbing activities, if applicable)</p>	<p>Applicant/Qualified Archaeologist or the Luiseño Native American Monitor</p>	<p>City of Oceanside</p>	

Table 1. Mitigation Monitoring and Reporting Program

Mitigation Measure	Implementation Timing	Responsible Party	Enforcing Agency	Date of Completion
<p>treatment could occur as prescribed by law. If suspected Native American remains are discovered, the remains shall be kept in-situ, or in a secure location in close proximity to where they were found, and the analysis of the remains shall only occur on-site in the presence of a Luiseño Native American monitor. By law, the Medical Examiner will determine within two working days of being notified if the remains are subject to his or her authority. If the Medical Examiner identifies the remains to be of Native American ancestry, he or she shall contact the Native American Heritage Commission (NAHC) within 24 hours. The NAHC shall make a determination as to the Most Likely Descendant.</p>				
Traffic and Circulation				
<p>MM-TRA-1: The project applicant will be required to implement a Voluntary Employer Commute Program in order to reduce trips. The program may include a carpool or vanpool system, subsidized or discount transit passes, bike amenities, commute trip reduction marketing, and/or preferential parking permit program. This mitigation measure would result in a VMT reduction of 6.2%.</p>	<p>Prior to project operation</p>	<p>Applicant</p>	<p>City of Oceanside</p>	
Tribal Cultural Resources				
<p>MM-CUL-1 through MM-CUL-9 (see Cultural Resources mitigation above)</p>	<p>-</p>	<p>-</p>		

Exhibit "A"

TO MMRP FOR EDDIE JONES WAREHOUSE MANUFACTURING AND DISTRIBUTION FACILITY PROJECT

PROJECT DESIGN FEATURES (PDF)

PDF-AQ-1: Require the cargo handling equipment including forklifts (forklifts and pallet jacks) and yard tractors for facility operation to be electric powered operation.

PDF-AQ-2: Standard construction practices that would be employed to reduce fugitive dust emissions include watering of the active sites two times per day, depending on weather conditions. Construction of Project components would be subject to SDAPCD Rule 55 – Fugitive Dust Control. Compliance with Rule 55 would limit fugitive dust that may be generated during grading and construction activities.

PDF-AQ-3: The applicant will incorporate the following applicable California Department of Justice Warehouse Project Best Practices measures as part of project construction and operation:

- Prohibiting grading on days with an Air Quality Index forecast of greater than 100 for particulates or ozone for the project area
- Forbidding idling of heavy equipment for more than 3 minutes
- Keeping on site and furnishing to the lead agency or other regulators upon request, all equipment maintenance records and data sheets, including design specifications and emission control tier classifications
- Conducting an on-site inspection to verify compliance with construction mitigation and to identify other opportunities to further reduce construction impacts
- Using paints, architectural coatings, and industrial maintenance coatings that have volatile organic compound levels of less than 10 grams per liter
- Providing information on transit and ridesharing programs and services to construction employees
- Forbidding trucks from idling for more than 3 minutes and requiring operators to turn off engines when not in use
- Posting both interior- and exterior-facing signs, including signs directed at all dock and delivery areas, identifying idling restrictions and contact information to report violations to the California Air Resources Board (CARB), the local air district, and the building manager
- Designing all project building roofs to accommodate the maximum future coverage of solar panels and installing the maximum solar power generation capacity feasible

-
- Running conduit to designated locations for future electric truck charging stations
 - Unless the owner of the facility records a covenant on the title of the underlying property ensuring that the property cannot be used to provide refrigerated warehouse space, constructing electric plugs for electric transport refrigeration units at every dock door and requiring truck operators with transport refrigeration units to use the electric plugs when at loading docks.
 - Oversizing electrical rooms by 25% or providing a secondary electrical room to accommodate future expansion of electric vehicle charging capability
 - Requiring facility operators to train managers and employees on efficient scheduling and load management to eliminate unnecessary queuing and idling of trucks
 - Posting signs at every truck exit driveway providing directional information to the truck route
 - Requiring that every tenant train its staff in charge of keeping vehicle records in diesel technologies and compliance with CARB regulations, by attending CARB-approved courses. Also requiring facility operators to maintain records on-site demonstrating compliance and make records available for inspection by the local jurisdiction, air district, and state upon request
 - Requiring tenants to enroll in the U.S. Environmental Protection Agency's SmartWay program, and requiring tenants who own, operate, or hire trucking carriers with more than 100 trucks to use carriers that are SmartWay carriers
 - Providing tenants with information on incentive programs, such as the Carl Moyer Program and Voucher Incentive Program, to upgrade their fleets

PDF-GHG-1: Photo-voltaic (PV) systems will be installed on the building to meet 50% of forecasted electricity demand, consistent with the City of Oceanside Climate Action Plan.

PDF-GHG-2: The applicant will participate in one of San Diego Gas & Electric's services for non-residential development such as the Comprehensive Audit Program or the Facility Assessment Service Program, no sooner than 1 year and no later than 2 years after initial building occupancy

1 PLANNING COMMISSION
2 RESOLUTION NO. 2025-P04

3 A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY
4 OF OCEANSIDE, CALIFORNIA APPROVING A DEVELOPMENT
5 PLAN, CONDITIONAL USE PERMIT, AND VARIANCE ON CERTAIN
6 REAL PROPERTY IN THE CITY OF OCEANSIDE

7 APPLICATION NO: D22-00001, CUP22-00001 & V22-00001

8 APPLICANT: RPG OCEANSIDE EDDY JONES WAY OWNER, LLC

9 LOCATION: 250 EDDIE JONES WAY (APNs: 145-021-29, 30 &32)

10 THE PLANNING COMMISSION OF THE CITY OF OCEANSIDE, CALIFORNIA DOES
11 RESOLVE AS FOLLOWS:

12 WHEREAS, there was filed with this Commission a verified petition on the forms
13 prescribed by the Commission requesting a Development Plan (D22-00001), Conditional Use
14 Permit (CUP22-00001), and Variance (V22-00001) for the Multi-Building and Truck Bay
15 Reduction Alternative (MBTRA) under the provisions of Articles 13, 41, and 43 of the Zoning
16 Ordinance of the City of Oceanside to permit the following:

- 17 a) A Development Plan for the MBTRA to construct a four-building warehouse,
18 manufacturing, and office facility totaling approximately 497,822 square feet of building
19 area; b) A Conditional Use Permit to allow for wholesaling, distribution, and storage with a
20 floor area greater than 50,000 square-feet and to allow trucking terminals with more than
21 six heavy trucks on the premises at one time; c) A Variance to allow a proposed flood wall
22 to exceed the maximum allowable wall height of eight (8) feet.

23 on certain real property described in the project description.

24 WHEREAS, the Planning Commission, after giving the required notice, did on the 10th
25 day of February 2025 conduct a duly-advertised public hearing as prescribed by law to consider
26 said application and heard and considered written evidence and oral testimony by all interested
27 parties on the above identified.

28 WHEREAS, the Planning Commission, after deliberation, introduced a motion to
approve said MBTRA project with an amendment to Condition 1b and Condition 11 to reduce

1 the maximum number of truck bays from 56 to a total of 34 bays with a caveat that truck bays
2 can be allocated between any of the four buildings at the discretion of the project applicant.

3
4 WHEREAS, pursuant to the California Environmental Quality Act of 1970, and State
5 Guidelines thereto; an Environmental Impact Report (EIR) was prepared and circulated for this
6 project;

7 WHEREAS, Chapter 8, Alternatives, of the Final EIR describes the potential impacts of
8 the Multi-Building and Truck Bay Reduction Alternative (MBTRA), this alternative has reduced
9 or similar less than significant impacts, with mitigation, to the proposed project.

10 WHEREAS, there is hereby imposed on the subject development project certain fees,
11 dedications, reservations and other exactions pursuant to state law and city ordinance;

12 WHEREAS, pursuant to Gov't Code §66020(d)(1), NOTICE IS HEREBY GIVEN that the
13 project is subject to certain fees, dedications, reservations and other exactions as provided below:

Description	Authority for Imposition
Public Facility (Commercial/Industrial)	Ord. No. 91-09 Reso. No. 15-R0638-1
School District Fee (Commercial/Industrial)	Ord. No. 91-34 OUSD Res. 13(12-13) CUSD Res. 21-1314
Traffic Signal & Thoroughfare (Commercial/Industrial)	Reso. No. 16-R0324-1
Drainage and Flood Control Fee	Ord. No. 85-23 Reso. No. 16-R0638-1
Wastewater System Capacity Buy-in Fee (Non-Residential and Multi-Family Residential)	Reso. No. 87-97 Ord. No. 15-OR0479-1 City Code 37.7.37
Water System Capacity Buy-in Fee (Residential and Non-Residential)	Reso. No. 87-96 Ord. No. 15-OR0480-1 City Code 37.7.37
San Diego County Water Authority (Residential and Non-Residential)	SDWA Ord. 2017

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26 WHEREAS, the fees listed above have been identified by the City as being applicable to
27 the project as proposed. Failure by the City to list an applicable fee above does not alleviate the
28 developer from paying all applicable fees at the time when such fees become due;

1 WHEREAS, unless otherwise provided by this resolution, all impact fees shall be
2 calculated and collected at the time and in the manner provided in Chapter 32B of the Oceanside
3 City Code and the City expressly reserves the right to amend the fees and fee calculations
4 consistent with applicable law;

5 WHEREAS, the City expressly reserves the right to establish, modify or adjust any fee,
6 dedication, reservation or other exaction to the extent permitted and as authorized by law;

7 WHEREAS, pursuant to Gov't Code §66020(d)(1), NOTICE IS FURTHER GIVEN that
8 the 90-day period to protest the imposition of any fee, dedication, reservation, or other exaction
9 described in this resolution begins on the effective date of this resolution and any such protest
10 must be in a manner that complies with Section 66020;

11 WHEREAS, pursuant to Oceanside Zoning Ordinance §4603, this resolution becomes
12 effective 10 days from its adoption in the absence of the filing of an appeal or call for review;

13 WHEREAS, the documents or other material which constitute the record of proceedings
14 upon which the decision is based will be maintained by the City of Oceanside Planning
15 Department, 300 North Coast Highway, Oceanside, California 92054.

16 WHEREAS, studies and investigations made by this Commission and on its behalf reveal
17 the following facts:

18 FINDINGS:

19 For the Development Plan (D22-00001):

- 20 1. The site plan and physical design of the project as proposed is consistent with the
21 purposes of the Zoning Ordinance because the proposed development complies with the
22 regulations established by the Limited Industrial (IL) Zone District. The project is
23 consistent with the purpose of the IL District, which is intended to provide areas
24 appropriate for a wide range of moderate to low-intensity industrial uses capable of
25 being located adjacent to residential areas with minimal buffering and attenuation
26 measures. The multi-building design orients the buildings to adequately screen loading
27 and maneuvering areas. Perimeter landscaping, enhanced tree plantings, and a 100-foot
28 biological buffer provide additional attenuation measures to ensure compatibility with
surrounding land uses.

- 1 2. The Development Plan as proposed conforms to the General Plan of the City, in that the
2 project is considered a light industrial use within an established industrial area and is
3 consistent and compatible with the Light Industrial land use designation, including
4 industrial Land Use Policies 2.1.A, 2.1.B, 2.12.C, 2.12.D, and 2.12.E, Economic
5 Development Element Policies EDE-3b-2 and EDE-2d-1, and Energy and Climate
6 Action Element Policies ECAE-1a-2 and ECAE-5a-7. Furthermore, the project is
7 consistent with the Oceanside Municipal Airport – Airport Land Use Compatibility Plan
8 (ALUCP) which considers manufacturing, warehousing, and distribution uses highly
9 compatible with airport operations. The project will optimize the redevelopment of an
10 underutilized industrial site and provide opportunities for a variety of industrial uses.
11 The project will also make a significant contribution towards addressing the City’s jobs-
12 to-housing ratio by introducing up to 499 permanent jobs as forecasted by the applicant.
- 13 3. The area covered by the Development Plan can be adequately, reasonably and
14 conveniently served by existing and planned public services, utilities and public
15 Facilities. The project site is served by existing services and was previously developed
16 as a manufacturing facility. The area was designed to accommodate industrial
17 development and the project, as conditioned, would comply with all City codes and
18 regulations necessary for redevelopment of the site.
- 19 4. The project as proposed is compatible with existing and potential development on
20 adjoining properties or in the surrounding neighborhood because the project was
21 designed in compliance with the regulations of the Zoning Ordinance and the Oceanside
22 Municipal Airport – Airport Land Use Compatibility Plan (ALUCP). The project is
23 found compatible with the airport because light industrial uses don’t conflict with airport
24 operations. The project complies with buffer and height limitations of the ALUCP and
25 the maximum intensity of people per acre. To the north, the project provides a 100 foot
26 habitat buffer and enhanced landscaping to screen the project from residential uses north
27 of the river. The San Luis Rey River corridor provides a natural buffer between
28 industrial uses on the south side of the river and residential uses north of the river. The
proposed buildings are also designed to adequately screen truck maneuvering areas and

1 loading docks from the adjacent neighborhood north of the site. The design features also
2 provide additional noise attenuation.

- 3 5. The site plan and physical design of the project is consistent with the policies contained
4 within Section 1.24 and 1.25 of the Land Use Element of the General Plan, the
5 Development Guidelines for Hillside, and Section 3039 of the Zoning Ordinance
6 because the property does not have slopes subject to the Hillside Ordinance.

7 For Conditional Use Permit (CUP22-00001):

- 8 1. The proposed location of the use is in accord with the objectives of the Zoning Ordinance
9 and the purposes of the district in which the site is located because the Limited Industrial
10 District is intended to provide areas appropriate for a wide range of moderate to low-
11 intensity industrial uses, including wholesaling, distribution and storage, located adjacent to
12 residential areas with minimal buffering and attenuation measures. The 31.79 acre site is
13 located adjacent to an airport which is an appropriate location for limited industrial projects
14 with warehouse space exceeding 50,000 square feet and a maximum of 34 truck bays. The
15 project is designed to accommodate a variety of limited industrial uses including
16 manufacturing, warehousing, distribution, and ancillary office space. The proposed
17 trucking terminals are designed with 34 truck bays divided among all four buildings. As
18 conditioned, trucks and trailers will be limited to parking at approved truck bays. All trucks
19 will access the project site from Benet Road, which is considered a secondary collector and
20 an acceptable roadway for truck traffic. Trucks will be prohibited from travelling north of
21 the San Luis Rey River on Benet Road.

- 22 2. The proposed location of the conditional use and the proposed conditions under which it
23 would be operated or maintained will be consistent with the General Plan; will not be
24 detrimental to the public health, safety or welfare of persons residing or working in or
25 adjacent to the neighborhood of such use; and will not be detrimental to properties or
26 improvements in the vicinity or to the general welfare of the city because the proposed
27 project is consistent with the Light Industrial General Plan land use designation and is
28 designed to be compatible with surrounding land uses in the area. The proposed buildings,
as designed, will screen truck maneuvering and loading areas from the adjacent

1 neighborhood to the north. Furthermore, a Noise Impact Analysis concluded that the
2 project's operational noise levels along the north property line will be below the maximum
3 noise level limits established by the City's Noise Control Ordinance and will not be
4 detrimental to persons residing in the neighborhood on the north side of the San Luis Rey
5 River. The proposed conditions of approval, mitigation measures, and project design
6 features identified in the Environmental Impact Report for the MBTRA ensure that the
7 project will not be detrimental to public health, safety, and welfare.

- 8 3. The proposed conditional use will comply with the provisions of this ordinance, including
9 any specific condition required for the proposed conditional use in the district in which it
10 would be located. The project will be conditioned to limit the number of truck bays to a
11 maximum of 34 and all heavy trucks and trailers must park at approved truck bay locations.
12 Heavy trucks will be prohibited from accessing the site on Alex Road or travelling north of
13 the river on Benet Road. The proposed project will also be conditioned to prepare a
14 Facilities Management Plan with a good neighbor policy to ensure all future tenants
15 operate in accordance with the conditions of approval and avoid any conflicts with
16 surrounding land uses.

17 For Variance (V22-00001):

- 18 1. That because of special circumstances or conditions applicable to the development site
19 including size, shape, topography, location or surroundings strict application of the
20 requirements of this ordinance deprive such property of privileges enjoyed by other
21 property in the vicinity and under identical zoning classification. The project site is located
22 adjacent to the San Luis Rey River and is located within a Special Flood Hazard Area
23 (Zone A99 per FEMA Flood Insurance Map). An existing levee was constructed to protect
24 the site from flooding, however, the levee has not been certified by FEMA and cannot be
25 viewed as an approved flood prevention system. Since elevating the entire 31.79-acre site
26 above the Base Flood Elevation (BFE) is not feasible, the applicant coordinated with the
27 City and FEMA to provide a flood wall as alternative floodplain mitigation. The proposed
28 flood wall will range in height from 7.9' to 9.9' to provide a consistent top of wall
elevation of 35.5' as required to exceed the BFE of 34'. Exceeding the maximum wall

1 height of eight feet in the IL District is required to provide necessary flood protection and
2 allow for proper grading of the site. Strict application of the requirements of the Zoning
3 Ordinance would deprive the project site of development privileges enjoyed by other
4 industrial zoned properties in the area. Furthermore, similar variances have been granted
5 for retaining walls exceeding maximum height for other projects in the IL District;

6 2. That granting the application will not be detrimental or injurious to property or
7 improvements in the vicinity of the development site, or to the public health, safety or
8 general welfare because the proposed flood wall has been designed specifically for the
9 subject property by a licensed engineer to ensure it will adequately function during a flood
10 event. The solid decorative masonry block wall system combined with proposed
11 landscaping will ensure an aesthetically pleasing design compatible with the project and
12 surrounding area.

13 3. That granting the application is consistent with the purposes of this ordinance and will not
14 constitute a grant of special privilege inconsistent with limitations on other properties in the
15 vicinity and in the same zoning district because flood protection mitigation is required for
16 all adjacent properties adjacent to the San Luis Rey River or in the same flood zone.
17 Similar variances for increased wall height have been approved in industrial zoning
18 districts. The proposed 7.9' to 9.9' flood wall will provide the necessary flood protection
19 for the development and is a practical solution to address a site constraint for a property
20 located in a flood zone.

21 NOW, THEREFORE, BE IT RESOLVED that the Planning Commission does hereby
22 approve Development Plan (D22-00001), Conditional Use Permit (CUP22-00001), and Variance
23 (V22-00001), subject to the following conditions:

24 **Planning:**

- 25 1. This resolution approves the following entitlements:
- 26 a) A Development Plan to construct a four-building warehouse, manufacturing, and
27 office facility totaling approximately 497,822 square feet of building area;
 - 28 b) A Conditional Use Permit to allow for wholesaling, distribution, and storage with
a floor area greater than 50,000 square-feet and to allow trucking terminals with

1 more than six heavy trucks on the premises at one time with a maximum of 34
2 truck bays divided among all four buildings;

3 c) A Variance to allow a proposed flood wall to exceed the maximum allowable
4 wall height of eight (8) feet. Exterior wall elevations would range from 7.9' to
5 9.9' above the exterior grade.

6 2. The Development Plan, Conditional Use Permit, and Variance shall expire on February
7 10, 2028 unless implemented as required by Articles 41 and 43 of the Zoning Ordinance
8 or a time extension is granted.

9 3. This development project approval shall become effective and final at the expiration of
10 the appeal period without the filing of a valid appeal. The filing of a valid appeal during
11 the appeal period shall stay the Planning Commission's decision pending review and
12 final decision by the City Council on the appeal. At such time that the development
13 project approval becomes effective and final, the applicant/developer shall either secure
14 a grading permit, a building permit, or timely file of a time extension request prior to the
15 expiration of the development approval. Failure by the applicant/developer to do so will
16 result in the expiration of the development approval and a new development project
17 application(s) submittal and approval will be required for the project to move forward.

18 4. The applicant, permittee or any successor-in-interest shall defend, indemnify and hold
19 harmless the City of Oceanside, its agents, officers or employees from any claim, action
20 or proceeding against the City, its agents, officers, or employees to attack, set aside, void
21 or annul the City's approval of this development project (D22-00001, CUP22-00001,
22 and Variance V22-00001). The City will promptly notify the applicant of any challenge,
23 claim, suit, action or legal proceeding against the City. The City will cooperate fully
24 with the applicant, permittee or any successor-in-interest in the legal defense of the
25 City's approving action.

26 5. A covenant or other recordable document approved by the City Attorney shall be prepared by
27 the property owner and recorded prior to grading permit issuance. The covenant shall provide
28 that the property is subject to this resolution and all listed conditions of approval.

- 1 6. Unless expressly waived, all current zoning standards and City ordinances and policies
2 in effect at the time building permits are issued are required to be met by this project.
3 The approval of this project constitutes the applicant's agreement with all statements in
4 the Description and Justification, Application and other materials and information
5 submitted with this application, unless specifically waived by an adopted condition of approval.
- 6 7. Prior to the transfer of ownership and/or operation of the site, the owner shall provide
7 written copy of the application, staff report and resolution for the project to the new
8 owner and/or operator. This notifications provision shall run with the life of the project
9 and shall be recorded as a covenant on the property.
- 10 8. This Development Plan, Conditional Use Permit, and Variance shall be called for review
11 by the Planning Commission if complaints are filed and verified as valid by the Code
12 Enforcement Office concerning the violation of any of the approved conditions or does
13 not conform with the information contained in or representations made in the
14 application, any supporting material submitted to the City or during any hearing on the
15 application.
- 16 9. Failure to meet any conditions of approval for this development shall constitute a
17 violation of the Development Plan, Conditional Use Permit, and Variance.
- 18 10. No deviations from the approved plans and exhibits shall occur without Planning
19 Division approval. Substantial deviations shall require a revision to the Development
20 Plan or a new Development Plan.
- 21 11. The Development Plan and Conditional Use Permit authorize a maximum of 34 truck
22 bays at the locations shown on the approved plans. No additional truck bays shall be
23 permitted unless a revision to the Development Plan and Conditional Use Permit is
24 approved by the Planning Commission. The applicant shall have the discretion to
25 allocate the 34 truck bays between any of the four buildings. Truck bay locations and the
26 total number of truck bays on site shall be identified on all future building plans.
- 27 12. Elevations, materials, colors, roofing materials and floor plans shall be substantially the
28 same as those approved by the Planning Commission. These shall be shown on plans
submitted to the Building Division and Planning Division.

- 1 13. The project shall comply with the provisions of the City's anti-graffiti (Ordinance No.
2 93-19/Section 20.25 of the City Code). These requirements, including the obligation to
3 remove or cover with matching paint all graffiti within 24 hours, shall be noted on the
4 Landscape Plan.
- 5 14. Prior to the issuance of any grading or building permit, the grading or building permit
6 plans shall be reviewed for consistency with the Planning Commission approved
7 discretionary application plans and exhibit. Building elevations, siding materials, colors,
8 roofing materials and floor plans shall be in substantial compliance with those plans and
9 exhibits approved by the Planning Commission.
- 10 15. Building permit plans shall demonstrate that all mechanical (HVAC) rooftop and
11 ground-mounted equipment will be completely screened from public view as required by
12 the Zoning Ordinance and that all mechanical HVAC equipment, screens and/or vents
13 shall be painted with non-reflective paint to match the roof.
- 14 16. Project signage was not approved as part of this project. All proposed signage shall be
15 reviewed and approved in conformance with the Zoning Ordinance prior to the issuance
16 of any sign permit.
- 17 17. Parking spaces shall be kept available and useable for the parking of vehicles at all
18 times.
- 19 18. Outdoor lighting shall be low emission, shielded, and directed away from neighboring
20 properties.
- 21 19. All fencing and walls constructed with the project shall be in conformance with the
22 approved Development Plan.
- 23 20. Heavy truck and trailer parking shall only occur at the approved truck bay locations.
24 No additional truck and/or trailer parking areas shall be permitted on the premises.
- 25 21. Prior to building occupancy, the property owner shall prepare a Facilities Management
26 Plan to be reviewed and approved by the City Planner and shall cover the following:
27 a) Property management and contact information.
28 b) All future tenants shall be provided conditions of approval and operational
requirements as provided in this resolution.

- c) Procedures for ensuring tenants comply with all conditions of approval.
- d) Security management plan with proposed security measures for providing appropriate security both on the premises and immediate vicinity of the site.
- e) Maintenance plan with comprehensive maintenance procedures for the project site, including the exterior building, landscaping areas, parking lots, sidewalks, and walkways to ensure that a high standard of maintenance exists at the site at all times. The maintenance plan shall include a policy for litter removal and include a commitment for the sweeping and cleaning of parking lots, sidewalks and other concrete surfaces at sufficient intervals to maintain a “like new” appearance. Wastewater, sediment, trash or other pollutants shall be collected on site and properly disposed of and shall not be discharged off the property or into the City’s storm drain system.
- f) Good neighbor policy with a point of contact to respond to issues regarding business operations or site conditions; policies for preventing or reducing nuisances including vehicle routes, number of trips, vehicle idling, backup alarms, facility noise, and light spillage; protocol and anticipated response time to complaints; and enforcement procedures.

22. Renewable Energy Facilities (Zoning Ordinance Article 30, Section 3048): The project shall install and maintain renewable energy facilities (e.g. solar photovoltaic systems) that supply at least 50 percent of forecasted electricity demand. Installation shall be completed prior to building occupancy or through the issuance of a bond with timing of installation to be approved by the City Planner.

23. Electric Vehicle Parking and Charging Facilities (Zoning Ordinance Article 30, Section 3048): Prior to issuance of building permits, the project shall comply with non-residential electric vehicle (EV) parking and charging facility requirements as provided in Table 2 of Article 30, Section 3048 of the Zoning Ordinance.

24. Urban Forestry Program (Zoning Ordinance Article 30, Section 3049): Prior to issuance of building permits, the project shall comply with the urban forestry standards outlined in Table 1 of Article 30, Section 3049 of the Zoning Ordinance.

- 1 a) Project site area of one acre or more must provide a minimum tree canopy area of
2 12 percent and a minimum permeable surface area of 22 percent.
- 3 b) The project must also provide a Landscape and Tree Canopy Management Plan
4 (LTCMP). The LTCMP shall include information regarding regular, seasonal,
5 and emergency maintenance, trash abatement, irrigation, tree/plant care, tree
6 replacement, insect and disease infestation prevention, integrated pest
7 management, and appropriate response process etc. Projects that do not maintain
8 landscape in a manner consistent with the approved LTCMP shall be subject to
9 code enforcement action.

10 25. Transportation Demand Management (TDM) (Zoning Ordinance Article 30, Section
11 3050): Prior to building occupancy, the project shall prepare and implement a
12 transportation demand management (TDM) plan that results in a minimum alternative
13 employee commute share of 20 percent. The alternative employee commute share shall
14 include all commute trips not involving combustion engine single-occupancy vehicles
15 (SOVs). Alternative employee commute modes include ridesharing, public transit, active
16 transportation, telecommuting, and zero-emission vehicles. TDM plans shall be
17 implemented within 12 months of full occupancy.

18 26. All mitigation measures identified in the Final EIR (SCH: 2022070365) and Mitigation,
19 Monitoring, and Reporting Program (MMRP) and Project Design Features (PDF) for the
20 project shall be complied with as stated in those documents. The applicant shall submit a
21 mitigation compliance binder (digital format) to the Planning Division documenting
22 compliance with all mitigation measures.

23 27. The Department of Environmental Health and Quality (DEHQ) Well Program has
24 records indicating there are 12 monitoring wells at the site, and environmental
25 assessments have identified a septic tank at the site. The septic tank and any wells on
26 the property must be properly destroyed under DEHQ permit prior to grading for the
27 project. If the wells will be retained for ongoing groundwater monitoring in conjunction
28 with the Response Plan, they must be protected in place during grading and construction

1 activities. Any wells damaged during grading/construction must be repaired to
2 standards under DEHQ permit.

3 **Traffic Engineering:**

4 28. The project shall be responsible for the fair share payment to the City, which shall be
5 \$49,595 to be paid to the City's Thoroughfare and Signal Account. The funds will be
6 used at the City's discretion for projects that will improve traffic safety and mobility in
7 the City of Oceanside. The \$49,595 shall be paid in full prior to issuance of any permit
8 (precise grading, building or otherwise) for any phase or any component of the project.
9 The \$49,595 fair share payment only satisfies the offsite improvement obligations. All
10 other onsite improvements such as roadway, sidewalk, bike trail/lane, etc, that is
11 contiguous to the project, or needed to provide access to the project shall be done at the
12 project developer's cost.

13 29. A sidewalk shall be installed along the project's frontage on Benet Road, connecting the
14 sidewalk to the existing sidewalk on the southeast corner of Benet Road at Eddy Jones
15 Way. This improvement shall be completed prior to the issuance of occupancy and must
16 meet the satisfaction of the City Traffic Engineer.

17 30. ADA-compliant pedestrian curb ramps shall be installed on the northeast and southeast
18 corners of Benet Road at Eddy Jones Way, on the northeast and southeast corners of the
19 project's driveway. This improvement shall be completed prior to the issuance of
20 occupancy and must meet the satisfaction of the City Traffic Engineer.

21 31. At the intersection of Eddy Jones Way and Benet Road, the project shall install a stop
22 sign, stop legend, and stop limit line on Eddy Jones. This improvement shall comply
23 with the CA-MUTCD guideline and be completed prior to the issuance of occupancy,
24 subject to the satisfaction of the City Traffic Engineer.

25 32. The project shall dedicate an easement on Benet Road to the City and construct the
26 proposed northbound right-turn lane onto the project access driveway. This
27 improvement shall be completed prior to the issuance of occupancy and must meet the
28 satisfaction of the City Traffic Engineer.

1 33. Prior to building occupancy, the project applicant shall submit a traffic management plan
2 for review and approval by the City Traffic Engineer and City Planner. The plan shall
3 include proposed long-haul truck routes, onsite/offsite vehicle circulation, and delivery
4 schedules.

5 34. Heavy truck trips to and from the project site shall be limited to the Benet Road access
6 point. All heavy trucks shall access the site from Highway 76 at Benet Road. No trucks
7 shall use Benet Road north of the San Luis Rey River. Ingress and egress of heavy
8 trucks on Alex Road shall be prohibited.

9 **Building:**

10 35. This development review checklist is not intended to be a complete review for any
11 project. Further review will be required during a Building permit application and plan
12 submittal. This checklist is intended to address any significant design considerations
13 based on the type of Building, location of building, and proposed use of a Building.

14 Beginning on January 1, 2023, Oceanside Development Services (ODS) is required by
15 State law to enforce the 2022 Edition of California Building Standards Codes (a.k.a.,
16 Title 24 of the California Codes of Regulations).

17 Every three years, the State adopts new model codes (known collectively as the
18 California Building Standards Code) to establish uniform standards for the construction
19 and maintenance of buildings, electrical systems, plumbing systems, mechanical
20 systems, and fire and life safety systems. Sections 17922, 17958 and 18941.5 of the
21 California Health and Safety Code require that the latest edition of the California
22 Building Standards code and Uniform Housing Code apply to local construction 180
23 days after publication.

- 24 • Part 2: The 2022 California Building Code (CBC).
- 25 • Part 2.5: The 2022 California Residential Code (CRC).
- 26 • Part 3: The 2022 California Electrical Code (CEC).
- 27 • Part 4: The 2022 California Mechanical Code (CMC).
- 28 • Part 5: The 2022 California Plumbing Code (CPC).
- Part 6: The 2022 California Energy Code

- Part 9: The 2022 California Fire Code (CFC)
- Part 11: The 2022 California Green Building Standards Code (CALGreen Code) This Part is known as the California Green Building Standards Code, and it is intended that it shall also be known as the CALGreen Code.

36. The building plans for this project shall be required by State law to be prepared by a licensed architect or engineer.

37. Separate/unique addresses may be required to facilitate utility releases. Verification that the addresses have been properly assigned by the City's Planning Division shall accompany the Building Permit application.

38. Compliance with the Federal Clean Water Act (BMP's) shall be demonstrated on the plans.

39. All outdoor lighting shall meet Chapter 39 of the City Code (Light Pollution Ordinance) and shall be fully shielded.

40. All electrical, communication, CATV, etc. service lines within the exterior lines of the property shall be underground (City Code Sec. 6.30).

41. A complete set of Soil Reports, Structural Calculations, Energy Calculations, & California Title 24 Energy Form(s) shall be required at time of plans submittal to the Building Division for plan check.

42. The specific requirements of the Soil Report must be incorporated into the plans and the Geo Technical Engineer must in writing indicate that the plans that have been submitted to the Building Division have been reviewed and meet the Soils Report recommendations.

43. A form or foundation survey shall be required prior to the placement of concrete to show the location of the new structure in respect to the property lines, known easements, and known setback lines. By obtaining a form survey the location of the foundation is checked prior to the placement of concrete, and can save costly corrective measures in case of an encroachment of a property line.

44. Construction waste management. Recycle and/or salvage for reuse a minimum of 65% of the nonhazardous construction and demolition waste in accordance with either CAL Green Section

1 4.408.2 Waste Management Plan, 4.408.3 Waste Management Company or 4.408.4 Waste
2 Stream Reduction Alternative.

3 The City of Oceanside has adopted the latest version of California's Green Building Standards
4 Code (CALGreen) and requires waste diversion of C&D materials from new construction,
5 commercial renovation, and most residential additions/alterations. To comply with the City of
6 Oceanside's requirements, you must submit a Waste Management Plan before permits are
7 issued and construction begins. Demonstrate how your project will fulfill the CALGreen waste
8 diversion requirement. Include an estimate of the amount of waste produced during construction
9 and demolition, the actual amount of waste produced, and the ways the different materials will
10 be diverted.

11 After construction is completed and during the final inspection, you will be required to sign an
12 affidavit stating that your project met the C&D diversion requirement in your Waste
13 Management Plan.

14 By diverting C&D materials, you are reusing resources, helping the City of Oceanside reach its
15 city-wide waste diversion goal of 75-90% by 2020, and potentially saving money on your
16 project

17 45. Electric Vehicle Charging for New Construction. Show electric vehicle charging wiring and
18 panels per the requirements found in CGC 4.106.4.

19 46. Water conserving plumbing fixtures. Plumbing fixtures (water closets and urinals) shall comply
20 with the following:

21 a) The effective flush volume of all water closets shall not exceed 1.28 gal/flush (CGC
22 403.1.1).

23 b) The effective flush volume of wall-mounted urinals shall not exceed 0.125 gal/flush. The
24 effective flush volume for all other urinals shall not exceed 0.5 gal/flush (CGC 403.1.2).

25 47. Operation and maintenance manual. An operation and maintenance manual will be provided to
26 the building occupant or owner per CGC 4.410.1.

27 48. Duct and mechanical systems protection. At the time of rough installation, during storage on the
28 construction site and until final startup of the heating, cooling and ventilating equipment, all
duct and other related air distribution components openings shall be covered with tape, plastic,

1 sheet metals, or other methods acceptable to the enforcing agency to reduce the amount of
2 water, dust or debris, which may enter the system per CGC 4.504.1.

3 49. Concrete slab foundations. A capillary break shall be installed if a slab on grade foundation
4 system is used. The use of a 4" thick base of 1/2" or larger clean aggregate under a 6 mil vapor
5 retarder with joint lapped not less than 6" will be provided per CGC 4.505.2 and CRC
6 R506.2.3.

7 50. The roof plan must indicate that it is Solar ready.

8 51. The manufacturing products and all raw materials must be shown to meet Building Code
9 requirements for use and storage in quantities that meet Table 307.1(1) of the Building Code.

10 52. All Occupancies must be shown on the plan to verify Fire resistive construction, rated walls,
11 corridors, etc.

12 53. The mechanical plans must show exhaust fans for all areas of hazardous fumes.

13 54. The plans must clearly show Hazardous materials that will be used.

14 55. Plumbing plans must show the correct disposal of manufacturing waste.

15 56. Electrical plans must show compliance with all equipment to be listed by a Nationally
16 Recognized Testing Agency or have third party review and approval.

17 57. The developer must show compliance with the 2022 CBC for Disabled Access including: a)
18 Parking; b) Access to building; c) Exiting; d) Bathroom facilities; e) Changes in elevation

19 58. The developer shall monitor, supervise and control all building construction and supportive
20 activities so as to prevent these activities from causing a public nuisance, including, but not
21 limited to, strict adherence with the following:

22 a) Section 6.25. – Construction hour limitations. It shall be unlawful to operate equipment or
23 perform any construction in the erection, demolition, alteration, or repair of any Building
24 or structure or the grading or excavation of land during the following hours:

- 25 • Before 7:00 a.m. and after 7:00 p.m. Monday through Saturday
- 26 • All day Sunday; and
- 27 • On any federal holiday.

28 Exceptions:

- An owner/occupant or resident/tenant of residential property may engage in a home improvement project between the hours of 9:00 a.m. and 5:00 p.m. on Sundays and holidays provided the project is for the benefit of said residential property and is personally carried out said owner/occupant or resident/tenant.
- The Building official may authorize extended or alternate hours of construction for the following circumstances:
 - i. Emergency work
 - ii. Adverse weather conditions
 - iii. Compatibility with store Business hours
 - iv. When the work is less objectionable at night than during daylight hours
 - v. Per the direction of the City Manager’s office for projects that have been determined that rapid completion is in the best interest of the general public (Ord. No. 19-OR0757-1, 1, 12-18-2019; Ord. No. 22-OR0685-1, 1, 10-5-2022)

Fire:

59. **EMERGENCY RESPONSE MAPS - Geo- Referenced Preplans:** Any new development, which necessitates updating of emergency response maps by virtue of new structures, hydrants, roadways or similar features, shall be required to provide map updates. Provide geo-referenced building plan in CAD (.dwg) format using the following coordinate system: NAD_1983_StatePlan_California_VI_FIPS_0406_Feet. Data deliverables (CAD and GIS) shall specifically include a site plan, building plan, all Utility shut-offs, fire sprinkler risers and shut-off valves, the fire department connection for sprinkler and class-I standpipe, all standpipe hose outlets, all stairwells, retail spaces, living units -numbers /locations, fire alarm panels, elevators, fire hydrants and all Knox boxes and key switch locations.
60. New structure shall be tested for Emergency responder radio coverage in accordance with Section 510 of the California Fire Code.
61. The applicant shall provide and maintain 100-foot -fire/fuel breaks to the satisfaction of the Oceanside Fire Department and Government Code 51182.

- 1 62. FIRE RESISTIVE CONSTRUCTION: Structure is required to be designed using state
2 fire marshal standards for fire resistive construction features per 2022 CBC, Chapter 7A
3 and/or CRC R337.
- 4 63. A fire protection plan which identifies ways to minimize and mitigate potential for loss
5 from wildfire exposure shall be submitted and reviewed by the Oceanside Fire
6 Department. Information regarding evacuation plan can be submitted within a CEQA
7 document.
- 8 64. Deferred Submittals:
- 9
 - 10 • Automatic Fire Sprinkler, CFC & NFPA 13
 - 11 • -Class-I Wet Standpipe, CFC & NFPA 14
 - 12 • -Fire Alarm System, CFC & NFPA 72
 - 13 • -Emergency Responder Radio Coverage, CFC Section 510
 - 14 • -Private Underground Fire Mains per CFC & NFPA 24
- 15 65. Fire apparatus access roads shall have an unobstructed improved width of not less than
16 28 or 35 feet (Based on location); curb line to curb line, and an unobstructed vertical
17 clearance of not less than 13 feet 6 inches. Access roads shall be all weather surface and
18 designed to support imposed loads of not less than 78,000 pounds. Secondary access is
19 required and be designed per Oceanside Fire standards.
- 20 66. Install multifamily, commercial-style fire hydrant. Minimum GPM shall be per CFC
21 Appendix B. Industrial fire hydrants shall have One 4-inch port and Two 2.5-inch ports.
22 Installation shall be as per Oceanside Water Department specifications. Maximum
23 spacing from one hydrant to another cannot exceed 400 feet from another. Maximum
24 distance from a fire hydrant to any fire department connection cannot exceed 40 feet.
- 25 67. Knox Key Boxes shall be provided. A master key for entry to all gates, enclosures and
26 equipment rooms or areas is required. Knox box shall be mounted in area approved by
27 fire dept. at height of 60 to 66 inches above grade. Knox box shall be 4400 series,
28 minimum of four will be required.
68. Gates or other devices that may obstruct fire access roadways shall be provided with
Knox Key switch with cover and all drive gates shall be equipped with approved

1 emergency traffic strobe sensor(s), which opens the gate on approach of emergency
2 vehicles. Gates shall have battery back-up or manual means of disconnect in case of
3 power failure.

4 69. Provide exit plan showing travel distance and occupant load to each exit, include in plan
5 occupant load for each room.

6 70. A lighted directory map, meeting current fire department standards, shall be installed at
7 each driveway entrance

8 71. *Note: This list is not meant to be complete. Additional fire and building code*
9 *requirements may apply based on formal plan submittal, intended use of building and*
10 *occupancy classification.*

11 **Engineering:**

12 72. For the demolition of any existing structure or surface improvements; grading plans
13 shall be submitted and erosion control plans be approved by the City Engineer prior to
14 the issuance of a demolition permit. No demolition shall be permitted without an
15 approved erosion control plan.

16 73. Design and construction of all improvements shall be in accordance with the City of
17 Oceanside's Engineers Design and Processing Manual, City Ordinances, standard
18 engineering, and specifications of the City of Oceanside, and subject to approval by the
19 City Engineer.

20 74. All right-of-way alignments, street dedications, exact geometrics, and widths shall be
21 designed, dedicated, and constructed or replaced in accordance with the City of
22 Oceanside Engineers Design and Processing Manual, and as required by the City
23 Engineer.

24 75. The owner/developer shall provide an updated Title Report dated within 6 months of the
25 grading plan application submittal.

26 76. The approval of the development plan/project shall not mean that closure, vacation, or
27 abandonment of any public street, right of way, easement, or facility is granted or
28 guaranteed to the owner/developer. The owner/developer is responsible for applying for
all closures, vacations, and abandonments as necessary. The application(s) shall be

1 reviewed and approved or rejected by the City of Oceanside under separate process (es)
2 per codes, ordinances, and policies in effect at the time of the application. The City of
3 Oceanside retains its full legislative discretion to consider any application to vacate a
4 public street or right of way.

5 77. The owner/developer shall submit to the City for processing a covenant attesting to the
6 project's development conditions. The approved covenant shall be recorded at the
7 County prior to the issuance of a grading permit.

8 78. All public improvement requirements shall be covered by a Development Improvement
9 Agreement and secured with sufficient improvement securities or bonds guaranteeing
10 performance and payment for labor and materials, setting of survey monuments, and
11 warranties against defective materials and workmanship before the approval of the
12 public improvement plans.

13 79. Prior to the issuance of a grading permit, a Lot Merger application (as defined by the
14 Subdivision Map Act), along with a Certificate of Compliance application, shall be
15 processed and filed with the City of Oceanside. The Lot Merger shall not be in effect
16 until all required documents are recorded at the County, which includes a Notice of Lot
17 Merger, Certificate of Compliance, and Grant Deeds.

18 80. Prior to the issuance of any building permits, all improvements including landscaping,
19 landscaped medians, and frontage improvements shall be under construction to the
20 satisfaction of the City Engineer.

21 81. Prior to the issuance of a Certificate of Occupancy permit, all improvements, including
22 landscaping, landscaped medians, and frontage improvements shall be completed to the
23 satisfaction of the City Engineer.

24 82. The owner/developer shall process a separate Right-of-Way dedication application to
25 provide a ROW dedication along the Alex Road cul-de-sac and 10-foot on Benet Road.
26 All ROW dedications are to be in fee, and the application shall be approved prior to the
27 issuance of a grading permit.

28 83. A traffic control plan shall be prepared in accordance with the City's traffic control
guidelines and approved by the City Engineer prior to the start of work within the public

1 Right-of-Way. Traffic control safety and implementation for construction or re-
2 construction of streets shall be in accordance with construction signing, marking, and
3 other protection as required by Caltrans' Traffic Manual and City Traffic Control
4 Guidelines. Traffic control plan implementation and hours shall be in accordance with
5 the approved traffic control plans.

6 84. Vehicular access rights to Benet Rd shall be relinquished to the City from all abutting
7 lots except at the proposed driveway(s). Process an access relinquishment application
8 with the City prior to the issuance of a grading permit, and record the approved
9 document prior to grading plan as-built acceptance.

10 85. An Encroachment Removal Agreement (ERA) application shall be submitted to the City
11 for the proposed 18-inch private storm drain located within the City's ROW within
12 Benet Road. The ERA shall be submitted for review prior to the issuance of a grading
13 permit and recorded at the County prior to the improvement plan as-built acceptance.

14 86. Benet Road shall be constructed with new sidewalk. Sidewalk improvements
15 (construct/replace) shall comply with current ADA requirements.

16 87. Alex Road shall be constructed with new curb, gutter and sidewalk. Sidewalk
17 improvements (construct/replace) shall comply with current ADA requirements.

18 88. An ADA-compliant pedestrian ramp shall be constructed at the cul-de-sac of Alex Road,
19 the Eddie Jones Way and Benet Road intersection, and other locations as required by the
20 City Engineer.

21 89. If hydraulically feasible, the existing storm drain headwall, found in the northwestern
22 portion of the project site, shall be integrated into the proposed on-site storm drain
23 system. The existing 24-inch concrete pipe (Per Improvement Plan R-9919) shall be
24 removed up to the storm drain cleanout located south of Eddie Jones Way.

25 90. Publicly-maintained pedestrian ramps (maintained by the City of Oceanside) must be
26 located entirely within the public right-of-way (ROW). Pedestrian ramps not located
27 entirely within the City's ROW shall be provided with a ROW dedication through a
28 separate ROW dedication application and shown on the improvement plans and grading

1 plans. The ROW dedication shall be submitted prior to the approval of the grading plans
2 and recorded prior to the grading plan as-built acceptance.

3 91. Minimum curb return radius at pedestrian ramp and driveway locations shall comply
4 with the City of Oceanside Engineers Design and Processing Manual.

5 92. Benet Road and Alex Road shall be provided with a 10-foot minimum parkway between
6 the face of the curb and the Right-of-Way line, and the design shall be displayed on the
7 improvement plans.

8 93. Sight distance requirements at the project driveway(s) or street shall conform to the sight
9 distance criteria as provided by Caltrans. The owner/developer shall provide a plan and
10 profile of the line of sight for each direction of traffic at each proposed driveway on the
11 grading plans.

12 94. Maintenance responsibilities for private driveways, parking lots, and roadways are the
13 responsibility of the property owner.

14 95. A pavement evaluation report shall be submitted for the proposed onsite pavement with
15 the grading plan application. Pavement sections for all public and private roadways,
16 driveways, and parking areas shall be based upon approved soil test requirements and
17 traffic indices identified within the City of Oceanside Engineers Design and Processing
18 Manual. The pavement design is to be prepared by the owner/developer's geotechnical
19 engineering firm and be approved by the City Engineer prior to the issuance of a grading
20 permit. Roadway alignments and geometric layouts shall be in conformance with the
21 City of Oceanside Engineers Design and Processing Manual.

22 96. A pavement evaluation report shall be submitted for offsite street pavements with the
23 grading plan application. The owner/developer shall contract with a geotechnical
24 engineering firm to perform a field investigation of the existing pavement on all streets
25 adjacent to the project boundary. The limits of the study shall be half-street width along
26 the project's Benet Road and Alex Road cul-de-sac frontage. The field investigation
27 shall be performed according to a specific boring plan prepared by a licensed
28 Geotechnical Engineer and approved by the City Engineer prior to the issuance of a
grading permit. In the absence of an approved boring plan, the field investigation shall

1 include a minimum of one pavement boring per every one hundred (100) linear feet of
2 street frontage.

3 Should the study conclude that the existing road pavement does not meet current
4 pavement thickness requirements set forth in the City of Oceanside Engineers Design
5 and Processing Manual, the Owner/developer shall remove and reconstruct the existing
6 pavement section in accordance with City requirements. Otherwise, the City Engineer
7 shall determine whether the Owner/developer shall: 1) Repair all failed pavement
8 sections, 2) header cut and grind per the direction of the City Engineer, or 3) Perform R-
9 value testing and submit a study that determines if the existing pavement meets current
10 City standards/traffic indices.

11 97. Proposed public improvements located within the City's ROW or onsite shall be
12 displayed on separate public improvement plans in accordance with the City's Engineers
13 Design and Processing Manual.

14 98. Any existing public or private improvements that are being joined to and that are already
15 damaged or damaged during the construction of the project, shall be repaired or replaced
16 as necessary by the developer to provide a competent and stable connection, and to the
17 City's satisfaction.

18 99. A precise grading plan, which includes proposed onsite private improvements, shall be
19 prepared, reviewed, secured, and approved prior to the issuance of any building permit.
20 The plan shall reflect all pavement, flatwork, landscaped areas, special surfaces, curbs,
21 gutters, medians, striping, signage, footprints of all structures, walls, drainage devices,
22 and utility services. Parking lot striping and any on-site traffic calming devices shall be
23 shown on the precise grading plans.

24 100. The project shall provide and maintain year-round erosion control for the site. Prior to
25 the issuance of a grading permit, an approved erosion control plan, designed for all
26 proposed stages of construction, shall be secured by the owner/developer with cash
27 securities or a Letter-of-Credit and approved by the City Engineer; a Certificate of
28 Deposit will not be accepted for this security.

1 101. The owner/developer shall develop and submit a draft neighborhood-notification flier to
2 the City for review. The flier shall contain information on the project, construction
3 schedule, notification of anticipated construction noise and traffic, and contact
4 information. Prior to the issuance of a grading permit, the approved flier shall be
5 distributed to area residents, property owners, and business owners located within a 500-
6 foot radius area of the project.

7 102. The owner/developer shall monitor, supervise, and control all construction and
8 construction-supportive activities, to prevent these activities from causing a public
9 nuisance, including but not limited to, ensuring strict adherence to the following:

10 a) Dirt, debris, and other construction material shall not be deposited on any public
11 street or into the City's storm water conveyance system.

12 b) All grading and related site preparation and construction activities shall be limited
13 to the hours of 7 AM to 6 PM, Monday through Friday. No engineering-related
14 construction activities shall be conducted on Saturdays, Sundays or legal holidays
15 unless written permission is granted by the City Engineer with specific limitations
16 to the working hours and types of permitted operations. All on-site construction
17 staging areas shall be located as far as possible (minimum 100 feet) from any
18 existing residential development. As construction noise may still be intrusive in the
19 evening or on holidays, the City of Oceanside Noise Ordinance also prohibits "any
20 disturbing excessive or offensive noise which causes discomfort or annoyance to
21 reasonable persons of normal sensitivity."

22 c) The construction site shall accommodate the parking of all motor vehicles used by
23 persons working at or providing deliveries to the site. An alternate parking site can
24 be considered by the City Engineer in the event that the lot size is too small and
25 cannot accommodate parking of all motor vehicles.

26 d) The owner/developer shall complete a haul route permit application (if required for
27 import/export of dirt) and submit it to the City of Oceanside Transportation
28 Engineering Section forty-eight hours (48) in advance of the beginning of work.
Hours of hauling operations shall be dictated by the approved haul route permit.

- 1 103. It is the responsibility of the owner/developer to evaluate and determine that all soil
2 imported as part of this development is free of hazardous and/or contaminated material
3 as defined by the City and the County of San Diego Department of Environmental
4 Health. Exported or imported soils shall be properly screened, tested, and documented
5 regarding hazardous contamination.
- 6 104. The approval of the development plan shall not mean that proposed grading or
7 improvements on adjacent properties (including any City properties/right-of-way or
8 easements) is granted or guaranteed to the owner/developer. The owner/developer is
9 responsible for obtaining written permission to grade or construct adjacent properties
10 prior to the issuance of a grading permit. Should such permission be denied, the
11 development plan shall be subject to going back to the public hearing or subject to a
12 substantial conformity review.
- 13 105. Prior to the issuance of a grading permit, a comprehensive soil and geologic
14 investigation shall be conducted for the project site. All necessary measures shall be
15 taken and implemented to assure slope stability, erosion control, and soil integrity; and
16 these measures shall be incorporated as part of the grading plan design. No grading shall
17 occur at the site without a grading permit.
- 18 106. Where proposed off-site improvements, including but not limited to slopes, public utility
19 facilities, and drainage facilities, are to be constructed, the owner/developer shall, at his
20 own expense, obtain all necessary easements or other interests in real property and shall
21 dedicate the same to the City of Oceanside as required. Owner/developer shall provide
22 documentary proof satisfactory to the City of Oceanside that such easements or other
23 interest in real property have been obtained prior to the issuance of any grading,
24 building, or improvement permit for this development/project. Additionally, the City of
25 Oceanside, may at its sole discretion, require that the owner/developer obtain at his sole
26 expense a title policy insuring the necessary title for the easement or other interest in real
27 property to have vested with the City of Oceanside or the owner/ developer, as
28 applicable.

1 107. Use of adjacent properties for construction without permission is prohibited. Developer
2 is required to obtain written permission from adjacent property owners allowing access
3 onto their site. There shall be no trespassing, grading, or construction of any kind on
4 adjacent properties without permission. "Failure to comply will result in the revocation
5 of the grading permit." This written permission shall be provided to the City prior to the
6 issuance of a grading permit.

7 108. Landscape and irrigation plans for disturbed areas shall be submitted to the City
8 Engineer prior to the issuance of a grading permit and approved by the City Engineer
9 prior to the issuance of building permits. Landscaping plans, including plans for the
10 construction of walls, fences, or other structures at or near intersections, must conform
11 to intersection sight distance requirements. Frontage and median landscaping shall be
12 installed and established prior to the issuance of any certificates of occupancy. Securities
13 shall be required only for landscape items in the public right-of-way. Any project fences,
14 sound or privacy walls, and monument entry walls/signs shall be shown on, bonded for,
15 and built from the approved landscape plans. These features shall also be shown on the
16 precise grading plans for purposes of location only. Plantable, segmental walls shall be
17 designed, reviewed, and constructed from grading plans and landscape/irrigation
18 design/construction shall be from landscape plans. All plans must be approved by the
19 City Engineer and a pre-construction meeting held prior to the start of any
20 improvements.

21 109. Unless an appropriate barrier is approved on a landscape plan, a minimum 42-inch high
22 barrier, approved by the City Engineer, shall be provided at the top of all slopes whose
23 height exceeds 20 feet or where the slope exceeds 4 feet and is adjacent to any streets, an
24 arterial street or state highway.

25 110. The drainage design shown on the conceptual grading/site plan, and the drainage report
26 for this development plan is conceptual only. The final drainage report and design shall
27 be based upon a hydrologic/hydraulic study that is in accordance with the latest San
28 Diego County Hydrology and Drainage Manual and is to be approved by the City
Engineer prior to the issuance of a grading permit. All drainage picked up in an

1 underground system shall remain underground until it is discharged into an approved
2 channel, or as otherwise approved by the City Engineer.

3 111. The project's drainage system shall not connect or discharge to another private storm
4 drain system without first obtaining written permission from the owner of the system.
5 The written permission letter shall be provided to the City prior to the issuance of a
6 grading permit. The owner/developer shall be responsible for obtaining any off-site
7 easements for storm drainage facilities.

8 112. All public storm drains shall be shown on separate public improvement plans. Public
9 storm drain easements shall be dedicated to the City where required.

10 113. Drainage facilities shall be designed and installed to adequately accommodate the local
11 storm water runoff, and shall be in accordance with the San Diego County Hydrology
12 Manual and the City of Oceanside Engineers Design and Processing Manual, and to the
13 satisfaction of the City Engineer.

14 114. The owner/developer shall place a covenant on the non-title sheet of the grading plan
15 agreeing to the following: "The present or future owner/developer shall indemnify and
16 save the City of Oceanside, its officers, agents, and employees harmless from any and all
17 liabilities, claims arising from any flooding that may occur on this site, and any flooding
18 that is caused by this site impacting adjacent properties".

19 115. Storm drain facilities shall be designed and constructed to allow inside travel lanes of
20 streets classified as a Collector or above, to be passable during a 100-year storm event.

21 116. Sediment, silt, grease, trash, debris, and pollutants shall be collected on-site and
22 disposed of in accordance with all state and federal requirements, prior to discharging
23 stormwater into the City drainage system.

24 117. Elevation adjustments and floodproofing shall be in accordance with City of Oceanside
25 Floodplain Management Regulations and Federal Emergency Management Agency
26 (FEMA) and National Flood Insurance Program (NFIP) requirements.

27 118. The owner/developer shall submit a finished construction elevation certificate on current
28 FEMA forms for each structure proposed in the development project. The finished
construction elevation certificate(s) shall be completed with surveyed information for

1 post construction and shall be submitted to the Engineering Division for review and
2 acceptance prior to Occupancy or Final Building Inspection.

3 For non-residential structures that are floodproofed, a floodproofing design certificate by
4 a California licensed civil engineer shall be submitted for review along with the finished
5 construction elevation certificate.

6 119. The project shall demonstrate compliance with all National Flood Insurance Program
7 (NFIP) regulations and City of Oceanside Floodplain Management Regulations and
8 obtain all relevant approvals from the Federal Emergency Management Agency
9 (FEMA), which may include but is not limited to Conditional Letter of Map Revision
10 (CLOMR) and Letter of Map Revision (LOMR).

11 The owner/developer shall submit an application for a Letter of Map Change (LOMC)
12 review with the City prior to, or concurrently with, the first submittal of any grading,
13 erosion, improvement, or building plan.

14 Should a CLOMR/LOMR be determined to be required by FEMA, the owner/developer
15 shall provide evidence to the City of Oceanside that a CLOMR has been obtained from
16 FEMA for the proposed revisions to the flood hazard areas prior to issuance of any
17 permit. After the grading activities or completion of flood-based improvements, the
18 owner/developer shall submit an application with FEMA and the City for a LOMR,
19 along with as-built plans, and any other documents required by FEMA to process and
20 receive the LOMR. Prior to the release of the grading bonds, the owner/developer shall
21 provide a copy of the FEMA-approved LOMR to the City of Oceanside.

22 Should approvals other than a CLOMR/LOMR be determined to be required by FEMA,
23 the owner/developer shall provide evidence of FEMA's conceptual approval of the
24 project and proposed encroachments prior to issuance of any permit as well as FEMA's
25 final approval after completion of the flood based improvements. Or, should no
26 approvals be required by FEMA, the owner/developer shall provide evidence to the City
27 prior to issuance of any permit that no such approval is required.

28 120. Development in any floodway is prohibited unless a California licensed civil engineer
prepares certification that encroachments shall not result in any increase in the base

1 flood elevation during the occurrence of the base flood discharge. A "No-Rise"
2 hydraulic analysis and certification shall be provided for any encroachments located in
3 the floodway. The analysis and certification must indicate that the base flood elevation
4 does not increase at all (greater than 0.00 feet) due to the encroachments.

5 121. The owner/developer shall comply with the provisions of the National Pollution
6 Discharge Elimination System (NPDES) General Permit for Storm Water Discharges
7 Associated with Construction Activity (General Permit) Water Quality Order 2009-
8 0009-DWQ. The General Permit continues in force and effect until a new General
9 Permit is issued or the SWRCB rescinds this General Permit. Only those
10 owners/developers authorized to discharge under the expiring General Permit are
11 covered by the continued General Permit. Construction activity subject to the General
12 Permit includes clearing, grading, and disturbances to the ground such as stockpiling, or
13 excavation that results in land disturbances of equal to or greater than one acre.

14 The owner/developer shall obtain coverage under the General Permit by submitting a
15 Notice of Intent (NOI) and obtaining a Waste Discharge Identification Number
16 (WDID#) from the State Water Resources Control Board (SWRCB). In addition,
17 coverage under the General Permit shall not occur until an adequate SWPPP is
18 developed for the project as outlined in Section A of the General Permit. The site
19 specific SWPPP shall be maintained on the project site at all times. The SWPPP shall be
20 provided, upon request, to the United States Environmental Protection Agency
21 (USEPA), SWRCB, Regional Water Quality Control Board (RWQCB), City of
22 Oceanside, and other applicable governing regulatory agencies. The SWPPP is
23 considered a report that shall be available to the public by the RWQCB under section
24 308(b) of the Clean Water Act. The provisions of the General Permit and the site
25 specific SWPPP shall be continuously implemented and enforced until the
26 owner/developer obtains a Notice of Termination (NOT) for the SWRCB.

27 Owner/developer is required to retain records of all monitoring information, copies of all
28 reports required by this General Permit, and records of all data used to complete the
NOT for all construction activities to be covered by the General Permit for a period of at

1 least three years from the date generated. This period may be extended by request of the
2 SWRCB and/or RWQCB.

3 122. The project is categorized as a stormwater-Priority Development Project (PDP). A final
4 Storm Water Quality Management Plan (SWQMP) and Operation & Maintenance
5 (O&M) Plan shall be submitted to the City for review at the final engineering phase.
6 Both documents are to be approved prior to the issuance of a grading permit.

7 123. The O&M Plan shall include an approved and executed Maintenance Mechanism
8 pursuant to the City of Oceanside BMP Design Manual (BDM). At a minimum, the
9 O&M Plan shall include the designated responsible party to manage the stormwater
10 BMP(s), employee training program and duties, operating schedule, maintenance
11 frequency, routine service schedule, specific maintenance activities, copies of resource
12 agency permits, cost estimate for implementation of the O&M Plan, non-refundable cash
13 security to provide maintenance funding in the event of non-compliance to the O&M
14 Plan, and any other necessary elements. The owner/developer shall complete and
15 maintain O&M forms to document all operation, inspection, and maintenance activities.
16 The owner/developer shall retain records for a minimum of 10 years. The records shall
17 be made available to the City upon request.

18 124. The owner/developer shall enter into a City-Standard Stormwater Facilities Maintenance
19 Agreement (SWFMA) with the City, obliging the owner/developer to maintain, repair,
20 and replace the Storm Water Best Management Practices (BMPs) structures identified in
21 the project's approved SWQMP, as detailed in the O&M Plan, in perpetuity.
22 Furthermore, the SWFMA will allow the City with access to the site for the purpose of
23 BMP inspection and maintenance, if necessary. The Agreement shall be approved by the
24 City Attorney's Office and recorded at the County Recorder's Office prior to the
25 issuance of a precise grading permit. A non-refundable **Security in the form of cash**
26 shall be required prior to the issuance of a precise grading permit. The amount of the
27 non-refundable security shall be equal to 10 years of maintenance costs, as identified by
28 the O&M Plan, but not to exceed a total of \$25,000. The owner/developer's civil
engineer shall prepare the O&M cost estimate.

- 1 125. The BMPs described in the project's approved SWQMP shall not be altered in any way
2 unless reviewed and approved by the City Engineer. The determination of whatever
3 action is required for changes to a project's approved SWQMP shall be made by the City
4 Engineer.
- 5 126. Prior to receiving a temporary or permanent occupancy permit, the project shall
6 demonstrate that all structural BMPs, including Storm Water Pollutant Control BMPs
7 and Hydromodification Management BMPs, are constructed and fully operational, are
8 consistent with the approved SWQMP and the approved Precise Grading Plan, and are
9 in accordance with San Diego RWQCB Order No. R9-2013-0001 §E.3.e. (1)(d).
- 10 127. All new extension services for the development of the project, including but not limited
11 to, electrical, cable, and telephone, shall be placed underground as required by the City
12 Engineer and current City policies.
- 13 128. Prior to the approval of plans and the issuance of a grading permit, the owner/developer
14 shall obtain all necessary permits and clearances from public agencies having
15 jurisdiction over the project due to its type, size, location, or infrastructure impact. The
16 list of public agencies includes, but is not limited to, Public Utility Companies, the
17 California Department of Transportation (Caltrans), the City of Carlsbad, the City of
18 Vista, Fallbrook, the County of San Diego, the U. S. Army Corps of Engineers, the
19 California Department of Fish & Game, the U. S. Fish and Wildlife Service, the San
20 Diego Regional Water Quality Control Board, and the San Diego County Health
21 Department.
- 22 129. The owner/developer shall comply with all the provisions of the City's cable television
23 ordinances, including those relating to notification as required by the City Engineer.
- 24 130. As part of the City's Opportunistic Beach Fill Permit, this project has been conditioned
25 to test proposed excavated material to determine suitability for deposit on city beaches
26 as part of the Beach Sand Replenishment program. Test results shall be provided as part
27 of the project geotechnical report which is required prior to approval of the grading plan
28 and issuance of the grading permit.

1 Suitable beach replenishment material shall be at least 75% sand with no more than a
2 10% difference in sand content between material at the source and discharge site.
3 Replenishment material shall contain only clean construction materials suitable for use
4 in the oceanic environment; no debris, silt, soil, sawdust, rubbish, cement or concrete
5 washings, oil or petroleum products hazardous/toxic/radioactive/munitions from
6 construction or dredging or disposal shall be allowed to enter into or be placed where it
7 may be washed by rainfall or runoff into waters of the United States. Any and all excess
8 or unacceptable material shall be completely removed from the site/work area and
9 disposed of in an appropriate upland site.

10 If the soil to be exported is determined to be suitable beach replenishment material, the
11 developer's contractor will coordinate with the City's Public Works Department to
12 determine the location for acceptance of the excavated material for spreading by Public
13 Works staff. **Coordination is required to occur a minimum of two weeks in advance**
14 of the need to place approved excavated material on the beach.

15 131. If shoring is required for the construction of the proposed development, the shoring
16 design plans shall be included within the grading plan set, and the structural design
17 calculations shall be submitted with the grading plan application.

18 132. This property is located in the vicinity of an airport, within what is known as an airport
19 influence area. As a result, the property may be exposed to some of the common
20 annoyances or inconveniences associated with airport operations (for example: noise,
21 vibration, or odors). An Airport Overflight Notification or Aviation Easement shall be
22 recorded per the Oceanside Municipal Airport Land Use Compatibility Plan and a copy
23 of either document shall be provided to the City prior to issuance of a grading permit.

24 133. Approval of this development project is conditioned upon payment of all applicable
25 impact fees and connection fees in the manner provided in Chapter 32B of the
26 Oceanside City Code. All traffic signal fees and contributions, highway thoroughfare
27 fees, park fees, reimbursements, drainage impact fees, and other applicable charges,
28 fees, and deposits shall be paid prior to the issuance of any building permits, in
accordance with City Ordinances and policies. The owner/developer shall also be

1 required to join in, contribute, or participate in any improvement, lighting, or other
2 special district affecting or affected by this project.

3 134. Upon acceptance of any fee waiver or reduction by the owner/developer, the entire
4 project will be subject to prevailing wage requirements as specified by Labor Code
5 section 1720(b) (4). The owner/developer shall agree to execute a form acknowledging
6 the prevailing wage requirements prior to the granting of any fee reductions or waivers.

7 135. If there are discrepancies in information between the conceptual plan and the conditions
8 outlined in the project's entitlement resolution (Conditions of Approval), the project's
9 entitlement resolution shall prevail.

10 **Landscaping:**

11 136. Landscape plans, shall meet the criteria of the City of Oceanside Landscape Guidelines
12 and Specifications for Landscape Development (latest revision), Water Conservation
13 Ordinance No.(s) 91-15 and 10-Ordinance 0412, Engineering criteria, City code and
14 ordinances, including the maintenance of such landscaping shall be submitted, reviewed
15 and approved by the City Engineer prior to the issuance of building permits.
16 Landscaping shall not be installed until bonds have been posted, fees paid, and plans
17 signed for final approval. In addition, a refundable cash deposit for the preparation of the
18 final As-built/ Maintenance Guarantee shall be secured with the City prior to the final
19 approval of the landscape construction plan. A landscape pre-construction meeting shall
20 be conducted by the landscape architect of record, Public Works Inspector, developer or
21 owner's representative and landscape contractor prior to commencement of the
22 landscape and irrigation installation.

23 The following landscaping items shall be required prior to plan approval and certificate
24 of occupancy:

25 137. Final landscape plans shall accurately show placement of all plant material such as but
26 not limited to trees, shrubs, and groundcovers.

27 138. Landscape Architect shall be aware of all utility, sewer, water, gas and storm drain lines
28 and utility easements and place planting locations accordingly to meet City of Oceanside
requirements.

- 1 139. Final landscape plans shall be prepared under the direct supervision of a Registered
2 Landscape Architect (State of California), with all drawings bearing their professional
3 stamp and signature.
- 4 140. All required landscape areas both public and private (including trees and palms in the
5 public rights-of-way) shall be maintained by owner, project association or successor of
6 the project (including public rights-of-way along Benet Drive and Alex Road). The
7 landscape areas shall be maintained per City of Oceanside requirements.
- 8 141. The As-built/ Maintenance Guarantee (refundable cash deposit) shall not be released
9 until the as-built drawings have been approved on the original approved Mylar
10 landscape plan and the required maintenance period has been successfully terminated.
- 11 142. Proposed landscape species shall fit the site and meet climate changes indicative to their
12 planting location. The selection of plant material shall also be based on cultural,
13 aesthetic, and maintenance considerations. In addition, proposed landscape species shall
14 be low water users as well as meet all fire department requirements.
- 15 143. All planting areas shall be prepared and implemented to the required depth with
16 appropriate soil amendments, fertilizers, and appropriate supplements based upon a soils
17 report from an agricultural suitability soil sample taken from the site.
- 18 144. Ground covers or bark mulch shall fill in between the shrubs to shield the soil from the
19 sun, evapotranspiration and run-off. All the flower and shrub beds shall be mulched to a
20 3" depth to help conserve water, lower the soil temperature and reduce weed growth.
- 21 145. The shrubs shall be allowed to grow in their natural forms. All landscape improvements
22 shall follow the City of Oceanside Guidelines.
- 23 146. Root barriers shall be installed adjacent to all paving surfaces where a paving surface is
24 located within 6 feet of a tree trunk on site (private) and within 10 feet of a tree trunk in
25 the right-of-way (public). Root barriers shall extend 5 feet in each direction from the
26 centerline of the trunk, for a total distance of 10 feet. Root barriers shall be 24 inches in
27 depth. Installing a root barrier around the tree's root ball is unacceptable.

28

- 1 147. All fences, gates, walls, stone walls, retaining walls, and plantable walls shall obtain
2 Planning Division approval for these items in the conditions or application stage prior to
3 1st submittal of working drawings.
- 4 148. For the planting and placement of trees and their distances from hardscape and other
5 utilities/ structures the landscape plans shall follow the City of Oceanside's (current)
6 Tree Planting Distances and Spacing Standards.
- 7 149. An automatic irrigation system shall be installed to provide coverage for all planting
8 areas shown on the plan. Low volume equipment shall provide sufficient water for plant
9 growth with a minimum water loss due to water run-off.
- 10 150. Irrigation systems shall use high quality, automatic control valves, controllers and other
11 necessary irrigation equipment. All components shall be of non-corrosive material. All
12 drip systems shall be adequately filtered and regulated per the manufacturer's
13 recommended design parameters.
- 14 151. All irrigation improvements shall follow the City of Oceanside Guidelines and Water
15 Conservation Ordinance.
- 16 152. The landscape plans shall match all plans affiliated with the project.
- 17 153. Landscape construction drawings are required to implement approved Fire Department
18 regulations, codes, and standards at the time of plan approval.
- 19 154. Landscape plans shall comply with Biological and/or Geotechnical reports, as required,
20 shall match the grading and improvement plans, comply with Storm Water Management
21 Plan (SWMP), Hydromodification Plan, or Best Management Practices and meet the
22 satisfaction of the City Engineer.
- 23 155. Existing landscaping on and adjacent to the site shall be protected in place and
24 Supplemented or replaced to meet the satisfaction of the City Engineer.
- 25 156. All pedestrian paving (both decorative and standard) shall comply with the most current
26 edition of the American Disability Act.
- 27 157. All landscaping, fences, walls, etc. on the site, in medians within the public right-of-way
28 and within any adjoining public parkways shall be permanently maintained by the
owner, his assigns or any successors-in-interest in the property. The maintenance

1 program shall include: a) normal care and irrigation of the landscaping b) repair and
2 replacement of plant materials (including interior trees and street trees) c) irrigation
3 systems as necessary d) general cleanup of the landscaped and open areas e)
4 maintenance of parking lots, walkways, enhanced hardscape, trash enclosures, walls,
5 fences, etc. f) pruning standards for street trees shall comply with the International
6 Society of Arboriculture (ISA) *Standard Practices for Tree Care Operations – ANSI*
7 *A300, Appendix G: Safety Standards, ANSI Z133; Appendix H; and Tree Pruning*
8 *Guidelines, Appendix F* (most current edition). Failure to maintain landscaping shall
9 result in the City taking all appropriate enforcement actions including but not limited to
10 citations. This maintenance program condition shall be recorded with a covenant as
11 required by this resolution.

12 158. In the event that the conceptual landscape plan (CLP) does not match the conditions of
13 approval, the resolution of approval shall govern.

14 **Solid Waste:**

15 159. The plans demonstrate enclosures at each end of the buildings, with space for all three
16 streams of service (landfill, recycling and organics). Each bin shall be labeled for
17 landfill, recycling, and organics.

18 160. The City of Oceanside reserves the right to review program and services levels and
19 request increases if deemed necessary. The City of Oceanside Municipal Code Chapter
20 13 requires that Oceanside residents, businesses and multifamily projects are to separate
21 all recyclable material from other solid waste. Additionally, the State of California
22 regulations requires all California businesses participate in Mandatory Recycling (AB
23 341) and Mandatory Commercial Organics Recycling (AB 1826 & SB 1383) as outlined
24 in the Oceanside Solid Waste code.

25 **Water Utilities:**

26 161. The developer will be responsible for developing all water and sewer utilities necessary
27 to develop the property. Any relocation of water and/or sewer utilities is the
28 responsibility of the developer and shall be done by an approved licensed contractor at
the developer's expense.

1 162. All Water and Wastewater construction shall conform to the most recent edition of the
2 *Water, Sewer, and Recycled Water Design and Construction Manual* or as approved by
3 the Water Utilities Director.

4 163. The property owner shall maintain private water and wastewater utilities located on
5 private property.

6 164. Water services and sewer laterals constructed in existing right-of-way locations are to be
7 constructed by an approved and licensed contractor at developer's expense.

8 165. The building may be served by a commercial master meter, but each tenant space shall
9 be equipped with a separate water sub-meter.

10 166. Provide a separate irrigation water meter for the development. An address assignment
11 will need to be completed for the meter, and can be processed through the City Planning
12 Department.

13 167. Buildings requiring an NFPA 13 automatic sprinkler system for fire protection shall
14 have a dedicated fire service connection to a public water main with a double check
15 detector backflow assembly. Location of the backflow assembly must be approved by
16 Fire Department.

17 168. Any proposed private onsite fire hydrants shall be served by a private fire main that is
18 looped onsite with two connections to an existing public water main. Each connection
19 shall have a double check detector assembly for backflow protection. Size-on-size hot
20 taps are not acceptable and cut-in tees shall have gate valves on all three ends.

21 169. The onsite sewer collection system shall be private, and an inspection manhole, as
22 described by the *Water, Sewer, and Recycled Water Design and Construction Manual*,
23 shall be constructed behind the property line prior to connection to the public sewer
24 system.

25 **The following conditions shall be met prior to the approval of engineering design plans.**

26 170. Any water and/or sewer improvements required to develop the proposed property will
27 need to be included in the improvement plans and designed in accordance with the
28 *Water, Sewer, and Recycled Water Design and Construction Manual*.

- 1 171. All public water and/or sewer facilities not located within the public right-of-way shall
2 be provided with easements sized according to the *Water, Sewer, and Recycled Water*
3 *Design and Construction Manual*. Easements shall be constructed for all weather
4 access.
- 5 172. No trees, structures or building overhang shall be located within any water or
6 wastewater utility easement.
- 7 173. Per City of Oceanside Ordinance No. 14-OR0565-1, the developer shall pay a recycled
8 water impact fee since the proposed project is not within 75 feet of a recycled water
9 main. The impact fee shall be established by submitting a formal letter requesting the
10 City to determine this fee, which is based on 75% of the design and construction cost to
11 construct a recycled water line fronting the property in Benet Road.
- 12 174. An inspection manhole for commercial waste as described by the *Water, Sewer, and*
13 *Recycled Water Design and Construction Manual*, shall be on each building sewer
14 lateral immediately behind the property line and the location shall be called out on the
15 approved engineering plans.
- 16 175. For the private sewer lift station; provide calculations outlining capacity of the pumps,
17 dwelling units served, capacity of the wet well, detention times, length and size of the
18 force main, and provision of any odor control equipment shall be submitted for review to
19 the Water Utilities Department. It shall be the responsibility of the design engineer to
20 ensure that lift station is adequately sized, has sufficient redundant measures (dual
21 pumps that will each handle estimated peak sewer flows, back-up power supply,
22 emergency by-pass connection for portable pump, alarm systems, high water alarms,
23 etc.), and complies with all applicable local, state, and federal regulations.
- 24 176. A Grease Interceptor, as required per City of Oceanside Ordinance 07-OR0021-1 & 18-
25 OR0021-1 relating to food service establishments shall be included in the private
26 collection system, when deemed necessary, in an appropriate outside location and shall
27 be maintained by the property owner. The grease interceptor shall be shown on
28 Engineering Plans with reference to Building Plans for design and detail.

1 177. An Oil and Sand Interceptor, as described by the latest adopted California Plumbing
2 Code Chapter 10, relating to garages, gasoline stations, wash racks or when deemed
3 necessary shall be shown on building plans at each building sewer in an appropriate
4 location and shall be maintained in accordance with the Fats, Oil, and Grease permit.
5 The location shall be shown on the approved Engineering Plans with reference to
6 Building Plans for design and detail.

7 178. Connections to a public sewer main with a 6-inch or larger sewer lateral will require a
8 new sewer manhole for connection to main per Section 3.3 of *Water, Sewer, and*
9 *Recycled Water Design and Construction Manual*.

10 179. Connection to an existing sewer manhole will require rehabilitation of the manhole per
11 City standards. Rehabilitation may include, but not be limited to, re-channeling of the
12 manhole base, surface preparation and coating the interior of the manhole, and replacing
13 the manhole cone with a 36" opening and double ring manhole frame and lid.

14 180. A separate irrigation meter and connection with an approved backflow prevention device
15 is required to serve landscaped areas and shall be displayed on the plans.

16 181. Provide peak irrigation flows per zone or control valve to verify size of irrigation meter
17 and reduced pressure principle backflow device on Landscape Plans.

18 182. Provide stationing and offsets for existing and proposed water service connections and
19 sewer laterals on plans.

20 183. Any water services or sewer laterals no in use by the proposed development or
21 redevelopment shall be abandoned in accordance with Water Utilities requirements.
22 Developer will be credited for any existing water meters that will be abandoned as part
23 of this development. Credit can be applied toward the purchase of any new water meters,
24 the amount of the current buy-in fee of the existing meter. Should the total credit exceed
25 proposed fees for new meters, the Developer will not be reimbursed the credit
26 monetarily.

27 //

28 //

The following conditions of approval shall be met prior to building permit issuance.

- 184. Show location and size of proposed water meter(s) on site plan of building plans. Show waterline from proposed meter to connection point to building. Water service shall have a RP backflow assembly per City Standard Drawing W-12 or W-13.
- 185. Show location and size of proposed sewer lateral(s) from property line or connection to sewer main to connection point at building.
- 186. Provide a fixture unit count table and supply demand estimate per the latest adopted California Plumbing Code (Appendix A) to size the water meter(s) and service line(s).
- 187. Provide drainage fixture unit count per the latest adopted California Plumbing Code to size sewer lateral for property.
- 188. If a Sand and Oil Separator is required, then building plans must show drainage fixture unit count and calculations per the latest California Plumbing Code to size oil and sand separator and show on plans the location, make and model of separator, inlet/outlet piping, and a plumbing schematic of the separator along with the required appurtenances at each building sewer lateral.
- 189. If a Grease Interceptor is required per City of Oceanside Ordinance 07-OR0021-1, then building plans must show sizing calculations per the latest California Plumbing Code, the location, the make and model, and plumbing schematic showing the required appurtenances at each building sewer lateral.
- 190. Water and Wastewater buy-in fees and the San Diego County Water Authority Fees are to be paid to the City at the time of Building Permit issuance per City Code Section 32B.7.

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1 PASSED AND ADOPTED Resolution No. 2025-P04 on February 10, 2025, by the
2 following vote, to wit:

3 AYES: Morrissey, Malik, Rosales, Balma, Dodds, Anthony, Ogden

4 NAYS:

5 ABSENT:

6 ABSTAIN:

7 
8 _____
9 Tom Morrissey, Chairperson
10 Oceanside Planning Commission

11 ATTEST:
12 
13 _____
14 Sergio Madera, Secretary

15 I, SERGIO MADERA, Secretary of the Oceanside Planning Commission, hereby certify that
16 this is a true and correct copy of Resolution No. 2025-P04.

17 Dated: February 10, 2025

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OCEANSIDE^{city of}CA

RECEIVED

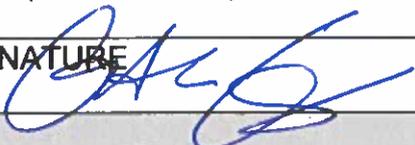
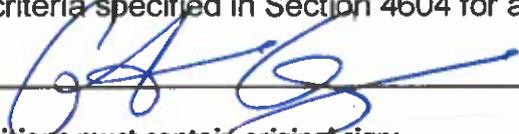
FEB 20 2025

City Clerk Department
300 North Coast Highway
Oceanside, CA 92054
Tel (760) 435-3000 Fax (760) 967-3922

OCEANSIDE CITY CLERK

APPEAL OF PLANNING COMMISSION ACTION

APPEALS MUST BE FILED WITH THE CITY CLERK'S OFFICE WITHIN 10 CALENDAR DAYS OF THE DATE OF FINAL ACTION

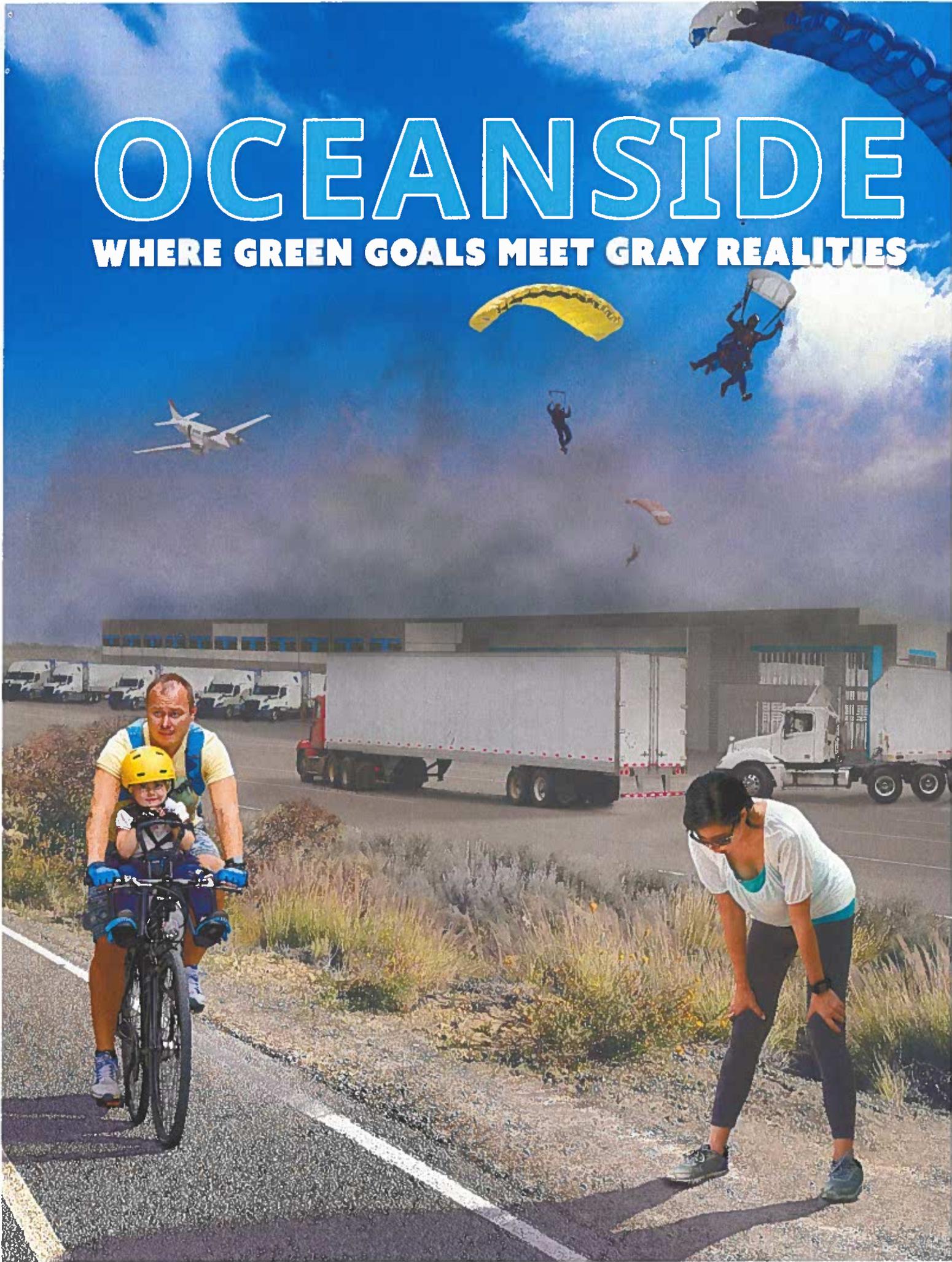
PROJECT/ACTION BEING APPEALED			
PROJECT NAME EDDIE JONES WAREHOUSE PROJECT		DATE OF FINAL DECISION FEBRUARY 10, 2025	
RESOLUTION NUMBER 2025-PO4 + 2025-PO5			
FORM OF APPEAL			
APPEAL FEE - \$1,838 <input type="checkbox"/> PETITION <input checked="" type="checkbox"/> (PLEASE SEE BELOW FOR EXPLANATION/SIGN OFF)			
LETTER INCLUDING A STATEMENT SPECIFICALLY IDENTIFYING THE PORTION(S) OF THE DECISION BEING APPEALED AND THE BASIS FOR THE APPEAL IS ATTACHED <input checked="" type="checkbox"/>			
PERSON FILING APPEAL			
NAME GRETCHEN GARY		Daytime Telephone: (760) 560-6103	
ADDRESS 471 TISHMAL COURT	CITY OCEANSIDE	STATE CA	ZIP 92058
APPEALED BY <input type="checkbox"/> APPLICANT (COMPANY/REP) <input type="checkbox"/> INTERESTED PARTY <input checked="" type="checkbox"/> PROPERTY OWNER WITHIN NOTICE AREA			
SIGNATURE 		DATE 2/19/2025	
CONTACT PERSON			
please use both emails: (IF DIFFERENT FROM PERSON FILING APPEAL)			
NAME gretchengary@gmail.com / cmccconnell4444@gmail.com			
ADDRESS	CITY	STATE	ZIP
Daytime Telephone	Fax Number	Email Address	
<p>Section 4604: To appeal by petition for a waiver of the appeal fee, the appeal must be accompanied by the signatures of 50% of the property owners within the noticed area or 25 signatures of the property owners or tenants within the noticed area, whichever is less.</p> <p>I hereby certify that this appeal is being submitted in accordance with the Zoning Ordinance and meets the criteria specified in Section 4604 for an appeal by petition.</p> <p>Signature:  Date: 2/19/2025</p> <p>NOTE: All petitions must contain original sign:</p>			

Received by: Leslie Huerta
Via: Counter
Copy to: CMO, CAO, DS.
Sergio, Rob, Clerk

h signer.

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES



OCEANSIDE SPEAKS OUT

Oceanside Citizen's Group – Collected 5,831 Signatures Opposing this Discretionary Project
OceansideSpeaksOut2.org · Facebook “Eddie Jones Project / Oceanside”

February 11, 2025

Oceanside City Council Members
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054

**APPEAL: CITY OF OCEANSIDE PLANNING COMMISSION DECISION – APPROVED
PC RESOLUTION NO. 2025-P04 + PC RESOLUTION NO. 2025-P05**

**RE: EDDIE JONES PROJECT DEVELOPMENT PLAN (D22-00001)
CONDITIONAL USE PERMIT (CUP22-00001) AND VARIANCE (V22-0001)
LOCATION: 250 EDDIE JONES WAY, OCEANSIDE, CA 92058**

INVESTORS:

10% = RAF Pacifica Group of Encinitas, CA

90% = The Carlyle Group, a multinational ‘private equity’ alternative asset management / financial services corporation headquartered in Washington DC, managing over \$447 Billion in assets internationally

SPECIFIC JUSTIFICATION / REASON FOR APPEAL:

Due to the unique topography / location, this discretionary project that will cause harm to residents and tourists.

Dear City of Oceanside Council Members,

The City of Oceanside’s Planning Commission approved the Eddie Jones Warehouse, Manufacturing, and Office Facility Project on February 10, 2025, allowing for the construction of four separate buildings ranging in size from 109,660 square feet to 134,015 square feet, for a cumulative total of 497,822 square feet of warehouse and distribution facilities at 250 Eddie Jones Way.

As representatives for the 236 residents living within 1500 feet of the Discretionary Project site’s notification area, we formally APPEAL the City Planning Commission’s

approval of these warehouse and distribution facilities on behalf of all residents of Oceanside.

WHO ARE WE:

“Oceanside Speaks Out” (OSO) is a grass roots citizens group organized over two years ago, to educate the public about the Eddie Jones Warehouse Project. The people who make up OSO vary widely: Oceanside residents, anyone who utilizes the Highway 76 corridor, and anyone who works here, or vacations here.

A total of 5,831 people have signed petitions to document their concerns about the Eddie Jones Discretionary Project.

CITY’S STRATEGIC VISION:

As shepherds of the City’s operation and development, City Council members are responsible for embracing the City of Oceanside’s strategic vision. The Eddie Jones Discretionary Project stands in direct conflict with the City’s General Plan – particularly the objective to develop Oceanside into a premier tourist destination. The proposed warehouse undermines this objective by introducing industrial scale pollution, noise and traffic congestion – factors that will deter visitors and degrade the quality of life for local residents.

CITY’S EXISTING ZONING ORDINANCE:

Per Article 13 of the City’s Zoning Ordinance an industrial facility for distribution and storage shall *NOT exceed* 50,000 square feet and allows *no more than* six (6) heavy trucks allowed on the premises at one time, unless the developer applies for a Conditional Use Permit (CUP).

**** It is important to note, that the ordinance does NOT specify the number of heavy truck TERMINALS. (Just number of trucks ON PREMISES as any one time.)**

**** Also, the ordinance does NOT regulate number of grade-level truck loading doors, whatsoever.**

The developer originally designed a single building with 114 heavy truck terminals (aka: “dock-high” terminals.) The current proposed four-building re-design asks for 56 heavy truck terminals, plus 45 “grade-level” truck loading doors, **for a total of 101 truck access points – a significantly larger number than zoning ordinance limit of 6 heavy trucks allowed on premises at one time.**

On February 10, 2025, the Planning Commission approved that the Eddie Jones Discretionary Project would be allowed up to thirty-four (34) heavy truck terminals

(presumably eight (8) heavy truck terminals at each of the three 109,000-132,000 square foot buildings and a 10 heavy truck terminals at the larger 134,000+ building.)

Of note, the Conditional Use Permit (CUP) as worded and approved does NOT address the number of heavy trucks allowed on the premises, just the number of truck terminals. *This is a glaring and confusing inconsistency between the zoning ordinance, the development plan and CUP.

The decision on February 10, 2025 includes built-in permission that, at any time in the future, a tenant may simply return to the Planning Commission to request additional heavy truck terminals.

**** The actual number could be as high as the original request of 114 truck terminals because the Planning Commission's RESOLUTION 2025-P05 provided blanket approval of the EIR in its entirety, written for the original 114 truck terminals.**

This open-ended approval does NOT limit the multi-building facility to thirty-four (34) truck terminals as specified in RESOLUTION 2025-P04. And, **the number of "grade-level" truck doors are unregulated, so the developer can simply increase the total truck access points back up to >100 at any time.**

The developer's insistence on a high number of truck access points effectively paves the way for any future tenant to use the location as a last mile delivery hub. If this becomes a reality, public input remains essential. (However, the burden on the public to monitor future Conditional Use Permit (CUP) requests on an ongoing basis is neither reasonable nor sustainable.)

DEVELOPER'S VISION:

The Eddie Jones Discretionary Project's proposed four (4) building footprint will be 288% larger than the previous TE Connectivity facility (old building was 172,300 square feet, compared to total of 497,822 square feet of the Eddie Jones Complex). There will be no escaping the view of these four (4) buildings, as they will consume all 31.79 acres of land. From every vantage point, they will dwarf all other structures in the San Luis Rey River Valley; so massive visually, they will destroy both perspective and appeal of the *San Luis Rey River*, and the *Mission San Luis Rey* an important marker of California's history founded in 1798.

The overall magnitude of the Eddie Jones building complex is not consistent nor compatible with the *size* of existing development in the vicinity and **buildings of historical significance.**

↘ *See EXHIBIT A – Comparison of Multi-Building Complex relative to Other Buildings

RAF Pacifica Group repeatedly says that the Eddie Jones Warehouse Project will be similar to their La Pacifica Project = three-building, 103 heavy truck terminal, multi-tenant industrial distribution and manufacturing business park located at 3801, 3809 and 3817 Ocean Ranch Blvd. This is an oversimplified comparison! **La Pacifica is located in an area with other like sized buildings, and is not adjacent to a residential community.** Nor does La Pacifica's property sit amongst family-focused recreational activities. Benet Road has entry/exit points onto the San Luis Rey River Trail, which poses a danger to bicycles, etc. Alex Road has entry/exit points onto the River Trail AND traverses immediately in front of the Prince Skate Park.

After this project is complete, Alex Road is scheduled to have a regular parade of light truck and delivery van traffic. That is, along with the skate board, bicycle and scooter traffic.

Other differences between La Pacifica and Eddie Jones is the soon-to-be developed OceanKamp which will bring even more traffic complexity to Alex Road; naïve tourists will be forced to navigate through traffic with light trucks and vans. The mixture of light trucks, delivery vans, bicycles, skate boards, and pedestrians, all intersecting in a future traffic circle area creates unnecessary risk for tourists.

Lastly, when studying the street terrain for access to La Pacifica there are multiple North-South and East-West approaches to these buildings. These buildings are in the middle of a typical grid pattern of established city streets. There are multiple approaches to the La Pacifica buildings, allowing for emergency vehicles in (ingress) and evacuations out (egress). Additionally, Ocean Ranch sits high on a breezy hilltop in an area **purposefully selected, planned and built for this kind of development.**

This is all distinctly different compared to the unique terrain and topography of a narrow and deep River Basin that has evolved over 80+ years in an unstructured piecemeal fashion. Adding four (4) massive warehouse buildings to the chaotic and inconsistent infrastructure of the San Luis Rey Valley is not the answer.

LOCAL TERRAIN AND TOPOGRAPHY:

The San Luis Rey River Valley surrounded by its **cliff walls creates a dish bowl effect that prevents typical grid pattern of established suburban streets.** The Airport Community is isolated on the North side of the River with only two (2) bridges at Benet and Fousat Roads, both one-lane-each-direction roads. Large businesses (AIM Recycling, S&R Towing, CalPortland), Prince of Peace Abbey and approximately 1200 homes are isolated across these two (2) bridges North of the River.

The Eddie Jones Discretionary Project's proposed business plan includes 1,286 Average Daily Trips (ADT) **consuming BOTH of these critical one-lane-each-direction-access points.** Eddie Jones truck traffic will change the Level of Service (LOS) traffic measurement for the intersection of Benet Rd and Highway 76 to move from Grade D to

F. (Grading system is A (best) to F (worst) conditions.) The domino effect is additional traffic and congestion on Highway 76 that serves 100,000+ citizens up the River Valley, East of this Discretionary Project.

Another unique characteristic of the unique topography of the San Luis Rey River Valley's cliff-sided topography is its ability to **amplify sound waves as you rise out of the Valley**. This results in significant sound impacts on the residents, visitors and worshipers at the Prince of Peace Abbey, and residents in the homes on the escalating hillsides North of the proposed site.

A third unique variable of the cliff lined River Valley is its **ability to channel and hold air pollutants**. Use of only 'daily thresholds of significance' (as was done in the Final EIR) does not fully characterize the air quality impacts to the Valley. A more accurate measure of air quality is based on the concentration of various air pollutants. Concentrations are affected by numerous other factors such as proximity to pollution generating businesses, weather conditions, etc.

Given the unique terrain / topography (and NOT a simple city grid pattern of street access) the developer's vision for a warehouse complex of buildings is unrealistic. A distribution warehouse complex in this location harms residents of the adjacent community AND compounds the problem of an already strained infrastructure for those commuting East and West along the narrow Highway 76 River Valley corridor.

LOCATION'S IMPACT ON RECREATION & TOURISM:

The proposed location for the Eddie Jones multi-building warehouse and distribution facilities will be sandwiched between tourist and community recreation locations.

Recreational and tourist activities adversely impacted are:

- a. "San Luis Rey River Trail" a 9-10 mile paved bicycle trail specifically designed to be *"tucked away from busy traffic"* (per City of Oceanside website.) *Note: This statement will **no longer be true** adjacent to daily operations of a diesel heavy truck facility.
- b. "Prince Memorial" aka: "Alex Road Skate Park" attracts children & teens from all of San Diego County, and skate boarders from around the world. This skate park is **only several hundred yards downwind** of the Discretionary Project.
- c. "Go Jump Oceanside" skydiving center on Airport Road which attracts thrill seekers from all over Southern California.
- d. "Pacific Coast Flyers Oceanside" provides rental aircraft to pilot and students at Oceanside's Bob Maxwell Municipal Airport.
- e. "OceanKamp" future wave pool for surf enthusiasts, with retail, dining, rock climbing, bicycle rental and proposed EVENT SPACE intended to draw attendees from all over Southern California.
- f. *Mission San Luis Rey*, a cornerstone of North San Diego County's past; a sacred place for many Californians.

Oceanside’s reputation and long-term prosperity are interconnected and dependent on tourism. Tourism is emphasized in the City’s General Plan and economic strategies. Yet, the proposed Eddie Jones Discretionary Project undermines these goals by creating an area of disproportional risk from air pollutants hovering in a dish bowl area full of recreational and tourist activities.

The Discretionary Project’s location is only two (2) miles from the ocean, an area that is central to a rising recreational area. The San Luis Rey River bicycle and walking trail serves to connect the City’s vibrant downtown, harbor and beach communities with interior tourist-centric destinations and activities. OceanKamp wave park will soon draw thousands of families with children to the surrounding area every day. **OceanKamp’s economic prospects and viability are dependent upon attracting *and retaining* tourists** – both important drivers in Oceanside’s strategy to expand its reputation as a ‘go to’ Southern California destination. Why would we deliberately place a Discretionary Project involving heavy diesel trucks right in the middle of our future gentrification plans for the area?

UNFOUNDED ASSUMPTIONS REGARDING JOB LEVEL IMPACT:

Tenants / occupants of the Eddie Jones buildings are unknown. Therefore, it is impossible to accurately assume that the Discretionary Project will attract “high paying biotech, med-tech and pharmaceutical jobs.” The concept of >100 truck access points (heavy truck terminals + grade-level doors) and the concept of “high paying biotech jobs” are mutually exclusive. Higher paying jobs in biotechnology (biomedical engineer, biochemist, research scientist, biostatisticians, pharmaceutical development director, chief medical officer, chief executive officer) are roles in research laboratories and executive offices. They do NOT work in buildings that have >100 truck access points, with 140 truck trips a day.

**** Note: this equates to 11 truck trips PER HOUR during peak morning commute = equivalent to one truck every 6 minutes waiting at the intersection of Highway 76 and Benet Rd with its 4 minute light cycle.**

Yes, the developer has built other projects that have biotech tenants, but that does not guarantee that future tenants of this Discretionary Project will be similar businesses. **In actuality, the four (4) building complex’s design with >100 truck access points (via combination of truck terminals and *unregulated* grade-level truck loaded doors), could just as easily attract “last mile” tenants.**

Last mile businesses are qualitatively distinct because of the mix of vehicles used for deliveries and the use of casual workers. Warehouse logistics facilities are notorious for their high turnover rate of employees. (Turnover rates in the industry as are high as 49%.) Often over time, last mile businesses are forced to attract employees from greater and greater distances, creating additional commute traffic in a city. The average hourly

earnings of workers in this sector, according to the Bureau of Labor Statistics is \$898 per week. This amounts to less than \$45,000 over a 50-week year.

↪ **See EXHIBIT B – Comment Letter from Teamsters Local Union No. 542*

FINANCIAL BENEFIT FOR THE CITY:

The executive summary of the Economic & Fiscal Impact Analysis for this Discretionary Project estimates the total NET surplus to the City of Oceanside as \$72,103 annually. Additionally, it is anticipated the City will benefit from one-time Discretionary Project development impact fees, of approximately \$2.1 million (paid as one-time fees for the initial development.) These numbers would certainly benefit the City's coffer. However, these revenue projections are not exclusive to warehouse distribution style building designs. **Other light industry building designs could generate similar one-time development fee income, and ongoing annual net income streams.** And, other light industry building designs exist, that would fit better in the location's unique topography and impact on the health and safety of residents, tourists and local businesses.

The true financial value for the City is rooted in the land.

No matter what future light industry building(s) are built on Eddie Jones Way, the discretionary project will generate development fee income and annual net income for the City. It does not even have to be a discretionary project that requires dozens of heavy trucks and an exception to the City's existing zoning ordinance.

It is the responsibility of all stakeholders (City Council Members, City Planners, local businesses and residents) to consider options and remember that this is a DISCRETIONARY PROJECT that will be in operation for multiple decades.

BUILDING DESIGN OPTIONS TO MATCH UNIQUE TOPOGRAPHY:

Modern light industrial facilities withOUT heavy truck traffic already exist in Oceanside and can generate similar revenue, jobs and long-term prosperity for the community. One example is "Oceanside Gateway Business Park" at Oceanside Blvd and Ord Way built on a similarly size parcel (37 acres). Oceanside Gateway has multiple multi-use industrial buildings with warehouse and office flex spaces. Individual units provide highly functional warehouse clearance heights, large grade-level doors and **independent unit designs conducive to support dozens of varied small businesses** (not just four (4) large tenants with the existing discretionary project design). Additionally, this kind of building complex better aligns with size and operations of existing businesses in the area south of the Airport runway and south of Highway 76 – all of which are zoned light industry and fall within existing zoning ordinance requirements.

↪ **See EXHIBIT C – Relatable Example: Oceanside Gateway Business Park*

NEW MULTI-BUILDING CONFIGURATION WITHOUT NOISE ANALYSIS:

The developer recently revised their Project significantly by changing to a multi-building configuration. **The EIR did not provide an analysis of the impact of this new *North-South orientation*; in particular, noise levels for the surrounding area.** Noise pollution from backup alarms of diesel heavy trucks, cargo vans, forklifts and supporting machinery will be **amplified by the cliff-sided topography of the San Luis Rey River Valley.**

We have independently performed the needed acoustic calculations. The new building configuration has improved the noise impact for some parts of the area. However, the critical San Luis Rey River Habitat, its associated wildlife, and the Wanis View homes will continue to experience **noise from warehouse and distribution facility operations at times with levels exceeding 5 times the background level.**

↘ **See EXHIBIT D - Acoustic Calculations New Multi-Building Configuration*

FUTURE BUILDING DESIGN CONSIDERATIONS:

To minimize noise in the cliff-lined dish bowl Valley, future building(s) on this property should be constructed in ***an East-West orientation with all access points and activity limited to the South side of the building.*** Additionally, all affiliated equipment use, traffic and parking of employee vehicles, service trucks, forklifts, any equipment with backup alarms, should be limited to the South side of the building. Hours of operation of future tenant(s) and ambient light generated by the facility should align with all stakeholders in the multi-use area.

Given the Discretionary Project's ***tourist-centric location upwind of key recreational activities***, we urge the City to consider imposing a full or partial net-zero Greenhouse-Gas (GHG) significance threshold for any development on this land. Requiring full or partial net-zero significance aligns with the City's own Climate Action Plan (CAP), the City's strategic goal of becoming a 'go to' Southern California tourist destination, and the California Air Resources Board's (CARB) Climate Change Scoping Plans. Additionally, it would protect the City and Applicant from California Environmental Quality Act (CEQA) GHG litigation.

CONDITIONS OF APPROVAL IN RES 2025-P04 FAILED TO INCLUDE WILDFIRE EVACUATION STUDY (WES) RECOMMENDATIONS:

We appreciate that the developer prepared the requested Fire Evacuation Time Study, but we disagree with the conclusion that adding 10 minutes to a worst-case scenario that already results in a 59 minute evacuation time is a "less than significant impact."

Furthermore, there are numerous recommendations in the Study that need to be incorporated into Project conditions. The document states: "The Wildfire Evacuation Study (WES) requires regular adjustment and continuous coordination by the Owner(s) and or Property Manager and fire/law enforcement agencies during each stage of the

construction process.” It also says that in order to evacuate more people (as will be required with this Discretionary Project) that emergency managers must “1. Provide more lead time to evacuate and 2. Control traffic levels during evacuations so fewer vehicles are trying to exit at the same time.”

This and other recommendations of this Study should have been formally incorporated into the Discretionary Project conditions of approval. Conditions of Approval in RES 2025-P04 failed to reference this report or incorporate any of its recommendations.

Additionally, in the Final EIR, the Wildfire Evacuation Study (WES) makes several questionable assumptions as to where a wildfire could start, the fuel loading, and the time available for residents to evacuate. More realistic assumptions of the “fire start location” and variations in wind direction and speed will greatly reduce the time residents will have to evacuate. With such a tight timeline, **the impact of the warehouse and associated heavy trucks will have on hindering evacuation will be very significant.**

↘ **See EXHIBIT E – Wildfire Safety: Inaccurate Evacuation Assumptions*

OVERSIMPLIFIED TRAFFIC MODEL:

The mathematical model used in this Discretionary Project’s traffic analysis simply equated one (1) heavy truck to two (2) passenger cars; however, there are clearly varying lengths of trucks, especially when comparing heavy diesels trucks versus mid-size trucks. Additionally, the model failed to include a variable for the weight of the trucks. **A heavily laden heavy truck moves at a vastly slower pace, than empty trucks and mid-sized trucks.** This oversimplification does not accurately reflect the true impact of trucks on overall traffic movement and **light cycle disruptions at Highway 76 and Benet Road that will be caused by 140 truck trips a day, with total of car + truck = 1,286 Average Daily Trips (ADT).**

The City’s Circulation Element includes an objective for acceptable Level of Service (LOS) Grade D or better on an average daily basis. The Eddie Jones Discretionary Project would **contribute to traffic at a failing intersection at Highway 76 and Benet Road; under several scenarios, LOS would be reduced to Grade F.**

The Discretionary Project proposes a “fair share” payment in the amount of \$50,000 to the City’s Thoroughfare and Signal Account for the City to use at its discretion to improve the traffic at ANY location in the City. This is a **staggeringly low number compared to the annual wear and tear City streets will incur from four (4) distribution warehouses creating an additional 140 truck trips a day, with total of car + truck = 1,286 Average Daily Trips (ADT).**

The Final EIR responses to comments on page RTC-97 states “Consistent with the City’s formally adopted VMT standards the Draft EIR uses SANDAG’s Employee VMT by census tract...”

This is misleading because the drivers of the heavy trucks, cargo vans, etc. accessing this Discretionary Project are not employees, and consequently NOT included in the analysis of VMT and the associated GHG emissions.

Additionally, the mitigation measures of incentivizing employees to commute in carpools, vanpools, etc. will have impact on total VMT, but **no impact whatsoever on drivers of the heavy trucks, cargo vans, etc. traveling to/from the distribution warehouse.**

TRANSPORTATION DEMAND MANAGEMENT (TDM) PLAN, NOT PROVIDED:

Other City Projects include the Draft TDM Plan that is required by City Ordinance and to comply with the City's Climate Action Plan (CAP). A TDM Plan is essential to determine if there is sufficient disclosure of transportation impact, to fully disclose additional trips that will be generated, and to provide opportunity for public comment. **This Discretionary Project did NOT include even a Draft TDM Plan. *Failure to provide this document leaves compliance with the CAP unknown, and this potentially significant impact, unaddressed.**

 **See EXHIBIT F – Failure to Provide TDM Plan*

CITY COMPLIANT WITH NEW STATE REGULATION?

California Assembly Bill (AB) 98, signed into law on September 29, 2024 regulates warehouses and trucking activities to reduce emissions to enhance community health. The bill mandates environmental and community health protections for warehouses larger than 250,000 square feet, with prescriptive requirements such as landscape buffering and truck access locations setback >500 feet from property line. AB 98 requires Cities to update their circulation elements, including identifying and updating established truck routes. **It also prohibits local agencies from approving these type of warehouse projects under certain criteria,** such as commercial roadway logistics, sensitive receptors, zoning and size of the facility.

SAN DIEGO MULTIPLE HABITAT CONSERVATION PROGRAM

No impartial biologist would ever recommend a project of this scale next to a critical nesting habitat for endangered species. The biology 'experts' hired to produce reports for the EIR have a conflict of interest.

The section of the San Luis Rey River along this discretionary project site provides critical breeding habitat for the endangered bird species *Least Bell's Vireo*. Figure 3 of the Biological Technical Report (BTR) within the EIR shows a number of documented occurrences of this species near or within the project boundary. The City should require **protocol surveys for this endangered species, as they were surprisingly not conducted.**

This was noted by the California Department of Fish and Wildlife (CDFW) in their public response to the Draft EIR.

Also, this area lies within Oceanside's Wildlife Corridor Planning Zone as designated in Oceanside's Subarea Plan, which is meant to protect another federally threatened bird species, the *Coastal California Gnatcatcher*. **United States Fish and Wildlife Service (USFWS) mentions this important detail in it's public comment letter and further explains that the federally-protected species could use this project site for dispersal.** The USFWS recommends that at least 50 percent of this property should be conserved as open space to adhere to the Subarea Plan.

Across the board, ALL wildlife in the San Luis Rey River Habitat will be severely affected by the traffic, noise, and exterior lighting of this discretionary four (4) building complex. Headlights from trucks will inevitably extend beyond the limits of the project and will disrupt nocturnal animals.

↘ **See EXHIBIT G – Impact on Sensitive San Luis Rey River Habitat*

KUMEYAAY NATION CULTURAL HABITAT

The City and developer are certainly aware, and it is worth highlighting the historical and cultural significance of this land. The Luiseno (also known as Payomkawichum) are a band of the Kumeyaay Nation; they are specifically associated with the San Luis Rey River Valley and *Mission San Luis Rey*. Past Archaeological assessments of the river basin and flood plain state that the area contains at least sixty-six (66) recorded archaeological sites. The location of the Eddie Jones Discretionary Project is well within the Luiseno cultural territory. Any new development will most likely encounter Luiseno sensitive cultural material. (*Note: In honor of the Kumeyaay Nation street name in the adjacent Wanis View residential community are Luiseno words. For example: "Wala" = Red-tailed Hawk and "Tishmal" = Hummingbird.)

UC DAVIS STUDY ON THE IMPACT OF WAREHOUSES:

According to the EPA, California has the highest levels of ozone in the country; much of which is due to the recent number of distribution warehouses built to support the surge in e-commerce. Diesel heavy trucks associated with distribution warehouses are large contributors to hazardous compounds that react in the presence of sunlight to create ozone. There is new evidence that air pollution from **warehouse distribution complexes results in significant health risks for people living in the area; in particular health risks for children, such as increased rates of asthma and cognitive disorders.** This concern is magnified in the San Luis Rey River Valley with its high cliffs and dish bowl topography.

↘ **See EXHIBIT H – Health Effects of California's Warehouse Boom*

CUMULATIVE IMPACT OF POLLUTANTS WITH NEARBY OCEANSIDE AIRPORT:

Numerous comments to the Draft EIR expressed concerns with the cumulative effects of various emissions/pollutants in the nearby area with emissions from diesel engine trucks (diesel particulate material). This was not adequately addressed in the EIR. For example, the proposed location for the Discretionary Project is adjacent to the largest source of airborne lead in Oceanside (the Airport) in the form of nanoparticles of lead dibromide. **The presence of diesel particulate material can increase the local exposure to lead by increasing the settling velocity of the combined, agglomerated particles.** Since there is no level of lead that is considered safe (US EPA, US CDC), this increased level of exposure should not be permitted. The question was put to the Planning Commission and Developer at the February 10th meeting: Has a Truck Terminal ever been located so close to an active runway of an airport using leaded fuel? – No one responded to this question.

↪ **See EXHIBIT I - Cumulative Lead Dibromide Analysis*

UNRESOLVED AIR QUALITY ISSUES:

In response to the Draft EIR in December, 2023 air quality professionals explicitly expressed concerns that the Eddie Jones Warehouse Project will create a *concentration* of greenhouse gases / pollutants, thus affecting air quality in the San Luis Rey River Valley.

Given the unique terrain and topography of a narrow and deep River Basin, this concentrating effect will have significant impact on thousands of Oceanside residents and businesses downwind of the Discretionary Project.

Use of only the daily thresholds of significance to determine significance does not fully characterize the air quality impacts to the nearby homes and businesses. This is because **impacts to nearby homes and businesses are related to the concentration of various air pollutants not their daily mass emission rates.** Concentrations are related not just to the daily emissions rates but also on numerous other factors such as proximity to homes and businesses and weather conditions.

The EIR inaccurately concludes that since the project level impacts are less than significant, then cumulative impacts would also be less than significant. **THIS LOGIC IS FLAWED.** Cumulative impact analysis also needs to address cumulative health risks. This was also missing in the EIR.

Cumulative impact analysis requires emissions from all current or future projects be evaluated to determine impacts. THIS WAS NOT DONE.

To measure air quality, the Final EIR mistakenly uses an old version of CalEEMod (2020.4.0). This misses critical details of this Discretionary Project's impact on climate. CalEEMod (2022.1.29) covers the important topics of climate risks, **environmental**

burdens, health, and equity impacts. These are salient concerns with the planned operations. In addition, CalEEMod, even up-to-date versions do not explicitly model the topography of a site. Instead it relies on generalized inputs and assumptions about the site. **This is extremely important for this Discretionary Project given the complex, “bowl” shape of the topography of the nearby area and its impact on pollutant retention.**

Another mistake is the Final EIR relies on CalEEMod despite comments on the importance of using the American Meteorological Society/EPA Regulatory Model (AERMOD) for this area. CalEEMod focuses on emissions generation. AERMOD focuses on pollutant dispersion– key to the complex topography of the area and provides important information on where pollutants go after they are released.

The Final EIR fails to adequately respond to the AERMOD results showing levels of cumulative NO_x and cumulative PM 10 exceeding Air Quality Standards.

The ‘experts’ hired to produce reports for the EIR clearly have a conflict of interest. No impartial air quality expert or environmental permitting specialist would create the misleading evidence that appears in the EIR. Here are a couple of examples:

Both the Draft and Final EIR go to great lengths to provide background information, with discussions about ambient air quality standards, prevailing local air quality, and project emissions (construction and operational) in Tables 4.2-1 through 4.2-3. However, at no point do they connect the project emissions with the ambient air quality standards, or impact of the project emissions on local air quality that was just discussed in their documents.

Instead, the Final EIR wanders off to compare project emissions with thresholds of significance. They never bother to translate project emissions into AQ impacts, how those project impacts compare against the AQ Standards or prevailing air quality that was presented just a few pages earlier in both the Draft EIR and Final EIR. So, why bother presenting eleven (11) pages of information about AQ Standards and prevailing air quality if you are not going to connect that information with project emissions?

↘ **See EXHIBIT J – Attorney Letter: Advocates for the Environment*

↘ **See EXHIBIT K – Attorney Letter (Mooney) & Environmental Permitting Specialist Technical Memorandum*

ENVIRONMENTAL INJUSTICE:

The proposed location of this Discretionary Project as well as areas to the east and south are among the highest levels of Poverty in all of San Diego County. From CalEnviroScreen 4.0, this area, census block 6073018603, has a **Poverty percentile of 92** (0- low 100-high) relative to all of California. This means that only 8 percent have a higher poverty rate. The City of **Oceanside average for comparison is 45 percentile.** This area is also the

highest level of Pollution burden (72 percentile) in Oceanside. Historically, the area has suffered a disproportionate burden of industry and associated pollution. Adding a facility like the Discretionary Project and the associated PC Resolution No. 2025-P04 + PC Resolution No. 2025-P05 allowing for numerous heavy duty trucks and an expanding building footprint will have the following long-term impacts on this already burdened area:

Perpetuation of Inequality: Environmental injustice exacerbates existing social and economic inequalities. It creates a cycle where marginalized communities are further disadvantaged.

Unjust Distribution of Harms: It's fundamentally unfair to concentrate environmental risks in areas where residents have the least resources and political power to fight back.

Cumulative Impacts: Poor areas often face multiple sources of pollution, leading to a higher overall burden and greater health risks.

Health disparities: These communities often experience higher rates of chronic diseases such as asthma, cancer, heart disease, and developmental problems.

This is due to their increased exposure to pollutants in the air, water, and soil.

Missed Economic Opportunities: Businesses may be hesitant to invest in areas with high pollution levels, hindering economic development.

LACK OF DUE PROCESS:

The original 566,905 square foot single building "Proposed Project" was described and analyzed in the Draft EIR, along with three project alternatives: No Development Alternative, Multi-Building Alternative, and Reduced Building Footprint Alternative. In the Fall of 2024 (shortly after California Assembly Bill (AB) 98 was signed into law), the Multi-Building Alternative was significantly modified by the developer with new building sizes, a new North-South orientation, and reduced number of truck terminals.

The release of the Final EIR on January 10, 2025 (CEQA's website) was the public's first published confirmation of this new Multi-Building Truck Bay Reduction Alternative (MBTRA). Yet, there was NO INDICATION that it was now the favored project design.

As the timeline rapidly narrowed, there was no formal opportunity for public response to the developer regarding the new North-South orientation. (*Note: the new North-South orientation severely impacts sound waves as they escalate up to hillside residences.) According to the City's Planning Department's email on January 13, 2025 public comment would be at the actual Planning Commission Meeting on February 10, 2025. Given there was no opportunity to comment until the date of the actual Planning Commission Meeting, there was not true due process.

Additionally, the City Planning Department Staff Report's *recommendation* of the MBTRA design (fourth iteration), was only made available to the public when the Agenda was posted on February 6, 2025; a mere 4 days before the decisive February 10, 2025 Planning Commission Meeting. This also did not provide the public adequate time to analyze or challenge the Staff's recommendation of this fourth iteration.

SPECIFIC ENTITLEMENTS REGARDING THE LEGAL RIGHTS OF THE PROPERTY (e.g. Permits, Licenses, Zoning):

Given the historical zoning designation of this land, the previous 60+ year tenant, and the proximity to the Oceanside Municipal Airport runway, Article 13 of the City's Zoning Ordinance should stand as is. Additionally, the General Plan land use designation of Light Industrial (LI) and a Zoning Designation of Limited Industrial (IL), is appropriate.

These facts are not contested.

**HOWEVER, THE EDDIE JONES DISCRETIONARY PROJECT'S
PC RESOLUTION NO. 2025-P04 + PC RESOLUTION NO. 2025-P05**

SHOULD HAVE BEEN DENIED

Per Article 13 of the Zoning Ordinance an industrial facility for distribution and storage shall *NOT exceed* 50,000 square feet and allows *no more than* six (6) heavy trucks on the premises at one time.

- a. **The Planning Commission should have followed the City's established Zoning Regulation.** The City has an obligation to balance resources for all stakeholders in the community.
- b. Limiting the number to six (6) heavy trucks at this facility at one time, has the following advantages:
 - i. ***Aligns with the historical use of the property*** which was a 172,300 square foot industrial manufacturing facility in operation at this location for 60+ years. (*Note: The previous facility maintained extensive grass open space, with a grove of mature Eucalyptus trees.)
 - ii. ***Aligns with ALL other Light Industrial buildings south of the Airport runway, and south of Highway 76.*** Constructing more of the same kind of buildings would be most welcomed. That is, a variety of industrial buildings with production areas, grade level doors to accommodate light trucks and delivery vehicles (NOT heavy trucks.)

** Article 13 Zoning Ordinance applies to all industrial south of the Airport and south of Highway 76, why should Eddie Jones Discretionary Project be the exception?
 - iii. Aligns with the fact that California State CALTRANS has confirmed, **there are no future CALTRANS funds earmarked for any road / traffic improvements** at the intersection of Highway 76 and Benet Rd.
 - iv. Aligns ***with the City of Oceanside's resources and budget to maintain and service roads*** due to repetitive heavy truck leading to / from this Project site. *Note: Since the

Discretionary Project would contribute traffic to a failing intersection (moves the LOS of the intersection at Benet Rd & Highway 76 from Grade D → F), the Project would be subject to a fair share monetary contribution in the amount of only \$50,000 – **which is a paltry sum compared to the cost the City will incur due to excess wear and tear of roads IN PERPETUITY...**

- v. Aligns with the *tourist-centric recreational activities downwind, and ecologically sensitive wildlife areas immediately adjacent* to this Discretionary Project site.

CONCLUSION:

The Planning Commission’s approval of the Eddie Jones Discretionary Project’s PC Resolution No. 2025-P04 + PC Resolution No. 2025-P05 should be reversed.

The City’s existing Article 13 of the Zoning Ordinance should stand, as is.

In reviewing the City Council’s Mission Statement, the Council represents the City’s citizens, ensuring “...that Oceanside is a desirable place to live, work, do business and to visit.” Given all the pros & cons of the Eddie Jones Discretionary Project, it is hard to understand how this project “... serves the best interests of all citizens.” Instead, it benefits external private equity investors (The Carlyle Group of Washington DC) and harms Oceanside residents and tourists.

Lastly, the Council has an established precedent, having rejecting the warehouse distribution center proposed near Oceanside’s Saint Cloud community; a proposal only a fraction of the size of this Eddie Jones Discretionary Project. The City Council unanimously rejected the project out of concern that it would disrupt the quality of life of residents in the area. That project was essentially just ONE of the four (4) buildings being proposed as part of this Eddie Jones Discretionary Project. The Airport Community deserves the same consideration as the Saint Cloud Community.

SIGNATURES IN SUPPORT OF THIS APPEAL:

With this Appeal Letter we submit 373 signatures obtained DURING the short eight (8) day period: Feb 11, 2025 to Feb 18, 2025

24 volunteers asked residents of the 236 homes within the 1500 feet notification area

236 homes approached

-34 not home

202 available responses



200 residents signed “NO” (99%)

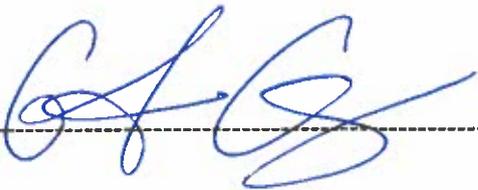
2 said they liked the project (<1%)

Signatures were also obtained from businesses in the Airport Community and residents of the *Prince of Peace Abbey*

Our Appeal is based on this Appeal Letter + ALL content in accompanying Exhibits (whether cited specifically or not), all written communications to the Planning Department, all audio/video recordings of proceedings before the City Council & Planning Commission, and all other records regarding this Discretionary Project maintained by the City of Oceanside and the City of Oceanside Planning Department, 300 North Coast Highway, Oceanside, California 92054.

Any portion of this Appeal Letter found to be unintentionally inaccurate does not invalidate any other section of this document or its Exhibits.

Thank you for your consideration,



2/19/2025

Gretchen Gary, 471 Tishmal Court, Oceanside, CA
Resident, representing the 236 residents within 1500 feet of the Project site's notification area.

*(*On a personal note: I am the Appellant of this letter and my master bedroom has unobstructed panoramic views of the property from all windows and balcony. Allowing a project like this to be built, invites full scale disruption to everyone's quality of life - both my family, as well as that of all my neighbors, **included those beyond 1500 feet of the project site.**)*

EXHIBITS

TABLE OF CONTENTS:

EXHIBIT A:

DEVELOPER'S VISION

Comparison of Multi-Building Complex relative to Other Buildings

EXHIBIT B:

UNFOUNDED ASSUMPTIONS REGARDING JOB LEVEL IMPACT

Comment Letter from Teamsters Local Union No. 542

EXHIBIT C:

BUILDING DESIGN OPTIONS TO MATCH UNIQUE TOPOGRAPHY

Relatable Example: Oceanside Gateway Business Park

EXHIBIT D:

NEW MULTI-BUILDING CONFIGURATION WITHOUT NOISE ANALYSIS

Acoustic Calculations New Multi-Building Configuration

EXHIBIT E:

WILDFIRE SAFETY FOR THE COMMUNITY

Inaccurate Evacuation Assumptions

EXHIBIT F:

TRANSPORTATION DEMAND MANAGEMENT (TDM) PLAN, NOT PROVIDED

Failure to Provide TDM Plan

EXHIBIT G:

SAN DIEGO MULTIPLE HABITAT CONSERVATION PROGRAM

Impact on Sensitive San Luis Rey River Habitat

EXHIBIT H:

UC DAVIS STUDY ON THE IMPACT OF WAREHOUSES

Health Effects of California's Warehouse Boom

EXHIBIT I:
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Cumulative Lead Dibromide Analysis

EXHIBIT J:
UNRESOLVED AIR QUALITY ISSUES
Attorney Letter: Advocates for the Environment

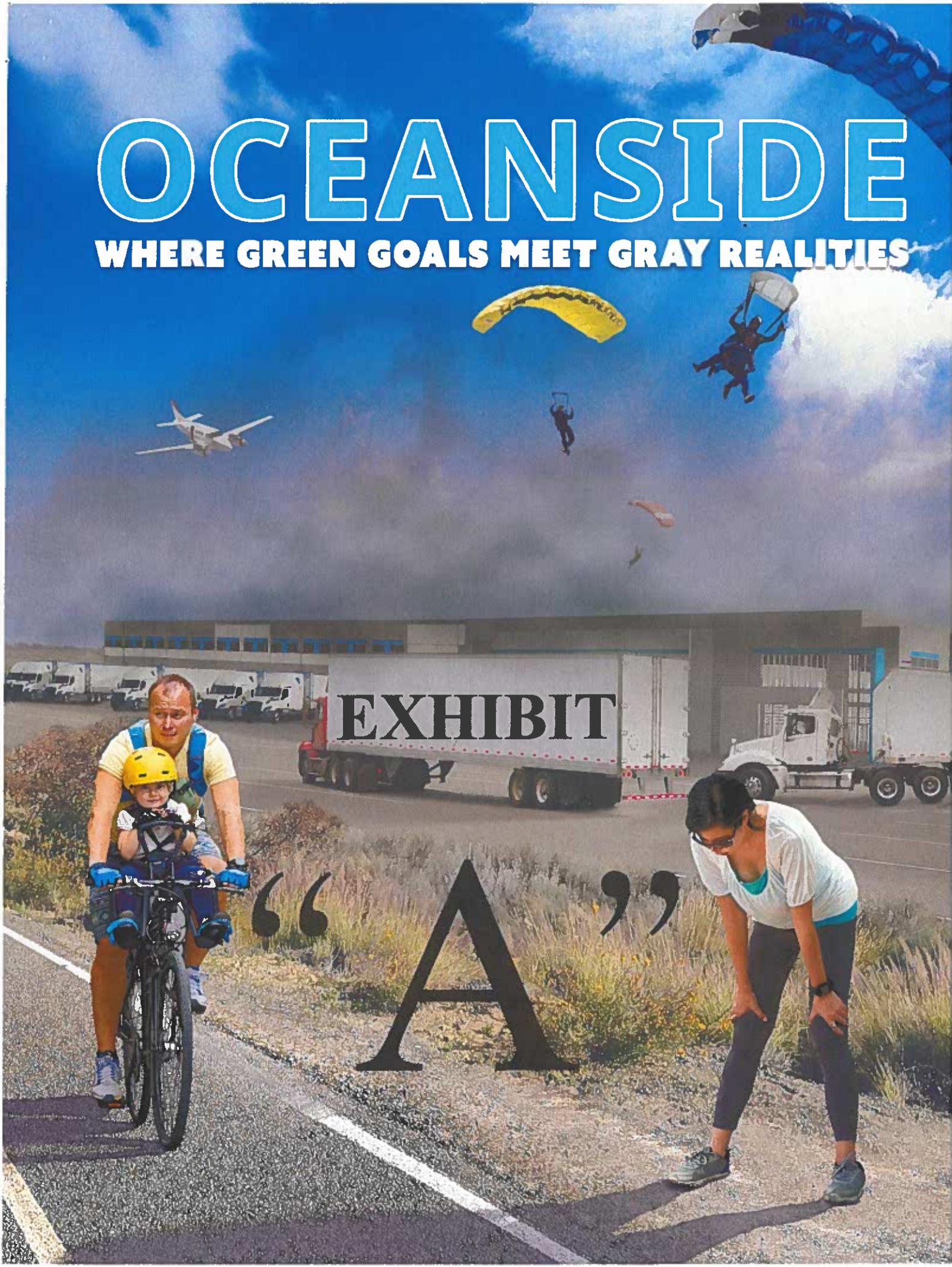
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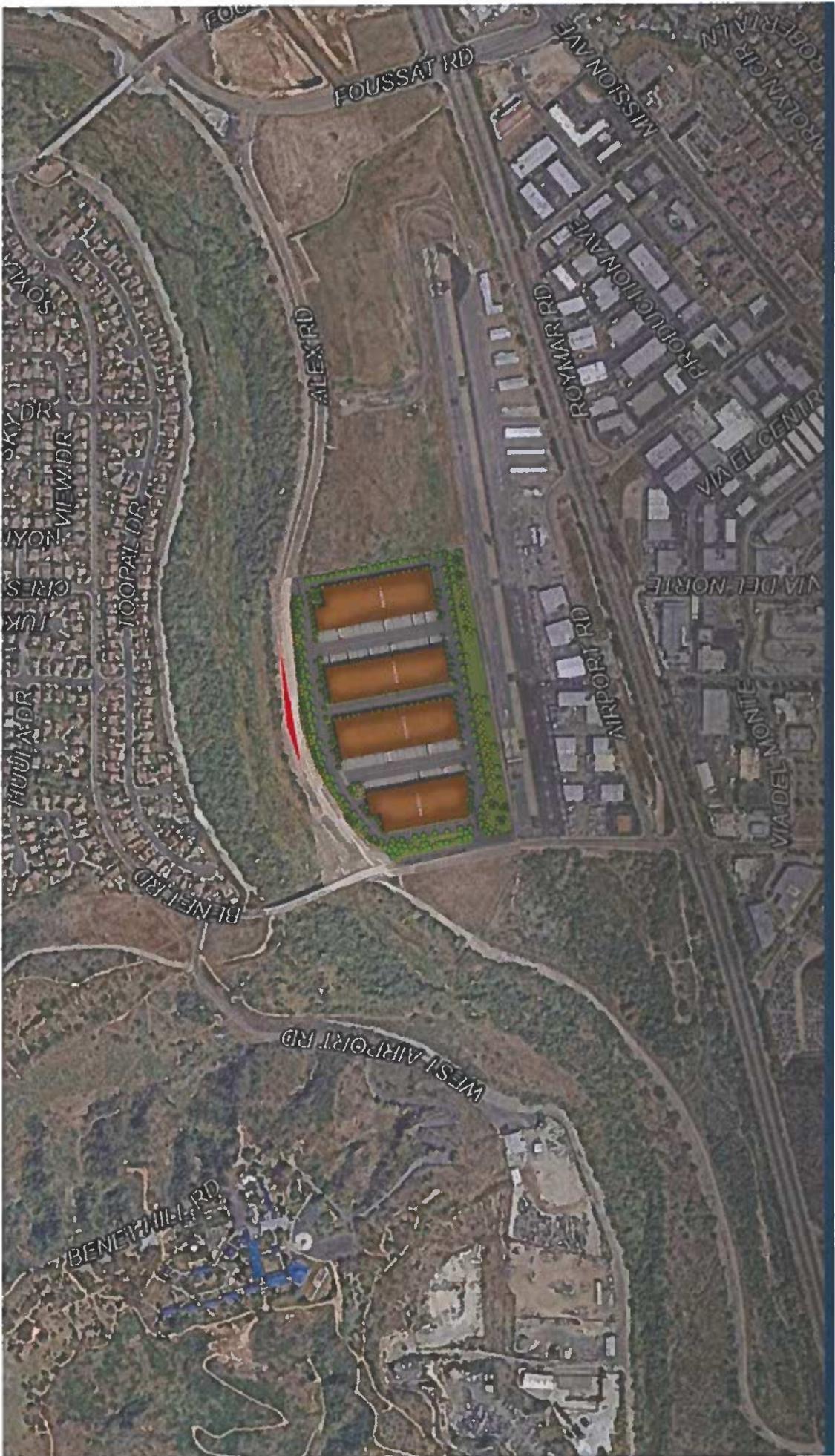
OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“A”



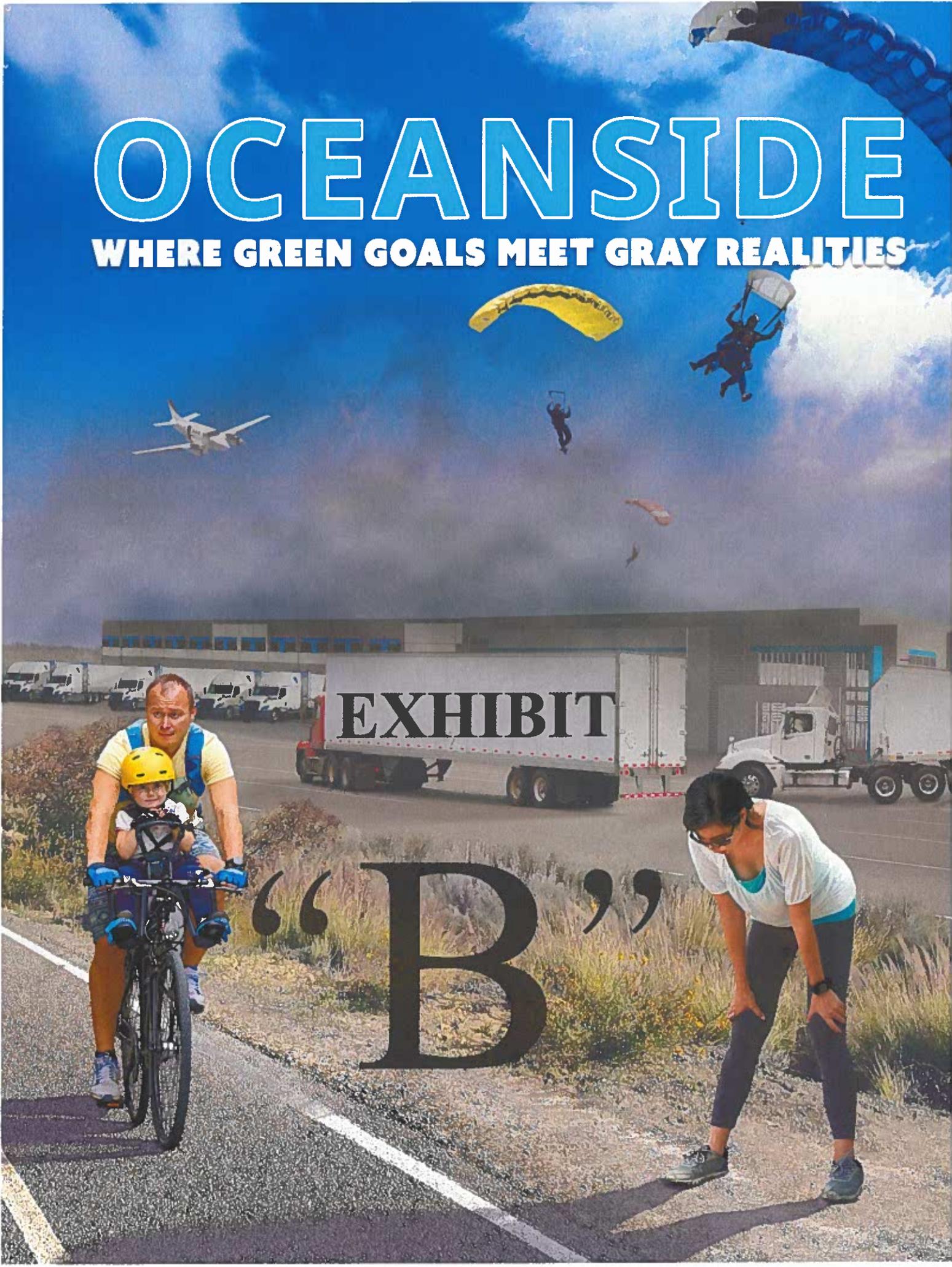


OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“B”





**TEAMSTERS, CHAUFFEURS,
WAREHOUSEMEN AND HELPERS
LOCAL UNION No. 542**

San Diego and Imperial Counties, California
and the City of Yuma, Arizona

Affiliated with
INTERNATIONAL BROTHERHOOD OF TEAMSTERS



Jaime Vasquez
Secretary-Treasurer
Dwayne Garrett
President
Mike West
Vice-President
Lynda Linville
Recording Secretary
Robert Moreno II
Trustee
Ed Swank
Trustee
Jennie Vasquez
Trustee

Rob Dmohowski
Principal Planner
City of Oceanside's Planning Division
300 North Coast Highway
Oceanside, California 92054

RE: Eddie Jones Warehouse Project, SCH# 2022070365

I. Introduction

This comment letter is submitted to the City of Oceanside in reference to the Draft Environmental Impact Report (DEIR) prepared for the "Eddie Jones Warehouse Project," and the associated entitlements. We reserve the right to clarify and supplement these comments as permitted by law and do not waive any issue or matter omitted herein as a result of error or omission by the City of Oceanside or the Applicant.

We are submitting this letter on behalf of Teamsters Local 542. We represent over 6,000 warehouse, logistics, delivery and related workers in San Diego County, including in Oceanside. As both residents of the community and workers in the industry, Teamsters have consistently organized to make sure that their industry is both a good employer and a good neighbor. We are committed to making this industry safe for its workers and its neighbors. We urge the City to refrain from adopting the DEIR and approving any related entitlements until further study is conducted and adequate mitigation measures are proposed and adopted. Our reasons are set forth below. Thank you for your consideration.

II. Project Setting and Background

A. *The Project*

The proposed project would be located on an approximately 31.79-acre site at 250 Eddy Jones Way in the City of Oceanside. The project site is located within the “Airport Neighborhood Planning Area” and is bound by the Oceanside Municipal Airport to the south, Benet Road to the west, the San Luis Rey River and recreational trail to the north, and vacant light industrial land to the east. The terminus of Alex Road also connects to the site at its northeast corner. The project site is approximately 900 feet north of the State Route 76 corridor. A vacant 172,300-square-foot industrial manufacturing facility was previously located on site prior to demolition in 2022; this would be replaced by over half a million square feet of industrial usage. The General Plan designation for the property is Light Industrial (LI), with the associated zoning category of Limited Industrial (IL).

Based on public reporting and statements by the developer, the proposed project consists of redevelopment of the project site with a new 566,905-square-foot “warehouse and distribution facility.” The proposed warehouse and distribution facility would consist of 369,415 square feet of warehouse area; 158,320 square feet of manufacturing space; and 39,170 square feet of office area, designed as a single building that could support multitenant occupancies.

Development of the proposed project would include 590 parking spaces for employee/visitor parking, 60 truck trailer parking stalls, and a vehicle circulation area. Loading bays are proposed on the north and south sides of the building, with a total of 114 truck terminals. The north side truck terminals would be facing the abutting San Luis Rey River. Access to the project site would be maintained and improved as necessary, with existing access points from Alex Road at the northeast corner and Benet Road at the southwest corner.

B. The Surrounding Area

The DEIR describes the area surrounding the site as follows:

The proposed project site is bound by the Oceanside Municipal Airport to the south, Benet Road to the west, the San Luis Rey River and recreational trail to the north and vacant light industrial land to the east. The terminus of Alex Road also connects to the site at its northeast corner. The project site is approximately 900 feet north of the Highway 76 corridor. The property was previously occupied by an approximate 172,300 square foot industrial manufacturing facility which was vacated in the summer of 2021 and demolished in 2022.

There is no mention, throughout the DEIR, of the Wanis View nature preserve area, north of the site, which has an ecological connection to the Mauro Preserve area. These two preserve areas, which also connect to the San Luis Rey River which directly abuts the site, are part of years-long efforts to reclaim land and return them to their natural, pre-settlement condition. Both the Wanis View and Andy Mauro preserves have received hundreds of thousands, up to millions, of dollars of investment in returning them to their pre-settlement condition and, importantly, have served as bird-sanctuaries, helping to return threatened species to the area. *See e.g.*, Nelson, Samantha, "Returning nature to its native roots," *The Coast News*, September 13, 2019 (retrieved November 20, 2023).

In fact, with minor exceptions, the DEIR throughout its length focuses entirely too narrowly on the Site itself, despite the fact that it is situated so closely to at least one existing residential area, a developing residential area (i.e., the Ocean Kamp development), a river, and several wildlife and nature preserves, specifically, the Wanis View and Andy Mauro preserves and bird sanctuaries. This failure to adequately describe with sufficient specificity the surrounding area's sensitivities, amounts to a legal inadequacy.

III. The Draft EIR is Inadequate as a Matter of Law and Policy

A. *Standard of Review*

The EIR is the “heart” of CEQA. *Cleveland National Forest Foundation v. San Diego Assn. of Governments* (2017) 3 Cal.5th 497, 511. This concept is repeated often because the purpose of CEQA is not to act as a comprehensive regulatory scheme for every possible area of environmental impact, but instead to provide the public and decision makers with adequate information to make reasoned and informed decisions on projects and their potential impacts on the environment. *Id.* This statutory purpose means that where there is conflicting evidence between which a local agency (such as the City of Oceanside) must choose, there is significant deference afforded to the local agency. However, when it comes to the *adequacy* of an EIR, less deference is required. *Sierra Watch v. Cty. of Placer*, 69 Cal. App. 5th 86, 95 (2021).

Therefore, a local agency should carefully consider whether an EIR has been adequately prepared; while deference by local decision-makers to the technical expertise of the preparers of an EIR is natural, the regulatory and legal adequacy in terms of what has been studied and the range of potential impacts should be carefully considered.

B. *Project Setting - Inadequacy*

An EIR’s nature as an information document means that properly situating a project in its environmental context is critical. *Sierra Watch v. Cty. of Placer*, 69 Cal. App. 5th 86, 95-96 (2021). An agency must, in its EIR, “include a description of the physical environmental conditions in the vicinity of the project,” which is referred to as the project’s “environmental setting.” (CEQA Guidelines, § 15125, subd. (a).) This description of the environmental setting often focuses on the

existing environmental conditions in the immediate vicinity of the project. But because “[k]nowledge of the regional setting is critical to the assessment of environmental impacts,” this description should also place “[s]pecial emphasis . . . on environmental resources that are rare or unique to that *region* and would be affected by the project.” (CEQA Guidelines, § 15125, subd. (c) (emphasis added). The agency must normally then use this description of the existing environmental setting as the “baseline against which predicted effects [of the project] can be described and quantified.” *Sierra Watch, supra*, citing *Neighbors for Smart Rail v. Exposition Metro Line Construction Authority* (2013) 57 Cal.4th 439, 447 (plur. opn.); see CEQA Guidelines, § 15125, subd. (a) (“This environmental setting will normally constitute the baseline physical conditions by which a lead agency determines whether an impact is significant.”).

Courts have repeatedly held that where an EIR contains an “inadequate description of the environmental setting for the project, a proper analysis of project impacts [i]s impossible.” *Galante Vineyards v. Monterey Peninsula Water Management Dist.* (1997) 60 Cal.App.4th 1109, 1122 (invalidating EIR with only passing references to surrounding viticulture); *Friends of the Eel River v. Sonoma County Water Agency* (2003) 108 Cal.App.4th 859, 873-75 (EIR’s description of environmental setting failed to describe regional setting for water diversions). Thus, if an EIR fails to include adequate information on the environmental setting that allows readers to understand the sensitivity of resources at stake, “prejudice is presumed.” *Sierra Club v. State Bd. of Forestry* (1994) 7 Cal.4th 1215, 1236-37 (“*Board of Forestry*”).

For the present case, the court’s decision in *San Joaquin Raptor/Wildlife Rescue Center v. County of Stanislaus* (1994) 27 Cal.App.4th 713, 729 (“*Raptor*”) is particularly apt. The project at issue there included 633 homes, a commercial area, and a park. *Id.* at 718. The project site lay near a wetland wildlife preserve, and a project park was situated adjacent to the San Joaquin River. *Id.*

at 724. The EIR purported to evaluate the development's impacts on waterfowl and other resources in the project area. *Id.* at 729. However, the court found that the EIR's information on the environmental setting was “incomplete and misleading” because it included little reference to the sensitive riparian resources in the *region*. *Id.* at 723-29. This omission violated CEQA as it “precludes this court from concluding that all the environmental impacts of the development project were identified and analyzed in the FEIR.” *Id.* at 729; *see also Cadiz Land Co., Inc. v. Rail Cycle* (2000) 83 Cal.App.4th 74, 92-95 (invalidating EIR for failing to provide contextual information regarding underlying aquifer and thereby precluding evaluation of “how soon depletion will occur” of this “valuable and relatively scarce resource in the region”).

The project setting description, or descriptions, throughout the DEIR are inadequate because they do not sufficiently incorporate any description of sensitive and protected nature reserves near the Site—and, importantly, along the identified trucking routes for the Project.

The DEIR's failure to adequately describe—or, indeed, even allude to—these nearby nature preserves raises serious questions about the adequacy of the entire document. The Wanis View and Mauro preserve have, as just one example, seemingly been effective in beginning to bring back populations of the coastal gnatcatcher, a federally threatened species that is also listed as a California Department of Wildlife species of special concern (SSC), thanks to the reintroduction of plant species on which they rely. Details of this restoration plan are public, and importantly, not speculative.¹ The Buena Vista Audubon Society (BVAS) has received cooperation and resources from the federal government, through the United States Marine Corps, to preserve and restore this land.

¹ See e.g., https://bvaudubon.org/wp-content/uploads/2021/01/Julies-restoration-details-for-website_FINAL.pdf

These nature reserves sit within two miles just north of the site, and share two roads that will service the Project, Benet Road and Alex Road. Importantly, these protected areas have been in development over the last handful of years, and therefore were not necessarily studied when the previous tenant of the project site was built.

As one example of the deficiency of the environmental setting description, the DEIR alludes to the gnatcatcher, acknowledging it is federally threatened and a CDW SSC. The gnatcatcher is a regionally native species. However, because no gnatcatchers, or the brush on which they rely, were observed “on the site,” no further analysis was conducted as to the possible impact of the Project. (DEIR at 4.3-5) (the gnatcatcher “was not observed *on site* during the reconnaissance survey, and there is no suitable habitat for this species *on the project site.*”) (emphasis added).

Another example of this failure to adequately set the baseline environmental setting is absence of adequate discussion of the San Luis Rey River, and in particular its fluctuating water levels. There does not seem to be any differential analysis that acknowledges the significant change in water levels of the San Luis Rey River at different times of the year.

The below images show the differing potential water levels at dry versus rainy times of year.



Runoff from urban impervious surfaces into the river and emptying into the Pacific Ocean have recently been blamed for dangerously high bacterial counts at Oceanside beaches.² As identified in the DEIR, the site directly abuts the San Luis Rey River, however analysis seems to have been limited to the dry season.

C. Traffic Analysis - Inadequacy - Classification

The assumptions incorporated into the DEIR's traffic analysis does not adequately capture the type of facilities actually permitted for the Project. (DEIR Appx. J at 30). The EIR's traffic study is based on the Institute of Transportation Engineers (ITE) Trip Generation Manual's classification of uses. Specifically, the "High Cube Warehouse" (HCW) use. The traffic defines a HCW as follows:

A high-cube warehouse (HCW) is a building that typically has at least 200,000 gross square feet of floor area, has a ceiling height of 24 feet or more, and is used primarily for the storage and or consolidation of manufactured goods (and to a lesser extent, raw materials) prior to their distribution to *retail locations or other warehouses*. (emphasis added)

This definition is taken directly from a 2016 ITE analysis.³ Given the potential uses for the site—and the fact that there is defined tenant for the site yet—this presumption underlying the traffic analysis is inadequate. This leaves the entire VMT/transportation inadequate.

The underlying inadequacy comes from the fact that "distribution facility" high cube warehouses are interstitial nodes in a logistical system—i.e., they sort and prepare packages for delivery to "retail locations" and "other warehouses," from where they will be purchased by

² See e.g., <https://www.sandiegouniontribune.com/communities/north-county/oceanside/story/2023-07-06/urban-runoff-likely-culprit-that-prompted-oceanside-beach-advisory>

³ See e.g., <https://www.ite.org/pub/?id=a3e6679a%2De3a8%2Dbf38%2D7f29%2D2961becdd498>

consumers or re-sorted for delivery to consumers. But the site will be outfitted for and could potentially be used as a distribution facility for direct-to-consumer deliveries, which entails a different VMT profile. There are no restrictions currently in the EIR preventing the use of the site for this type of use.

The difference between a warehouse-to-warehouse facility and a warehouse-to-consumer (or “last mile”) facility is that the type, frequency, and routes of vehicles miles generated by the latter are inherently different. Commercial vans and passenger vehicles are used for direct-to-consumer deliveries. Because the deliveries are going to residential areas, the vehicle miles traveled are not as predictable or fixed because they will fluctuate with the season, the total level of retail demand in the economy, and routes will be determined by residential purchasing patterns. New residential developments nearby a “last-mile” type facility will naturally increase demand and therefore VMT.

The use of this underlying assumption would not necessarily cause a problem for the CEQA analysis, except that there is nothing in the entitlements to be issued to the project, or in the EIR itself, that limits the use of the site to the land use studied in the traffic analysis. While an EIR does not need to study every possibility, reasonably foreseeable uses must be studied. This is a well-tested and long-established principle of CEQA: that development that can “foreseeably” result from the entitlements associated with an EIR must be considered and studied. *See Laurel Heights Improvement Ass'n v. Regents of Univ. of Cal.*, 47 Cal. 3d 376, 396 (1988). Typically an EIR preparer handles this requirement by considering the most-intense use or mix of uses being entitled. However in this instance, the preparer studied *an* intense use, but by failing to consider a different character of use, did not study foreseeable impacts.

“Last-mile” type facilities are qualitatively distinct because of the mix of vehicles used for deliveries, which include passenger vehicles—i.e., vehicles owned by employees (or “gig” workers who complete deliveries on a frictional or occasional basis). The existence of a “last-mile” type facility has been known to cause unique impacts. A recent study completed for the journal *Research in Transportation Economics*, demonstrated quantitatively and qualitatively that home delivery of packages has unique impacts on residential areas.⁴ A proposed use that will specifically send vehicles, including passenger vehicles and commercial vans, into residential areas where there are “sensitive receptors” such as homes for the elderly, schools, children, hospitals, etc., naturally will have a different impact particularly over the long term, from a facility that sends freight over essentially settled routes to fixed facilities in industrial/warehousing and densely commercial zones.

It may well be that such a use does not have a different aggregate impact on vehicle miles traveled, air quality, use of public resources (such as emergency response and public safety response), etc., but that is a distinct question. When considering the *adequacy* of an EIR, as opposed to the substantiveness of the evidence within it, the local agency should err on the side of requiring more completeness.

D. Traffic Analysis - Inadequacy - Mitigation Measure

The inadequacy of the proposed mitigation measure for the transportation impact is directly related to the foregoing issue of classification and study of the proposed use.

⁴ Travis Fried, Rishi Verma, Anne Goodchild, “Ecommerce and environmental justice in metro Seattle,” *Research in Transportation Economics*, Volume 103, 2024, 101382, ISSN 0739-8859, <https://doi.org/10.1016/j.retrec.2023.101382>

The traffic analysis found that the VMT that would result from the project would exceed thresholds of significance by just under 3%. (DEIR Appx. J at 5). In order to bring this impact under thresholds of significance, the DEIR proposed a mitigation measure based on the San Diego Area Governments (SANDAG) “Mobility Management VMT Reduction Calculator,” specifically a “Voluntary Employer Commute Program.” (DEIR Appx. J at 7).

This mitigation measure fails in two respects: first, because it does not consider the potential that the facility could be used for “last-mile” delivery, i.e., delivery to home consumers; and second, because its voluntary nature is overly speculative.

First, the proposed mitigation does not adequately address how “employee”-focused mitigations (e.g., carpools etc.) can operate if the employment model requires a significant amount—or indeed, any substantial amount—of passenger vehicles be used for the operation of the facility’s purpose (e.g., home delivery of packages). The use of passenger vehicles, or leased commercial vans, for home delivery of packages is standard industry practice. As of 2021, for example, Amazon used as many as 140,000 “flex” drivers (e.g., part-time drivers completing deliveries in their personal vehicles) in a two year period according to court filings.⁵ The use of commercial vans is plainly necessary since semi-trailer trucks cannot make deliveries on residential roads. Therefore, “carpooling” and shared rides *to and from* a facility are not adequate to reduce vehicle miles traveled; a certain subset of employees will arrive at a facility, then leave again to make deliveries, either in their car or a commercial van.

The proposed mitigation measure is based on reducing the number of vehicle miles traveled by employees of the project to and from the site, by encouraging use of carpools, bicycles, and transit. (DEIR Appx. J at 7):

⁵ <https://www.cnet.com/tech/services-and-software/amazon-flex-drivers-to-receive-payments-in-61-million-tip-settlement/>

Future employers will be given an outline of employee commute trip reductions to include disseminating information about SANDAG's iCommute program, carpools, vanpools, subsidized or discounted transit passes, bike amenities, trip reduction marketing, and preferential parking permit program.

This is plainly inadequate in a scenario where some significant portion of those employees arrive at the facility not to stay and work for a full shift, but to pick up packages and leave again (and, potentially, to do so more than once per shift). Again, while a "last-mile" facility has not been confirmed for the site, *no* tenant has been confirmed for the site; and a "last-mile" type facility would be legally entitled to operate without further environmental review. Therefore it must be considered a reasonably foreseeable use of the site, and one that this mitigation measure does not account for. As it is inadequate, it does not bring the VMT impact below the threshold of significance, and therefore is a significant impact that has not been mitigated and must be addressed with statement of overriding consideration or otherwise acknowledged as such for the public and decision-makers.

Secondly, this mitigation measure is overly speculative. Even if the measure was adequate despite the potential use of the site for home deliveries, this mitigation measure is completely voluntary; there are not even specific incentives spelled out in the EIR which would encourage the use of these VMT-mitigating practices, such as carpools, vanpools, etc. The SANDAG-provided mitigation tools does, presumably, include more specificity about available inducements, such as subsidies for transit, but these are not spelled out in the DEIR; and in any case, would remain voluntary on *two* levels; first, the employer would need to agree, and then the employees themselves would have to avail themselves of these alternatives. There is nothing in the DEIR which adequately addresses how or why such volunteering would occur.

This mitigation measure requires extensively more discussion to be adequate. While the SANDAG tool relies on census data, there is nothing in the DEIR which accounts for whether a

distribution facility of this type will be able to hire sufficiently from the surrounding such that, for example, any significant or substantial number of employees could commute to work via bicycle (especially given that access to the site is substantially via freeway). Warehouse logistics facilities are notorious for their high “churn” and turnover rate, which suggests that employees will, over the lifetime of the project, have to come from further and further away—or at least that the immediate vicinity of the site cannot be relied upon for the labor pool. Turnover rates in the industry are as high as 49%.⁶ The median home price in Oceanside as of November 2023 is \$899,000, up almost 13% year on year.⁷ The average hourly earnings of workers in this sector, according to the Bureau of Labor Statistics, is \$23.04 over 39 hours per week, or \$898 per week. That amounts to less than \$45,000 over a 50-week year, just over half of the median household income of Oceanside.⁸ These relatively low wages strongly suggest that workers for the facility are unlikely to come from Oceanside itself—or that even if they do in the first year or so, the high rates of turnover will require any employer to look further and further out for employees.

There is no discussion of these factors in the mitigation measure, which simply assumes a static workforce concentrated enough that bicycling and carpooling (voluntarily, to boot) will have a meaningful impact on VMT. The mitigation measure is therefore inadequate.

IV. Conclusion

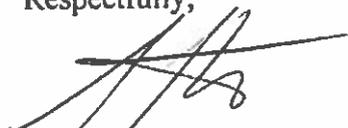
⁶ See e.g., Miller, Jen A., *Supply Chain Dive* “Pay is only one piece of the warehouse worker retention puzzle”<https://www.supplychaindive.com/news/pay-is-only-one-piece-of-the-warehouse-worker-retention-puzzle/625646/>

⁷ https://www.realtor.com/realestateandhomes-search/Oceanside_CA/overview

⁸ <https://www.census.gov/quickfacts/oceansidecitycalifornia>

For all the foregoing reasons, the proposed DEIR is inadequate and the project requires further study. We strongly urge the City not to adopt the DEIR in its current state, and instead to require further, more specific study and mitigation measures adequate to the potential impacts.

Respectfully,



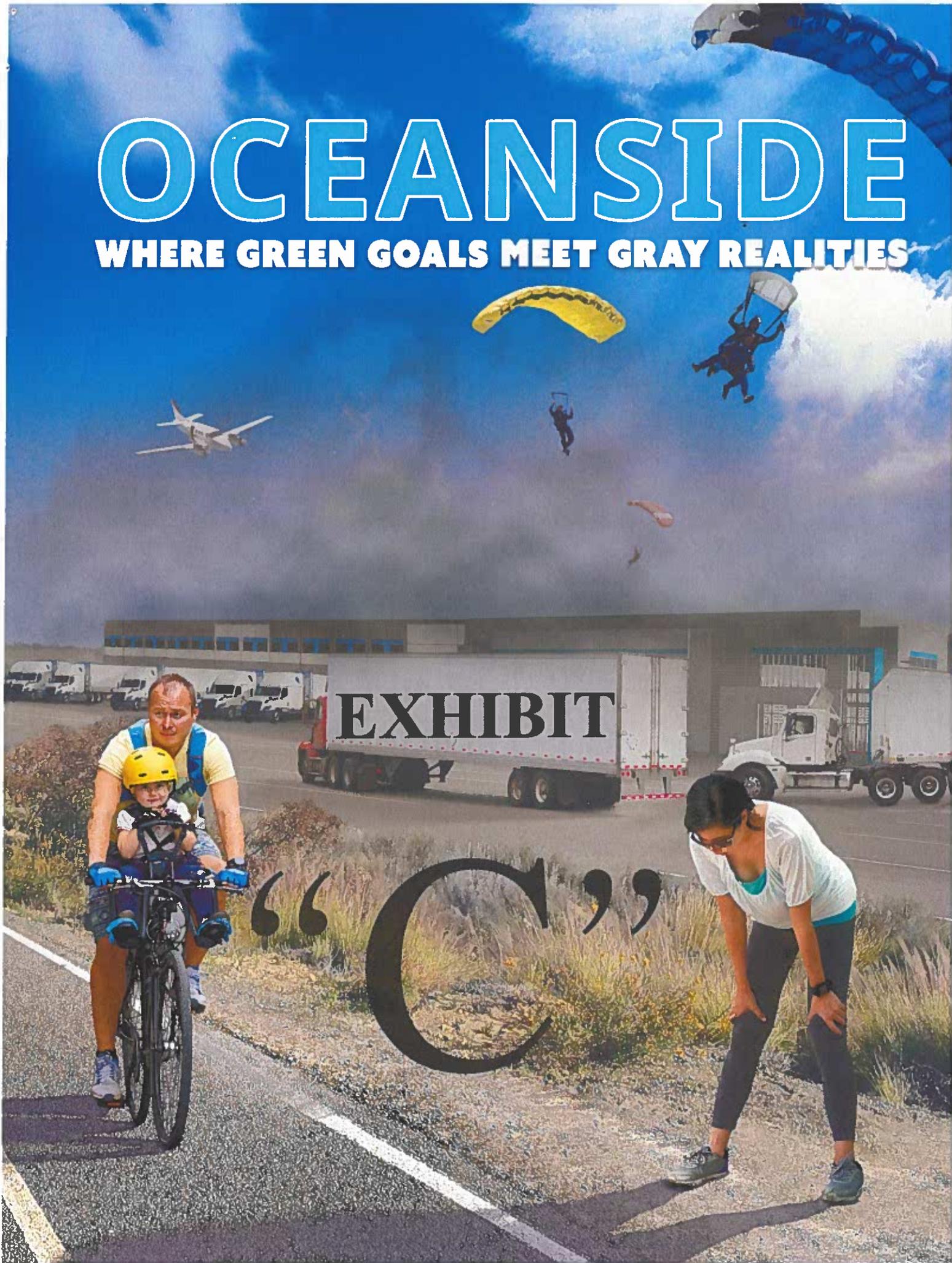
Salvador Abrica
Political Coordinator

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“C”





Relatable Example

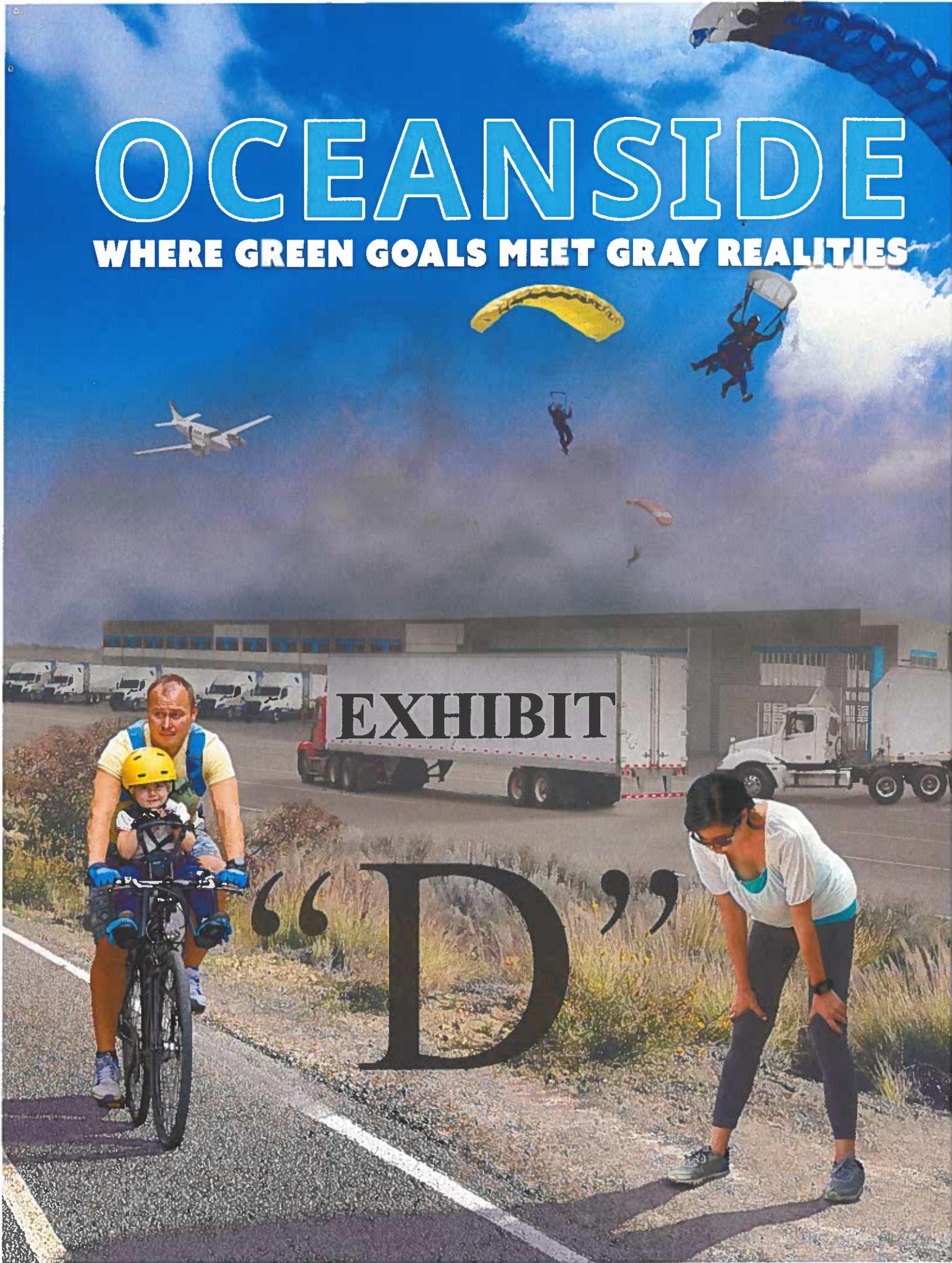
- **BUSINESS PARK “OCEANSIDE GATEWAY” - 37 acres**
 - **Oceanside Blvd & Ord Way**
 - **Multiple multi-use industrial buildings with warehouses & office flex spaces**
 - **Highly functional warehouse clearance heights**
 - **Large grade-level doors**
 - **Design promotes growth of additional small businesses**
-

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“D”



Response to the recently released Environmental Impact Report

(RAF PACIFICA GROUP)

Noise Issues with the Fourth Iteration of the Project

Prepared by Michael Tenhover (January 24, 2025)

*The following memo describes concerns with the Fourth Iteration of the “eddie jones warehouse project” (the “Project”). The new building configuration has improved the noise impact for some parts of the area. However, **the critical Wildlife region** in the adjacent San Luis Rey River and the **Wanis View homes** continue to have unacceptably high levels of noise from warehouse operations.*

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Calculation Methodology for Fourth Iteration of Project

VI. Appendix 2:

Calculation of Noise Levels for original Project design [1] (Dec 2024).

VII. Appendix 3:

References

I. Major Issue with the EIR- Fourth Iteration- related to noise

- a. Unacceptable levels of noise from warehouse operations are still present in the River and Wanis View Homes areas.

II. Noise levels for Fourth Iteration and other building layouts.

The Noise levels for the Fourth Iteration have been calculated (see Appendix 1). This includes the locations: River, Wanis View Homes, Wanis View Preserve, and Prince of Peace Abbey (see photo/diagram below)

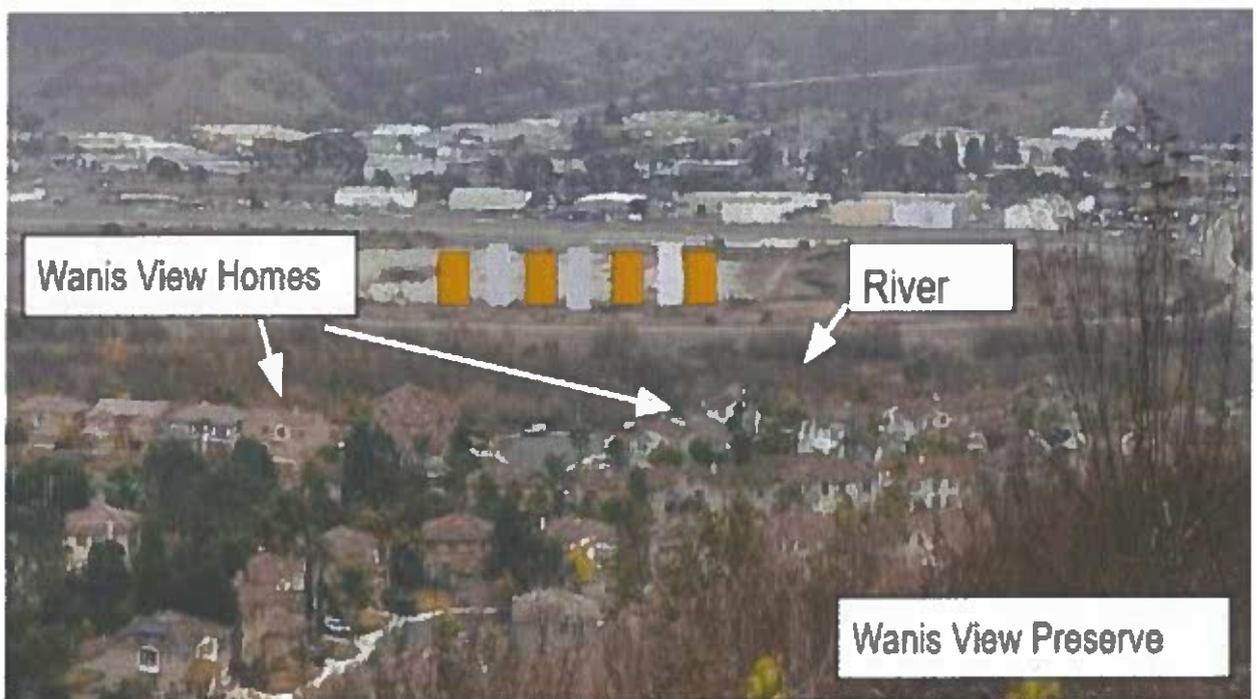


Figure 1. Photo of Project Site taken from the Wanis View Wildlife Preserve (.33.225231155953736, -117.3580229412305) The Fourth Iteration buildings (in brown) are shown with the proposed North-South Orientation.

Figure 2 shows the results of the calculations of noise levels for the original Project design [1], the Fourth Iteration (with 4 buildings going North to South) and a

modified Fourth Iteration (with the 4 buildings going from East to West). In general, the transition from the Original Design to the Fourth Iteration has improved the Noise situation.

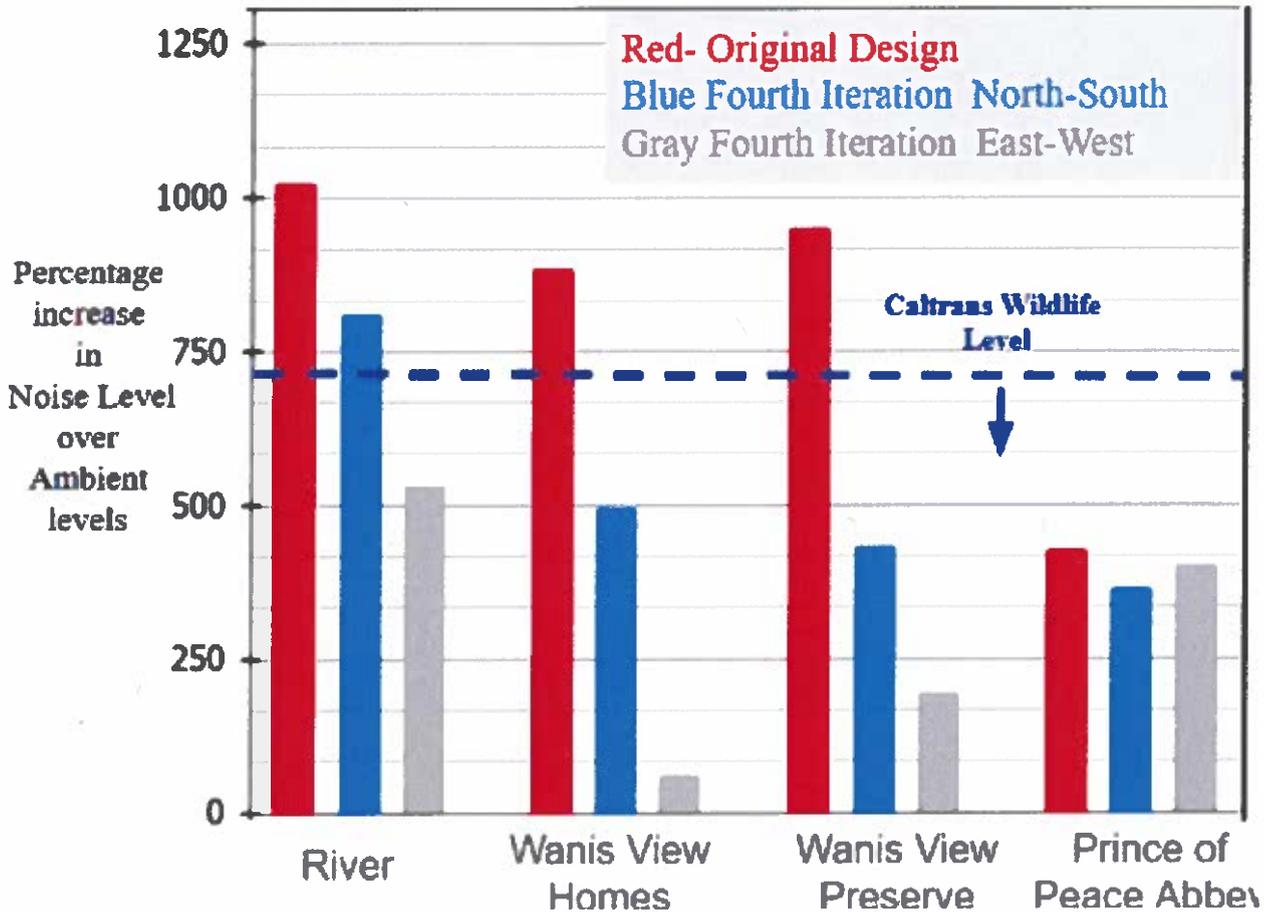


Figure 2. Noise Levels due to warehouse operations at the locations noted for various building layouts. Red is the original Project design. Blue is the Fourth Iteration design with the 4 buildings having a North-South orientation, and Gray is a modified Fourth Iteration with an East-West orientation for the 4 buildings.

In the case of River, the red and blue bars represent the area to the North of the Project, while the gray bar represents the area to the West of the Project.

Comments:

1. The Fourth Iteration continues to have unacceptable noise levels in the River area. (blue bar) This area is a critical component of the local wildlife biodiversity and wildlife corridor (see previous memo Dec 4, 2023 [1]). These noise levels exceed Caltrans guidelines for wildlife areas.

2. The noise levels for the River can be improved by adopting a modified Fourth Iteration with buildings going East to West. (gray bar) With this modification, the relevant section of the River is now West of the site. The distance is slightly larger in this case (from warehouse operations to River) and because of the road there, the ambient noise level is higher. Levels are still high but within Caltrans guidance.
3. The proposed Fourth Iteration has a high level of noise for the Wanis View Homes (blue bar).
4. The noise level for the Wanis View homes can be improved by changing the building layout to an East-West orientation (gray bar) when combined with other features (see section III. Recommendations, item 2 below)
5. Other layouts are possible for the Project that will have improved- lower noise levels than the North-South Fourth Iteration and the original design. (see section IV.)
6. The Fourth Iteration with either a North-South or East-West orientation has reduced the noise level for the Wanis View Preserve.
7. The noise levels for the Prince of Peace Abbey have not improved with the Fourth Iteration introduction.

III. Recommendations– Issues to be resolved.

1. **To resolve the Noise issue in the River area** a number of changes will be required. Changing the building layout to an East-West orientation improves the situation. In addition to this, other desirable measures to further lower the noise levels should be considered: These would include: 1) Strict restrictions on the number of trucks on-site at any specific time- a number

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determined by Noise level calculations. 2) No truck travel along the North section of the Project- i.e. parallel to the River. 3) Restrictions on truck idling and forklift operations near the West ends of the channels . 4) No truck bays on the West section.

2. **To resolve the noise issue for the Wanis View homes.** The predicted, “worse case” noise values for the Fourth Iteration are not high enough to constitute a human health hazard. However they are high enough to impact the quality of life for the residents there. The Project should consider a design in which the buildings have an East-West orientation as long as no trucks move parallel to the River area in the extreme North section of the property. Truck movement

should be restricted to the areas between the buildings and the areas adjacent to the airport.

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IV. Alternate Layouts for Fourth Iteration to address Noise Concerns: East West Layout

Good:



Better



Noise Concerns with Fourth Iteration– the Project Jan 2025

Appendix I. Modification of Noise Calculations for "Fourth Iteration" Parallel buildings with trucks, forklifts, etc. moving between them.

The Fourth Iteration introduces trucks, forklifts, etc. stationed and moving between four buildings (channels), three channels in total. This is substantially different from the original Project design and thus warrants a new look at expected noise levels. In this exercise, it is also assumed that fewer trucks will be moving, idling or other substantial noise generating activities along the North section of the Project than was in the Original design. If this is not the case, the calculations will need to be redone as

this will lead to a considerable increase in Noise for both the River and the Wanis View homes. Basic information used in the present memo is from reference 1.

The new problem with the Fourth Iteration to be addressed considers acoustic sources between buildings, which is approximated as a channel with vertical sides of finite height, parallel non-absorbing walls ($\alpha \ll 1$) and a once reflecting ground surface. This is similar to the problem of sound propagation in a tunnel or waveguide, in which reflections from the surrounding surfaces play an important role. [2-9] The effect of multiple reflections is modeled by the insertion of an infinite series of image sources for the acoustic source (rays). Each of the three channels of the Project (between the four separate buildings) are treated separately and added together with the usual methods.

According to the ray model, the acoustic power flow (P) across a channel cross-section at distance x from the source is obtained by the incoherent sum of the free space power flows from each of the image sources.

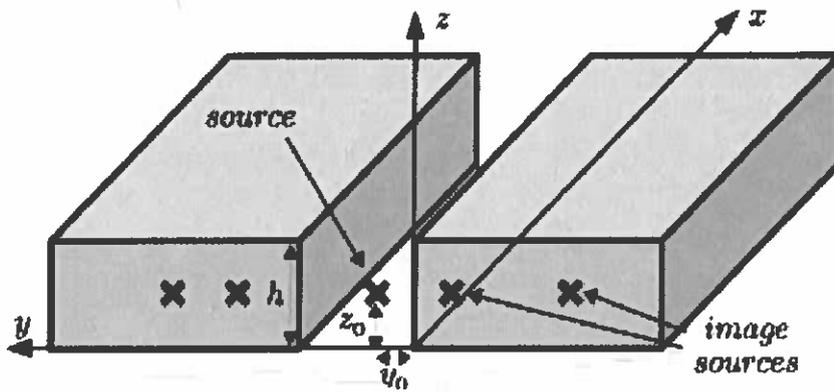
When x is much greater than h (building height) and with absorption at the walls ($\alpha > 0$) this can be approximated by:

$$P \sim \frac{2}{\pi} \int_0^{\frac{\pi}{2}} \frac{(1 - \alpha)^{x \tan \theta}}{\sqrt{1 + \left(\frac{x}{h \cos \theta}\right)^2}} d\theta.$$

The power flow (for constant building height-h, $\alpha \ll 1$ and small values of z_0) which can be approximated as:

$$P = \frac{2}{\pi} \int_0^{\frac{\pi}{2}} \frac{1}{\sqrt{1 + \left(\frac{x}{h \cos \theta}\right)^2}} d\theta$$

The various



quantities are defined in the adjacent diagram

$$x = \cos \varphi \cos \theta,$$

$$y - y_0 = \cos \varphi \sin \theta,$$

$$z = \sin \varphi,$$

Outside of the channels, the standard wave equation is used.

$$\nabla^2 p - \frac{1}{c^2} \frac{\partial^2 p}{\partial t^2} = 0$$

In which p is the acoustic pressure, c is the propagation speed, t is time.

The future owners/operators of the warehouse are unknown (at least not disclosed at the time of this note). The expected and future level of activity is also unknown. Still, a worst case for noise can be described.

Three figures of merit are relevant:

1. Number of active truck bays
2. The location of the bays in the Project
3. Time to unload/load the trucks.

With advances in automation, innovations in processing, the unload/load times are expected to drop over time and the level of activity of the warehouse could increase. The worst case would therefore be a quasi-continuous movement of trucks/forklifts/backup alerts accelerating, moving, idling, loading/unloading and moving between the four buildings of the Fourth Iteration. The results of this analysis

is shown in Figure 2 above.

Appendix II. Noise level calculations for original Project design memo:

Response to the recently released Draft Environmental Impact Report (RAF PACIFICA GROUP) Devastating Impact of a Proposed Warehouse Project on Essential Wildlife Corridors Prepared by Wanis View Wildlife Preserve Volunteers (submitted Dec 4, 2023) [1]

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Relevant Section on Noise Calculations:

Inputs: Literature data, discussions with an industry expert, visits to nearby warehouse operations. Background sound levels were measured at the various locations, both at daytime and nighttime.

Truck Warehouses are very noisy places, some of the noise levels (at 50 feet) are:

Source	Sounds Level (dBa) at 50 feet
Truck Idle	80-96
Truck accelerating	90
Engine Noise	78
Exhaust Noise	88

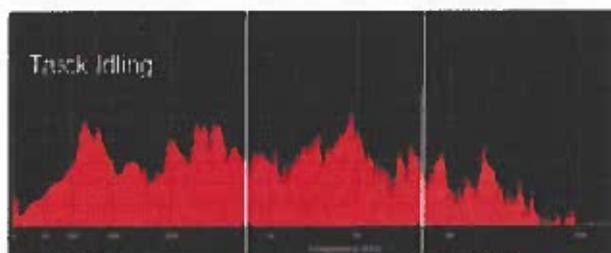
Low speed truck movement	88
Compressors/exhaust fans/HVAC	95
Trailer Refrigeration Units	96-104
Back-up alarms	80

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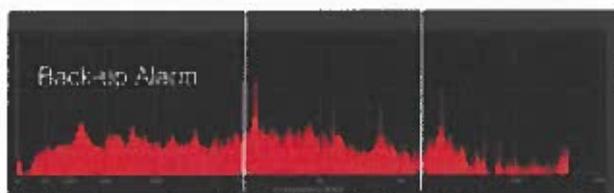


Frequency Spectrum of sounds produced in a diesel truck warehouse



The White bars show the part of the Spectrum that is most important for Birds and other wildlife

dBa values are related more to human hearing (frequency 2000 to 6000 Hz)



These three dominant sounds at a warehouse all produce considerable energy in the frequency range of importance for wildlife (1000-4000 Hz, the region inside the white vertical bars in the above figure). The back-up alarms are actually designed to produce sound at 1200 Hz making them highly problematic for wildlife. Note the low frequency contributions in the truck idling and truck accelerating charts.

A noise signal component can be characterized with four quantities: amplitude, frequency content, time, and phase. It can be mathematically described as

$$x(t) = \sum a_i \cos(\omega_i t + \alpha_i) + \sum \sum b_{ij}(t) u(t - t_j) \times \cos(\omega_{ij} t + \alpha_{ij})$$

where a_i and $b_{ij}(t)$ denote the amplitude of a signal component, ω_i and ω_{ij} represent the frequency, $u(t)$ is the step function, t_j is the instant at which an event occurs, and α_i

and α_{ij} are phases of signal components.

The subject calculations are three dimensional in nature, but ignore some acoustic effects related to the terrain and topography of the nearby hills and canyons. The

nature of the ground in nearby areas would tend to reflect and perhaps focus some of the acoustic energy. The current calculations consider only noise from these sources: trucks accelerating, trucks idling, HVAC, back-up alarms. Other noise sources will depend on the specifics of the warehouse operations. For example, significant additional noise will be present if refrigerated operations are in place.

The usual correction factor for vegetation is applied to the final results. This does not completely exist now, but it is assumed the warehouse developer could add this to help reduce noise levels. The existing vegetation along the Wanis Nature trail is included. The ground-level (road surface) of the warehouse would be some 8-10 meters above sea level. The San Luis Rey river in that region is 4-5 meters above sea level. The Wanis nature trail is 14 meters above sea level. The Wanis View Preserve ranges from 8 to 60 meters above sea level.

Two Cases Considered:

1. Worst Case— assumptions and rationale.

The wildlife preserves and corridors are meant to function forever. The Wanis View Preserve agreement terms are “in perpetuity”. We cannot predict the future owners/operators of the warehouse and thus the level of activity. Still, a worst case for noise can be described. Two figures of merit are relevant. One is the possible numbers of truck bays and the other is the time to unload/load the trucks. With advances in automation, innovations in processing, the unload/load times are expected to drop over time and the level of activity of the warehouse could increase. The worst case would therefore be a quasi-continuous movement of trucks accelerating, moving, idling, loading/unloading, pausing as they move to enter/exit

the warehouse.

2, Typical Case– assumptions and rationale.

A typical case is considered in which there are on average 2 trucks pulling in/out of the facility, 4 trucks idling, HVAC, and 4 back-up alarms in use.

$$\nabla^2 p - \frac{1}{c^2} \frac{\partial^2 p}{\partial t^2} = 0$$

Calculation Details: The starting point for the calculations is solving the standard wave equation:

In which p is the acoustic pressure, c is the propagation speed , t is time.

In evaluating the **Worst Case**, the continuous movement of trucks gives a cylindrical symmetry to the acoustic source (line sources parallel to the river basin). The form of p (r,t) in this case is best expressed by the H₀ and H₁ Hankel functions.

Location	Distance (feet)	Calculated Noise Level Increase
River	340	1025%
Wanis Nature Trail	1179	944%
Wanis View Preserve	1790	954%

Typical Case. This is a case in which a number of effectively point sound sources are distributed in space. These sources will be uncorrelated. Each of these point sources emit spherically symmetric acoustic waves with a boundary condition (reflection) on the hard ground surface as well as the building itself.

Location	Distance (feet)	Calculated Noise Level Increase

River	340	1012%
Wanis Nature Trail	1179	398%
Wanis View Preserve	1790	0%

Confirmation of Methodology used in this analysis.

The reliability of the current calculation methods was checked by comparing them to measurements over the area in question. Hwy 76 was used as a noise source (cylindrically symmetric source) located south of the warehouse. The measurements were conducted north of the warehouse site, so this includes all the terrain in question. The sound intensity was measured at various distances from Hwy 76 during early morning rush hour. (This corresponds to 76 dBa at 50 feet). Measurements at various locations were done in the absence of aircraft, wind and bird noise). Background noise levels were measured at early morning hours with minimal traffic on Hwy 76. At Rush Hour:

2400 feet from Hwy 76	Increase in Noise Level
Measurement	288%
Calculation	264%

Measurement-Calculated values are within 8%.

Appendix III. References

1. Memo prepared by Wanis View Preserve Volunteers in response to DEIR, submitted Dec 4, 2024.
2. Francis M. Wiener; Charles I. Malme; Creighton M. Gogos Sound Propagation in Urban Areas J. Acoust. Soc. Am. 37, 738–747 (1965 3. J. Kang. Urban sound environment. Taylor and Francis, 2006 4. R. H. Lyon. Role of multiple reflections and reverberation in urban noise propagation. Journal of

Applied Physics 111, 114906 (2012)

5. Miguel Molero, Simon Felix, Vincent Pagneux, and Olivier Richoux.. Sound propagation in periodic urban areas Soc. Am., 55(3):493–503, March 1974.

Noise Concerns with Fourth Iteration– the Project Jan 2025

15

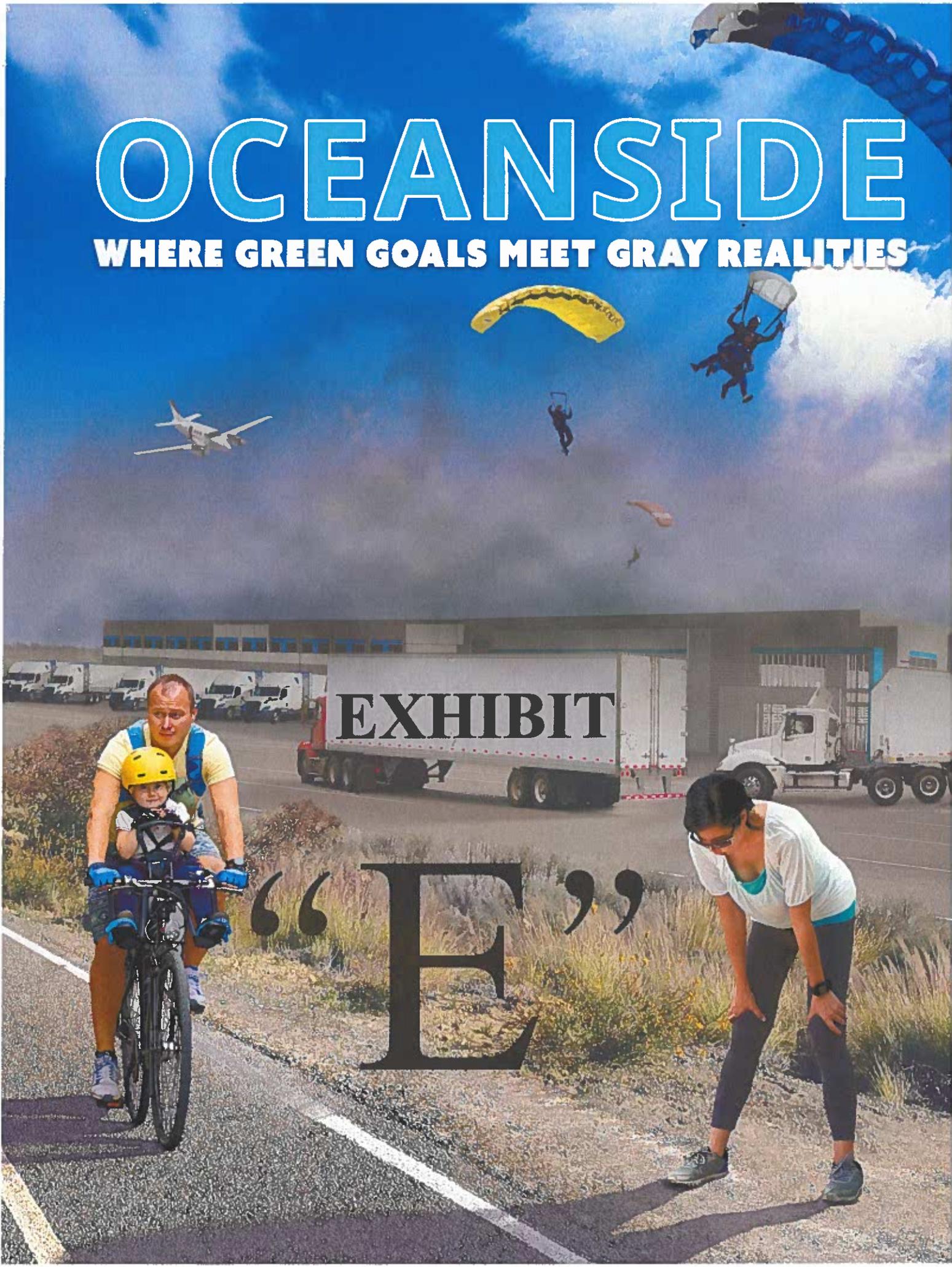
6. M. Hornikxa, M. Dohmena, K. Conenb, T. van Hooffc, The wind effect on sound propagation over urban areas: Predictions for generic urban sections Building and Environment Volume 144, 15 October 2018
7. M R Ismail The propagation of noise through the urban fabric Proc. J.O.A. Vol 22 Pan 2 (2000)
8. Wolfgang Probst Prediction of Sound radiated from Tunnel Openings March 2010 Noise Control Engineering Journal 58(2)
9. David Peter Hewett Sound Propagation in an Urban Environment thesis Term 2010

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“E”



Response to the recently released Final Environmental Impact Report

(RAF PACIFICA GROUP)

WildFire Issues with the Fourth Iteration of the Project

Prepared by Michael Tenhover (February 7, 2025)

The following memo describes concerns with the Fourth Iteration of the “eddie jones warehouse project” (the “Project”). The Wildfire Evacuation Study (WES) section is seriously flawed with its identification of where a wildfire will start, what areas should be of concern, available evacuation times for the community, and the amount of time it will take for a Wildfire to threaten the nearby community

Table of Contents

- I. Major Issue with the FEIR Fourth Iteration - related to Wildfire
- II. FireWise/Cal Fire identification of Areas of Concern- Fire Safety
- III. Recommendations- Studies that must be completed before the Project can be considered for approval

Appendix I- Fire Safety Issues- Planning Commission August 2023

I. Major Issue with the FEIR- Fourth Iteration- related to Wildfire

In the FEIR, the Wildfire Evacuation Plan has used a number of questionable and unreasonable assumptions. Based on these, the plan states that in the event of a Wildfire, the Wanis View community will have 2 hours before they are threatened by the fire. They also state that with the Project and the issues with evacuation routes, a worse case for the time to evacuate is 1 hour and 32 minutes. **None of these assumptions/results seemed to be accurate.**

Specific Problems:

- 1. The Study has assumed a start location for the Fire, shown below:**



Figure 1.

The fire is assumed to start at the RED symbol on the Right/Middle of the figure above (GPS coordinates shown). This is in an area with low levels of fuel as well as low levels of fuel to the East and South and as far away from the area as possible. This seems to be a “best case” for both slow fire propagation as well as distance from the Wanis View Homes and the Project.

It is not surprising that the study found this fire to move slowly and provide time for evacuation, but it is not at all realistic.

- 2. Studies need to include both other start points and variations in wind direction.**

Also shown in the above figure are Yellow symbols that are equally (if not even more likely for a wildfire to start). The WES needs to use the same methodology to evaluate the fire behavior for these locations. They also need to include variations in wind direction– not just East, but NorthEast, and even NorthWest.

Wildfires can start from natural sources but also can be the result of open fires (homeless camps cooking, heating), off-road vehicles, and arsonists.

3. Fuel Loads in the area are highly variable

The area in question can have huge variations in the fuel loading depending on the amount of seasonal rain, brush abatement measures, and summer heat. This input also needs to be included in the fire models.

4. The appropriate goal of a WES.

The goal of this exercise should be to find the minimum time for a fire to reach the homes, not the maximum time a fire can burn in the area before being a threat.

It is easy to see that a Wildfire starting on any of the Yellow symbols will be an almost immediate threat to the community.

5. Evacuation Times for the Community.

We are lucky in Oceanside to have excellent leaders in both our Fire and Police Departments. However, even these capable groups will find it difficult to evacuate this complex neighborhood in the time that will be available. Rather than the two hours claimed in the WES, there may be as little as 30 minutes to perform this essential task, depending on where the wildfire will start and wind direction. The added difficulty might be the day of the week as well as time of day for the evacuation. **With such a tight timeline, the impact of the Project and associated heavy trucks will have on hindering evacuation will be very significant.**

II. FireWise/CAI Fire identification of areas of concern.

Volunteers in the Wanis View area formed a Firewise program in 2023. In part of the certification process they worked with Cal Fire to identify areas of concern for Fire Safety. This exercise and results were presented to the Oceanside City Council in August 2023 (Appendix I). Shown below is the result of this evaluation:

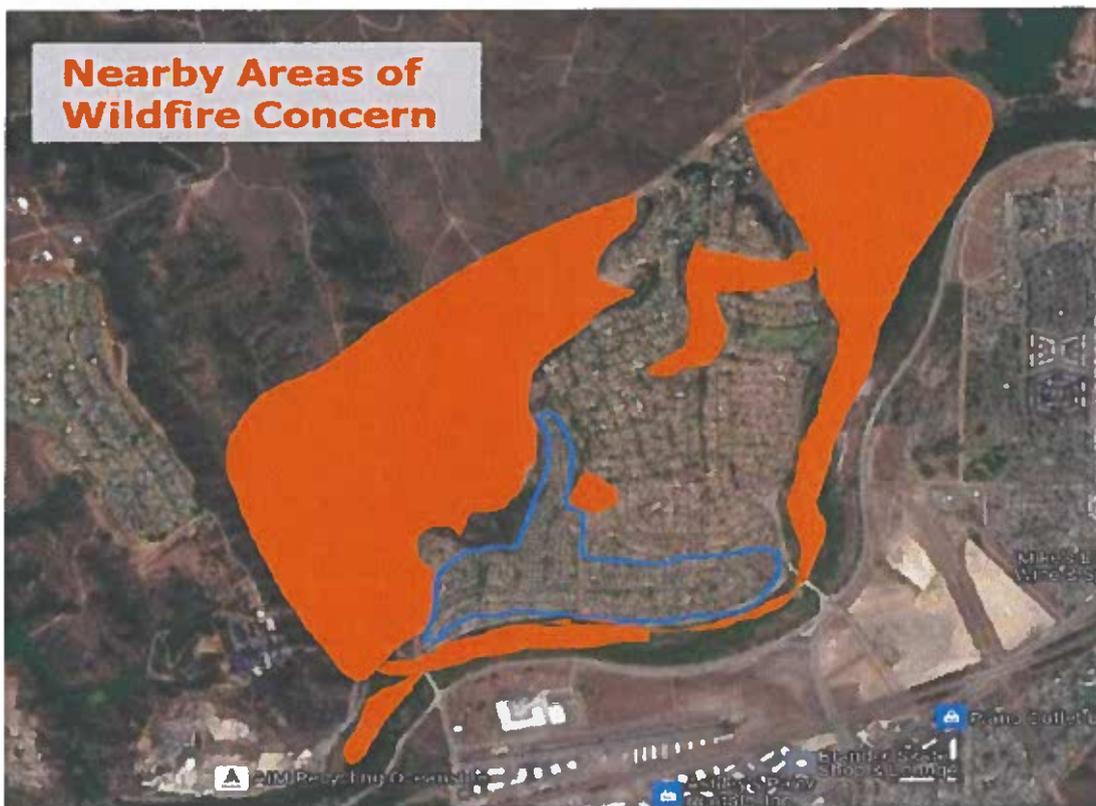


Figure 2

The Orange areas are places based on fuel, terrain, and wind direction that were designated “Areas of Concern”. This information is not considered in the WES

and paints a very different picture of where wildfire could start, where abundant fuel is sometimes present, and the proximity to the residents.

III. Recommendations- Studies that must be completed before the Project can be considered for approval.

They must include:

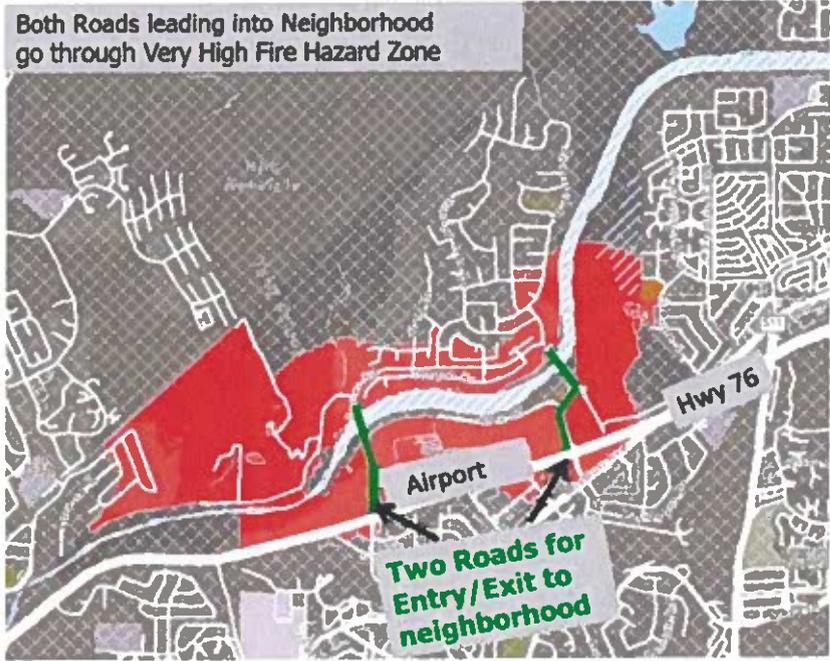
1. As described above we need studies that provide information on the “most dangerous” scenarios, not just one favorable for the Project.
2. These should include at a minimum, the locations shown in Figure 1 - Yellow symbols.
3. The studies must also look at variations in wind direction and fuel loads

Appendix I. Presentation on Fire Safety Issues Oceanside City Council (August 2023)



FIREWISE PROGRAM– Only Oceanside Area in CAL FIRE’s Very High Fire Hazard Zone

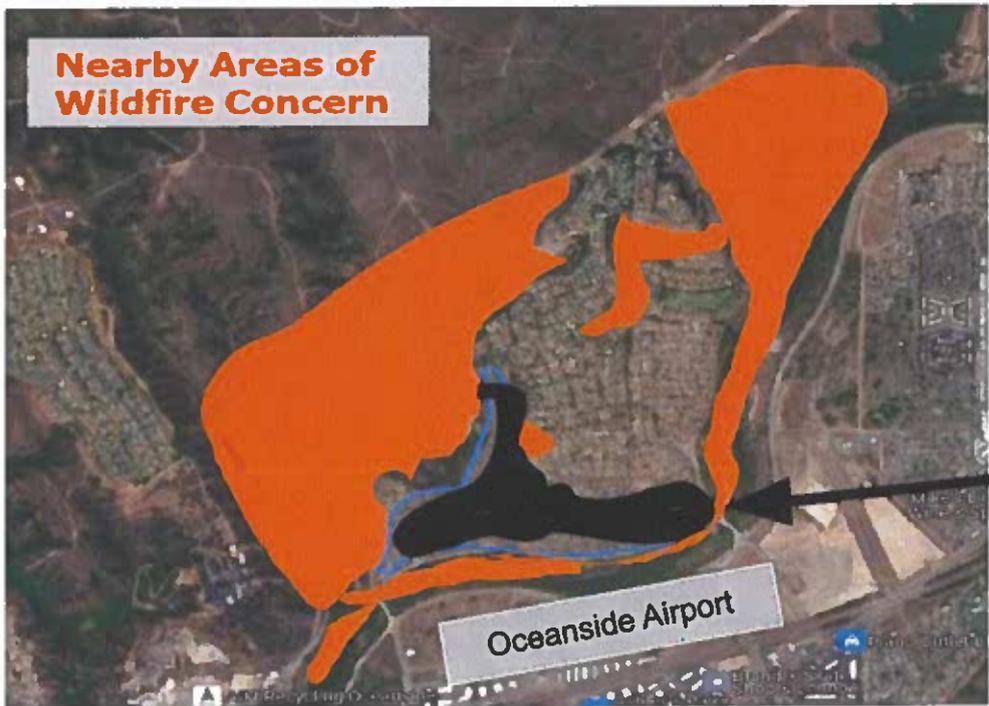




Evacuation and
Emergency Vehicle

We only have two
roads, both of
which are in the
Fire Hazard Zone

5000 residents



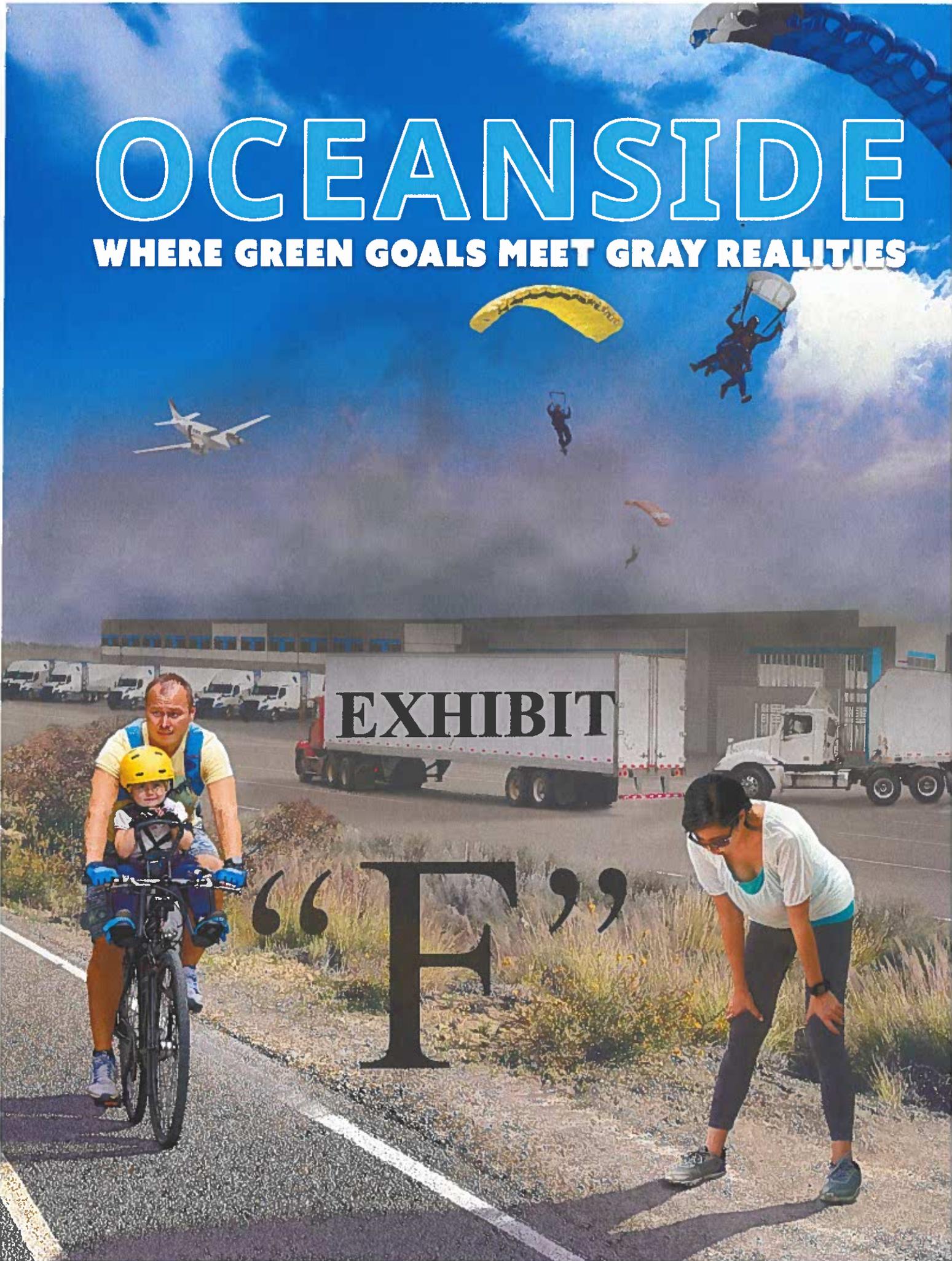
Our
Program
Area

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“F”





February 7, 2025

City of Oceanside Planning Commission
Sent via Email

Subject: Comments on Eddy Jones Warehouse Final EIR and Project

Honorable Chair and Commissioners

The Eddy Jones Warehouse Project is one you might be inclined to approve. It is an industrial use on industrial zoned land. It sat vacant for several years—providing no jobs. According to the analysis done for the Economic Development Element of our General Plan, our greatest job producing need is for even more land to be zoned industrial.

But this project has failed to address critical impacts it will have on our community. This is a project that requires a Conditional Use Permit (CUP) because it exceeds the limits of 50,000 square feet and no more than 6 heavy duty trucks at one time. This requirement for a CUP gives you broad discretion to add Conditions of Approval and ensure that the remaining concerns with this project are fully addressed.

The following are the key issues we ask you to address in the CUP:

- **Edge effects of this development on the adjacent sensitive habitat along the San Luis Rey River**

Attached are standard conditions of approval used by Oceanside for many years after the MHCP was adopted in 2003 and then got buried. Not 100% of them apply to every project. But they were all considered and incorporated as appropriate for development next to sensitive habitat. The responses to comments verify that many of these have been ignored. Feral cats are estimated to kill over 2 billion birds a year in this country. The sensitive river habitat supports endangered Least Bell's Vireo and California Coastal Gnatcatcher, among others. We submit that throughout this region one can walk behind industrial or commercial buildings and find that someone is leaving out food and water for feral cats. It is not sufficient to just say the project won't do that—it needs to be conditioned to do so. Also, the concern about lighting is not just building lighting that is required to be low sodium and directed to not spillover to sensitive habitat. The headlights from these trucks extend beyond the limits of the project and can disrupt many nocturnal animals. There is a reason for every one of these conditions—and all need to be considered.

- **GHG and Air Quality impacts from all of this truck traffic has not been adequately addressed**

The FEIR responses to comments on page RTC-97 states “Consistent with the City’s formally adopted VMT standards the DEIR uses SANDAG’s Employee VMT by census tract...” The drivers of the trucks accessing this facility are not employees, and consequently they are not included in the analysis of VMT and the associated GHG emissions. It is a step in the right direction that the project added the recommended environmental justice mitigation measures. The Inland Empire has had extensive experience with large warehouses like this. Included are attachments that discuss some of the litigation, settlement, analysis and recommended mitigation measures that have affected that area. Our local Air Pollution Control District has a study underway to develop local standards for these large warehouses. But the requirement for a CUP allows you to incorporate the recommendations from others who have studied this issue.

- Impact on safe emergency evacuation

We appreciate that the developer of this project prepared the requested Fire Evacuation Time Study. But we disagree with the conclusion that adding 10 minutes to a worst-case scenario that already results in a 59 minute evacuation time is a “less than significant impact.” Furthermore, there are numerous recommendations in the study that need to be incorporated into project conditions. These include “The Wildfire Evacuation Study will require regular adjustment and continuous coordination by the Owner(s) and or Property Manager and fire/law enforcement agencies during each stage of the construction process.” It also says that in order to evacuate more people(as will be required with this project) that emergency managers must “ 1. Provide more lead time to evacuate and 2. Control traffic levels during evacuations so fewer vehicles are trying to exit at the same time.” It further notes the need for “diligent public education and emergency personnel training and familiarity,” and the need to require notification of the presence of special needs populations to the Fire Safety Coordinator. All of the recommendations of this study need to be formally incorporated into the project conditions of approval. **Please note that the project Conditions of Approval in Res 2025-PO4 do not reference this report or incorporate any of its recommendations.**

- Limit of “56 trucks” is not sufficient, or accurate

The project description for this new alternative specifically says it will include bays for “56 heavy trucks plus 45 grade level loading docks for small trucks.” All trucks will contribute to the traffic, GHG and air quality impacts. But of course heavy duty trucks will have the greatest impact. This needs to correctly limit the project to “ 56 heavy trucks and no more than a total of 101 truck loading docks”.

- TDM Plan has not been provided

Other city projects include the draft TDM Plan that is required by City Ordinance and to comply with the CAP. That makes it possible to determine if it appears to be sufficient to address the additional trips that will be generated, and to provide an opportunity for public comment. This project has not included even the draft TDM plan. Failure to provide this document leaves

compliance with the CAP unknown and leaves this a this a potentially significant impact that has not been addressed.

Thank you for considering our comments.

Sincerely,
Diane Nygaard
On behalf of Preserve Calavera

Attachments:

- A. MHCP/C SAP Edge Effect Conditions
- B. Warehouse Mitigation Measures

Link Attachments:

- C. [Warehouse Litigation 7-21-2021 v2](#)
- D. [State of Emergency Public Health Request](#)
- E. [Industrial Warehousing Report - Revised 2018](#)

Attachment A: MHCP/C SAP Edge Effect Conditions

City of Oceanside - Standard Project Conditions

SELECT AS APPROPRIATE - FOR PROJECTS LOCATED ADJACENT TO RESERVE AREAS OR WITH OTHERWISE SENSITIVE HABITAT ISSUES

1. A qualified biologist shall be retained by the applicant to review the final grading plans, access routes and staging areas, monitor all aspects of construction, educate contractors about the biological sensitivities associated with the area and ensure compliance with mitigation measures.
2. The qualified biologist shall conduct a training session for all project personnel prior to any grading/construction activities. At a minimum the training shall include a description of the target species of concern, its habitats, the general provisions of the Endangered Species Act (Act) and the MHCP, the need to adhere to the provision of the Act and the MHCP, the penalties associated with violating the provisions of the Act, the general measures that are being implemented to conserve the target species of concern as they relate to the project, any provisions for wildlife movement, and the access routes to and project site boundaries within which the project activities must be accomplished.
3. A water pollution and erosion control plan shall be developed that describes sediment and hazardous materials control, dewatering or diversion structures, fueling and equipment management practices and other factors as deemed necessary. Erosion control measures shall be monitored on a regularly scheduled basis, particularly during time or rainfall. Corrective measures shall be implemented in the event erosion control strategies are inadequate. Sediment/erosion control measures shall be continued at the project site until such time as the revegetation efforts are successful at soil stabilization.
4. The footprint of habitat disturbance shall be minimized to the maximum extent feasible. Access to sites shall be via pre-existing access routes to the greatest extent possible.
5. The upstream and downstream limits of project disturbance plus lateral limits of disturbance on either side of the stream shall be clearly defined and marked in the field and reviewed by the biologist prior to initiation of work.
6. Placement of equipment and personnel within environmentally sensitive habitat areas stream channels or on sand and gravel bars, banks and adjacent upland habitats used by target species of concern shall be avoided. Activities that can not be conducted without

placing equipment or personnel in sensitive habitats shall be timed to avoid the breeding season of the target species of concern.

7. When stream flows must be diverted, the diversions shall be conducted using sandbags or other methods requiring minimal instream impacts. Silt fencing or other sediment trapping materials shall be installed at the downstream end of the construction activity to minimize the transport of sediments off-site. Settling ponds where sediment is collected shall be cleaned out in a manner that prevents the sediment from re-entering the stream. Care shall be exercised when removing silt fences, as feasible, to prevent debris or sediment from returning to the stream.
8. Equipment storage, fueling and staging areas shall be located to minimize risks of direct drainage into riparian areas or other environmentally sensitive habitats. These designated areas shall be located in such a manner as to prevent runoff from entering sensitive habitats. All necessary precautions shall be taken to prevent the release of cement or other toxic substances into surface waters. All project related spills of hazardous materials shall be reported to appropriate entities including but not limited to the City of Oceanside, FWS, and CDFG, SWQCB and shall be cleaned up immediately and contaminated soils removed to approved disposal areas.
9. Erodible fill material shall not be deposited into water courses. Brush, loose soils, or other similar debris material shall not be stockpiled within the stream channel or on its banks.
10. Stockpiling of materials and other aspects of construction staging shall be limited to disturbed areas without native vegetation, areas to be impacted by project development or in non sensitive habitats.
11. "No-fueling zones" shall be established within a minimum of 10 meters (33 feet) from all drainages and fire sensitive areas.
12. Scheduling of construction activities shall minimize potential impacts to biological resources. Construction adjacent to drainages shall occur during periods of minimum flow (i.e. summer through first rain of fall) to avoid excessive sedimentation and erosion and to avoid impacts to drainage dependent species. Construction near riparian

areas or other sensitive habitats shall be scheduled to avoid the breeding season (March through September) and potential impacts to breeding bird species.

13. Construction activities during the breeding season (dates to be determined depending upon species of concern- some start in Feb March through September) shall be limited to those that will not produce significant noise impacts (i.e. noise levels greater than 60 dBL_{eq} –decibels equivalent sound level) at the edge of the habitat of concern.
14. Conduct preconstruction surveys at potential impact areas between mid-May and mid-June.
15. Human and pet access to preserve areas shall be limited to designated trails by use of natural vegetation, topography, signs and limited fencing.
16. Artificial lighting adjacent to the preserve area shall be eliminated except where essential for roadway, facility use and safety and security purposes. Where use of artificial lighting is necessary it shall be limited to low-pressure sodium sources. Use of low voltage outdoor or trail lighting, spotlights or bug lights is prohibited. All light sources shall be shielded so that lighting is focused downward to restrict any light spillover onto sensitive habitat.
17. The HOA shall establish an education program for homeowners regarding responsible pet ownership. The program shall encourage a) keeping pets indoors, especially at night; b)having pets neutered or spayed to reduce unwanted reproduction and long-range wandering; c)belling of cats to reduce their effectiveness as predators; d) prohibiting release of unwanted pets into the wild; e) keeping dogs on leashes when walking them on trails in preserve areas.
18. The HOA shall establish a feral animal removal program.
19. The qualified biologist shall monitor construction activities throughout the duration of the project to ensure that all practicable measures are being employed to avoid incidental disturbance of habitat and any target species of concern outside the project footprint. Construction monitoring reports shall be completed and provided to the City of Oceanside, FWS and CDFG summarizing how the project is in compliance with applicable conditions. The project biologist shall be empowered to halt work activity if

necessary and to confer with staff from the City of Oceanside, FWS and CDFG to ensure the proper implementation of species and habitat protection measures.

20. The removal of native vegetation shall be avoided and minimized to the maximum extent practicable. Temporary impacts shall be returned to pre-existing contours and revegetated with appropriate native species. All revegetation plans shall be prepared and implemented consistent with Appendix C (Revegetation Guidelines of the Final MHCP Plan – Volume II) and shall require written concurrence of the FWS and CDFG.
21. To avoid attracting predators of the target species of concern, the project site shall be kept clean of debris as possible. All food related trash items shall be enclosed in sealed containers and regularly removed from the site. Pets of project personnel shall not be allowed on site where they may come in contact with any listed species.
22. Construction employees shall strictly limit their activities, vehicles, equipment, and construction materials to the proposed footprint and designated staging areas and routes of travel. The construction area(s) shall be the minimal area necessary to complete the project and shall be specified in the construction plans. Construction limits shall be fenced with orange snow screen. Exclusion fencing shall be maintained until the completion of all construction activities. All employees shall be instructed that their activities are restricted to the construction areas.
23. Any habitat destroyed that is not in the identified project footprint shall be disclosed immediately to the City of Oceanside, FWS and CDFG and shall be compensated at a minimum ratio of 5:1.
24. If dead or injured listed species are located, initial notification must be made within three working days, in writing to the Service's Division of Law Enforcement in Torrance California and by telephone and in writing to the applicable jurisdiction, Carlsbad Field Office of the FWS, and CDFG.
25. The City of Oceanside shall have the right to access and inspect any sites of approved projects including any restoration/enhancement area for compliance with project conditions and BMPs. The FWS and CDFG may accompany the City representatives on this inspection.

26. Any planting stock to be brought onto the site for landscaping or ecological restoration shall be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to Argentine ants, fire ants, and other insect pests. Any planting stock found to be infested with such pests shall not be allowed on the project site or within 300 ft of natural habitats. The stock shall be quarantined, treated or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats.
27. New utility lines or towers or modification of existing utility lines or towers shall implement designs that preclude or minimize harm to wildlife due to collisions or electrocution. Information on such designs is available at www.migratorybirds.fws.gov/issues/towers.
28. Use bridges, instead of culverts for all major riparian crossings and regional wildlife movement corridors. The site of the riparian crossing and its importance as a wildlife corridor should dictate the design. (Where appropriate based on site specific survey results) Wildlife undercrossings shall be designed and implemented (for new roads or road improvement projects that could disrupt wildlife movements or result in increased road kill). Such undercrossings, along with any necessary wildlife fencing or other facilities, shall be designed based on best available information to maximize use of the undercrossing by species of concern. Undercrossing design shall strive to maximize the openness index (widthXheight/length), minimize traffic noise within the crossing, use skylight openings within the underpass to allow for vegetative cover within the underpass, use appropriate fencing to funnel wildlife into the crossing rather than across the road surface, and screen the undercrossing opening and access path with natural vegetation. Undercrossing design shall be subject to review and approval by the City of Oceanside, FWS and CDFG prior to issuance of grading permits.
29. All mitigation sites shall be conserved through fee title acquisition or conservation easement, and proof of recordation shall be provided to the City of Oceanside prior to land disturbance.
30. Use of retaining walls shall be minimized. Development on the site shall be configured to existing topography to minimize grading and landform alteration.

31. Existing natural drainages and watersheds shall be maintained. The project shall restore or minimize changes to natural hydrological processes.

32. Detention basins shall incorporate earthen berms to allow growth of natural vegetation.

Note : There are additional specific conditions in areas of equestrian use

CONDITIONS SPECIFIC TO BUENA VISTA LAGOON

33. The Buena Vista Lagoon boundary (and/or wetland area) shall be delineated and criteria used to identify any wetlands existing on site shall be those of Section 30121 of the Coastal Act. Mapping of wetlands, conditions to protect sensitive resources and siting of development shall be done in consultation and subject to approval of DFG.

34. Landscaping on the site shall be utilized as a visual buffer and shall be compatible with the surrounding native vegetation and preserved open space through installation of native, non-invasive, drought tolerant plant species.

35. Approved landscaping shall be installed immediately upon completion of construction and maintained by the property owner in good growing condition for the life of the development.

36. Landscaping screening of structures with specimen trees and fire-retardant vegetation of substantial height, shall be required to screen and soften the view of structures from _____ (Interstate 5/ Buena Vista Lagoon etc.) and public vista points.

37. A HOA shall implement a landscape management plan that includes herbicide/pesticide management and removal of invasive species..

38. On-site sensitive biological resources areas inclusive of the 100-ft buffer area shall be left in their natural state (or restored with native drought tolerant vegetation) and used only for those passive activities allowed as a condition of permit approval. The permissible passive activities and any other conditions of the permit shall be incorporated into a covenant of easement that shall be recorded against title to the property. The USFWS and DFG shall be named beneficiaries to any covenant of easement recorded pursuant to this condition.

39. The use of chemical pesticides for mosquito control is prohibited (rely on biological agents).

40. Access to buffer and sensitive habitat areas is prohibited during the breeding season (see species specific guidelines for breeding season dates) except for emergency access.

Attachment B: Warehouse Mitigation Measures

Warehouse Mitigation Measures¹

- 1) Trucks certified to meet or exceed ARB's 0.02 g/bhp-h optional low-NOx emissions standard (i.e., near-zero or zero emission).
- 2) Tier 4 or cleaner construction equipment.
- 3) Solar photovoltaic panels on site sufficient to supply all electric energy demands for the office space, air conditioning and dark shell lighting of the project.
- 4) Solar ready roofs.
- 5) Three minute limit on all diesel idling.
- 6) Roundabouts at major intersections.
- 7) Air Quality Complaints. Prior to the start of grading, developer must post legible, durable, weather-proof signs, of a size to be easily readable from the street, at all construction entrances, which state in English and Spanish (i) that diesel trucks servicing the Project site shall not idle for more than 3 minutes; and (ii) the name and telephone numbers of an authorized individual such as the Project Superintendent to be contacted to resolve dust and air quality complaints, and a phone number to the local air district to report violations. The signs must remain posted on the property until construction is complete. All legitimate dust complaints must be resolved within 24 hours of receipt.
- 8) A minimum of 250-foot building setbacks from adjacent properties, and a larger buffer from residential and other sensitive receptor facilities based upon site-specific analyses.
 - 9) Maximize use of native plants in landscaped areas.
 - 10) Maximize use of drought-tolerant landscape materials.
 - 11) Maximize harvesting of rainwater and project drainage.
 - 12) Design streets to capture runoff to irrigate medians and parkways (zero curb design).
 - 13) Provide on-street truck parking turnouts.
 - 14) Exceed Title 24 by at least 15%.
 - 15) Accommodate alternate forms of transportation including, public transportation (bus), charging stations for electric cars, carpooling, and bicycles.
 - 16) Install a sufficient number of electric vehicle charging stations to accommodate 30% of the projected number of employee vehicles. Electric charging units shall meet or exceed Level 2 Electric Vehicle Service Equipment standards.
 - 17) Provide preferential parking locations for ZEVs and carpool/vanpool vehicles.
 - 18) Zero-emission fork lifts and yard goats, or near-zero emission CNG using RNG if electric powered equipment is not readily available.
 - 19) Electric plug-in capacity for all trucks equipped with transportation refrigeration units (TRUs), and TRUs shall be limited to diesel run-time of 15 minutes.
 - 20) Promote the riding of bicycles, through the provision of bike racks / storage, showers and changing rooms.
 - 21) Reduce 'heat-island' effect by incorporating lighter paving materials where possible and light roofing materials on all structures.

¹ Based on document prepared by Richard Drury of Lozeau Drury, LLP. Edited and revised by Joe Lyou, Coalition for Clean Air.

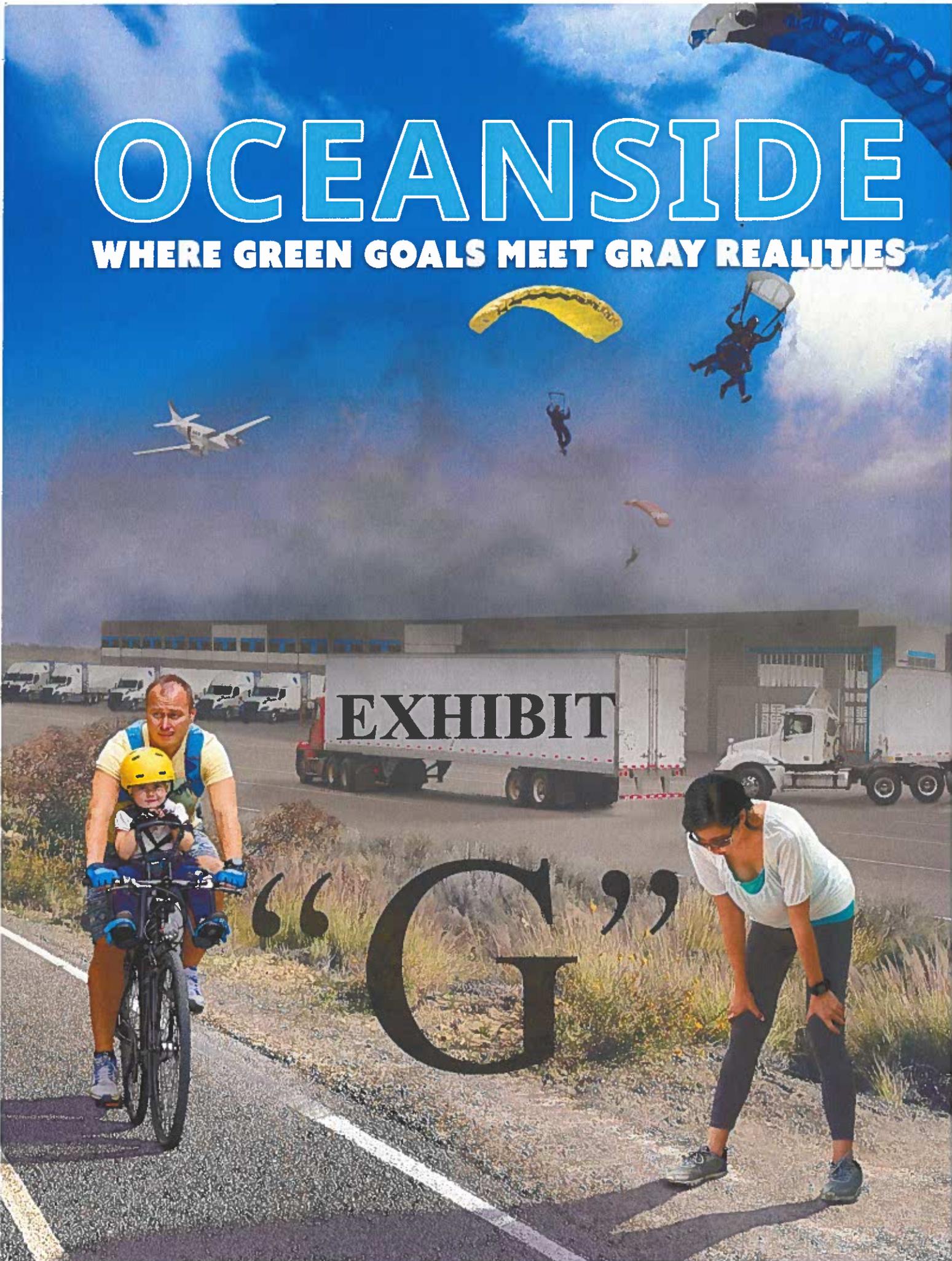
- 22) **Employ adequate shielding features to ensure zero light spill off-site.**
- 23) **Minimize water use in restrooms. Use zero or ultra-low flow urinals, dual flush toilets, and EPA certified WaterSense high efficiency fixtures.**
- 24) **Employ a recycling program.**
- 25) **Divert construction waste from landfills.**
- 26) **Incorporate recycled materials where feasible.**
- 27) **Incorporate low-emitting adhesives, paints, coatings, and flooring systems.**
- 28) **Make the best use of day-light into the interior spaces.**
- 29) **If project changes use of agricultural land, create an agricultural easement for comparable agricultural land (production) in California.**
- 30) **All LEED-certified buildings.**
- 31) **Use non-reflective solar panels.**
- 32) **All sites to be gated and manned 24/7 to monitor/regulate truck access.**
- 33) **Build or arrange for a renewable LNG/CNG fueling station(s) as appropriate to support low-NOx trucks.**
- 34) **Construct sound walls and utilize rubberized asphalt.**
- 35) **Use non-diesel emergency backup generators.**
- 36) **Provide funding for work force development & education.**
- 37) **Create a first source hiring program that encourages and assists local residents in securing facility-related jobs.**
- 38) **Provide funding to local air districts for air quality improvement projects.**
- 39) **Construct active transportation paths and nature trails to the benefit of the community.**
- 40) **Locate truck check-in points sufficiently interior to the project to preclude queuing of trucks onto public streets.**
- 41) **Provide rest areas with free Wi-Fi and restrooms for truck drivers.**

OCEANSIDE

WHERE GREEN GOALS MEET GRAY REALITIES

EXHIBIT

“G”



Dear City of Oceanside Planning Commission:

I am writing to express my opposition to the Eddies Jones warehouse project. I do not live close by but am deeply concerned by the potential impacts to Oceanside residents as well as sensitive wildlife. There must be more suitable locations for distribution centers with over 600 parking spaces and 57 truck bays than right next to our largest and most important wildlife corridor. It's truly baffling to see a project of this scale in this location with a parking lot right next to the San Luis Rey River trail.

As a wildlife biologist, I cannot emphasize enough how important the San Luis Rey River is to our local wildlife, as it serves as the largest continuous wildlife corridor in the area. Its importance was acknowledged in Oceanside's Subarea Plan, the City's commitment to the San Diego Multiple Habitat Conservation Program. The section of the San Luis Rey River near the project site provides critical breeding habitat for the endangered bird species Least Bell's Vireo. Figure 3 of the Biological Technical Report (BTR) within the EIR shows a number of documented occurrences of this species near or within the project boundary. However, the report contradicts this information by stating that 1) "Least Bell's Vireo is known to occur approximately 1.4 miles east of the project site," which is false and 2) "there is no suitable habitat to support this species within the project site," which we know is also false from the documented observations.

I urge the City to require protocol surveys for this endangered species, as they were surprisingly not conducted. This was also noted by the California Department of Fish and Wildlife (CDFW) in their public response to the draft EIR. I have worked with this species along the San Luis Rey River and know they often nest at the southern edge of the river's riparian habitat. Therefore, it is very likely that they use the northern edge of the project boundary, which is less than 100 feet away from the riparian

habitat. I strongly encourage that a larger buffer zone be implemented to protect this sensitive species.

Also this area lies within Oceanside's Wildlife Corridor Planning Zone as designated in Oceanside's Subarea Plan, which is meant to protect another federally threatened bird species, the Coastal California Gnatcatcher. United States Fish and Wildlife Service (USFWS) mentions this important detail in its public comment letter and further explains that this federally-protected species could utilize the project site for dispersal.

USFWS specifically mentions the following from Section 5.3.1.1 of the Subarea Plan (General Development Standards for the Wildlife Corridor Planning Zone), and recommends adherence to this policy:

"removal of native habitats shall be avoided to the maximum extent feasible, without precluding reasonable use of the property. New development on existing properties larger than 2 acres within this zone shall conserve at least 50 percent of the parcel as open space and may remove no more than 25 percent of the coastal sage scrub habitat."

The Final EIR responded to this comment by stating that "This entire property was previously developed and therefore not subject to the General Development Standard." The previous operation at this site ceased about five years ago, thereby allowing the land to revert to a more natural state with minimal disturbance, which increased its biological value. We should not consider every parcel as developed simply because it was developed once in the past because the habitat could now support our local sensitive species for foraging, nesting or dispersal.

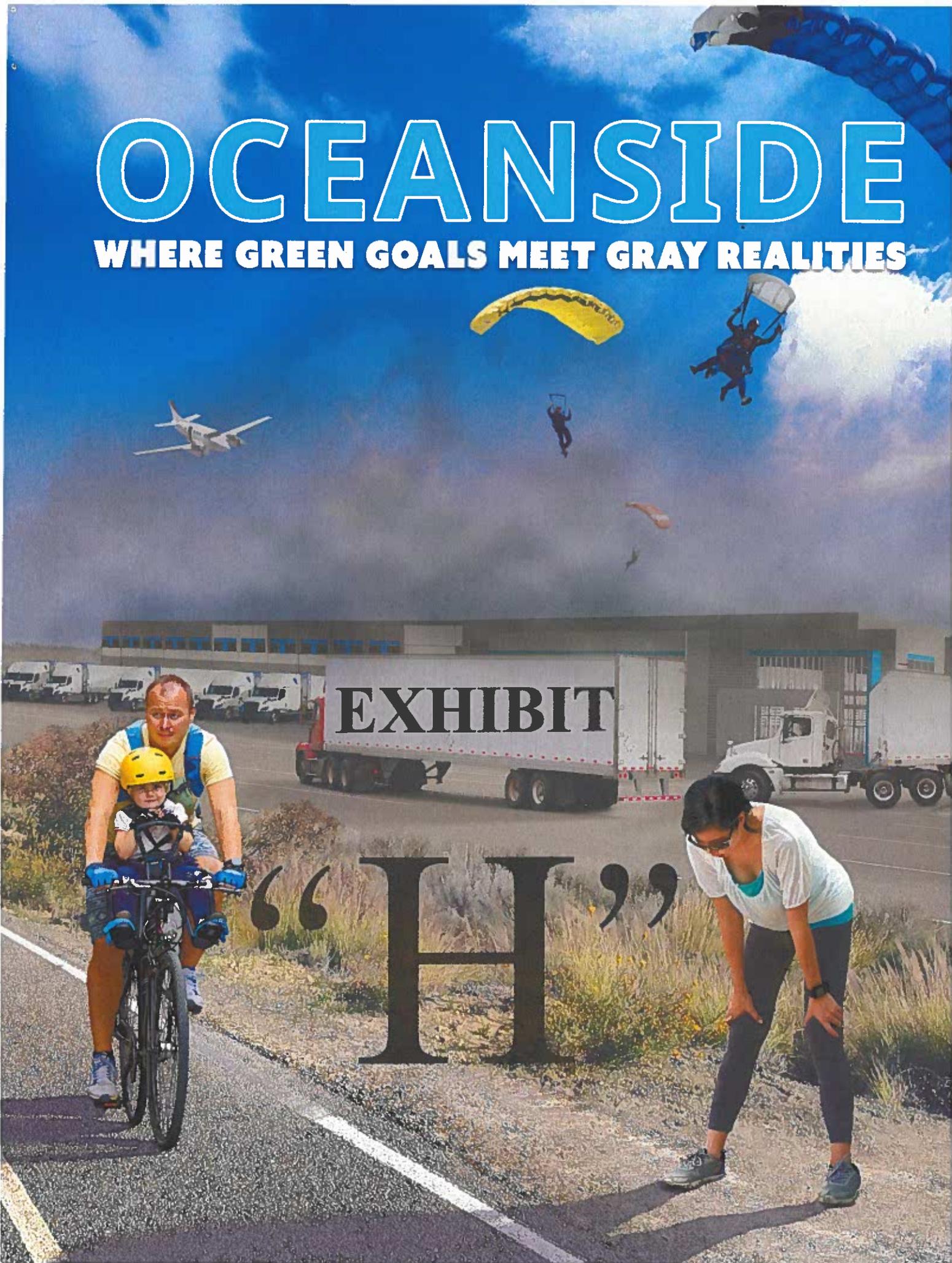
I therefore concur with USFWS that at least 50 percent of the parcel should be conserved as open space to adhere to the Subarea Plan.

I urge the City to please stick to its commitments to protect our sensitive wildlife per the Subarea Plan. Thank you for considering my recommendations.

Sincerely,
Jennifer Jacobs
Feb 10, 2025

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“H”



Environmental Health Sciences Center

Science for a healthy planet



Health Effects of California's Warehouse Boom Raise Concerns Among Residents and Policy Makers

by Angelina Angelo with Julianne Ng | October 02, 2023

Quick Summary

- California's warehouse boom has raised significant concerns over long-term

environmental justice and health equity repercussions.

- Children & underrepresented communities are especially vulnerable to the impacts of the warehouse boom.
- Lawmakers are actively pushing for the implementation of a buffer zone between warehouses and "sensitive receptors."
- As the long-term health effects of the warehouse boom continue to be unknown, it becomes increasingly crucial to increase efforts in bridging the gaps between science and policy to protect overburdened California communities.

California has experienced an unprecedented surge in the expansion of e-commerce and, subsequently, [a dramatic boom in warehouse construction](#). The warehouse boom, while contributing to economic growth, has also raised significant concerns over the long-term environmental justice and health equity repercussions.

Environmental Concerns

The boom in warehouses has raised [significant health concerns](#), especially in already overburdened communities. The exponential growth of warehouses in the Inland Empire is intrinsically tied to its critical location in the nation's goods movement system. In 1980, the Inland Empire hosted 234 large warehouses (over 10,000 sq ft), by January 2023, this



had skyrocketed to over [4,000](#). This growth will have massive impacts on air quality here with similar impacts elsewhere in the state, including the Central Valley.

California's dependence on diesel-powered vehicles has produced large increases in nitrogen oxide (NOx) emissions, creating ozone, a significant health concern. [Ozone](#) is a hazardous compound that forms in the atmosphere when nitrogen oxide (NOx) and volatile organic compounds (VOCs) react in the presence of sunlight. [According to the EPA](#), in 2009, California had the highest levels of ozone concentration in the country; since then, this has only increased. In June of 2023, six California counties, San Bernardino, Riverside, Los Angeles, Tulare, Kern, and Fresno, ranked the highest for

most polluted due to ozone and particle pollution, according to the [American Lung Association](#) and [CalEnviroScreen 4.0](#).

[Diesel engines](#) emit a mixture of harmful air pollutants, including NOx. In addition to ozone, Diesel Particulate matter (DPM) is [responsible for a staggering 70%](#) of the total cancer risk attributable to air pollution. [According to the EPA](#), prolonged exposure to high levels of DPM poses dangerous health risks, such as exacerbating respiratory conditions like asthma, and can worsen pre-existing conditions like lung or heart disease. However, it's the long-term health effects that are of increasing concern for scientists, health practitioners, and communities.

Populations of Concern

Children are especially vulnerable to the health impacts of the warehouse boom. In California, over 300 warehouses are 1000 feet or less from 139 schools, and an additional 600 warehouses are located within 1500 feet, [according to a recent report](#). This toxic geography places thousands of children in the crosshairs. Children, with their still-developing respiratory systems and outdoor play, are at heightened risk for long-lasting health issues resulting from exposure to air pollution. Read [more about concerns for children's environmental health here](#).

Underrepresented communities are also of concern. According to [CalMatters](#), the adverse effects of warehouse pollution and related traffic congestion are overwhelmingly concentrated in neighborhoods inhabited primarily by Latino communities and low-income communities. Other [new research](#) shows the population living within half a mile of a warehouse is 64% people of color in California.

What Next?

The emergence of air pollution and its associated health risks, particularly for children and the region's residents, demands immediate attention and proactive measures. The warehouse expansion plans in California show no signs of slowing; the Inland Empire alone currently has one billion sq feet of warehouse space operating, with 170 million sq ft of warehouse space currently approved or pending, according to a [CalMatters State of Emergency Public Health Request](#).

In response to these health concerns, lawmakers are actively pushing for the



implementation of a buffer zone between warehouses and “sensitive receptors” such as daycare centers, schools, and hospitals to reduce exposure to air pollution. [Assembly Bill 2840](#), which would provide a buffer zone of 1,000 feet between diesel truck traffic emissions generated by any new warehouse facilities and defined these sensitive receptors, has passed the Assembly and

is currently in the Senate. [Assembly Bill 1000](#), a similar policy that bans land permits within 1,000 feet of sensitive receptors, did not pass earlier this year. Many California residents, educators, and local officials are beginning to protest new warehouse builds, according to [CalMatters](#).

As the long-term effects of the warehouse boom on both environmental and health effects continue to be unknown, it becomes increasingly crucial to put more effort into bridging the gaps between science and policy to protect overburdened California communities.

If you found this blog interesting, [register](#) for our [annual retreat on November 8th](#) to keep this vital conversation going.

The author would like to acknowledge and thank Dr. Jonathan London for his contribution of expertise and editing of this blog post.



Angelina Angelo produced this article with support from Julianne Ng, EHSC student assistant in communications & media.

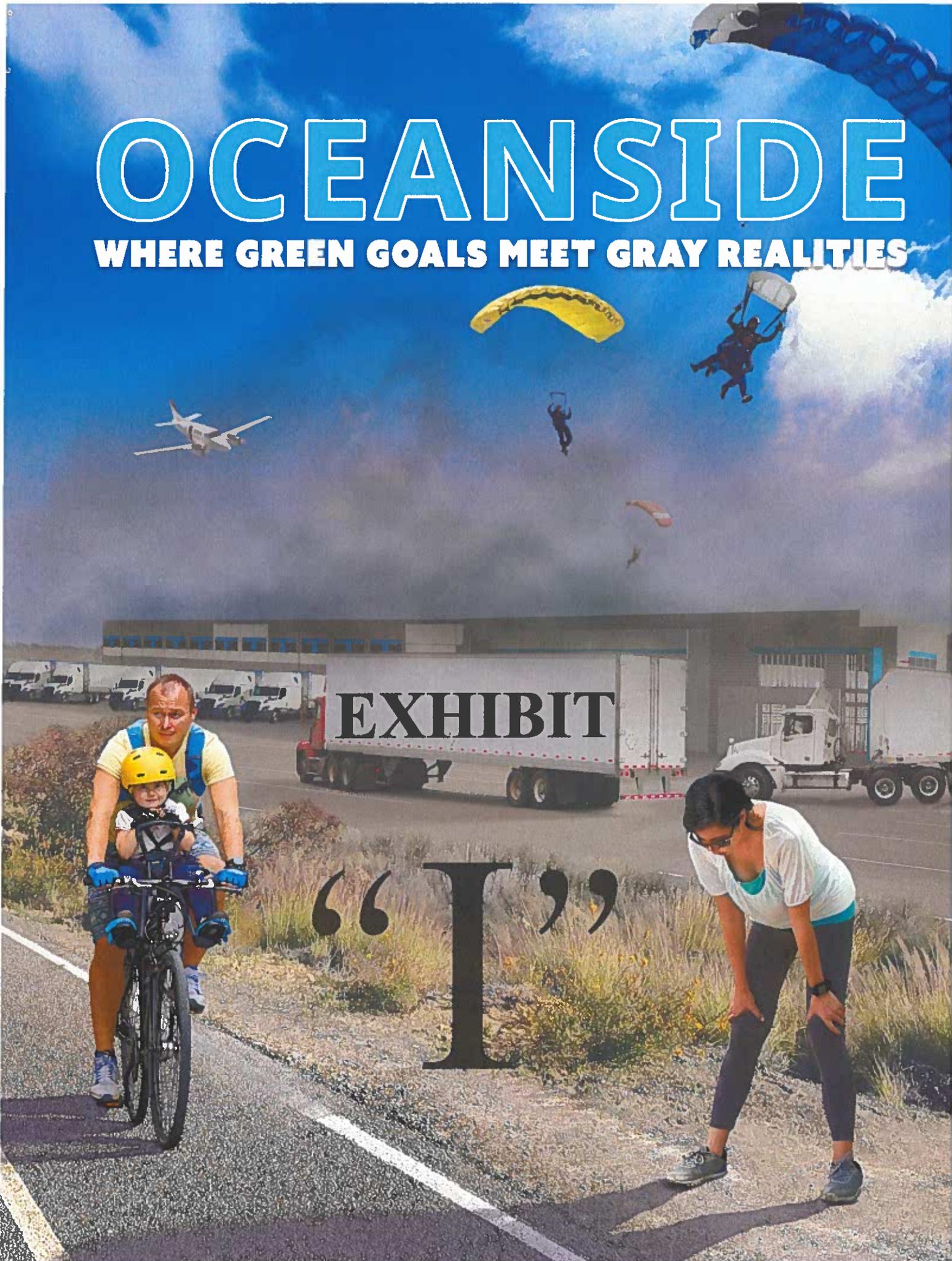
Angelina is an EHSC editorial assistant & undergraduate writer for the communications department and an undergraduate student at UC Davis studying Human Development. She is an aspiring writer with a focus on science communication.

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EXHIBIT

“ I ”



Response to the recently released Environmental Impact Report

(RAF PACIFICA GROUP)

Health Issues with the Fourth Iteration of the Project: Synergetic Hazardous Particulate Material

Prepared by Michael Tenhover (February 8, 2025)

The following memo describes concerns with the Fourth Iteration of the “eddie jones warehouse project” (the “Project”) related to the Health of near-by residents and the large number of people who use the skate park, bicycle river trail, GoJump, the future OceanKamp and other nearby areas for recreation. The concern centers around placing a warehouse producing copious amounts of Diesel Particulate Material near the Bob Maxwell Memorial Field (Oceanside Airport), a facility that uses leaded fuel for aviation.

Table of Contents

- I. Major Issue with the EIR Fourth Iteration - Lead dibromide/Diesel Exhaust Synergy.
- II. Recommendations
- III. Pollution in the area near the Project

Appendix 1: Lead dibromide nanoparticles- size and morphology

Appendix 2 EIR descriptions of Lead and DPM

Appendix 3: Possible mechanism of particle agglomeration

References

I. Major Issue with the EIR- Fourth Iteration

Synergistic Combination of Lead dibromide and diesel exhaust particles:

While the EIR describes the concentration and impact of DPM on the nearby areas, it fails to consider the fact that there are other health risks at the site.

- a. The proposed location of the Project is in the highest Lead exposure zone in Oceanside. One source of this lead is the lead dibromide particles (LDB) from piston engine aircraft at the Oceanside Airport.
- b. The project, with the diesel engine trucks will be a source of PM10 and PM2.5— partially combusted hydrocarbon particles (DPM)
- c. The possibility of agglomeration of LDB/DPM is a concern that can lead to a synergistic effect resulting in particles that are more toxic, have reduced settling time, have higher exposure to nearby people and are more easily retained in human lungs. The physics and chemistry of the situation point towards this being a real concern, but without data taken on-site with DPM production near the Oceanside Airport it is speculative.
- d. In contrast to most hazardous materials, there is no safe level for lead exposure. Any action to reduce the lead concentration and exposure time is a benefit to life and livelihoods. The maximum contaminant level goal for lead is zero. (as set by the US EPA). In addition, lead is a cumulative poison and once absorbed, it is difficult for the body to excrete- so the level will build up in the body over time.
- e. The optics of this situation should be a concern for the City— locating this Project in proximity to the Airport runway

II. Recommendations

1. Caution is warranted in locating a DPM producing facility like the proposed Project near a local airport runway. The low annual rainfall, coastal-exposure environment of the Project seems to be the perfect conditions for LDB/DPM agglomeration.

2. Limiting the number of trucks on-site would be a useful measure to minimize the possible health risks for the nearby community and recreation areas.
3. Exclusive use of biofuels for all diesel vehicles
4. Low emission trucks (EV and hybrid) would also help minimize the health risks.

III. Current Pollution in the area near the proposed Project:

Figure 1 below shows the relative lead levels in a wide section of the City of Oceanside [1]. The location of the Project is at the Lead=69 marker. That is, the highest lead level in the City. Since the phase-out of leaded gasoline in automobiles, the largest source of atmospheric lead is aircraft aviation fuel containing tetraethyl lead. The proposed location of the Project is adjacent to the Oceanside Airport. As described in Appendix I, the lead containing particles are lead dibromide with a nanometer particle size. There is an associated partially combusted hydrocarbon/oxide layer on these particles.



Diesel Emissions: The exhaust from a modern diesel engine contains a wide variety of particle sizes. These are very large compared to the LDB particles described above. (see Appendix 2)

As described in Appendix 3, the concern here is the possible agglomeration of the ultrafine LDB and coarse DPM particles. The EIR discloses information on both Lead and DPM, but does not consider the effects of a combination of them.

These combined/agglomerate particles will have:

1. Reduced settling times compared to LDB
2. Increased human lung retention
3. Enhanced toxicity.

1. **Setting times.** The settling time is the time for particles to deposit from a fluid (in this case air) to a surface. For a first order approximation, Stokes Law is used to calculate the settling velocities. Larger particles will settle much faster than small particles. Under moderate conditions, the settling time for these agglomerates could be hours/minutes compared to days/weeks for the LDB (Appendix 3). This reduction in settling time has the effect of localizing the LDB containing agglomerates near the sources of DPM (the Project) instead of having it disperse and dissipate.
2. **Increase lung retention.** Related to settling time, the localization of the agglomerates to areas near the Project increases the exposure for people compared to the normally widely dispersed nanosize LDB particles. Nanoparticle inhalation is not well understood in terms of retention in human lungs (very fine particles are sometimes expelled by respiration for example). But, larger agglomerates such as being mentioned here (micron-size), are retained at high probabilities []
3. **Enhanced toxicity.** The presence of the LDB on the DPM greatly increases the toxicity of the agglomerate. The LDB particles, bound by van der Waals, capillary, electrostatic forces to the DPM can be mechanically removed from the DPM in the lung (they are not chemically bound), thus delivering nanosize lead particles deep in the lungs with a 100% chance of

being absorbed. The DPM particle will also be a source of irritation and partially combusted hydrocarbons (carcinogenic).

Figure 2. shows the areas that will be most affected by issues mentioned in this memo. (circled in yellow). This region contains some of Oceanside's most valuable current and future recreation opportunities as well as numerous homes/residents. DPM Source is the proposed Project which includes the warehouse, parking lots, entrances, and transits to and from Hwy 76.



Figure 2. Map of local region.

Appendix 1: Lead dibromide nanoparticles- size and morphology

Lead Pollution from Aircraft Engines. The fuel used is leaded Avgas with tetraethyl lead and a halogen scavenger such as 1,2-Dibromoethane. The lead particles produced from Aviation fuel are very different from those found in lead-

fueled automobile exhaust. (much smaller particle size) In the aviation case the lead phase mostly occurs as 4 nanometer (0.00000015 inches in diameter) beads of lead dibromide nanocrystals. [3] Lead dibromide (LDB) is an ionic compound with a formula $PbBr_2$ which has a very low solubility in water. These LDB nanocrystals are aggregated in a matrix of partially combusted hydrocarbons. The resultant aggregates range from 6 to 16 nanometers. These ultrafine particles are subject to Brownian Motion and can have settling times ranging from days to weeks (depending on ambient conditions- ozone, hydroxyl concentrations) They are readily dispersed by wind and can travel great distances, reducing their impact on the local environment. High humidity and rain can clear the air of LDB by forming water/LDB agglomerates and thus decreasing the settling time.

Overall the particle size of emissions from gasoline piston-engine, aircraft and automobiles is much smaller than that found in diesel-fuel trucks. The diesel fuel is intrinsically much higher in aromatics and high molecular weight hydrocarbons. Diesel (16- 18 carbon atoms) versus gasoline (4-12 carbon atoms).

Example Santa Monica Airport:

LDB Particle emissions from aircraft: The table below shows the measured LDB emissions (percent of total emission) for the Santa Monica Airport [4]. The four major emission events are listed. With the exception of Approach, all of these emission events would be close to the Project location in the case of the Oceanside Airport.

Location	Percent LDB emission	Location relative to Project
Climb-out	29%	300-1300 feet
Approach	27%	> 3000 feet
Taxi	15%	400 feet
Run-up	13%	120 feet

Appendix: 2 DPM and Lead– From the Project EIR

Diesel Particulate Matter (DPM). DPM is part of a complex mixture that makes up diesel exhaust. Diesel exhaust is composed of two phases, gas and particle, both of which contribute to health risks. More than 90% of DPM is less than 1 micrometer in diameter (about 1/70th the diameter of a human hair), and thus is a subset of PM_{2.5}. DPM is typically composed of carbon particles (“soot,” also called black carbon) and numerous organic compounds, including over 40 known cancer-causing organic substances. Examples of these chemicals include polycyclic aromatic hydrocarbons, benzene, formaldehyde, acetaldehyde, acrolein, and 1,3-butadiene.

Note added: the PM_{2.5} DPM particles typically have a carbon core with aggregated carbon nanoparticles, metal oxides, sulfates and silicates.

Lead. Lead is a potent neurotoxin that accumulates in soft tissues and bone over time. The major sources of lead emissions have historically been motor vehicles (such as cars and trucks) and industrial sources. Because lead is only slowly excreted, exposures to small amounts of lead from a variety of sources can accumulate to harmful levels. Effects from inhalation of lead near the level of the ambient air quality standard include impaired blood formation and nerve conduction. Lead can adversely affect the nervous, reproductive, digestive, immune, and blood-forming systems. Symptoms can include fatigue, anxiety, short-term memory loss, depression, weakness in the extremities, and learning disabilities in children.

Appendix 3: Possible mechanisms of particle agglomeration

Agglomeration is the process of loosely gathering particles into a cluster. In the present case, the agglomeration mechanism analysis starts with the DPM. The DPM has a complex surface chemistry [] and a number of agglomeration mechanisms are possible for the LDB particles to attach to them including van der Waals, electrostatics, and capillary effects. This is the classic case of very fine particles, propelled by Brownian motion moving about larger stable particles. This

process is enhanced by the nanoparticle's high surface to volume ratio. The presence of oxygen-containing functional groups on the DPM surface, which can be formed during combustion and also through atmospheric aging processes, significantly influences the amount of chemisorbed water. Chemisorbed water can significantly affect the behavior of DPM particles, including their ability to act as condensation nuclei.

The agglomeration will also depend on humidity, the time particles are in proximity, and temperature.

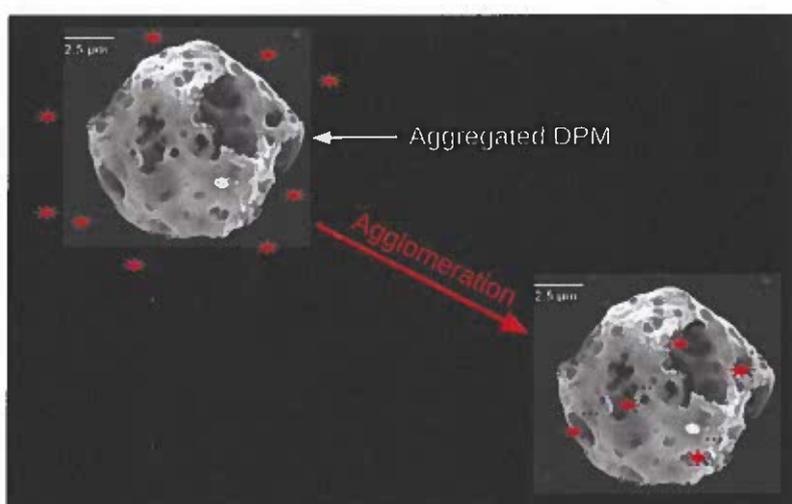
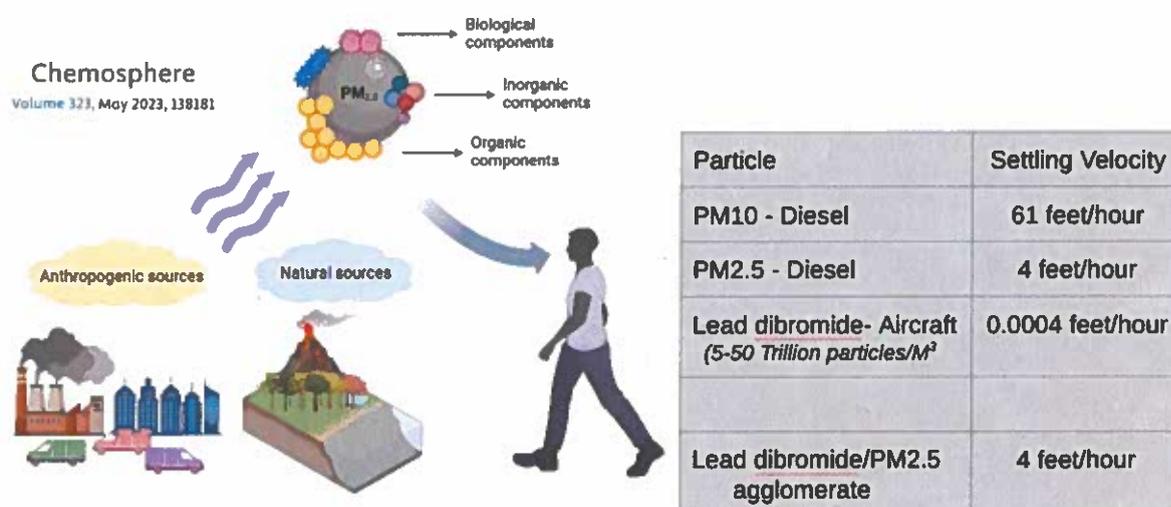


Figure 2 above shows the before and after states of the agglomeration process. The large particle shown is a DPM and the small red particles are meant to represent LDB nanoparticles. The LDB particles are not to scale and would be much smaller than shown in the figure. The product particles (LDB + DPM) have reduced settling times than the starting LDB due to the large size of the agglomerate.

Developing predictions for the agglomeration process is a difficult problem when the particles are in the nanoscale range (5–20 nm) due to the physics of the system being at the intersections between quantum mechanics and discrete particle mechanics. The chemisorbed water layer present on particles is known to have a very low surface energy and can dominate the agglomeration process.

Ref [6] describes this type of process for DPM/soot particles and various other environmental species. The figure below is from this reference showing a particle with PM_{2.5} and the various types of agglomerated organic and inorganic species. The present concern is for the case of DPM from the Warehouse and nearby LDB for the Airport as the inorganic component. The results of the calculator using Stokes law on Settling velocity is also shown.



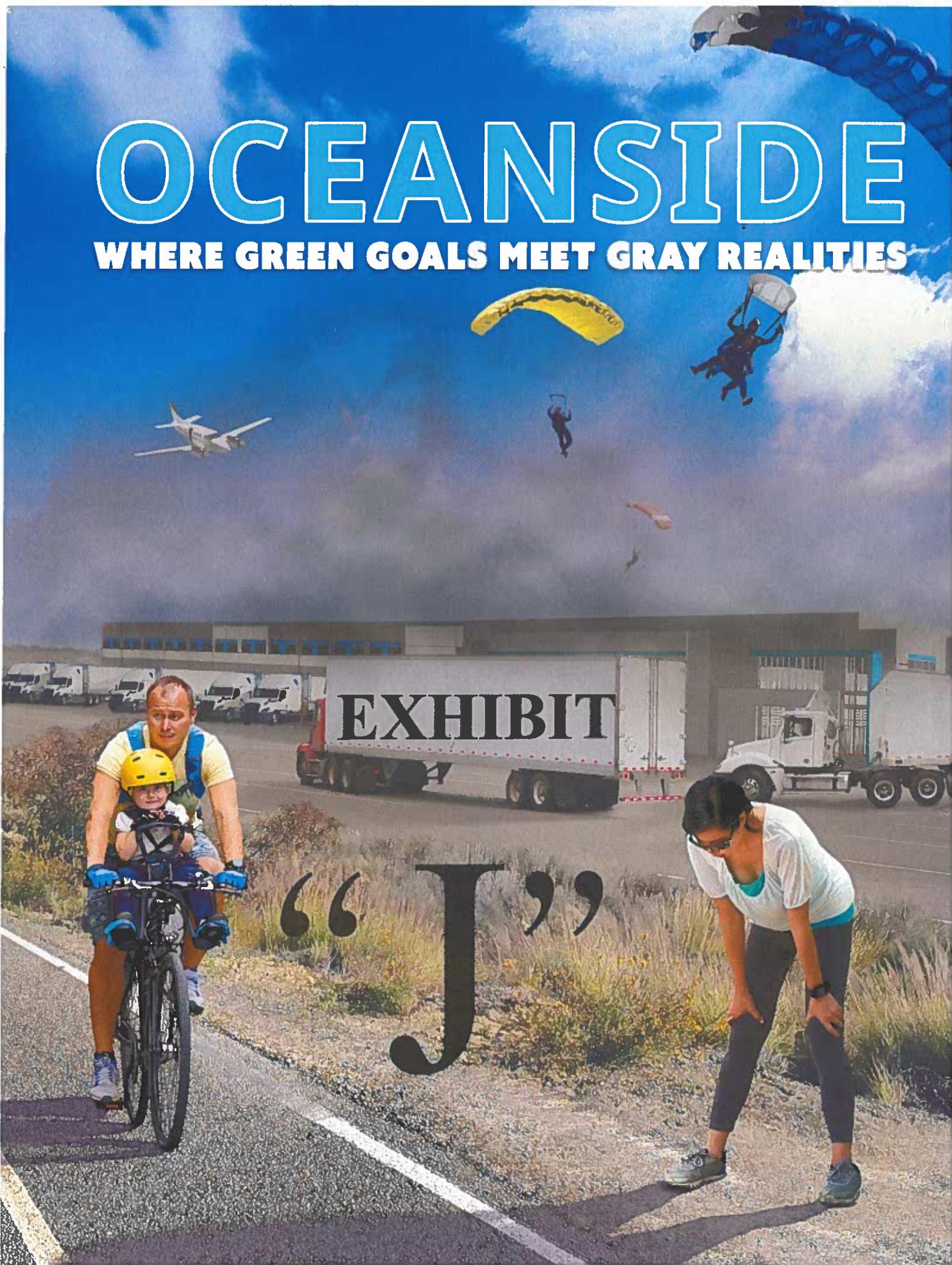
Note: No level of Lead exposure is considered safe. (US EPA)

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2. B.G. Kim et al Allergy Asthma Immun Res 8 2015
3. J. D. Griffith Atmospheric Pollution Research 11 (2020)
4. EPA Document # 2023-23247
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EXHIBIT

“J”

January 10, 2024

Advocates for the Environment

A non-profit public-interest law firm
and environmental advocacy organization



Robert Dmohowski
Principal Planner
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054

Via U.S. Mail and email to rdmohowski@oceansideca.org

re: Comments on the Environmental Impact Report for the Eddie Jones Warehouse
Manufacturing and Distribution Facility Project, SCH No. 2022070365

Dear Mr. Dmohowski:

Advocates for the Environment submits the comments in this letter regarding the Environmental Impact Report (EIR) for the Eddie Jones Warehouse Manufacturing and Distribution Facility Project (Project). The Project Site is located at the intersection of Alex Road and Benet Road in the City of Oceanside (City), San Diego County. The Project proposes to construct a 566,905 square-foot warehouse facility on the 31.79-acre site. The warehouse would include 114 truck terminals.

Advocates for the Environment is a public interest law firm and advocacy organization with the mission to educate the public about the law as it pertains to the environment and provide legal services in support of environmental causes. We have reviewed the EIR and submit comments regarding the sufficiency of the EIR's Greenhouse-Gas (GHG) analysis under the California Environmental Quality Act (CEQA).

The City Should Require the Project to be Net-Zero

Given the current regulatory context and technological advancements, a net-zero significance threshold is feasible and extensively supportable. GHG emissions from buildings, including indirect emissions from offsite generation of electricity, direct emissions produced onsite, and from construction with cement and steel, amounted to 21% of global GHG emissions in 2019. (IPCC Sixth Assessment Report, Climate Change 2022, WGIII, Mitigation of Climate Change, p. 9-4.) This is a considerable portion of global GHG emissions. It is much more affordable to construct new building projects to be net-zero than to obtain the same level of GHG reductions by expensively retrofitting older buildings to comply with climate change regulations. Climate damages will keep increasing until we reach net zero GHG emissions, and there is a California state policy requiring the state to be net-zero by 2045. It therefore is economically unsound to construct new buildings that are not net-zero.

10211 Sunland Blvd., Shadow Hills, CA 91040 (818) 650-0030 X101 dw@aenv.org

Environmental groups have achieved tremendous outcomes by litigation under CEQA. Two of the largest mixed-use development projects in the history of California, Newhall Ranch (now FivePoint Valencia), and Centennial (part of Tejon Ranch) decided to move forward as net-zero communities after losing CEQA lawsuits to environmental groups. The ability for these large projects to become net-zero indicates that it is achievable, even for large-scale developments. The Applicant for this Project should do the same.

We urge the City to adopt net-zero as the GHG significance threshold for this project. This threshold is well-supported by plans for the reduction of GHG emissions in California, and particularly the CARB Climate Change Scoping Plans. The CARB 2017 Scoping Plan states that “achieving no net additional increase in GHG emissions, resulting in no contribution to GHG impacts, is an appropriate overall objective for new development.” (CARB 2017 Scoping Plan, p. 101.) Additionally, the CARB 2022 Scoping Plan reaffirms the necessity of a net zero target by expressing: “it is clear that California must transition away from fossil fuels to zero-emission technologies with all possible speed ... in order to meet our GHG and air quality targets.” (CARB 2022 Scoping Plan, p. 184.) CARB further encourages a net-zero threshold in its strategies for local actions in Appendix D to the 2022 Scoping Plan. (CARB 2022 Scoping Plan, Appendix D p. 24-26.)

Moving this Project forward as a net-zero project would not only be the right thing for the City to do, but also would also help protect the City and the Applicant from CEQA GHG litigation.

GHG Significance Analysis

The City estimated the Project’s emissions at approximately 7,172.55 metric tons carbon dioxide equivalent (MTCO_{2e}) per year. The City used the City of Oceanside’s Climate Action Plan (CAP) to streamline the CEQA analysis under CEQA Guidelines Section 15183.5. (EIR, p. 4.7-23.) Even though the Project exceeded the CAP bright-line threshold of 900 MTCO_{2e}, the City claimed that the Project would have a less-than-significant impact because it asserts that it was consistent with the CAP Consistency Checklist (CAP Checklist) (EIR, p. 4.7-24.) Yet, the City failed to account for several of the CAP Checklist items. Accordingly, the Project would not be consistent with the CAP Checklist and therefore is unable to rely upon the City of Oceanside CAP as a streamlining document to demonstrate a less-than-significant impact.

The EIR adopts the two CEQA Guidelines Appendix G significance criteria, “Would the project generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?” (EIR, p. 45.7-23) and “Would the project generate conflict with an applicable plan, policy, or regulation adopted for the purpose of reducing the emissions of greenhouse gases?” (EIR p. 4.7-27.) The EIR’s analysis of the Project’s consistency with both of these thresholds is deeply flawed.”

Consistency with the CAP Checklist

The City bases its evaluation under the first threshold on consistency with the City of Oceanside's Climate Action Plan (the CAP). The Project is inconsistent with the CAP. First, there is a "brightline CAP threshold" of 900 MTCO_{2e}/year, and the Project's estimated emissions of 7,173 MTCO_{2e} greatly exceeds this threshold. Second, the CAP's goal is to reduce emissions to 4 MTCO_{2e}/capita by 2030 and 2 MTCO_{2e}/capita by 2050. Given that the "project is expected to employ 590 workers," (EIR, p. 5.13-6), its annual GHG emissions will be $7,173 / 590 = 12.2$ MTCO_{2e}/capita. This greatly exceeds both CAP goals.

The City claimed that "the proposed project is consistent with the CAP Consistency Checklist adopted by the City to ensure that the GHG emission targets identified in the CAP are achieved." (EIR, p. 4.7-27.) However, in its incomplete list of checklist items in Table 4.7-7 of the EIR, the City ignored several relevant project-level items. Chapter 5 of the current City of Oceanside CAP outlines the "Project Review Checklist," which includes specific CAP measures in Table 11. The EIR did not demonstrate consistency with three out of seven CAP measures that are identified as Checklist items on Table 11.

First, the Checklist requires Smart Growth Policies which require project-level implementation including promoting new employment-generating land uses within Smart Growth Opportunity Areas (SGOAs). (CAP, p. 5-2). The checklist items listed in Table 4.7-7 of the EIR did not include this aspect, even though this Project would involve non-negligible vehicle trip generation.

Second, Table 11 includes a measure for Expanded Electric Vehicle Charging Infrastructure, in which any project with parking spaces must allow for installation of electric vehicle charging stations on a portion of its parking spaces. (CAP, p. 5-2). Here, the Project would include 590 parking spots (EIR, p. ES-2), so it would be required to adhere to this measure by prewiring a portion of those parking spots to be ready for electric vehicle charging stations in order to be consistent with this measure of the CAP Checklist. However, the Project specifications do not include any such prewiring and this CAP measure was not mentioned anywhere in the GHG analysis.

Third, to be consistent with the CAP Checklist as the City claims that the Project would be, the Project must assign preferential parking spaces to zero emission vehicles to 12% of its parking spaces through the adoption of a Clean Air Vehicle Parking Ordinance; and all industrial uses with parking spaces must adhere to this measure to be consistent with the CAP. (CAP, p. 5-2.) This measure is applicable here because the Project is anticipated to be industrial and involve parking (EIR, p. ES-2.)

Overall, the failure to include, analyze, or account for these three required CAP Checklist measures invalidates the City's claim that the Project would be consistent with the CAP Checklist. The checklist items listed in Table 4.7-7 are an incomplete account of the requirements under the Oceanside CAP to be consistent with the Checklist in its entirety (i.e., including relevant checklist

items listed in Table 11). Therefore, the City cannot rely on the CAP Checklist to demonstrate that the Project would have a less-than-significant impact on GHGs.

Inconsistency with Applicable Plans

The City did not appropriately apply the second of the two thresholds: whether the Project would “[c]onflict with an applicable plan, policy or regulation adopted for the purpose of reducing the emissions of greenhouse gases.” (EIR, p. 4.7-22.) This language requires that the EIR analyze the Project’s consistency with *all* other applicable plans, not just the plans that the City prefers to analyze. However, the City solely analyzed consistency with the CAP, ignoring all other applicable plans, policies, and regulations for the reduction of GHGs, including, but not limited to, the 2022 Scoping Plan for Achieving Carbon Neutrality from the California Air Resources Board (2022 Scoping Plan), Senate Bill 32, which requires statewide GHG emissions to be reduced to 40% below 1990 levels by 2030, and Executive Order B-55-18 to achieve carbon neutrality by 2045—within the Project’s projected lifespan. The Project is also inconsistent with CARB 2017 Scoping Plan, which sets as a goal the reduction of GHG emissions to 6 MTCO_{2e}/capita by 2030 and 2 MTCO_{2e} by 2050.

Not only did the EIR not include any analysis for how the Project would be consistent with these measures, but the use of diesel and other GHG-emitting fossil fuels would prevent the achievement of at least some of these applicable plans and policies. Therefore, the Project would not only have a significant impact under the CAP, but also the framework that the City adopted when it specified a review focused on conflicts with applicable plans, policies, and regulations.

The EIR’s Reported Overall Emissions Differs from the CalEEMod Overall Outputs

CalEEMod was used as a model to estimate anticipated Project emissions. The CalEEMod analysis was included Appendix B to the EIR and was inconsistent with the EIR itself, resulting in inaccurate and misleading information for the public and decision-makers. These inconsistencies could have resulted in erroneous or underreported GHG emissions quantifications which undermine the EIR’s usefulness as an informational document.

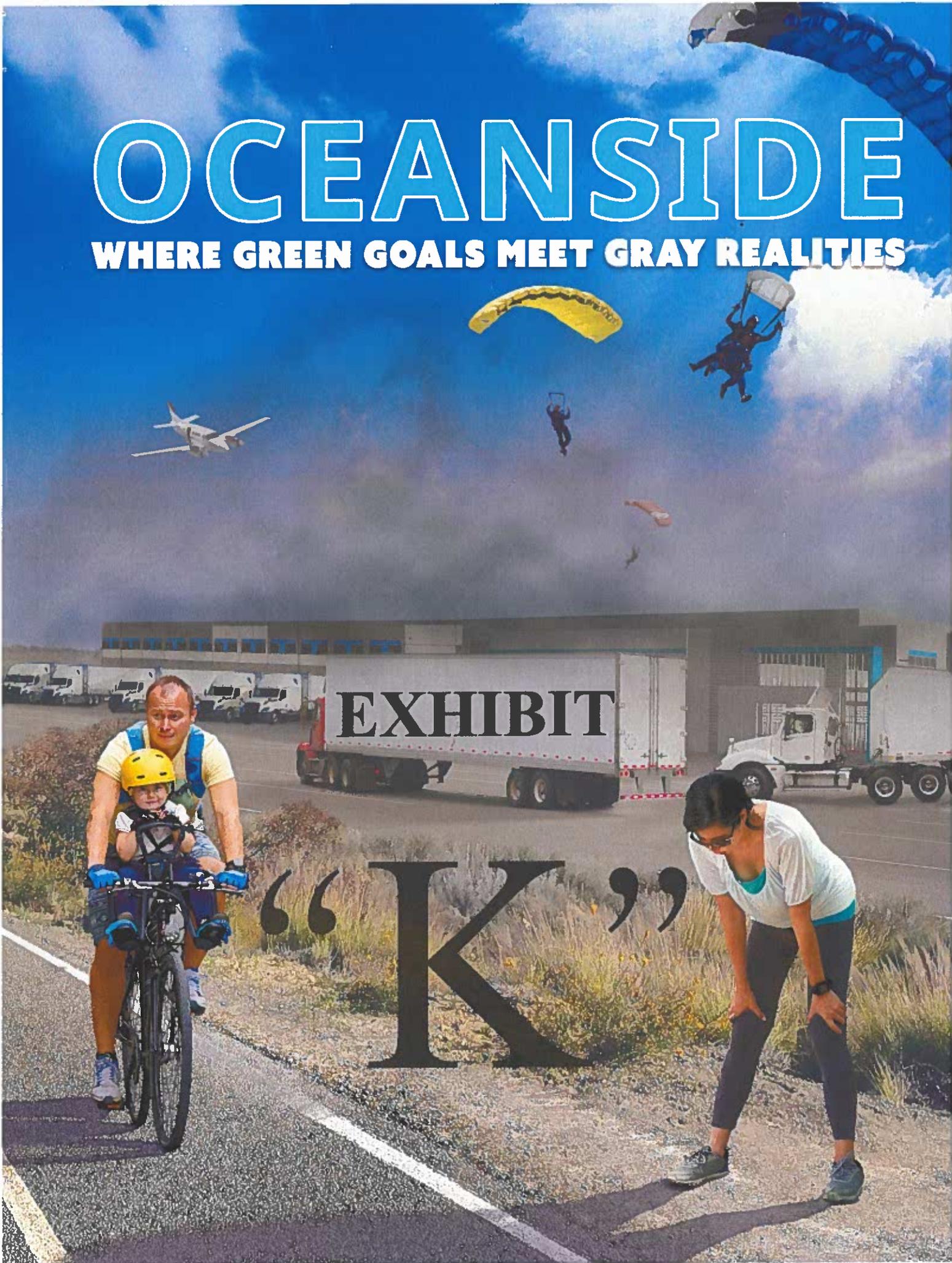
When a lead agency makes an estimate for project emissions, it should carefully and completely demonstrate that the emissions outputs from the model are consistent with the analysis in the EIR, to be accurate and not misleading about the GHG impact of the Project. Here, however, the CalEEMod overall outputs do not fully align with the information in the EIR. CalEEMod was run in three different runs: (1) Annual, (Operational emissions of approximately 8,142); (2) Summer (Operational emissions of approximately 31,178 MTCO_{2e}); and (3) Winter (Operational emissions of approximately 30,442 MTCO_{2e}). (Appendix B.)

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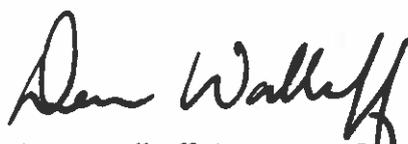
Notably, the operational emissions estimate included in the EIR is a much lower number of 7141.86 (subtracting amortized construction emissions), which the City claimed was from CalEEMod calculations, but does not align with the CalEEMod outputs included in Appendix B. It is not clear how this quantification was reached. The "Winter" and "Summer" CalEEMod runs were not included in the totals or mentioned in the EIR at all. There is also no indication of why the CalEEMod outputs for Winter and Summer are each more than four times the amount of the estimate reported in the EIR. Nor did the City discuss why it omitted these outputs from the EIR, which are much higher than the estimate that the City used for its significance analysis. This is confusing, misleading, and not supported by substantial evidence.

Conclusion

For the reasons given in this letter, the EIR is not in conformance with CEQA requirements. Notably, the significance analysis was incorrect, and the City should have concluded that the Project would contribute to a significant GHG impact because it is over the CAP screening threshold and inconsistent with the CAP. Besides the emissions quantification and streamlined checklist review, which was insufficient to demonstrate a consistency with the CAP, the City did not provide any evidence to support a finding of less-than-significant impact, and therefore the determinations in the EIR are not supported by substantial evidence.

Please put Advocates for the Environment on the interest list to receive updates about the progress of this Project.

Sincerely,



Dean Wallraff, Attorney at Law
Executive Director, Advocates for the Environment

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December 29, 2023

VIA EMAIL

rdmohowski@oceasideca.org

Rob Dmohowski
Principal Planner
City of Oceanside's Planning Division
300 North Coast Highway
Oceanside, CA 92054

**Re: Draft Environmental Impact Report for Eddie Jones Warehouse,
Manufacturing and Distribution Facility Project (SCH No. 2022070365)**

Dear Mr. Dmohowski:

The following comments on the Draft Environmental Impact Report ("DEIR") for the Eddie Jones Warehouse, Manufacturing and Distribution Facility Project (SCH No. 2022070365) are submitted on behalf of the Eddie Jones Go Fund Me Trust. As an initial matter, the Eddie Jones Go Fund Me Trust objects to the DEIR and the Project as the DEIR fails to meet the requirements of the California Environmental Quality Act ("CEQA") Public Resources Code, section 21000 *et seq.* Thus, the DEIR is legally inadequate.

A. THE CALIFORNIA ENVIRONMENTAL QUALITY ACT

"CEQA is a comprehensive scheme designed to provide long-term protection to the environment. [Pub. Resources Code, §21001.] In enacting CEQA, the Legislature declared its intention that all public agencies responsible for regulating activities affecting the environment give prime consideration to preventing environmental damage when carrying out their duties. [Pub. Resources Code, § 21000(g).] CEQA is to be interpreted 'to afford the fullest possible protection to the environment within the reasonable scope of the statutory language.' [*Friends of Mammoth v. Board of Supervisors* (1972) 8 Cal.3d 247, 259]". (*Mountain Lion Foundation v. Fish & Game Commission* (1997) 16 Cal.4th 105, 112; *see also Sierra Club v. County of Fresno* (2018) 6 Cal.5th 502, 511.) The DEIR, "with all its specificity and complexity, is the mechanism prescribed by CEQA to force informed decision making and to expose the decision-making process to public scrutiny. (*Planning and Conservation League v. Department of Water Resources* (2000) 83 Cal.App.4th 892, 910; citing *No Oil, Inc. v. City of Los Angeles* (1974) 13 Cal.3d 68, 86.) This interpretation remains the benchmark for judicial interpretation of CEQA. (*Laurel Heights Improvement Association v. Regents of the University of California* ("Laurel Heights I") (1988) 47 Cal.3d 376, 390, quoting *Bozung v. Local Agency Formation Commission* (1975) 13 Cal.3d 263, 274.) As the *Laurel Heights I* court noted, "[i]t is, of course, too late to argue for a grudging, miserly reading of CEQA." (*Laurel Heights I, supra*, 47 Cal.3d at 390.) The "foremost principle under CEQA is that the Legislature intended the act 'to be interpreted in

such manner as to afford the fullest possible protection to the environment within the reasonable scope of the statutory language.” (*Sierra Club, supra*, 6 Cal.5th at 511, quoting *Friends of Mammoth, supra*, 8 Cal.3d at 259.)

The EIR is “the heart of CEQA” and “an environmental alarm bell whose purpose is to alert the public and its responsible officials to environmental changes before they have reached the ecological point of no return.” (*Id.* at 392.) The EIR is the “primary means” of ensuring that public agencies “take all action necessary to protect, rehabilitate, and enhance the environmental quality of the state.” (*Id.*, quoting Pub. Resources Code, § 21001(a).) The EIR is also a “document of accountability,” intended “to demonstrate to an apprehensive citizenry that the agency has, in fact, analyzed and considered the ecological implications of its actions.” (*Laurel Heights I, supra*, 47 Cal.3d at 392 (quoting *No Oil, Inc., supra*, 13 Cal.3d at 86.) Thus, “[t]he EIR process protects not only the environment but also informed self-government.” (*Ibid.*)

The central purpose of an EIR is to identify the significant environmental effects of the proposed project, and to identify ways of avoiding or minimizing those effects through the imposition of feasible mitigation measures or the selection of feasible alternatives. (Pub. Resources Code, §§ 21002, 21002.1(a), 21061.) “The basic purpose of an EIR is to provide public agencies and the public in general with detailed information about the effect [that] a proposed project is likely to have on the environment; to list ways in which the significant effects of such a project might be minimized; and to indicate alternatives to such a project.” (Pub. Resources Code, § 21061; *see* Title 14 Cal. Code Regulations (“CEQA Guidelines”) § 15003(b)-(e); *Sierra Club v. County of Fresno, supra*, 6 Cal.5th at 511; *Mountain Lion Foundation v. Fish & Game Commission, supra*, 16 Cal.4th at 113.) Thus, an EIR must provide information to government decision-makers and the public about the potential significant environmental effects of proposed projects (CEQA Guidelines, § 15002(a)(1);) and disclose to the public the reasons for approval of a project that may have significant environmental effects. (*Id.*, § 15002(a)(4).) This informed decision making and public participation constitutes the fundamental cornerstones of the CEQA process. (*See Citizens of Goleta Valley v. Board of Supervisors* (1990) 52 Cal.3d 553, 564; *Laurel Heights I, supra*, 47 Cal.3d 376.)

A legally adequate EIR must “facilitat[e] ‘informed decision making and informed public participation.’” (*Sierra Club, supra*, 6 Cal.5th at 513, quoting *California Native Plant Society v. City of Santa Cruz* (2009) 177 Cal.App.4th 957, 988.) “To facilitate CEQA’s informational role, the EIR must contain facts and analysis, not just the agency’s bare conclusions or opinions.” (*Laurel Heights I, supra*, 47 Cal.3d at 404-405, quotation omitted.) “And . . . a sufficient discussion of significant impacts requires not merely a determination of whether an impact is significant, but some effort to explain the nature and magnitude of the impact.” (*Sierra Club, supra*, 6 Cal.5th at 519, citing *Cleveland Nat’l Forest Found. v. San Diego Assn. of Govts.* (2017) 3 Cal.5th 497, 514-515.) An EIR lacking such information does not “includ[e] enough detail ‘to enable those who did not participate in its preparation to understand and to consider meaningfully the issues raised by the proposed project.’” (*Sierra Club, supra*, 6 Cal.5th at 516, quoting *Laurel Heights I, supra*, 47 Cal.3d at 405.)

An EIR also “must” include “a disclosure of the ‘analytic route the agency traveled from evidence to action.’” (*Laurel Heights I, supra*, 47 Cal.3d at 404, quoting *Topanga Assn. for a Scenic Community v. County of Los Angeles* (1974) 11 Cal.3d 506, 515.) If this “‘analytic route’ has not been revealed,” it is impossible for “others, be they courts or constituents, [to] intelligently analyze the logic of the [agency’s] decision.” (*Citizens for Quality Growth v. City of Mt. Shasta* (1988) 198 Cal.App.3d 433, 441.) “The data in an EIR must not only be sufficient in quantity, it must be presented in a manner calculated to adequately inform the public and decision makers, who may not be previously familiar with the details of the project. ‘Information “scattered here and there in EIR appendices” or a report “buried in an appendix,” is not a substitute for ‘a good faith reasoned analysis.’” (*Vineyard Area Citizens for Responsible Growth v. City of Rancho Cordova* (2007) 40 Cal.4th 412, 442 (“*Vineyard*”), quoting *California Oak Foundation v. City of Santa Clarita* (2005) 133 Cal.App.4th 1219, 1239.) Finally, the “audience to whom an EIR must communicate is not the reviewing court but the public and the government officials deciding on the project. That a party’s briefs to the court may explain or supplement matters that are obscure or incomplete in the EIR, for example, is irrelevant, because the public and decision makers did not have the briefs available at the time the project was reviewed and approved. The question is therefore not whether the project’s significant environmental effects can be clearly explained, but whether they were.” (*Vineyard, supra*, 40 Cal.4th at 443.) “‘Whatever is required to be considered in an EIR must be in that formal report; what any official might have known from other writings or oral presentations cannot supply what is lacking in that report.’” (*Laurel Heights I, supra*, 47 Cal.3d at 405.) Thus, the “preparation and circulation of an EIR is more than a set of technical hurdles for agencies and developers to overcome. The EIR’s function is to ensure that government officials who decide to build or approve a project do so with a full understanding of the environmental consequences and, equally important, that the public is assured those consequences have been taken into account. [Citation.] For the EIR to serve these goals it must present information in such a manner that the foreseeable impacts of pursuing the project can actually be understood and weighed, and the public must be given an adequate opportunity to comment on that presentation before the decision to go forward is made.” (*Vineyard, supra*, 40 Cal.4th at 449-450, citing *Laurel Heights, supra*, 47 Cal.3d at 391-392.)

CEQA requires a lead agency to adopt feasible alternatives or feasible mitigation measures that can substantially lessen a project’s significant environmental impacts. (Pub. Resources Code, § 21002; CEQA Guidelines, § 15002(a)(3); *Sierra Club v. Gilroy City Council* (1990) 222 Cal.App.3d 30, 41.) For that reason, “[t]he core of an EIR is the mitigation and alternatives sections.” (*Citizens of Goleta Valley v. Board of Supervisors, supra*, 52 Cal.3d at 564.) CEQA requires the preparation of an EIR in order to identify the significant effects on the environment of a project, so that measures to mitigate or avoid those effects, or alternatives that avoid those effects, can be devised. (Pub. Resources Code, §§ 21002.1(a), 21060.) CEQA Guidelines section 15126.4 requires that the Final EIR describe all feasible measures that can minimize significant adverse impacts of the project. CEQA does not allow an agency to defer analysis of impacts and mitigation measures. (CEQA Guidelines § 15126.4(a)(1)(B).)

Compliance with the procedural requirements of CEQA sets the stage for development of

mitigation measures and alternatives. Without a proper procedural foundation, a local agency cannot comply with CEQA's mandate that public agencies should not approve projects as proposed if there are feasible alternatives or feasible mitigation measures available which would substantially lessen the significant environmental effects of such projects. (Pub. Resources Code § 21002.)

B. THE DRAFT ENVIRONMENTAL IMPACT REPORT FAILS TO COMPLY WITH CEQA

1. Section 2: Environmental Setting

CEQA requires that “an EIR must include a description of the physical environmental conditions in the vicinity of the project, as they exist at the time the notice of preparation is published, or if no notice of preparation is published, at the time environmental analysis is commenced, from both a local and regional perspective. This environmental setting will normally constitute the baseline physical conditions by which a lead agency determines whether an impact is significant. (CEQA Guidelines, § 15125, subd. (a).)” (*Neighbors for Smart Rail v. Exposition Metro Line Construction Authority* (2013) 57 Cal.4th 439, 469.) “The selected baseline must result in a reliable evaluation of a project’s impacts.” (*Id.* at 470.)

In the present matter, the DEIR failed to include the nearby wildlife preserves such as Wanis View Wildlife Preserve and the wildlife corridors in the environmental setting. Having failed to disclose or discuss the wildlife preserves and/or wildlife corridor the DEIR fails to inform the decisionmakers and the public whether the Project may have potentially significant impacts to biological resources. (*See* discussion below, Section 4.3 Biological Resources.)

2. Section 4.2: Air Quality

An EIR is legally when adequate when it fails to include a discussion of project related air pollution impacts and fails to describe the nature and magnitude of the significant impacts on public health that would result from the project. (*Sierra Club v. County of Fresno, supra*, 6 Cal.5th at 520.) An EIR must provide information sufficient to allow interested parties “to understand and to consider meaningfully the issues the proposed project raises.” (*Id.* at 510.)

In the present matter, the DEIR failed to adequately analyze and disclose discussion and analysis of the Project’s impacts on air quality. As set forth in the Technical Memorandum from Ray Kapahi, Environmental Permitting Specialists (“EPS”) the DEIR relies upon an old version of the CalEEMod Emissions model.¹ As set forth in the in the EPS Technical Memorandum, the current version of CalEEMod addresses the Project’s risk to the climate. Additionally, the EPS Technical Memorandum points out that the DEIR’s use of only the daily thresholds of significance to determine significance does not fully characterize the air quality impacts to the nearby homes. Moreover, as discussed in the EPS Technical Memorandum, the DEIR fails to provide the required analysis to regarding air quality impacts to nearby sensitive receptors.

¹ A copy of the EPS Technical Memorandum is attached to these comments as Exhibit A.

Instead, the DEIR simply concludes that the impacts would be less than significant. When a DEIR's conclusion lacks analysis or omits the magnitude of the impacts it violates CEQA's informational requirements. (*Id.* at 514.).

The DEIR's also fails to provide an analysis of cumulative impacts associated with air quality. As indicated in the EPS Technical Memorandum, the DEIR concludes that since project level impacts are less than significant, then cumulative impacts would be less than significant. This is not the analysis that CEQA requires. Cumulative impacts are "two or more individual effects which, when considered together, are considerable or which, when considered together, are considerable or which compound or increase other environmental impacts. (CEQA Guidelines, § 15355.) A cumulative impact is an impact created by the combination of the project reviewed in the EIR together with other projects causing related impacts. (CEQA Guidelines, 15130(a)(1).) In determining whether the DEIR must analyze a cumulative impact, the agency must make two determinations: 1) is the combined impact of the project and other projects significant? And, is the project's incremental effect cumulatively considerable? (*See* CEQA Guidelines, § 15130(a).) Thus, when a project-specific impact may be insignificant, there can still be a related cumulative impact that is significant. (*Id.*; *see also Environmental Protection Information Center v. Development of Forestry & Fire Protection* (2008) 44 Cal.4th 459, 524.)

As discussed by EPS, the cumulative impact analysis fails to address the proposed Ocean Kamp Project as well as the cumulative health risks. Despite the DEIR's failure to address the cumulative impacts associated with Ocean Kamp, there are significant cumulative air quality impacts for multiple air pollutants. (*See* EPS Technical Memorandum at 3-4.) This failure to address the Project's cumulative air quality impacts violates CEQA's requirements. (*See* CEQA Guidelines, § 15130(a).)

The DEIR's approach also contradicts the California Attorney General's guidance for evaluating warehouse projects. (*See Warehouse Projects: Best Practices and Mitigation Measures to Comply with the California Environmental Quality Act*, State of California Department of Justice, ("Warehouse Projects") at 7.)² With respect to evaluating air quality and greenhouse gas emissions, the Attorney General states:

When analyzing cumulative impacts, thoroughly considering the project's incremental impact in combination with past, present, and reasonably foreseeable future projects, even if the project's individual impacts alone do not exceed the applicable significance thresholds.

Thus, the DEIR clearly fails to provide an adequate cumulative impacts analysis with respect to air quality.

² A copy of the *Warehouse Projects: Best Practices and Mitigation Measures to Comply with the California Environmental Quality Act*, State of California Department of Justice, is attached as Exhibit B.

The EPS Technical Report found that emissions of NO_x, PM₁₀ and PM_{2.5} would violate the ambient air quality standards by a substantial margin. (EPS Technical Report at 4.) As demonstrated in report, the spatial distribution of pollutant concentration demonstrates that the project exceeds the applicable air quality standards. (*Id.*) This also results in higher cumulative impacts as the DEIR fails to include emissions from the Oceanside Municipal Airport.

The EPS Technical Report also points out that the DEIR's conclusion that air quality impacts are less than significant are misleading lack substantial evidence, as future emissions from manufacturing and truck emissions are largely unknown. Thus, the conclusion is speculative and not supported by substantial evidence. (*See King v. Gardiner Farms v. County of Kern* (2020) 45 Cal.App.5th 814, 666.)

The DEIR also fails to adequately address the air pollution based upon the entire expected length of truck trips truck trips. The DEIR states the "[T]he light-duty, medium-heavy-duty and heavy-duty truck trip lengths were based upon the 40 miles and assumed to be 100% of primary trips. (DEIR at 4.2-21.) The DEIR references assumptions made by the San Diego County Air Quality Management Districts' 2016 AQMP. (*Id.*) Nothing in the record, however, indicates that these assumptions are consistent with the operations of this warehouse project. As discussed by the Attorney General:

Disclosing air pollution from the entire expected length of truck trips. CEQA requires full public disclosure of a project's anticipated truck trips, which entails calculating truck trip length based on likely truck trip destinations, rather than the distance from the facility to the edge of the air basin, local jurisdiction, or other truncated endpoint. All air pollution associated with the project must be considered, regardless of where those impacts occur. (*Warehouse Projects* at 7.)

Thus, the DEIR needs to evaluate this warehouse project and calculate the expected length of truck trips in to adequately disclose and analyze the Project's air quality impacts.

3. Section 4.3: Biological Resources

"A prejudicial abuse of discretion occurs if the failure to include relevant information precludes informed decisionmaking and informed public participation, thereby thwarting the statutory goals of the EIR process." [Citations.] (*Banning Ranch Conservancy v. City of Newport Beach* (2017) 2 Cal.5th 918, 942.) In this matter, the DEIR precludes informed decisionmaking and informed public participation by failing to include relevant information regarding biological resources. More specifically the nearby wildlife preserves and wildlife corridors.

The comments submitted by the Wanis View Wildlife Preserve demonstrate that the DEIR's failure to address the Project's potentially significant impact to wildlife, including protected bird nesting, wildlife communication, and wildlife movement. (*See Comments prepared the Wanis View Wildlife Preserve Volunteers ("Wanis View").*) As demonstrated in the

comment letter, the Project has the potential for significantly impacting a wildlife corridor that includes the Wanis View Wildlife Preserve, San Luis Rey River and SeaCliff Preserve. Also, as discussed in the Wanis View comments there are a number of special interest species within the Wanis View Preserve that were never mentioned in the DEIR. The DEIR's failure to disclose the required information is prejudicial regardless of whether a different outcome would have resulted if the public agency had complied with the law. (Pub. Resources Code, § 21005(a); *Neighbors for Smart Rail, supra*, 57 Cal.4th at 463; *Banning Ranch Conservancy, supra*, 2 Cal.5th at 942.)

4. Section 4.5: Energy

The DEIR fails to comply with CEQA's requirement for analysis and disclosure regarding the Project's energy consumption. The DEIR concludes that "[t]he project would not result in a potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources during project construction; impacts related to temporary energy consumption during construction of the project would be less than significant." (DEIR, 4.5-15.) The DEIR further concludes that "energy consumption associated with the operation of the project would not result in a potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources; impacts would be less than significant." (DEIR, 4.5-17.) Both of these conclusions are not supported by substantial evidence and the DEIR fails to contain a discussion on whether the Project could increase its reliance on renewable energy sources. (See *California Clean Energy Committee v. City of Woodland* (2014) 225 Cal.App.4th 173, 213; see also *League to Save Lake Tahoe et al. v. County of Placer* (2022) 75 Cal.App.5th 63, 164-168.)

CEQA requires an EIR to evaluate whether the project would result in wasteful, inefficient or unnecessary consumption of energy resources. An agency's failure to undertake "an investigation into renewable energy options that might be available or appropriate for a project" violates CEQA. (*California Clean Energy Committee v. City of Woodland, supra*, 225 Cal.App.4th at 213.) CEQA defines "energy conservation" as the "wise and efficient use of energy." (CEQA Guidelines, App. F, § I.) The "wise and efficient use of energy" is achieved by "(1) decreasing overall per capita energy consumption, (2) decreasing reliance on fossil fuels such as coal, natural gas and oil, and (3) increasing reliance on renewable energy resources." (*Id.*)

"CEQA requires an EIR to analyze a project's energy consumption. (§ 21100, subd. (b)(3); Guidelines, § 15126.4, subd. (a)(1), Appendix F.) If analysis of the project's energy use reveals that the project may result in significant environmental effects due to wasteful, inefficient, or unnecessary use of energy, or wasteful use of energy resources, the EIR shall mitigate that energy use. (Guidelines, § 15126.2, subd. (b).)" (*League to Save Lake Tahoe, supra*, 75 Cal.App.5th at 166.)

Noting that compliance with the California Building Energy Efficiency Standards (Cal.Code Regs., tit. 24, part 6 (Title 24)) does not constitute an adequate analysis of energy.

(*Ukiah Citizens for Safety First v. City of Ukiah* (2016) 248 Cal.App.4th 256, 264-65 (*Ukiah Citizens*)).) Similarly, the court in *California Clean Energy Committee* held unlawful an energy analysis that relied on compliance with Title 24, that failed to assess transportation energy impacts, and that failed to address renewable energy impacts. (*California Clean Energy Committee v. City of Woodland, supra*, 225 Cal.App.4th at 209-13.) As such, the EIR's reliance on Title 24 compliance does not satisfy the requirements for an adequate discussion of the Project's energy impacts.

“Guidelines section 15126.2, subdivision (b), and Appendix F to the Guidelines thus indicate an EIR should address the project's potential to increase its use of renewable energy sources for at least two purposes. First, when the EIR analyzes the project's energy use to determine if it creates significant effects, it should discuss whether any renewable energy features could be incorporated into the project. (Guidelines, § 15126.2, subdivision (b).) The EIR's determination of whether the potential impact is significant is to be based on this discussion. Second, if the EIR concludes the project's impact on energy resources is significant, it should consider mitigating the impact by requiring uses of alternate fuels, particularly renewable ones, if applicable. (Guidelines, Appendix F., II. D. 4.)” (*League to Save Lake Tahoe et al. v. County of Placer, supra*, 75 Cal.App.5th at 167.)

With respect to construction, the DEIR discusses the fuel consumption from construction equipment. (DEIR 4.5-13 to 4.5-15.) The DEIR concludes that the project would not significantly affect the overall demand for petroleum considering the project's minimal contribution towards demand. (DEIR 4.5-15.) The DEIR further concludes that the energy demands of diesel and gasoline would be small relative to statewide and local demands for fuel. (*Id.*) The DEIR further states that the Project would be commensurate with typical construction projects. (*Id.*) Thus, business as usual. This is not what CEQA or CEQA requires. The EIR omits any discussion or analysis with respect to construction of whether the project could increase its reliance on renewable energy sources to meet its energy demand. (*See California Clean Energy Committee v. City of Woodland, supra*, 225 Cal.App.4th at 213; *see also League to Save Lake Tahoe et al. v. County of Placer, supra*, 75 Cal.App.5th at 164-168.)

As for energy consumption regarding operation of the Project, the DEIR provides a summary of the Project's energy requirements and that the Project would meet the Title 24 requirements/standards. (DEIR, 4.5-17-4.5-17.) The DEIR also concludes that although the Project would result in an increase in natural gas and electricity over the City's typical annual natural gas and electricity consumption, the result would be a nominal increase. (*Id.*, 4.5-19.) The then determines that the Project's “resultant increase in energy demand would not exceed the available capacity of SDG&E [San Diego Gas & Electric] servicing infrastructure to the site or beyond and would be consistent with local and regional plans for usage of the project site the energy consumption with that usage.” (*Id.*)

Again, this is not what CEQA requires. Simply stating that the Project will comply with Title 224 does not constitute an adequate analysis of energy. (*Ukiah Citizens for Safety First v. City of Ukiah* (2016) 248 Cal.App.4th 256, 264-65; *California Clean Energy Committee, supra*,

225 Cal.App.4th at 209-213 (inadequate energy analysis that relied on compliance with Title 24, but failed to assess transportation energy impacts, and that failed to address renewable energy impacts.) The DEIR contains no discussion or analysis of any renewable energy features that could be incorporated into the project. This failure violates CEQA.

Also, it should be noted that a significant portion of the Project is to serve as a distribution facility which includes significant a significant amount semi-trucks coming and going to the Project. The Project includes 60 tractor/truck/trailer parking stalls and sixty-seven (67) loading bays for tractor/trailers. Despite the fact that the Project operation will include a constant flow of semi-trucks coming and going on a daily basis consuming significant amounts of energy, nothing in the DEIR's section regarding energy consumption provides any discussion whether renewable energy features could be incorporated into the project to reduce energy consumption regarding the energy consumed by the semi-trucks. The failure to include renewable energy options that might be available or appropriate for the project violates CEQA. (*California Clean Energy Committee, supra*, 225 Cal.App.4th at 209.)

5. Section 4.14: Traffic & Circulation

The DEIR fails to adequately address the Project's potentially significant impacts to traffic safety. As discussed in the report prepared by Greenlight Traffic Engineering the DEIR fails to address the increased safety risk of mixing greater percentages of heavy truck traffic with passenger car traffic. (Greenlight Report at 3.)³ The Greenlight report concludes that although the growth in truck traffic resulting from the Project raises serious concern regarding road safety, the DEIR fails disclose and/or analyze this heightened safety risk resulting from the Project.

This potential impacts associated with truck traffic and warehouse projects is further illustrated by the Attorney General:

Warehouse facilities inevitably bring truck and passenger car traffic. Truck traffic can present substantial safety issues. Collisions with heavy-duty trucks are especially dangerous for passenger cars, motorcycles, bicycles, and pedestrians. These concerns can be even greater if truck traffic passes through residential areas, school zones, or other places where pedestrians are common and extra caution is warranted. (*Warehouse Projects* at 11.)

The DEIR simply omits any discussion regarding the increased risk to traffic safety with the large influx of large trucks and trailers to the area.

With respect to vehicle miles travelled ("VMT"), the DEIR determined that the Project would have a significant impact on VMT as it would result in 87.9 of the national average which exceeds the VMT threshold by 2.9. (DEIR 4.14-8.) The DEIR then relies upon MM-TRA-1

³ A copy of the Greenlight Traffic Engineering's Peer Review Report is attached as Exhibit C.

required to implement a Voluntary Employer Commuter Program. The EIR then concludes that the Voluntary Employer Commuter Program would result in a VMT reduction of 6.2%. (DEIR at 4.14-8.) That brings the VMT to below 85% - the VMT threshold – and a determination that after the Mitigation Measure-TRA-1, the impact to VMT would be less than significant. (DEIR at 4.14-10.) It is unclear from the DEIR how the City determined that the implementation of MM-TRA-1 would result in a VMT reduction of 6.2%. The determination that MM-TRA-1 would result in a VMT reduction of 6.2% must be supported by substantial evidence.⁴ (See *King v. Gardiner Farms v. County of Kern*, supra, 45 Cal.App.5th at 666; *Sierra Club v. County of San Diego* (2014) 231 Cal.App.4th 1152, 1168; *Communities for a Better Environment v. City of Richmond* (2010) 184 Cal.App.4th 70, 95.) Nothing in DEIR explains how the 6.2% was derived or determined.

Moreover, mitigation measure is for a voluntary program. A voluntary mitigation measure does not constitute an enforceable mitigation measure as required by CEQA. (Pub. Resources Code, § 21081.6(b); CEQA Guidelines, 15126.4(a)(2); *Gray v. County of Madera* (2008) 167 Cal.App.4th 1099, 1116.) Mitigation measures must be enforceable so that they will actually be implemented, not adopted and ignored. (*Federation of Hillside & Canyon Associations v. City of Los Angeles* (2000) 83 Cal.App.4th 1252, 1261.) The DEIR fails to explain how a voluntary program is enforceable to insure that there are sufficient reductions in VMT to support the conclusion that the potentially significant impact would be less than significant.

6. Section 4.17: Wildfire

The DEIR recognizes that the project site is within a Very High Fire Hazard Severity Zone (VHFHSZ). The DEIR also recognizes that the vegetation in the San Luis Rey River corridor to the north could present a wildfire risk, but then is somewhat dismissive asserting that land uses to the south and east are largely urban and do not present a wildfire risk. (DEIR at 4.17-2.) VHFHSZ is Cal-Fire's highest level of fire hazard. (Gov't Code, § 51178.)

Based upon comments received it is clear that the DEIR failed to adequately address the Project's impact on evacuation of the area in the event of a wildfire. The comments from residents of the area with personal knowledge of traffic flow and the street system have clearly demonstrated that the numerous trucks entering and exiting Benet Road and Highway 76 will impact evacuation and emergency vehicle movement in case of wildfire.

⁴ CEQA defines "substantial evidence" as "enough relevant information and reasonable inferences from this information that a fair argument can be made to support a conclusion, even though other conclusions might also be reached. Whether a fair argument can be made ... is to be determined by examining the whole record before the lead agency. Argument, speculation, unsubstantiated opinion or narrative, evidence which is clearly erroneous or inaccurate ... does not constitute substantial evidence." (CEQA Guidelines, § 15384(a).)

These personal observations of the neighbors and residents to the Project constitutes substantial evidence. (*Pocket Protectors v. City of Sacramento* (2005) 124 Cal.App.4th 903, 937.) Relevant personal observations of area residents on nontechnical subjects, such as aesthetics and traffic qualify as substantial evidence for a fair argument. (*Id.*, *Ocean View Estates Homeowner's Assn., Inc. v. Montecito Water District* (2004) 116 Cal.App.4th 396, 402; *Citizens Ass'n for Sensible Development v. County of Inyo* (1985) 172 Cal.App.3d 151, 173 (owner of adjacent property may, based upon personal observations, testify to existing traffic conditions). Thus, while an individual may not be an expert, their firsthand observations should not casually be dismissed as immaterial because "relevant personal observations are evidence." (*Ocean View Estates Homeowners Assn., Inc. v. Montecito Water Dist.*, *supra*, 116 Cal.App.4th at 402.)

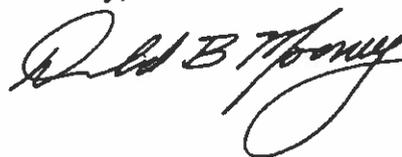
Additionally, Greenlight's peer review came to a similar conclusion regarding wildfire and evacuation. After reviewing the DEIR, Greenlight concluded that "the City has not adequately planned for emergency evacuation in the event of wildfire, nor has the developer identified how this risk will be mitigated with the increase in traffic congestion." (Greenlight at 3.)

The lack of discussion and analysis regarding wildfire risk, and in particular impacts to evacuation means the DEIR fails as an informational document. This violates the most basic purpose of an EIR to provide public agencies and the public in general with detailed information about the effect [that] a proposed project is likely to have on the environment; to list ways in which the significant effects of such a project might be minimized; and to indicate alternatives to such a project." (Pub. Resources Code, § 21061; *see* CEQA Guidelines § 15003(b)-(e); *Sierra Club v. County of Fresno*, *supra*, 6 Cal.5th at 511; *Mountain Lion Foundation v. Fish & Game Commission*, *supra*, 16 Cal.4th at 113.)

C. Conclusion

The DEIR fails to meet CEQA's most basic requirements of informed decision making and informed public participation. (*See Sierra Club v. County of Fresno*, *supra*, 6 Cal.5th at 513.) Thus, the DEIR is legally inadequate.

Sincerely,



Donald B. Mooney
Attorney for Eddie Jones
Go Fund Me Trust

Attachments

cc: Client

EXHIBIT A

EXHIBIT A



TECHNICAL MEMORANDUM

To: Justin Floyd
Eddie Jones GoFundMe Trust
Oceanside, California

Date: December 20, 2023

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Subject: Review and Analysis of Project Level and Cumulative Impacts to Air Quality Related to the Proposed Eddie Jones Warehouse, Oceanside, California

INTRODUCTION

I have reviewed the air quality impacts presented in the October 2023 Draft Environmental Impact Report (DEIR) for the Eddie Jones Warehouse and Distribution Facility in Oceanside, CA. The proposed project is to be located on a 31.79-acre site at 250 Eddy Jones Way in Oceanside. The project site is adjacent to the Oceanside Municipal Airport and is approximately 1,000 feet South of a residential area (Figure 1).

There were two main objectives in preparing this analysis:

1. Review the DEIR for accuracy and completeness
2. Evaluate project level and cumulative impacts to air quality

The overall goal is to determine if the proposed project would cause significant air quality impacts to homes and businesses near the project site. A project is considered to have a significant air quality impact if one or more ambient (outside) air quality standards are violated. Similarly, cumulative impacts are considered significant if impacts from the proposed project plus impacts from other nearby projects (either existing or proposed) would violate the ambient air quality standards. These standards cover the so called "Criteria Air Pollutants" that include:

- Carbon Monoxide (CO)
- Oxides of Nitrogen (NOx)
- Particulate Matter (PM10 or PM2.5)

A copy of these standards is attached. An air quality standard consists of an averaging time and a numerical concentration. For example, the federal 1-hour NOx standard is 100 parts per billion or 188 micrograms per cubic meter (ug/m3).

In addition to the Criteria Air Pollutants, a project is considered to have significant air quality impact if it releases toxic air contaminants (TACs). Unlike for criteria air pollutants that have air quality standards in terms of concentration, TACs are regulated in terms of health risks. For example, a cancer risk of 1 in a million from exposure to a TAC. Emissions of TACs are considered significant if cancer risk exceed 10 in a million or if non-cancer risk exceeds a hazard index of 1. TACs include pollutants such as benzene from car exhaust and diesel particulate from construction equipment and trucks.

REVIEW OF DEIR

The air analysis appears in Section 6.4.2 of the DEIR and in a separate document "Air Quality and Greenhouse Gas Emissions Technical Report" September 2023. The DEIR and the Technical Report conclude that the project would not cause any significant air quality impact nor would contribute to any new violations of the air quality standards. The Technical Report further asserts that since project level impacts are insignificant, then cumulative impacts would also be insignificant. The report cites the San Diego Air Pollution Control District (SDAPCD) significance thresholds as the basis for determining the significance of impacts.

As shown in the next section of this memorandum, the construction phase of the project would violate multiple air quality standards at a project level. These violations are exacerbated when the project is viewed with other existing or planned project. The impacts from the operational (occupancy) phase cannot be fully determined as there is substantial uncertainty as to future tenants and what activities may occur at the site. Therefore, future impacts from for the operational phase remain largely unknown.

My specific findings are as follows:

Old Version of the Emissions Model Used

The DEIR used an old version of the CalEEMod Emissions model to calculate daily and annual emissions and then compared these emissions against the thresholds of significance. Version 2020.4.0 was used instead of the current version 2022.1.

The current version of CalEEMod provides, among other things, details of the project's risk to the climate which is a key issue for this project. Since pollutant emission rates are at the core of the impact analysis and conclusions presented in the DEIR, this is a critical oversight.

Air Quality Impacts are Significant

Use of only the daily thresholds of significance to determine significance does not fully characterize the air quality impacts to the nearby homes and businesses. This is because impacts to nearby homes are related to the concentration of various air pollutants not their daily mass emission rates. Concentrations

are related not just to the daily emission rates but also on numerous other factors such as proximity to homes and businesses and local weather conditions.

The CEQA Guidelines Appendix G¹ specifically requires the evaluation of pollutant concentration to sensitive receptors. The DEIR however, provides no analysis that addresses this issue but merely concludes that impacts would be less than significant.

Cumulative Impact Analysis is Incomplete

The DEIR concludes that since project level impacts are less than significant, then cumulative impacts would also be less than significant². This logic is flawed. Under this interpretation, the only project would significant cumulative impacts would be those that have project level significant impacts. Under this reasoning for example, 10 projects each with project level impact less than significant would insignificant cumulative impact. This is clearly false.

Cumulative impact analysis requires that emissions from all current or future project be evaluated to determine impacts. This was not done. As discussed later in this memorandum, I evaluated the emissions from the proposed Ocean Kamp project and found significant cumulative air quality impacts for multiple air pollutants. Cumulative impact analysis also needs to address cumulative health risks. This was also missing in the DEIR.

Impacts from Emissions of Toxic Air Contaminants May be Significant

The DEIR analyzed public health risks (presented in Table 4.2-1). These risks do not take into account the fact that future truck/manufacturing emissions are mostly unknown. For example, trucks equipped with transport refrigeration units (TRUs) had small and highly polluting diesel engines that release DPM. Emissions from such engines were not quantified nor included in the risk analysis.

IMPACTS TO AIR QUALITY

As noted previously, air quality impacts depend on the exposure concentration of various air pollutants. I used an air dispersion model to calculate the pollutant concentration based on the daily project and cumulative emission rates of several air pollutants.

METHODOLOGY

My analysis focused on the following air pollutants.

Air Pollutant	Maximum Mitigated Emission Rate Construction Phase <i>Source: DEIR Table 8</i>		Applicable Air Quality Standard
	Pounds/Day	Pounds/Hour	
Oxides of Nitrogen (NOx)	42.29	5.29	188 ug/m3 over 1 hour (Federal)
Respirable Particulate Matter (PM10)	10.29	1.28	50 ug/m3 over 24 hours (State)
Fine Particulate Matter (PM2.5)	5.76	0.72	35 ug/m3 over 24 hours (State)

Note: Hourly emissions during the construction phase equal lbs/day divided by 8 hrs/day

¹ This is acknowledged in Section 4.2.3 of the DEIR.

² See discussion in the Executive Summary under “Cumulative Impacts”. Page VIII.

For cumulative impacts, I used the emission rates provided in Table 5.1-2 of the Ocean Kamps SEIR³. The air dispersion model (AERMOD) used in the analysis has been designated as "Preferred Model" by the Environmental Protection Agency (EPA) and by the SDAPCD. A description of this model is attached. In addition to project emission rates, application of this model requires the use of hourly weather data (wind speed, wind direction, temperature, etc.). I used 3 years of hourly weather data from Camp Pendleton for the period 2019 to 2021. A total of 26,280 hours of weather data were utilized. These data were processed by SDAPCD and made available to us. The modeling area used in the analysis is shown in Figure 2.

The results are displayed in terms of numerical concentration in ug/m3 as well as contour plots showing the spatial distribution of pollutant concentration in the vicinity of the project site.

FINDINGS

My analysis found that emissions of NOx, PM10 and PM2.5 would violate the ambient air quality standards by a substantial margin as summarized below.

Air Pollutant	Maximum Off-Site Pollutant Concentrations Construction Phase (ug/m3)		Applicable Air Quality Standard ug/m3
	Project Level	Cumulative	
Oxides of Nitrogen (NOx) 1-hour	300	3,267	188
Respirable Particulate Matter (PM10) 24-hour	21.9	260.1	50
Fine Particulate Matter (PM2.5) 24-hour	12.3	51.1	35

The spatial distribution of pollutant concentration is shown in Figures 3 to 6. In these figures, areas in red exceed the applicable air quality standard.

Actual cumulative impacts are expected to be higher since they do not include emissions from the Oceanside Municipal Airport.

For the operational phase, the DEIR concludes that air quality impacts are less than significant and that cancer risk is 1.33 in a million⁴. However, these conclusions are misleading as future emissions from manufacturing and truck emissions are largely unknown.

³ Final Supplemental Environmental Impact Report. July 2022.

⁴ Table 4.2-13 Eddie Jones Warehouse DEIR October 2023.

CONCLUSIONS

On the basis on my review of the DEIR and on the modeling analysis presented in this Memorandum, my conclusions are as follows.

1. The emissions estimates and conclusions presented in the DEIR are questionable as they are based on an outdated emissions model.
2. Air quality impacts are significant both at a project level and cumulatively. This is based on calculated pollutant concentration in the vicinity of the project
3. The analysis of cumulative impact was incomplete. Emissions from existing and future sources and projects were completely ignored.
4. Future emissions and health risks to the public remain largely unknown and therefore difficult to reliably quantify future impacts

ATTACHMENTS

Figures

Air Quality Standards

Description of AERMOD Dispersion Model

Qualifications

Figures

Figure 1
Project Location



Source: Eddie Jones Warehouse Draft EIR

Figure 2

Layout of Modeling Domain

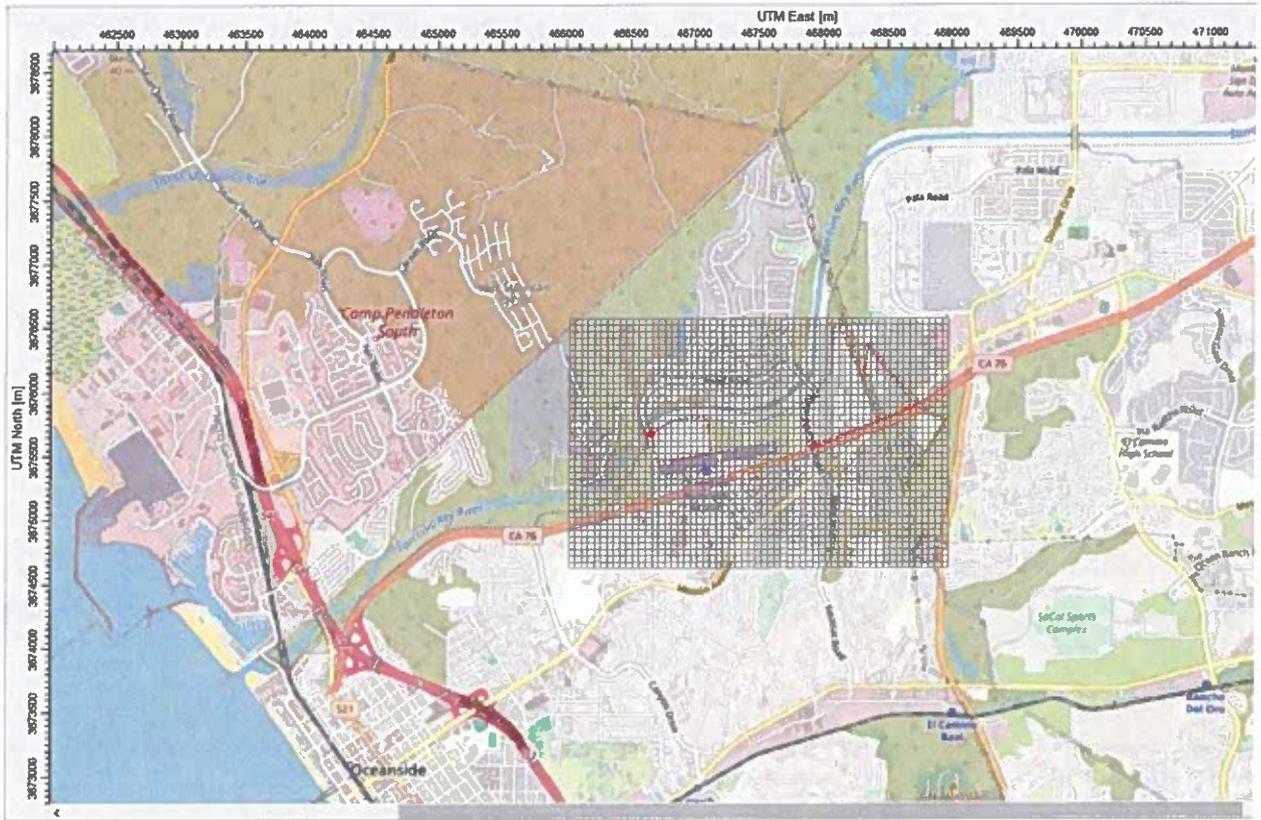


Figure 3

**Spatial Distribution of Project Level 1 Hour NO_x in ug/m³
Areas Inside the Contour Labelled 188 exceeds the Air Quality Standard**

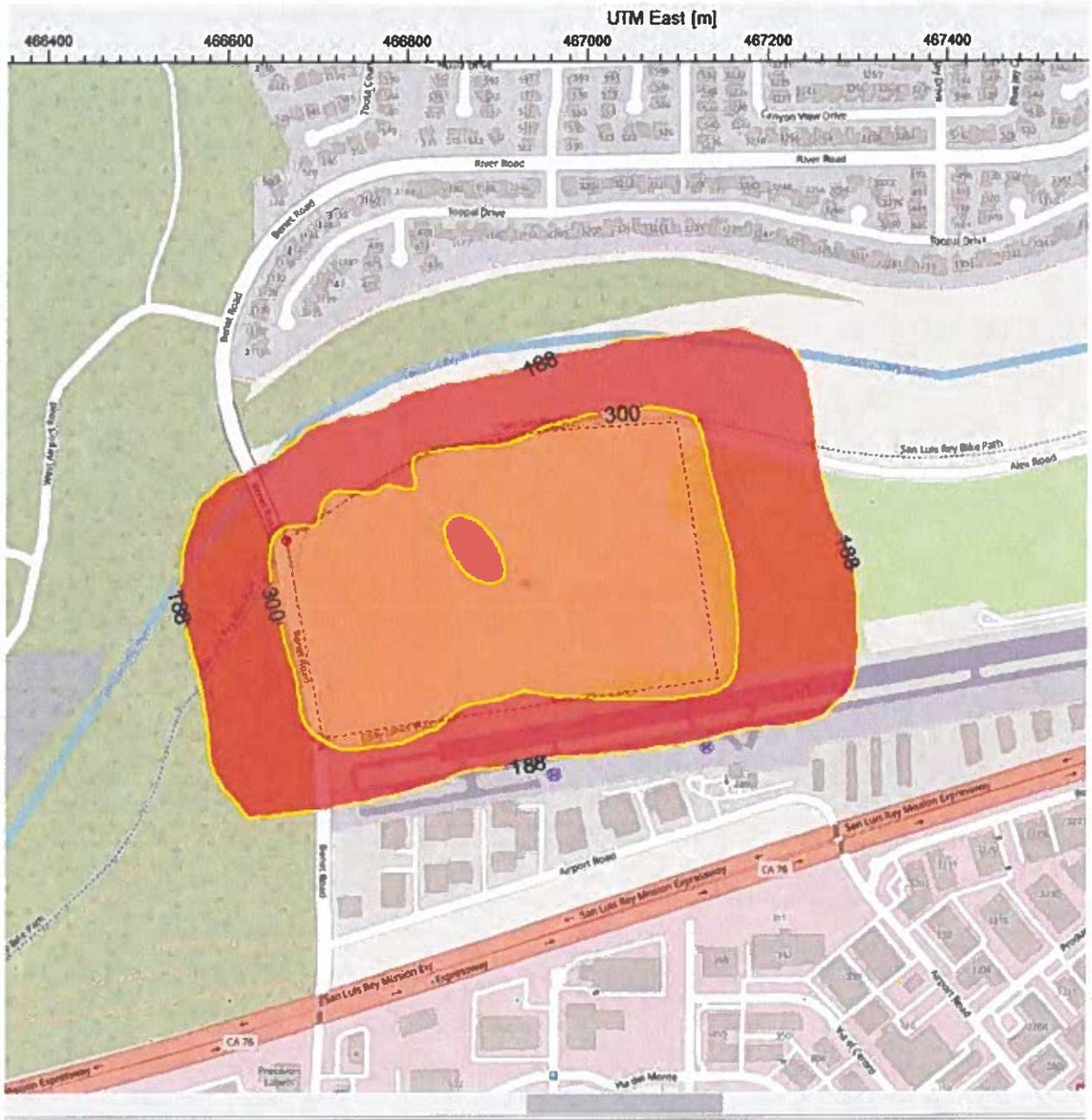


Figure 4

Spatial Distribution of Cumulative 1 Hour NOx
Areas Inside the Contour Labelled 188 exceeds the Air Quality Standard

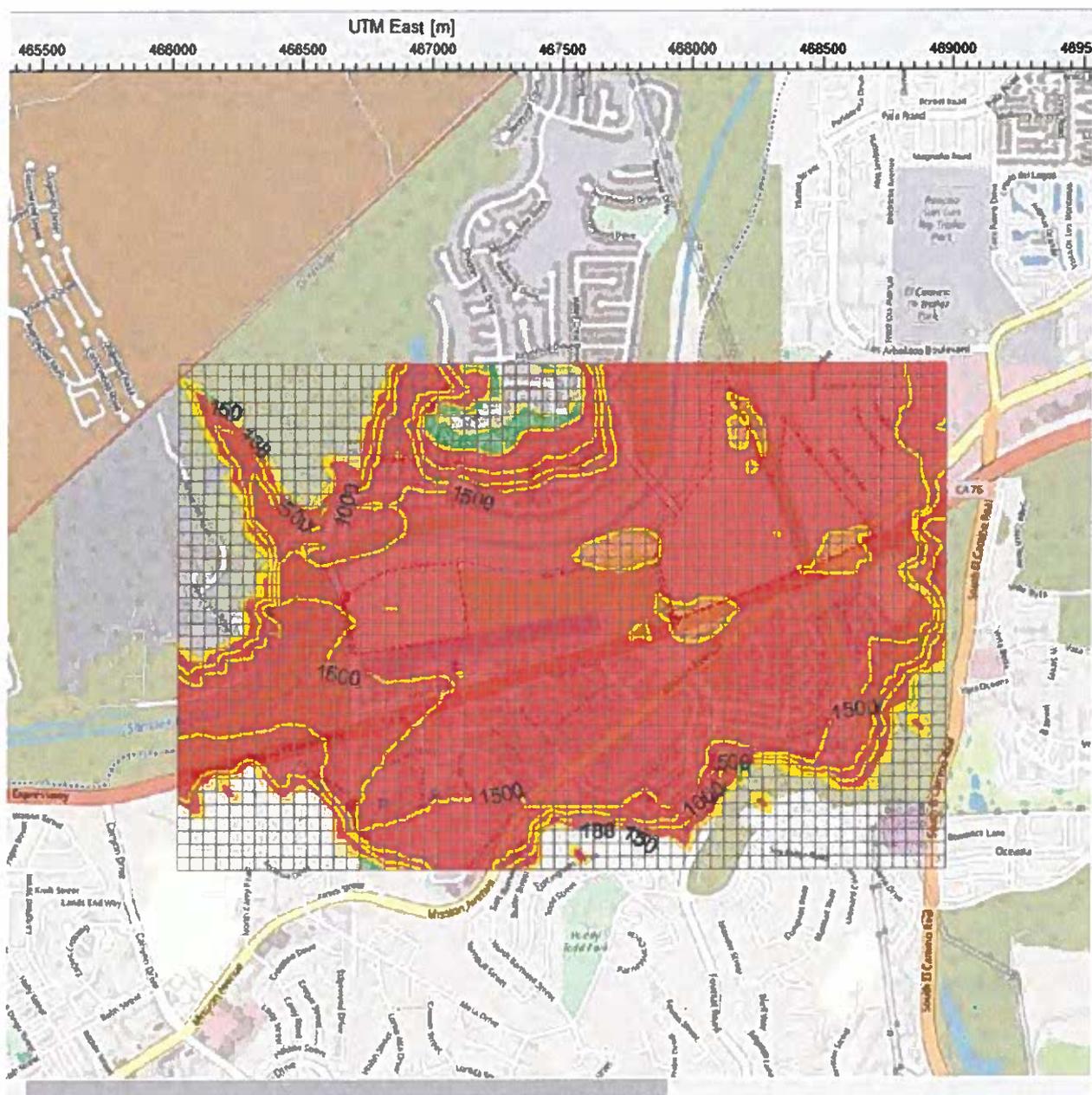


Figure 5

Spatial Distribution of Cumulative 24 Hour PM10 Concentration in ug/m3
Areas Inside the Contour Labelled 50 exceeds the Air Quality Standard

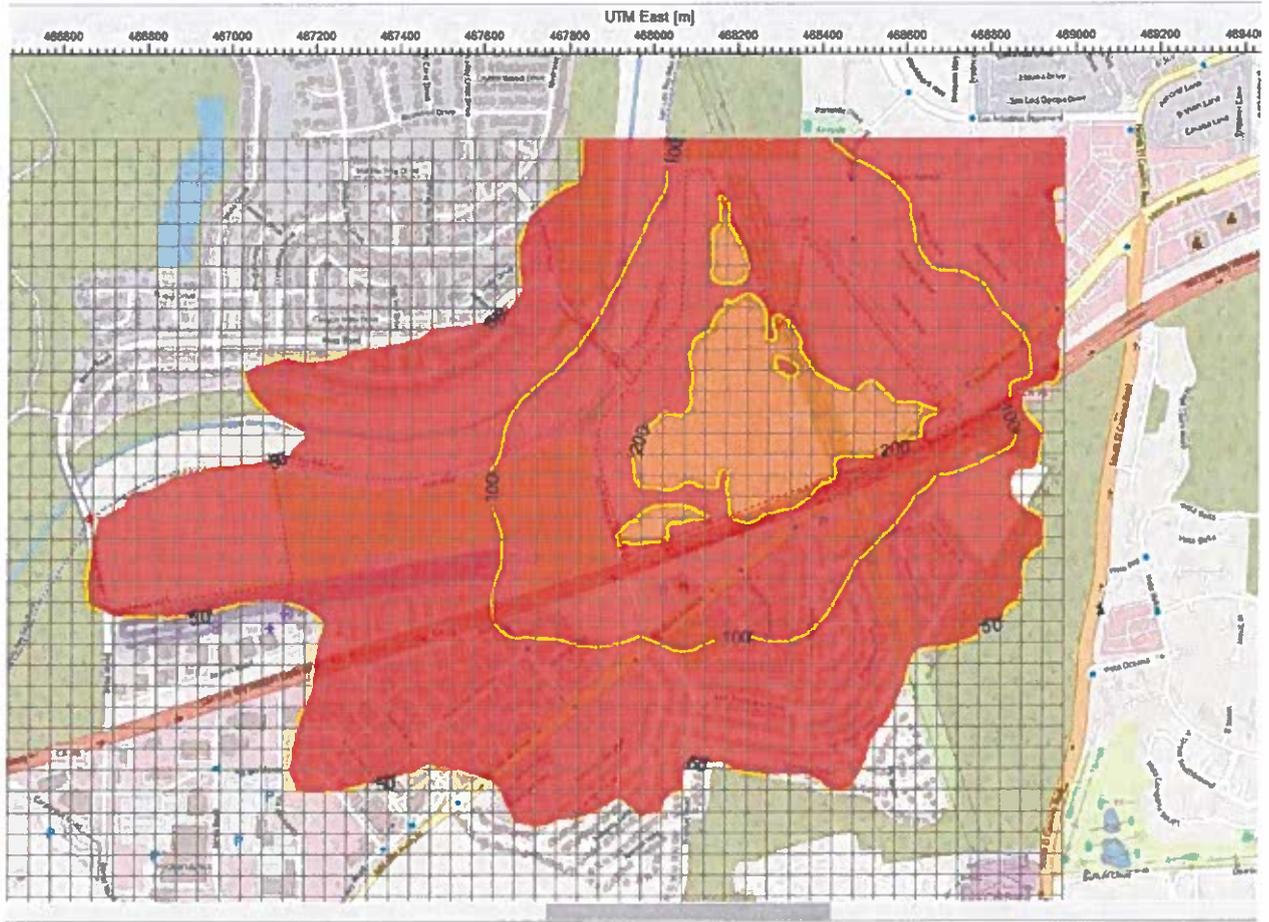
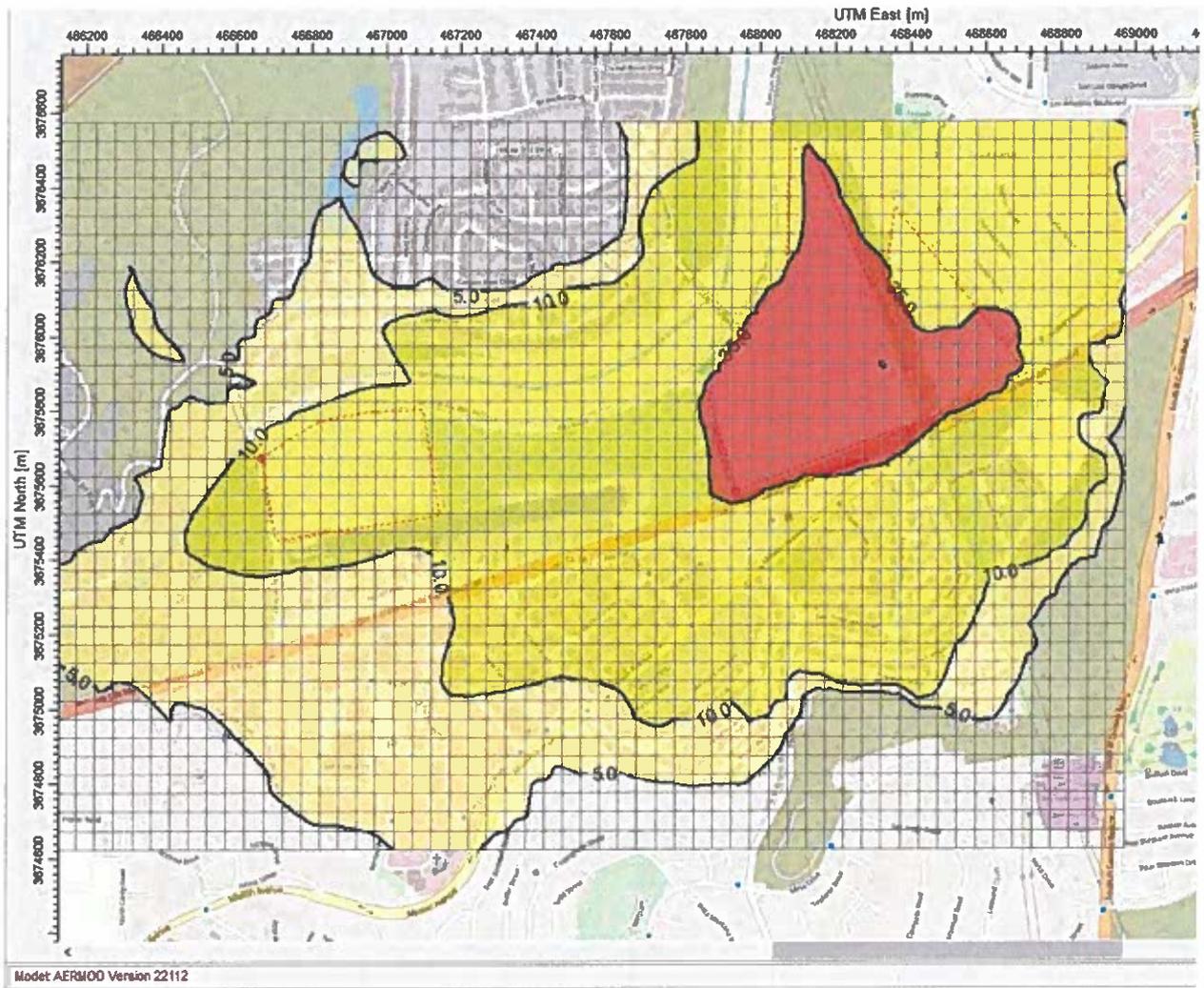


Figure 6

**Spatial Distribution of Cumulative PM_{2.5} Concentration in ug/m³
Areas Inside the Contour Labelled 35 exceeds the Air Quality Standard**



Air Quality Standards

Ambient Air Quality Standards

Pollutant	Averaging Time	California Standards ¹		National Standards ²		
		Concentration ³	Method ⁴	Primary ^{3,5}	Secondary ^{3,6}	Method ⁷
Ozone (O ₃) ⁸	1 Hour	0.09 ppm (180 µg/m ³)	Ultraviolet Photometry	—	Same as Primary Standard	Ultraviolet Photometry
	8 Hour	0.070 ppm (137 µg/m ³)		0.070 ppm (137 µg/m ³)		
Respirable Particulate Matter (PM ₁₀) ⁹	24 Hour	50 µg/m ³	Gravimetric or Beta Attenuation	150 µg/m ³	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	20 µg/m ³		—		
Fine Particulate Matter (PM _{2.5}) ⁹	24 Hour	—	—	35 µg/m ³	Same as Primary Standard	Inertial Separation and Gravimetric Analysis
	Annual Arithmetic Mean	12 µg/m ³	Gravimetric or Beta Attenuation	12.0 µg/m ³	15 µg/m ³	
Carbon Monoxide (CO)	1 Hour	20 ppm (23 mg/m ³)	Non-Dispersive Infrared Photometry (NDIR)	35 ppm (40 mg/m ³)	—	Non-Dispersive Infrared Photometry (NDIR)
	8 Hour	9.0 ppm (10 mg/m ³)		9 ppm (10 mg/m ³)	—	
	8 Hour (Lake Tahoe)	6 ppm (7 mg/m ³)		—	—	
Nitrogen Dioxide (NO ₂) ¹⁰	1 Hour	0.18 ppm (339 µg/m ³)	Gas Phase Chemiluminescence	100 ppb (188 µg/m ³)	—	Gas Phase Chemiluminescence
	Annual Arithmetic Mean	0.030 ppm (57 µg/m ³)		0.053 ppm (100 µg/m ³)	Same as Primary Standard	
Sulfur Dioxide (SO ₂) ¹¹	1 Hour	0.25 ppm (655 µg/m ³)	Ultraviolet Fluorescence	75 ppb (196 µg/m ³)	—	Ultraviolet Fluorescence; Spectrophotometry (Pararosaniline Method)
	3 Hour	—		—	0.5 ppm (1300 µg/m ³)	
	24 Hour	0.04 ppm (105 µg/m ³)		0.14 ppm (for certain areas) ¹¹	—	
	Annual Arithmetic Mean	—		0.030 ppm (for certain areas) ¹¹	—	
Lead ^{12,13}	30 Day Average	1.5 µg/m ³	Atomic Absorption	—	—	High Volume Sampler and Atomic Absorption
	Calendar Quarter	—		1.5 µg/m ³ (for certain areas) ¹²	Same as Primary Standard	
	Rolling 3-Month Average	—		0.15 µg/m ³		
Visibility Reducing Particles ¹⁴	8 Hour	See footnote 14	Beta Attenuation and Transmittance through Filter Tape	No National Standards		
Sulfates	24 Hour	25 µg/m ³	Ion Chromatography			
Hydrogen Sulfide	1 Hour	0.03 ppm (42 µg/m ³)	Ultraviolet Fluorescence			
Vinyl Chloride ¹²	24 Hour	0.01 ppm (26 µg/m ³)	Gas Chromatography			

See footnotes on next page ...

1. California standards for ozone, carbon monoxide (except 8-hour Lake Tahoe), sulfur dioxide (1 and 24 hour), nitrogen dioxide, and particulate matter (PM10, PM2.5, and visibility reducing particles), are values that are not to be exceeded. All others are not to be equaled or exceeded. California ambient air quality standards are listed in the Table of Standards in Section 70200 of Title 17 of the California Code of Regulations.
2. National standards (other than ozone, particulate matter, and those based on annual arithmetic mean) are not to be exceeded more than once a year. The ozone standard is attained when the fourth highest 8-hour concentration measured at each site in a year, averaged over three years, is equal to or less than the standard. For PM10, the 24 hour standard is attained when the expected number of days per calendar year with a 24-hour average concentration above $150 \mu\text{g}/\text{m}^3$ is equal to or less than one. For PM2.5, the 24 hour standard is attained when 98 percent of the daily concentrations, averaged over three years, are equal to or less than the standard. Contact the U.S. EPA for further clarification and current national policies.
3. Concentration expressed first in units in which it was promulgated. Equivalent units given in parentheses are based upon a reference temperature of 25°C and a reference pressure of 760 torr. Most measurements of air quality are to be corrected to a reference temperature of 25°C and a reference pressure of 760 torr; ppm in this table refers to ppm by volume, or micromoles of pollutant per mole of gas.
4. Any equivalent measurement method which can be shown to the satisfaction of the ARB to give equivalent results at or near the level of the air quality standard may be used.
5. National Primary Standards: The levels of air quality necessary, with an adequate margin of safety to protect the public health.
6. National Secondary Standards: The levels of air quality necessary to protect the public welfare from any known or anticipated adverse effects of a pollutant.
7. Reference method as described by the U.S. EPA. An "equivalent method" of measurement may be used but must have a "consistent relationship to the reference method" and must be approved by the U.S. EPA.
8. On October 1, 2015, the national 8-hour ozone primary and secondary standards were lowered from 0.075 to 0.070 ppm.
9. On December 14, 2012, the national annual PM2.5 primary standard was lowered from $15 \mu\text{g}/\text{m}^3$ to $12.0 \mu\text{g}/\text{m}^3$. The existing national 24-hour PM2.5 standards (primary and secondary) were retained at $35 \mu\text{g}/\text{m}^3$, as was the annual secondary standard of $15 \mu\text{g}/\text{m}^3$. The existing 24-hour PM10 standards (primary and secondary) of $150 \mu\text{g}/\text{m}^3$ also were retained. The form of the annual primary and secondary standards is the annual mean, averaged over 3 years.
10. To attain the 1-hour national standard, the 3-year average of the annual 98th percentile of the 1-hour daily maximum concentrations at each site must not exceed 100 ppb. Note that the national 1-hour standard is in units of parts per billion (ppb). California standards are in units of parts per million (ppm). To directly compare the national 1-hour standard to the California standards the units can be converted from ppb to ppm. In this case, the national standard of 100 ppb is identical to 0.100 ppm.
11. On June 2, 2010, a new 1-hour SO_2 standard was established and the existing 24-hour and annual primary standards were revoked. To attain the 1-hour national standard, the 3-year average of the annual 99th percentile of the 1-hour daily maximum concentrations at each site must not exceed 75 ppb. The 1971 SO_2 national standards (24-hour and annual) remain in effect until one year after an area is designated for the 2010 standard, except that in areas designated nonattainment for the 1971 standards, the 1971 standards remain in effect until implementation plans to attain or maintain the 2010 standards are approved.
 Note that the 1-hour national standard is in units of parts per billion (ppb). California standards are in units of parts per million (ppm). To directly compare the 1-hour national standard to the California standard the units can be converted to ppm. In this case, the national standard of 75 ppb is identical to 0.075 ppm.
12. The ARB has identified lead and vinyl chloride as 'toxic air contaminants' with no threshold level of exposure for adverse health effects determined. These actions allow for the implementation of control measures at levels below the ambient concentrations specified for these pollutants.
13. The national standard for lead was revised on October 15, 2008 to a rolling 3-month average. The 1978 lead standard ($1.5 \mu\text{g}/\text{m}^3$ as a quarterly average) remains in effect until one year after an area is designated for the 2008 standard, except that in areas designated nonattainment for the 1978 standard, the 1978 standard remains in effect until implementation plans to attain or maintain the 2008 standard are approved.
14. In 1989, the ARB converted both the general statewide 10-mile visibility standard and the Lake Tahoe 30-mile visibility standard to instrumental equivalents, which are "extinction of 0.23 per kilometer" and "extinction of 0.07 per kilometer" for the statewide and Lake Tahoe Air Basin standards, respectively.

Description of AERMOD

AERMOD VIEW

GAUSSIAN PLUME AIR DISPERSION MODELS

Model Descriptions

AERMOD

The AMS/EPA Regulatory Model (AERMOD) is the next generation air dispersion model based on planetary boundary layer theory. AERMOD contains essentially the same options as ST3 with few exceptions.

AERMOD fully incorporates the PRIME building downwash algorithms, advanced depositional parameters, local terrain effects, and advanced meteorological turbulence calculations.

ST3

ST3 (Industrial Source Complex Model) is a steady-state Gaussian plume model used to assess pollutant concentrations from a wide variety of sources associated with an industrial complex.

The ST3 model accounts for:

- Settling and dry deposition of particles
- Building downwash
- Point, area, line, open pit, flare, and volume sources
- Flat and complex terrain

PRIME

ST3 with the Plume Rise Enhancements (PRIME) model incorporates two important features:

- Enhanced plume dispersion coefficients due to the building turbulent wake.
- Reduced plume rise caused by a combination of descending streamlines in the lee of the building and the increased entrainment in its wake.

Leading Air Dispersion Models Under One Interface

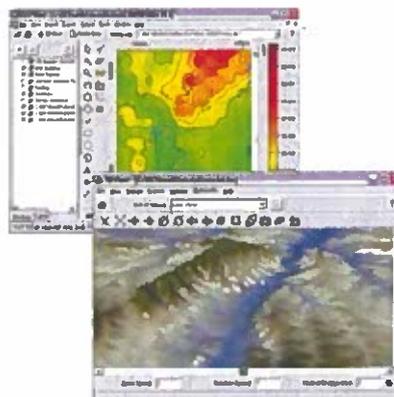
AERMOD View is a complete and powerful air dispersion modeling package which seamlessly incorporates the popular U.S. EPA models into one interface: AERMOD, ST3, and PRIME. These models are used extensively to assess pollution concentration and deposition from a wide variety of sources.



AERMOD View Tools

AERMOD View provides all the tools you need to get your air quality analysis done on time, including:- Easy and intuitive graphical interface

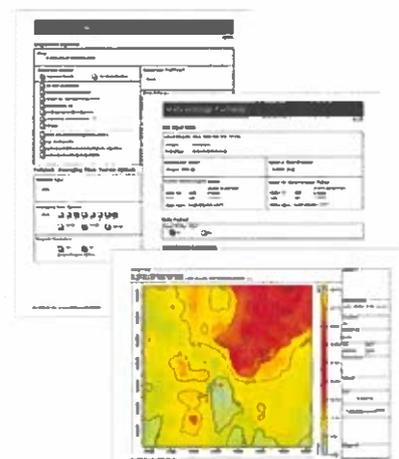
- Data entry in Metric and English units
- Imports a variety of base map formats
- Supports the major digital terrain formats
- Powerful 3D visualization
- Building downwash analysis
- Meteorological pre-processing
- Automatic contouring of results
- Rapid model comparisons
- Report-ready output



Impressive Report-Ready Output

AERMOD View lets you impress with integrated report generation.

- Summarize your modeling input in professionally designed reports
- Custom 3D views of your project site and/or modeling results can also be generated in professional report format
- Customize information to be included
- Print from AERMOD View or save your report to file



Digital Terrain Data

AERMOD View supports a wide variety of digital elevation terrain data formats. Quick import of terrain elevations will save you time and avoid costly hand-made errors.

- USGS DEM
- GTOPO30 DEM
- U.K. DTM
- U.K. NTF
- XYZ Files
- CDED 1-degree
- AutoCAD DXF

Import multiple DEMs and AERMOD View will automatically combine each area and zone for your project.

Complete support for AERMAP is also included, making terrain processing for your AERMOD project a snap!

Terrain grid files for deposition analysis can be automatically created from digital terrain files.

Building Downwash

Buildings can radically influence the dispersion of pollutants. AERMOD View provides all the necessary tools to effectively and quickly complete your building downwash analysis.

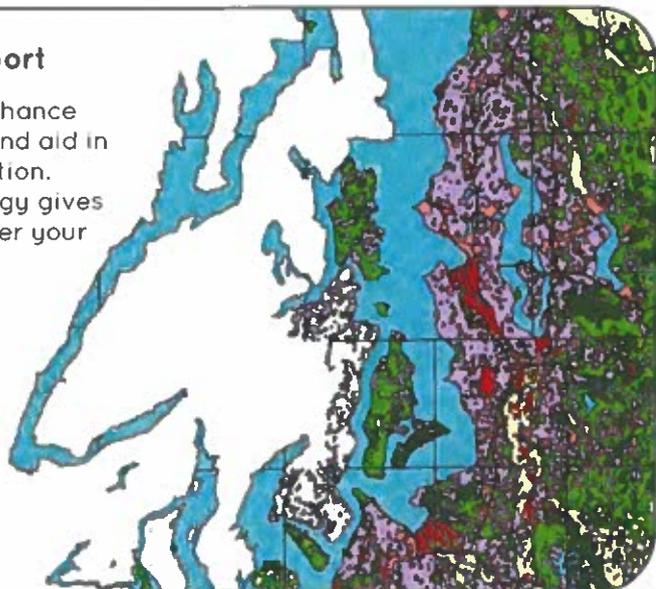
With easy import of building coordinates from AutoCAD base maps and options to digitize buildings, your downwash analysis can be performed quickly.



Extensive Map Support

Import base maps to enhance your modeling project and aid in model results interpretation. Integrated GIS technology gives you complete control over your modeling site.

- Bitmap
- USGS DLG
- USGS LULC
- AutoCAD DXF
- ESRI Shapefile
- JPEG
- TIFF/GeoTIFF
- MrSID



Graphical Input

Avoid the hassle and errors of entering locations by hand from paper maps. Specify sources and receptors graphically. After defining an object graphically you automatically have access to the related text mode window in which you can further modify parameters.

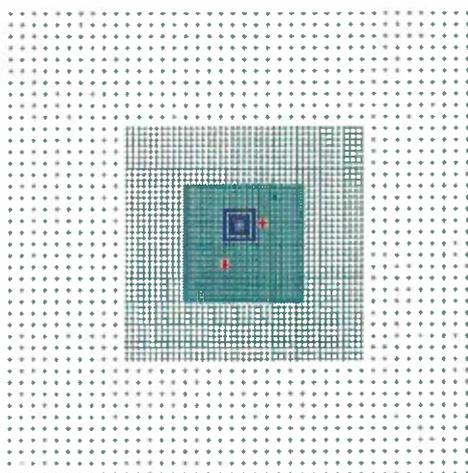
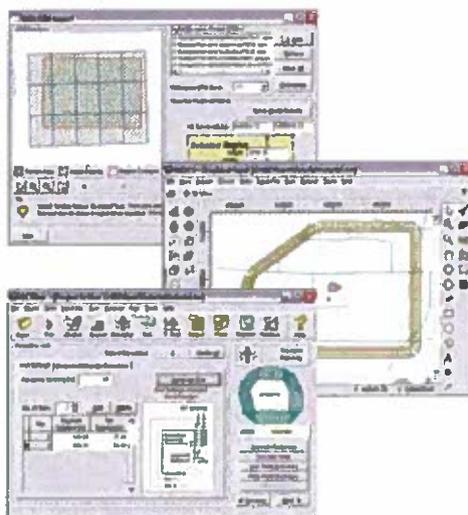
Automatically eliminate receptors inside the facility property line.

Receptors (unlimited)

- Cartesian Grids
- Polar Grids
- Drecte Cartesian Receptors
- Drecte Polar Receptors
- Cartesian Plant Boundary
- Polar Plant Boundary
- Fenceline Grid
- Multi-Tier Grid (Risk Grid)
- Flagpole Receptors

Sources (unlimited)

- Point
- Area (square, rectangular, circular, polygon)
- Volume
- Open Pit
- Flare
- Line



AERMOD VIEW

PROFESSIONAL VISUALIZATION & ADVANCED TOOLS TO BOOST YOUR PRODUCTIVITY

High-Impact 3D Visualization

AERMOD View features powerful 3D visualization tools unlike other software that requires you to purchase yet another software package just to view plain, static 3D plots. Nothing communicates your modeling better than images, and AERMOD View provides powerful, dynamic 3D tools built right into its interface.

Understand the effects of topography by displaying your model results with 3D terrain. Make your final report clear and concise by visualizing all your data.

Complete visualization of your imported terrain is just a click away! Click the 3D Terrain icon and your project is transformed into a fully customizable 3D view using your terrain elevation data. Zoom, rotate, and save views in true 3D.

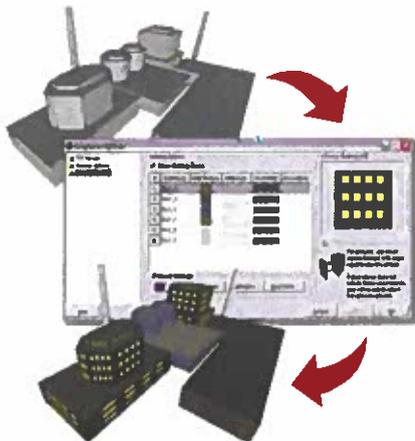
Apply terrain contours to your 2D site view for professional maps, or change to 3D views with a single click. View your site and surrounding terrain in true 3D. Sources and buildings appear in context with your site terrain.

Custom Textures

Apply custom textures to buildings to further increase building realism - say goodbye to bland polygons!

Clear and realistic communication is essential and your model should make an impression.

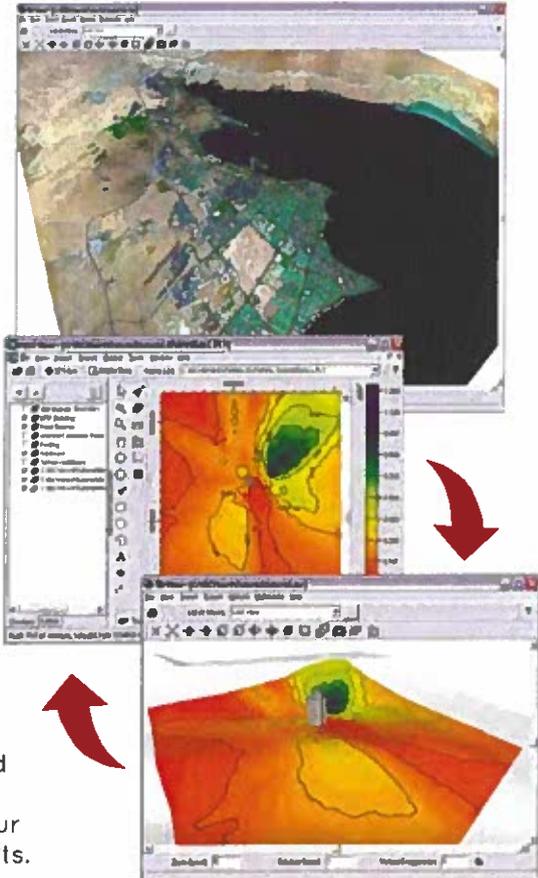
Real-time lighting effects, true color shading and textures bring your site to life like no other package can!



Integrated Contouring

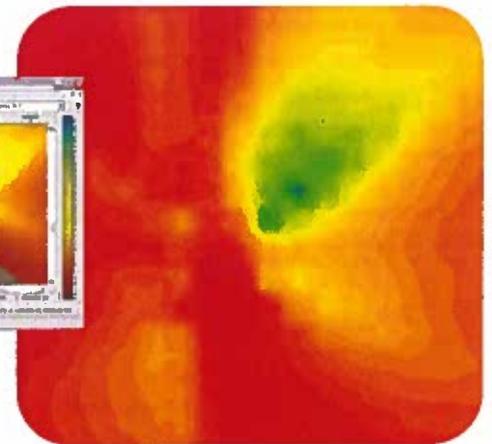
AERMOD View features integrated post-processing with automatic gridding, blanking, shaded contour plotting, and posting of your results.

- Customize contour levels, color shading, transparency, contour labels, fonts, and more.
- Multiple levels of transparency
- True color palette
- Save and edit any palette
- Extensive terrain contouring options
- Color ramp customization



Export Options

- ESRI Shapefiles
- Bitmaps
- Enhanced Metafiles



Multiple Chemicals

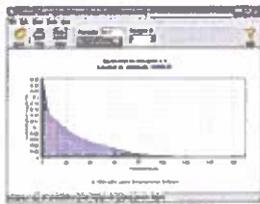
The Multi-Chemical Run utility will boost your productivity drastically when analyzing the contributions of each pollutant from sources emitting multiple chemicals.

Simply specify the pollutants emitted by each source, define emission rates, and click Run. The Multi-Chemical Run utility takes care of the rest, reducing your model run time to a fraction of the time it would take traditionally.

- Unlimited sources
- Unlimited receptors
- Unlimited pollutants (>1000)
- Block averages
- Rolling averages
- Chemical-specific plotfiles

Percent View

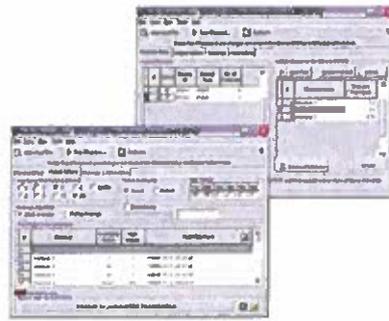
Percent View takes the hassle out of performing modeling runs that require percentiles or rolling averages. Run the model and have these calculations automatically computed.



Risk Assessment Projects

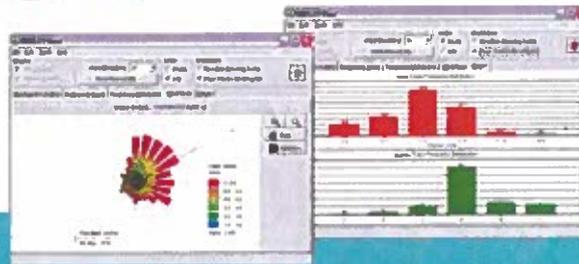
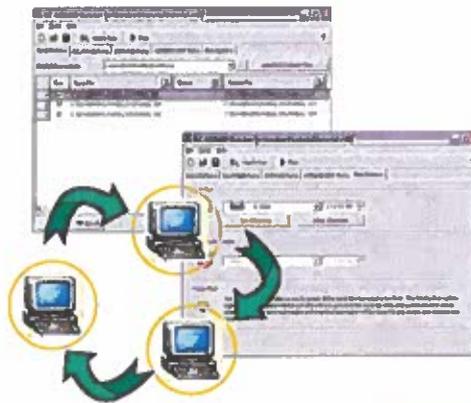
Customized air modeling support for leading risk assessment protocols, the U.S. EPA - OSW Human Health Risk Assessment Protocol, and the U.S. EPA - OSW Screening Level Ecological Risk Assessment Protocol. Simply select "Risk Mode" to model in accordance with these guidelines.

Quickly generate the files required for ACE2588 risk assessment model.



AERMOD Batcher T

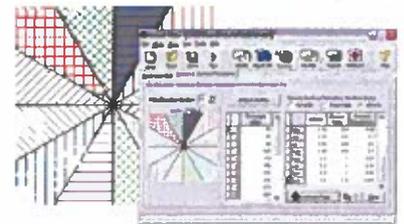
The AERMOD Batcher is designed to let you easily perform multiple modeling runs. Simply specify the input files for the projects you wish to run, click Run and AERMOD Batcher will run all your projects. This is ideal for large modeling runs, which is often required for risk assessment projects.



Meteorological Tools

Aermet View

Aermet View is the meteorological preprocessor that guides you through easy steps to prepare your on-site and off-site meteorological data for use with AERMOD.



Rammet View

Rammet View is the meteorological preprocessor that prepares data for use with the ST3 and PRIME models. Rammet View includes a set of tools which allow conversion of your own met data into the required format.



WRPLOT View

Wind rose plots, frequency tables, and graphs can be generated automatically from surface data files in SCRAM, CD144, HUSWO, TD-3505, CARB, and SAMSON formats or from and AERMET preprocessed met data files. Import from Excel is also supported



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LakesTM
Environmental

Qualifications



Statement of Qualifications Environmental Permitting Specialists (EPS)

EPS is a specialized consulting firm focusing on air quality impact analysis, permitting, health risk assessments and odor control and analysis. The firm is headquartered in Sacramento, California and has been providing these services since 1995. Firms such as Sunsweet Growers, Amazon, El Dorado County Cannabis Growers Alliance, Teichert Construction, Sacramento City Unified School District, Blue Diamond Growers and many others have used our services. These firms they recognize the complexity related to controlling odors and of everchanging environmental regulations and prefer to have a specialized firm handle these issues on their behalf.

Practice Areas

- Construction Industries
- Solid Waste
- Cannabis Cultivation
- Energy Production
- Food Industries

In addition to providing services to private sector, EPS routinely provides technical support services to various County Planning Departments and air pollution control districts. These include:

- Amador Air District and Planning Department
- Calaveras Air Pollution Control District and Planning Department
- Mariposa County Departments of Planning and Environmental Health
- Placer County Air Pollution Control District
- Great Basin Air Quality Management District

Contact:

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Ray Kapahi

Senior Air Quality Consulting Engineer



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Practice Areas

- Air Quality Permitting
- Odor Modeling and Control
- Health Risk Assessment
- Computational Fluid Dynamics
- Greenhouse Gas Analysis
- Atmospheric Dispersion Modeling

Industries

- Solid Waste
- Energy Production
- Construction and Mining
- Cannabis Cultivation
- Oil and Gas Production
- Food Industries

Education and Training

- BSc. Physics (1972)
- MEng. Chemical Engineering (1975)
- CARB Accredited Green House Gas (GHG) Lead Verifier with Specialization in Process Emissions and Electricity Transactions (2009)

News

- Presentation "Numerical Modeling of Landfill Gas and Odors" 33rd International Conference on Solid Waste Technology and Management. March 11 to 14, 2018, Annapolis, MD.
- Presentation "Integrated Approach to Effective Odor Control at Landfills and Composting Facilities" Wastecon 2016, Indianapolis, IN.

EXPERIENCE

Over 30 years of experience in analyzing air quality and odor impacts, permitting of stationary sources, and preparation of environmental impact documents. Mr. Kapahi assists a broad range of clients and assists them to identify and meet their regulatory obligations.

The scope of his experience includes siting of new landfills, waste to energy plants, obtaining conditional use permits from City and County Governments for new projects or expansion of existing projects. Specific experience and skills include preparation of emission inventories, analysis and measurements of odors, dispersion modeling, oversight of air quality monitoring, analysis of impacts to public health, responding to public comments, and appearing before City and County Planning Boards and Commissions as an expert witness on behalf of clients.

Following approvals for new facilities or expansion of existing facilities, Mr. Kapahi continues to work with clients to ensure on-going compliance.

REPRESENTATIVE PROJECTS

Air Quality Modeling and Permitting

- **Permitting of a Powdered Milk Plant (Turlock, CA)**
Evaluate emissions of various air pollutants from the proposed 30 million gallons per year mild processing/drying facility. Demonstrate compliance with local and state air quality regulations, including regulation of toxic air pollutants.
- **Permit Revisions for an Existing Fruit Dehydration Facility (Yuba City, CA)**
Assisted a major food processor in revising their operating permits to allow for additional steam production. Worked cooperatively with the local air district to ensure timely issuance of the revised permits.
- **Permitting of a Waste to Energy Plant (Fort Irwin, CA)**
Quantify emissions from a proposed 34 tons per day solid waste to energy project. Analyze emissions associated with pyrolysis and subsequent utilization of synthetic gas to generate 1.5 MW of electric power. Prepare the necessary permit applications and supporting documentation.
- **Permitting of a CBD Oil Extraction Facility (Mendota, CA)**
Quantify emissions from a proposed solvent extraction process. Assist in design of an RTO VOC control system. The facility was permitting in 2019 and is currently operating.

Publications and Presentations

Presentation "Use of Advanced Models to Control Fugitive Odors from Composting Sites". US Compost Council Annual Meeting, January 2015, Austin, TX.

"Air Emissions from Landfills and Transfer Stations – Do they Increase Public Health Risks?" Presented at Quad State Environmental Conference, Pigeon Forge TN, Sept 2015.

"Risks of Carbon Credit Invalidation Under California's Cap-and-Trade Program", Presented at the 2014 Air and Waste Management Association Annual Conference. June 24-27, 2014. Long Beach, CA

"Estimate of VOC Emissions from Sludge Drying", Presented at the 1995 SWANA Conference. November 1995, Baltimore, MD.

"Use of Biofilters to Control VOCs", Biocycle, February 1995.

"Impacts of the 1990 Clean Air Act Amendments", San Jose Business Journal, March 24, 1994.

"Modeling Fine Particulates" in Municipal Waste Incineration Risk Assessment, Edited by Curtis Travis, Plenum Press, 1990.

Specialized Training

Calculating Tank Emissions. Trinity Consultants. Los Angeles, CA February 1-2, 2020.

Accidental Release Modeling Workshop. Trinity Consultants. Dallas, TX November 1-2, 2018.

HARP2 (Risk Assessment Model) Training at California Air Resources Board. Redding, CA

Hearing Board Variance Training – California Air Resources Board (1995)

Air Emissions and Odors from Wastewater – University of Texas, Austin (1994)

Professional Affiliations

Air and Waste Management Association (Board Member)

American Institute of Chemical Engineers (Member)

Member Technical Advisory Committee (TAC) for the California Energy Commission

Odor Analysis and Mitigation

• **Ventilation System for Odor Control (Anaheim, CA)**

Advanced computational fluid mechanics (CFD) models were used to predict the air flow and building pressure to identify the location, size and number of exhaust fans required to remove odors from the transfer station building.

• **Analysis of Potential Odors from Outdoor and Indoor Cannabis Cultivation (Georgetown and Somerset, Eldorado County, CA)**

EPS is working cooperatively with growers and El Dorado County Planning Department to evaluate odors associated with indoor and outdoor cannabis cultivation. Through use of on-site odor measurements and dispersion models, EPS has been able to project intensity of future odors from new cannabis operations and demonstrate compliance with the County's Ordinance limiting odors at the property lines and at nearby homes.

• **Analysis and Control of Fugitive Dust and Odors from a Soil Blending Facility (Stockton, CA)**

Advanced computational fluid mechanics (CFD) models were used to predict the air flow and movement of fugitive dust at a soil blending facility. With this information, the client was able to install appropriate mitigation services to mitigate off-site migration of fugitive dust.

• **Review of Odor Control Systems for Cannabis Cultivation and Distribution Facilities (Palm Springs, CA)**

EPS evaluated the *odor* control system for over 15 different odor cultivation and distribution facilities in Palm Springs. The effectiveness of the proposed system was evaluated and recommendations were made to the City to Palm Springs.

Analysis of Public Health Risks

• **Analysis of Public Health Risks Associated with Composting Operations (Napa County, CA)**

Estimate the types and amounts of toxic air contaminants (TAC) released from green waste and food waste composting. An air dispersion model was used with local wind data to determine the concentration of each TAC. The concentration estimates were supplemented with toxicity data to quantify public health risks from exposure to the various toxic pollutants.

• **Analysis of Public Health Risks from Proposed Asphalt Plant (Kern County, California)**

Analyze emissions of any toxic air pollutants from a proposed 250 tons per day asphalt plant. Emissions from aggregate drying, propane combustion and asphalt oil were quantified. Acute and chronic public health risks from exposure to various toxic pollutants were calculated.

Ray Kapahi, DBA Environmental Permitting Specialists Current and Recent Projects

Project	Location	Description	Client
Preparation of Initial Sturt and mitigated negative declaration (ISMND) for Proposed Exploratory Oil and Gas Well	Kern County, CA	<ul style="list-style-type: none"> • Quantify air and greenhouse gas emissions from proposed exploratory oil and gas well • Assess the significance of air and GHG impacts • Recommend mitigation • Analyze public health risks 	RAB Consulting, Fairfield, CA
Analysis of Public Health Risks from Proposed Warehouse Buildings	Stockton, CA	<ul style="list-style-type: none"> • Evaluate public health risks associated with proposed 2.8 million square feet warehouse development on a 20 acre lot • Evaluate health impacts associated with construction and operational (occupancy) phases 	First Industrial Realty Trust, Chicago, IL
Review of Permit Application for a Biomass to Energy Plant	Mariposa County, CA	<ul style="list-style-type: none"> • Review permit application and associated support documents • Confirm emissions and use of best available control technology (BACT) • Recommend issuance or denial of air permits 	Amador Air District, Jackson, CS
Analysis of Air Quality and Public Health Risks from Proposed Mining, Aggregate, Concrete and Asphalt plant Complex	Newman (Stanislaus County), California	<ul style="list-style-type: none"> • Estimate emissions from various phases of mining, aggregate production, asphalt and concrete production and recycling plant • Determine significant of air quality and public health impacts 	Calaveras Building Materials, Fresno, CA
Preparation of CEQA Documents for Proposed Asphalt Plant	Delano (Kern County), CA	<ul style="list-style-type: none"> • Quantify air and greenhouse gas emissions from a proposed hot-mix drum type asphalt plant • Determine emission rates of toxic air pollutants • Prepare health risk assessment to quantify cancer risk to workers and the public from exposure to future toxic air emissions 	Jaxon Enterprises, Redding, CA

Project	Location	Description	Client
Air monitoring of Carbon Monoxide at a School Transportation Center	Sacramento, CA	<ul style="list-style-type: none"> • Monitor ambient concentrations of carbon monoxide (CO) at the school bus yard • Determine if CO concentration exceed the state 1-hour standard 	Law Firm Spinelli, Donald & Nott, Sacramento, CA
Impacts to Future Residents from Emissions from Vehicles and Trail Locomotives	Sacramento, CA	<ul style="list-style-type: none"> • Evaluate existing concentrations of toxic air contaminants at the proposed 150 unit housing development • Evaluate impacts from emissions from trucks and cars travelling along US Hwy 50 • Evaluate emissions from diesel train locomotives travelling along adjacent tracks 	EAH Housing, San Rafael, CA
Evaluation of Odors from Proposed Outdoor Cannabis Cultivation	Somerset and Georgetown (El Dorado County)	<ul style="list-style-type: none"> • Review plans for outdoor cannabis cultivation • Model future odor emissions to determine compliance with El Dorado County odor ordinance • Recommend odor mitigation (if required) 	Multiple Confidential Clients
Dispersion Modeling of Emissions from Proposed Lithium Extraction Plant	Salton Sea (Imperial County, CA)	<ul style="list-style-type: none"> • Calculate emissions from proposed emergency electric generator • Set-up modeling domain, source height and location and modeling grid • Calculate impacts from various criteria air pollutants 	RCH Group, Sacramento, CA
Analysis of Public Health Risks from Proposed Warehouse Land Use Development	Richmond, CA	<ul style="list-style-type: none"> • Evaluate public health risks associated with proposed 324,000 square feet warehouse development on a 30 acre lot • Evaluate health impacts associated with construction and operational (occupancy) phases 	Scannell Properties, Lafayette, CA

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Current Billing Rates – January 2023

Permitting of Stationary Sources -----\$175/hr
Air Quality Impact Analysis, CEQA Review -----\$175/hr
Expert Witness, Legal Testimony, Attend Hearings and
Legal Depositions ----- \$350/hr
All other work -----as negotiated

Expenses:

Expenses are billed at actual costs plus 10%. Personal Mileage charged at \$.55/mile.

Billing:

Billing is provided at the close of each month. Clients are invoiced within the first five business days following the end of the month.

Payment:

Payment terms are 30 days from receipt of invoice subject to prior arrangements. Late charges are assessed at 1.5% of past due balance monthly from payment due date.

EXHIBIT B

EXHIBIT B



Warehouse Projects: Best Practices and Mitigation Measures to Comply with the California Environmental Quality Act

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In carrying out its duty to enforce laws across California, the California Attorney General's Bureau of Environmental Justice (Bureau)¹ regularly reviews proposed warehouse projects for compliance with the California Environmental Quality Act (CEQA) and other laws. When necessary, the Bureau submits comment letters to lead agencies regarding warehouse projects, and in rare cases the Bureau has filed litigation to enforce CEQA.² This document builds upon the Bureau's work on warehouse projects, collecting information gained from the Bureau's review of hundreds of warehouse projects across the state.³ It is meant to help lead agencies pursue CEQA compliance and promote environmentally-just development as they confront warehouse project proposals.⁴ While CEQA analysis is necessarily project-specific, this document provides information on feasible best practices and mitigation measures, nearly all of which have been adapted from actual warehouse projects in California.

I. Background

In recent years, the proliferation of e-commerce and rising consumer expectations of rapid shipping have contributed to a boom in warehouse development.⁵ California, with its ports, population centers, and transportation network, has found itself at the center of this trend. In 2020, the Ports of Los Angeles, Long Beach, and Oakland collectively accounted for over 34% of all United States international container trade.⁶ The Ports of Los Angeles and Long Beach alone generate about 35,000 container truck trips every day.⁷ Accordingly, the South Coast Air Basin now contains approximately 3,000 warehouses of over 100,000 square feet each, with a total warehouse capacity of approximately 700 million square feet, an increase of 20 percent over the last five years.⁸ This trend has only accelerated, with e-commerce growing to

¹ <https://oag.ca.gov/environment/justice>.

² <https://oag.ca.gov/environment/ceqa>; *People of the State of California v. City of Fontana* (Super. Ct. San Bernardino County, No. CIVSB2121829); *South Central Neighbors United et al. v. City of Fresno et al.* (Super. Ct. Fresno County, No. 18CECG00690).

³ This September 2022 version revises and replaces the prior March 2021 version of this document.

⁴ Anyone reviewing this document to determine CEQA compliance responsibilities should consult their own attorney for legal advice.

⁵ As used in this document, "warehouse" or "logistics facility" is defined as a facility consisting of one or more buildings that stores cargo, goods, or products on a short- or long-term basis for later distribution to businesses and/or retail customers.

⁶ Data from the Bureau of Transportation Statistics, Container TEUs (Twenty-foot Equivalent Units) (2020), <https://data.bts.gov/stories/s/Container-TEU/x3fb-aeda/> (Ports of Los Angeles, Long Beach, and Oakland combined for 14.157 million TEUs, 34% of 41.24 million TEUs total nationwide) (last accessed September 18, 2022).

⁷ U.S. Dept. of Transportation, Federal Highway Administration, *FHWA Operations Support – Port Peak Pricing Program Evaluation* (2020), available at <https://ops.fhwa.dot.gov/publications/fhwahop09014/sect2.htm> (last accessed September 18, 2022).

⁸ South Coast Air Qual. Mgmt. Dist., *Final Socioeconomic Assessment for Proposed Rule 2305 – Warehouse Indirect Source Rule – Warehouse Actions and Investments to Reduce Emissions (WAIRE) Program and Proposed Rule 316 – Fees for Rule 2305*, at 7-8, 41 (May 2021).

13% of all retail sales and 2021 being a second consecutive record year for new warehouse space leased.⁹ The latest data and forecasts predict that the next wave of warehouse development will be in the Central Valley.¹⁰

When done properly, these activities can contribute to the economy and consumer welfare. However, imprudent warehouse development can harm local communities and the environment. Among other pollutants, diesel trucks visiting warehouses emit nitrogen oxide (NO_x)—a primary precursor to smog formation and a significant factor in the development of respiratory problems like asthma, bronchitis, and lung irritation—and diesel particulate matter (a subset of fine particulate matter that is smaller than 2.5 micrometers)—a contributor to cancer, heart disease, respiratory illnesses, and premature death.¹¹ Trucks and on-site loading activities can also be loud, bringing disruptive noise levels during 24/7 operation that can cause hearing damage after prolonged exposure.¹² The hundreds, and sometimes thousands, of daily truck and passenger car trips that warehouses generate contribute to traffic jams, deterioration of road surfaces, and traffic accidents.

These environmental impacts also tend to be concentrated in neighborhoods already suffering from disproportionate health impacts and systemic vulnerability. For example, a comprehensive study by the South Coast Air Quality Management District found that communities located near large warehouses scored far higher on California’s environmental justice screening tool, which measures overall pollution and demographic vulnerability.¹³ That

⁹ U.S. Census Bureau News, Quarterly Retail E-Commerce Sales 4th Quarter 2021 (February 22, 2022), https://www.census.gov/retail/mrts/www/data/pdf/ec_current.pdf (last accessed September 18, 2022); CBRE Research, *2022 North America Industrial Big Box Report: Review and Outlook*, at 2-3 (March 2022), available at <https://www.cbre.com/insights/reports/2022-north-america-industrial-big-box#download-report> (last accessed September 18, 2022).

¹⁰ CBRE Research, *supra* note 9, at 4, 36; New York Times, *Warehouses Are Headed to the Central Valley, Too* (Jul. 22, 2020), available at <https://www.nytimes.com/2020/07/22/us/coronavirus-ca-warehouse-workers.html>.

¹¹ California Air Resources Board, Nitrogen Dioxide & Health, <https://ww2.arb.ca.gov/resources/nitrogen-dioxide-and-health> (last accessed September 18, 2022) (NO_x); California Air Resources Board, Summary: Diesel Particulate Matter Health Impacts, <https://ww2.arb.ca.gov/resources/summary-diesel-particulate-matter-health-impacts> (last accessed September 18, 2022); Office of Environmental Health Hazard Assessment and American Lung Association of California, Health Effects of Diesel Exhaust, <https://oehha.ca.gov/media/downloads/calenviroscreen/indicators/diesel4-02.pdf> (last accessed September 18, 2022) (DPM).

¹² Noise Sources and Their Effects, <https://www.chem.purdue.edu/chemsafety/Training/PPETrain/dblevels.htm> (last accessed September 18, 2022) (a diesel truck moving 40 miles per hour, 50 feet away, produces 84 decibels of sound).

¹³ South Coast Air Quality Management District, “Final Socioeconomic Assessment for Proposed Rule 2305 – Warehouse Indirect Source Rule – Warehouse Actions and Investments to Reduce Emissions (WAIRE) Program and Proposed Rule 316 – Fees for Rule 2305” (May 2021), at 4-5.

study concluded that, compared to the South Coast Air Basin averages, communities in the South Coast Air Basin near large warehouses had a substantially higher proportion of people of color; were exposed to more diesel particulate matter; had higher rates of asthma, cardiovascular disease, and low birth weights; and had higher poverty and unemployment rates.¹⁴ Each area has its own unique history, but many of these impacts and vulnerabilities reflect historic redlining practices in these communities, which devalued land and concentrated poverty, racial outgroups, and pollution into designated areas.¹⁵

II. Proactive Planning: General Plans, Local Ordinances, and Good Neighbor Policies

To systematically guide warehouse development, we encourage local governing bodies to proactively plan for logistics projects in their jurisdictions. Proactive planning allows jurisdictions to prevent land use conflicts before they materialize and direct sustainable development. Benefits also include providing a predictable business environment, protecting residents from environmental harm, and setting consistent expectations jurisdiction-wide.

Proactive planning can take many forms. Land use designation and zoning decisions should channel development into appropriate areas. For example, establishing industrial districts near major highway and rail corridors but away from sensitive receptors¹⁶ can help attract investment while avoiding conflicts between warehouse facilities and residential communities. Transition zones with lighter industrial and commercial land uses may also help minimize conflicts between residential and industrial uses.

In addition, general plan policies, local ordinances, and good neighbor policies should set minimum standards for logistics projects. General plan policies can be incorporated into existing economic development, land use, circulation, or other related general plan elements. Many jurisdictions alternatively choose to consolidate policies in a separate environmental justice element. Adopting general plan policies to guide warehouse development may also help

¹⁴ *Id.* at 5-7.

¹⁵ Beginning in the 1930s, federal housing policy directed investment away from Black, immigrant, and working-class communities by color-coding neighborhoods according to the purported “riskiness” of loaning to their residents. In California cities where such “redlining” maps were drawn, nearly all of the communities where warehouses are now concentrated were formerly coded “red,” signifying the least desirable areas where investment was to be avoided. See University of Richmond Digital Scholarship Lab, Mapping Inequality, <https://dsl.richmond.edu/panorama/redlining/#loc=12/33.748/-118.272&city=los-angeles-ca> (Los Angeles), <https://dsl.richmond.edu/panorama/redlining/#loc=13/32.685/-117.132&city=san-diego-ca> (San Diego), <https://dsl.richmond.edu/panorama/redlining/#loc=11/37.81/-122.38&city=oakland-ca> (Oakland), <https://dsl.richmond.edu/panorama/redlining/#loc=13/37.956/-121.326&city=stockton-ca> (Stockton), <https://dsl.richmond.edu/panorama/redlining/#loc=12/36.751/-119.86&city=fresno-ca> (Fresno) (all last accessed September 18, 2022).

¹⁶ In this document, “sensitive receptors” refers to residences, schools, public recreation facilities, health care facilities, places of worship, daycare facilities, community centers, or incarceration facilities.

jurisdictions comply with their obligations under SB 1000, which requires local government general plans to identify objectives and policies to reduce health risks in disadvantaged communities, promote civil engagement in the public decision making process, and prioritize improvements and programs that address the needs of disadvantaged communities.¹⁷

Local ordinances and good neighbor policies that set development standards for all warehouses in the jurisdiction are a critical and increasingly common tool that serve several goals. When well-designed, these ordinances direct investment to local improvements, provide predictability for developers, conserve government resources by streamlining project review processes, and reduce the environmental impacts of industrial development. While many jurisdictions have adopted warehouse-specific development standards, an ordinance in the City of Fontana provides an example to review and build upon.¹⁸ Good neighbor policies in Riverside County and by the Western Riverside Council of Government include additional measures worth consideration.¹⁹

The Bureau encourages jurisdictions to adopt their own local ordinances that combine the strongest policies from those models with measures discussed in the remainder of this document.

III. Community Engagement

Early and consistent community engagement is central to establishing good relationships between communities, lead agencies, and warehouse developers and tenants. Robust community engagement can give lead agencies access to community residents' on-the-ground knowledge and information about their concerns, build community support for projects, and develop creative solutions to ensure new logistics facilities are mutually beneficial. Examples of best practices for community engagement include:

- Holding a series of community meetings at times and locations convenient to members of the affected community and incorporating suggestions into the project design.
- Posting information in hard copy in public gathering spaces and on a website about the project. The information should include a complete, accurate project description, maps and drawings of the project design, and information about how the public can provide input and be involved in the project approval process. The

¹⁷ For more information about SB 1000, see <https://oag.ca.gov/environment/sb1000>.

¹⁸ <https://oag.ca.gov/system/files/attachments/press-docs/Final%20Signed%20Fontana%20Ordinance.pdf> (last accessed September 18, 2022).

¹⁹ For example, the Riverside County policy requires community benefits agreements and supplemental funding contributions toward additional pollution offsets, and the Western Riverside Council of Governments policy sets a minimum buffer zone of 300 meters between warehouses and sensitive receptors. <https://www.rivcocob.org/wp-content/uploads/2020/01/Good-Neighbor-Policy-F-3-Final-Adopted.pdf> (last accessed September 18, 2022) (Riverside County); <http://www.wrcog.cog.ca.us/DocumentCenter/View/318/Good-Neighbor-Guidelines-for-Siting-Warehouse-Distribution-Facilities-PDF?bidId=> (last accessed September 18, 2022) (Western Riverside Council of Governments).

information should be in a format that is easy to navigate and understand for members of the affected community.

- Providing notice by mail to residents and schools within a certain radius of the project and along transportation corridors to be used by vehicles visiting the project, and by posting a prominent sign on the project site. The notice should include a brief project description and directions for accessing complete information about the project and for providing input on the project.
- Providing translation or interpretation in residents' native language, where appropriate.
- For public meetings broadcast online or otherwise held remotely, providing for access and public comment by telephone and supplying instructions for access and public comment with ample lead time prior to the meeting.
- Partnering with local community-based organizations to solicit feedback, leverage local networks, co-host meetings, and build support.
- Considering adoption of a community benefits agreement, negotiated with input from affected residents and businesses, by which the developer provides benefits to the affected community.
- Creating a community advisory board made up of local residents to review and provide feedback on project proposals in early planning stages.
- Identifying a person to act as a community liaison concerning on-site construction activity and operations, and providing contact information for the community liaison to the surrounding community.
- Requiring signage in public view at warehouse facilities with contact information for a local designated representative for the facility operator who can receive community complaints, and requiring any complaints to be answered by the facility operator within 48 hours of receipt.

IV. Warehouse Siting and Design Considerations

The most important consideration when planning a logistics facility is its location. Warehouses located in residential neighborhoods or near sensitive receptors expose community residents and those using or visiting sensitive receptor sites to the air pollution, noise, traffic, and other environmental impacts they generate. Therefore, placing facilities away from sensitive receptors significantly reduces their environmental and quality of life harms on local communities. The suggested best practices for siting and design of warehouse facilities does not relieve lead agencies' responsibility under CEQA to conduct a project-specific analysis of the project's impacts and evaluation of feasible mitigation measures and alternatives; lead agencies' incorporation of the best practices must be part of the impact, mitigation and alternatives analyses to meet the requirements of CEQA. Examples of best practices when siting and designing warehouse facilities include:

- Per California Air Resources Board (CARB) guidance, siting warehouse facilities so that their property lines are at least 1,000 feet from the property lines of the nearest sensitive receptors.²⁰
- Providing adequate amounts of on-site parking to prevent trucks and other vehicles from parking or idling on public streets and to reduce demand for off-site truck yards.
- Establishing setbacks from the property line of the nearest sensitive receptor to warehouse dock doors, loading areas, and truck drive aisles, and locating warehouse dock doors, loading areas, and truck drive aisles on the opposite side of the building from the nearest sensitive receptors—e.g., placing dock doors on the north side of the facility if sensitive receptors are near the south side of the facility.
- Placing facility entry and exit points from the public street away from sensitive receptors—e.g., placing these points on the north side of the facility if sensitive receptors are adjacent to the south side of the facility.
- Ensuring heavy duty trucks abide by the on-site circulation plans by constructing physical barriers to block those trucks from using areas of the project site restricted to light duty vehicles or emergency vehicles only.
- Preventing truck queuing spillover onto surrounding streets by positioning entry gates after a minimum of 140 feet of space for queuing, and increasing the distance by 70 feet for every 20 loading docks beyond 50 docks.
- Locating facility entry and exit points on streets of higher commercial classification that are designed to accommodate heavy duty truck usage.
- Screening the warehouse site perimeter and onsite areas with significant truck traffic (e.g., dock doors and drive aisles) by creating physical, structural, and/or vegetative buffers that prevent or substantially reduce pollutant and noise dispersion from the facility to sensitive receptors.
- Planting exclusively 36-inch box evergreen trees to ensure faster maturity and four-season foliage.
- Requiring all property owners and successors in interest to maintain onsite trees and vegetation for the duration of ownership, including replacing any dead or unhealthy trees and vegetation.
- Posting signs clearly showing the designated entry and exit points from the public street for trucks and service vehicles.
- Including signs and drive aisle pavement markings that clearly identify onsite circulation patterns to minimize unnecessary onsite vehicle travel.
- Posting signs indicating that all parking and maintenance of trucks must be conducted within designated on-site areas and not within the surrounding community or public streets.

²⁰ CARB, Air Quality and Land Use Handbook: A Community Health Perspective (April 2005), at ES-1. CARB staff has released draft updates to this siting and design guidance which suggests a greater distance may be warranted in some scenarios. CARB, Concept Paper for the Freight Handbook (December 2019), available at https://ww2.arb.ca.gov/sites/default/files/2020-03/2019.12.12%20-%20Concept%20Paper%20for%20the%20Freight%20Handbook_1.pdf (last accessed September 18, 2022).

V. Air Quality and Greenhouse Gas Emissions Analysis and Mitigation

Emissions of air pollutants and greenhouse gases are often among the most substantial environmental impacts from new warehouse facilities. CEQA compliance demands a proper accounting of the full air quality and greenhouse gas impacts of logistics facilities and adoption of all feasible mitigation of significant impacts. Although efforts by CARB and other authorities to regulate the heavy-duty truck and off-road diesel fleets have made excellent progress in reducing the air quality impacts of logistics facilities, the opportunity remains for local jurisdictions to further mitigate these impacts at the project level. Lead agencies and developers should also consider designing projects with their long-term viability in mind. Constructing the necessary infrastructure to prepare for the zero-emission future of goods movement not only reduces a facility's emissions and local impact now, but it can also save money as demand for zero-emission infrastructure grows. In planning new logistics facilities, the Bureau strongly encourages developers to consider the local, statewide, and global impacts of their projects' emissions.

Examples of best practices when studying air quality and greenhouse gas impacts include:

- Fully analyzing all reasonably foreseeable project impacts, including cumulative impacts. In general, new warehouse developments are not ministerial under CEQA because they involve public officials' personal judgment as to the wisdom or manner of carrying out the project, even when warehouses are permitted by a site's applicable zoning and/or general plan land use designation.²¹
- When analyzing cumulative impacts, thoroughly considering the project's incremental impact in combination with past, present, and reasonably foreseeable future projects, even if the project's individual impacts alone do not exceed the applicable significance thresholds.
- Preparing a quantitative air quality study in accordance with local air district guidelines.
- Preparing a quantitative health risk assessment in accordance with California Office of Environmental Health Hazard Assessment and local air district guidelines.
- Refraining from labeling compliance with CARB or air district regulations as a mitigation measure—compliance with applicable regulations is required regardless of CEQA.
- Disclosing air pollution from the entire expected length of truck trips. CEQA requires full public disclosure of a project's anticipated truck trips, which entails calculating truck trip length based on likely truck trip destinations, rather than the distance from the facility to the edge of the air basin, local jurisdiction, or other truncated endpoint. All air pollution associated with the project must be considered, regardless of where those impacts occur.

²¹ CEQA Guidelines § 15369.

- Accounting for all reasonably foreseeable greenhouse gas emissions from the project, without discounting projected emissions based on participation in California’s Cap-and-Trade Program.

Examples of measures to mitigate air quality and greenhouse gas impacts from construction are below. To ensure mitigation measures are enforceable and effective, they should be imposed as permit conditions on the project where applicable.

- Requiring off-road construction equipment to be hybrid electric-diesel or zero-emission, where available, and all diesel-fueled off-road construction equipment to be equipped with CARB Tier IV-compliant engines or better, and including this requirement in applicable bid documents, purchase orders, and contracts, with successful contractors demonstrating the ability to supply the compliant construction equipment for use prior to any ground-disturbing and construction activities.
- Prohibiting off-road diesel-powered equipment from being in the “on” position for more than 10 hours per day.
- Using electric-powered hand tools, forklifts, and pressure washers, and providing electrical hook ups to the power grid rather than use of diesel-fueled generators to supply their power.
- Designating an area in the construction site where electric-powered construction vehicles and equipment can charge.
- Limiting the amount of daily grading disturbance area.
- Prohibiting grading on days with an Air Quality Index forecast of greater than 100 for particulates or ozone for the project area.
- Forbidding idling of heavy equipment for more than three minutes.
- Keeping onsite and furnishing to the lead agency or other regulators upon request, all equipment maintenance records and data sheets, including design specifications and emission control tier classifications.
- Conducting an on-site inspection to verify compliance with construction mitigation and to identify other opportunities to further reduce construction impacts.
- Using paints, architectural coatings, and industrial maintenance coatings that have volatile organic compound levels of less than 10 g/L.
- Providing information on transit and ridesharing programs and services to construction employees.
- Providing meal options onsite or shuttles between the facility and nearby meal destinations for construction employees.

Examples of measures to mitigate air quality and greenhouse gas impacts from operation include:

- Requiring all heavy-duty vehicles engaged in drayage²² to or from the project site to be zero-emission beginning in 2030.

²² “Drayage” refers generally to transport of cargo to or from a seaport or intermodal railyard.

- Requiring all on-site motorized operational equipment, such as forklifts and yard trucks, to be zero-emission with the necessary charging or fueling stations provided.
- Requiring tenants to use zero-emission light- and medium-duty vehicles as part of business operations.
- Forbidding trucks from idling for more than three minutes and requiring operators to turn off engines when not in use.
- Posting both interior- and exterior-facing signs, including signs directed at all dock and delivery areas, identifying idling restrictions and contact information to report violations to CARB, the local air district, and the building manager.
- Installing solar photovoltaic systems on the project site of a specified electrical generation capacity that is equal to or greater than the building's projected energy needs, including all electrical chargers.
- Designing all project building roofs to accommodate the maximum future coverage of solar panels and installing the maximum solar power generation capacity feasible.
- Constructing zero-emission truck charging/fueling stations proportional to the number of dock doors at the project.
- Running conduit to designated locations for future electric truck charging stations.
- Unless the owner of the facility records a covenant on the title of the underlying property ensuring that the property cannot be used to provide refrigerated warehouse space, constructing electric plugs for electric transport refrigeration units at every dock door and requiring truck operators with transport refrigeration units to use the electric plugs when at loading docks.
- Oversizing electrical rooms by 25 percent or providing a secondary electrical room to accommodate future expansion of electric vehicle charging capability.
- Constructing and maintaining electric light-duty vehicle charging stations proportional to the number of employee parking spaces (for example, requiring at least 10% of all employee parking spaces to be equipped with electric vehicle charging stations of at least Level 2 charging performance)
- Running conduit to an additional proportion of employee parking spaces for a future increase in the number of electric light-duty charging stations.
- Installing and maintaining, at the manufacturer's recommended maintenance intervals, air filtration systems at sensitive receptors within a certain radius of facility for the life of the project.
- Installing and maintaining, at the manufacturer's recommended maintenance intervals, an air monitoring station proximate to sensitive receptors and the facility for the life of the project, and making the resulting data publicly available in real time. While air monitoring does not mitigate the air quality or greenhouse gas impacts of a facility, it nonetheless benefits the affected community by providing information that can be used to improve air quality or avoid exposure to unhealthy air.
- Requiring all stand-by emergency generators to be powered by a non-diesel fuel.
- Requiring facility operators to train managers and employees on efficient scheduling and load management to eliminate unnecessary queuing and idling of

trucks.

- Requiring operators to establish and promote a rideshare program that discourages single-occupancy vehicle trips and provides financial incentives for alternate modes of transportation, including carpooling, public transit, and biking.
- Meeting CalGreen Tier 2 green building standards, including all provisions related to designated parking for clean air vehicles, electric vehicle charging, and bicycle parking.
- Designing to LEED green building certification standards.
- Providing meal options onsite or shuttles between the facility and nearby meal destinations.
- Posting signs at every truck exit driveway providing directional information to the truck route.
- Improving and maintaining vegetation and tree canopy for residents in and around the project area.
- Requiring that every tenant train its staff in charge of keeping vehicle records in diesel technologies and compliance with CARB regulations, by attending CARB-approved courses. Also require facility operators to maintain records on-site demonstrating compliance and make records available for inspection by the local jurisdiction, air district, and state upon request.
- Requiring tenants to enroll in the United States Environmental Protection Agency's SmartWay program, and requiring tenants who own, operate, or hire trucking carriers with more than 100 trucks to use carriers that are SmartWay carriers.
- Providing tenants with information on incentive programs, such as the Carl Moyer Program and Voucher Incentive Program, to upgrade their fleets.

VI. Noise Impacts Analysis and Mitigation

The noise associated with logistics facilities can be among their most intrusive impacts to nearby sensitive receptors. Various sources, such as unloading activity, diesel truck movement, and rooftop air conditioning units, can contribute substantial noise pollution. These impacts are exacerbated by logistics facilities' typical 24-hour, seven-days-per-week operation. Construction noise is often even greater than operational noise, so if a project site is near sensitive receptors, developers and lead agencies should adopt measures to reduce the noise generated by both construction and operation activities.

Examples of best practices when studying noise impacts include:

- Preparing a noise impact analysis that considers all reasonably foreseeable project noise impacts, including to nearby sensitive receptors. All reasonably foreseeable project noise impacts encompasses noise from both construction and operations, including stationary, on-site, and off-site noise sources.
- Adopting a lower significance threshold for incremental noise increases when baseline noise already exceeds total noise significance thresholds, to account for the cumulative impact of additional noise and the fact that, as noise moves up the decibel scale, each decibel increase is a progressively greater increase in sound

pressure than the last. For example, 70 dBA is ten times more sound pressure than 60 dBA.

- Disclosing and considering the significance of short-term noise levels associated with all aspects of project operation (i.e. both on-site noise generation and off-site truck noise). Considering only average noise levels may mask noise impacts sensitive receptors would consider significant—for example, the repeated but short-lived passing of individual trucks or loading activities at night.

Examples of measures to mitigate noise impacts include:

- Constructing physical, structural, or vegetative noise barriers on and/or off the project site.
- Planning and enforcing truck routes that avoid passing sensitive receptors.
- Locating or parking all stationary construction equipment as far from sensitive receptors as possible, and directing emitted noise away from sensitive receptors.
- Verifying that construction equipment has properly operating and maintained mufflers.
- Requiring all combustion-powered construction equipment to be surrounded by a noise protection barrier
- Limiting operation hours to daytime hours on weekdays.
- Paving roads where truck traffic is anticipated with low noise asphalt.
- Orienting any public address systems onsite away from sensitive receptors and setting system volume at a level not readily audible past the property line.

VII. Traffic Impacts Analysis and Mitigation

Warehouse facilities inevitably bring truck and passenger car traffic. Truck traffic can present substantial safety issues. Collisions with heavy-duty trucks are especially dangerous for passenger cars, motorcycles, bicycles, and pedestrians. These concerns can be even greater if truck traffic passes through residential areas, school zones, or other places where pedestrians are common and extra caution is warranted.

Examples of measures to mitigate traffic impacts include:

- Designing, clearly marking, and enforcing truck routes that keep trucks out of residential neighborhoods and away from other sensitive receptors.
- Installing signs in residential areas noting that truck and employee parking is prohibited.
- Requiring preparation and approval of a truck routing plan describing the facility's hours of operation, types of items to be stored, and truck routing to and from the facility to designated truck routes that avoids passing sensitive receptors. The plan should include measures for preventing truck queuing, circling, stopping, and parking on public streets, such as signage, pavement markings, and queuing analysis and enforcement. The plan should hold facility operators responsible for violations of the truck routing plan, and a revised plan should be required from any new tenant that occupies the property before a business license

is issued. The approving agency should retain discretion to determine if changes to the plan are necessary, including any additional measures to alleviate truck routing and parking issues that may arise during the life of the facility.

- Constructing new or improved transit stops, sidewalks, bicycle lanes, and crosswalks, with special attention to ensuring safe routes to schools.
- Consulting with the local public transit agency and securing increased public transit service to the project area.
- Designating areas for employee pickup and drop-off.
- Implementing traffic control and safety measures, such as speed bumps, speed limits, or new traffic signs or signals.
- Placing facility entry and exit points on major streets that do not have adjacent sensitive receptors.
- Restricting the turns trucks can make entering and exiting the facility to route trucks away from sensitive receptors.
- Constructing roadway improvements to improve traffic flow.
- Preparing a construction traffic control plan prior to grading, detailing the locations of equipment staging areas, material stockpiles, proposed road closures, and hours of construction operations, and designing the plan to minimize impacts to roads frequented by passenger cars, pedestrians, bicyclists, and other non-truck traffic.

VIII. Other Significant Environmental Impacts Analysis and Mitigation

Warehouse projects may result in significant environmental impacts to other resources, such as to aesthetics, cultural resources, energy, geology, or hazardous materials. All significant adverse environmental impacts must be evaluated, disclosed and mitigated to the extent feasible under CEQA. Examples of best practices and mitigation measures to reduce environmental impacts that do not fall under any of the above categories include:

- Appointing a compliance officer who is responsible for implementing all mitigation measures, and providing contact information for the compliance officer to the lead agency, to be updated annually.
- Creating a fund to mitigate impacts on affected residents, schools, places of worship, and other community institutions by retrofitting their property. For example, retaining a contractor to retrofit/install HVAC and/or air filtration systems, doors, dual-paned windows, and sound- and vibration-deadening insulation and curtains.
- Sweeping surrounding streets on a daily basis during construction to remove any construction-related debris and dirt.
- Directing all lighting at the facility into the interior of the site.
- Using full cut-off light shields and/or anti-glare lighting.
- Requiring submission of a property maintenance program for agency review and approval providing for the regular maintenance of all building structures, landscaping, and paved surfaces.
- Using cool pavement to reduce heat island effects.

- Planting trees in parking areas to provide at least 35% shade cover of parking areas within fifteen years to reduce heat island impacts.
- Using light colored roofing materials with a solar reflective index of 78 or greater.
- Including on-site amenities, such as a truck operator lounge with restrooms, vending machines, and air conditioning, to reduce the need for truck operators to idle or travel offsite.
- Designing skylights to provide natural light to interior worker areas.
- Installing climate control and air filtration in the warehouse facility to promote worker well-being.

IX. Conclusion

California's world-class economy, ports, and transportation network position it at the center of the e-commerce and logistics industry boom. At the same time, California is a global leader in environmental protection and environmentally just development. The guidance in this document furthers these dual strengths, ensuring that all can access the benefits of economic development. The Bureau will continue to monitor proposed projects for compliance with CEQA and other laws. Lead agencies, developers, community advocates, and other interested parties should feel free to reach out to us as they consider how to guide warehouse development in their area.

Please do not hesitate to contact the Environmental Justice Bureau at ej@doj.ca.gov if you have any questions.

EXHIBIT C

EXHIBIT C

Eddie Jones Industrial Redevelopment Local Transportation Study and Vehicle Miles Traveled Peer Review Report

Prepared for

Mr. Justin Floyd
Wanis View Estates/Eddie Jones GoFundMe
c/o Avalon Management
3618 Ocean Ranch Blvd
Oceanside, CA 92056

Prepared by



December 26, 2023



1 ABBREVIATIONS

Abbreviation	Meaning
ADT	Average Daily Traffic
CEQA	California Environmental Quality Act
COO	City of Oceanside
DEIR	Draft Environmental Impact Report
HCM	Highway Capacity Manual
ITE	Institute of Transportation Engineers
LOS	Level of Service
LTS	Local Transportation Study
NHTSA	National Highway Traffic Safety Administration
PCE	Passenger Car Equivalent
PE	Professional Engineer
PTOE	Professional Traffic Operations Engineer
SF	Square Feet
VMT	Vehicle Miles Traveled

2 SCOPE AND PROCESS

Category	Findings
Scope	The scope of work consisted of: <ul style="list-style-type: none"> Review of the Eddie Jones Industrial Redevelopment Project (Project) LTS and VMT Analysis. Preparation of peer review traffic engineering comments.
Traffic Impact Analysis Review Process and Limitations	This report is based on significant and relevant findings obtained during a review of Project TIS by Scott Kelley, PE, PTOE of Greenlight Traffic Engineering, LLC (Reviewer). This report has been developed in accordance with the preparer's understanding of the Project details, as well as the preparer's professional experience related to preparing and reviewing traffic and transportation studies. In addition, all review findings are based on available and provided documentation.
Examined Documents	The following documents were examined as part of this report: <ul style="list-style-type: none"> Eddie Jones Industrial Redevelopment Project Draft LTS (LOS Engineering Inc., August 2023) Eddie Jones Industrial Redevelopment Project Draft VMT (LOS Engineering Inc., April 2022)

3 PROPOSED PROJECT

Category	Findings
Project Developer	Eddie Jones Industrial
Project Traffic Engineer	LOS Engineering Inc.
Project Description	Proposed redevelopment of Manufacturing and Warehousing facility.
Project Location	250 Eddie Jones Way in Oceanside, California.
Project Access	The Project will access be accessed via driveways on Benet Road and Alex Road.
Project Jurisdiction(s)	All study area streets are under jurisdiction of COO.

4 EXECUTIVE SUMMARY

Greenlight Traffic Engineering completed a review of the Eddie Jones Industrial Redevelopment Project Draft Local Transportation Study and Draft Vehicle Miles Traveled documents prepared by LOS Engineering Inc. in August 2023 and April 2022, respectively.

The proposed project is a mix of approximately 30% manufacturing and 70% warehousing with a total of up to 568,000 SF of building space. The project will replace an existing manufacturing business with 172,305 SF resulting in a net building increase of approximately 395,695 SF.

The LTS report provides a non-CEQA analysis as required by the City of Oceanside. The VMT report determines if there is a potential CEQA VMT transportation impact.

The Draft VMT Analysis appears to be consistent with applicable standards and procedures; therefore, there are no comments pertaining to the Draft VMT Analysis. However, we have identified concerns with several assumptions within the LTS analysis. A summary of those items are detailed below.

4.1 Trip Assignment

Within the DEIR, the Project Description section, ES.2.2, notes the restriction of the Alex Road access to passenger vehicles only, while Benet Road is to be used by heavy truck traffic. However, the LTS does not clearly identify the trip assignment and distribution for both passenger vehicle and truck traffic through the transportation network to confirm that the modeling and analysis results are consistent with the assumptions in ES.2.2.

4.2 Passenger Car Equivalent

The PCE, as defined in Exhibit 12-25 of the Highway Capacity Manual (HCM) 6th Edition, is formulated for Basic Freeway and Multilane Highway Segments marked by continuous traffic flow. In the context of the Project, the arterial and collector roads nearby experience interrupted traffic flow due to traffic signals along the corridors. Additionally, the warehousing use is expected to see primarily large trucks. Therefore, a higher PCE should be used to more adequately represent the future capacity and Project impacts on the roadway network within the study area.

Impact of Trucks on Signalized Intersection Capacity (Washburn and Cuz-Casas, June 2010) indicates that the PCE estimated in the HCM is low. Per the study's abstract, "The PCE values determined from this study are 1.8, 2.2, and 2.8 for small, medium, and large trucks, respectively. A model for estimating startup lost time based on the same small, medium, and large truck classifications was also developed."

In conclusion, the utilization of the PCE as outlined in the HCM 6th Edition may not fully capture the intricacies of traffic dynamics in the study area. Considering the projected increase in large trucks associated with warehousing use, the need for a higher PCE is emphasized to more accurately represent the future capacity and potential impacts of the Project on the roadway network.

4.3 Project Trip Generation

It is recommended to employ the fitted curve equation for car trips related to Land Use Code 140: Manufacturing. This is because this equation has the capability to accurately represent the increased traffic volumes associated with this particular land use category.

4.4 Heavy Vehicle Percentage

In the analysis, a default heavy vehicle factor of 2% is utilized. The area within the Project vicinity will experience an increase in heavy vehicle traffic due to the Project and additional planned developments. The increase in other project truck trips should be considered at the study intersections to accurately evaluate the impact of the Project on the study area roadway network.

4.5 Traffic Safety

With planned development, the study area vicinity is expected to see a significant increase in large truck traffic within the coming years. The study does not address the impact of the increased safety risk of mixing greater percentages of heavy truck traffic with passenger car traffic. According to NHTSA Traffic Safety Facts (April 2022), "In 2020 large trucks accounted for 9 percent of all vehicles involved in fatal crashes and 5 percent of all vehicles involved in injury and property-damage-only crashes. Large trucks accounted for 5 percent of all registered vehicles ..." NHTSA Traffic Safety Facts (April 2022). Large trucks represent 5% of the vehicle population; however, they are involved in 9% of fatal crashes. Therefore, large trucks are nearly twice as likely to be involved in fatal crashes compared to passenger vehicles.

According to *Trucks a significant cause of severe accidents, study finds* (Taylor & Francis, December 5, 2013), trucks account for 8% of highway traffic, but have a disproportionate impact on fatal road crashes, contributing to 11% of such incidents. A study published in the International Journal of Injury Control and Safety Promotion revealed that a mere 1% increase in truck volume significantly raises the probability of severe crashes, emphasizing the concerning correlation between higher truck traffic and elevated crash risks.

According to a blog published in Omnitracs' Road Ahead Blog, *The Unacceptable Increase in Truck-Involved Crashes and Fatalities* on August 24, 2022, by Don Osterberg (Safety Advisor, Omnitracs), truck crash fatalities increased by 48% from 2009 to 2019, with nearly 5,000 deaths in 2020. Despite a 32% rise in truck miles driven, fatalities rose by 48%. The data suggests a need for improved safety measures as truck-involved crash injuries soared by 115% in 2020. Analysis reveals that a significant percentage of crashes occur on non-interstate roads, with 27% at four-way intersections and 57% on rural roads. Truck drivers, despite lower alcohol involvement, had a higher rate of previous crashes (23%).

In conclusion, the impending growth in large truck traffic, as anticipated with planned development in the study area, raises serious concerns about road safety. The LTS omits an examination of the heightened safety risk arising from increased interaction between heavy trucks and passenger cars.

In addition, the City has not adequately planned for emergency evacuation in the event of a wildfire, nor has the developer identified how this risk will be mitigated with the increase in traffic and congestion. The LTS has not addressed this risk. The rise in truck traffic poses the risk of increased congestion during emergency situations, potentially impeding the safe and timely evacuation of residents in the vicinity.

5 SUMMARY OF TECHNICAL COMMENTS

Item No.	Sheet	Comment
Eddie Jones Industrial Redevelopment Project Draft LTS (LOS Engineering Inc., August 2023)		
1.	General	The section ES.2.2 Project Description of the Draft Environmental Impact Report (DEIR) reports states the following: "The Alex Road access would be limited to passenger vehicles, and heavy truck traffic would be limited to the Benet Road access point." However, the LTS overlooks the inclusion of this information, lacking distinct trip assignment and distribution for truck and vehicle traffic.
2.	General	The Passenger Car Equivalent (PCE), as determined by Exhibit 12-25 in the Highway Capacity Manual (HCM) 6 th Edition, is designed for Basic Freeway and Multilane Highway Segments characterized by uninterrupted traffic flow. In the context of the Project, the arterial and collector roads in the vicinity exhibit interrupted traffic flow due to the ingress and egress associated with the development. Consequently, a higher PCE should be considered to accurately account for the traffic conditions on these roads surrounding the Project.
3.	General	The conditions of Existing plus Project, Near Term plus Project, and Horizon Year 2030 plus Project require revision following the updates based on the provided comments. This is essential to accurately depict the Level of Service (LOS) results and the impacts of the project on the surrounding area.
4.	17	Project Traffic Generation Calculation of Project-generated car trips using the ITE Trip Generation Manual Land Use Code 140: Manufacturing should utilized the fitted curve equations.

Item No.	Sheet	Comment
5.	17	Project Traffic Generation For the analysis, a default heavy vehicle factor of 2 is employed. Given that the Project, along with additional background developments, is anticipated to augment heavy vehicle traffic in the study area, it is imperative to apply a suitable heavy vehicle factor. This adjustment is necessary to accurately assess the impact of the Project on the surrounding area in terms of increased truck traffic.
6.	18-19	Figures 8-9 The trip distribution, trip assignment, and related analysis results need to be updated to accurately reflect vehicle access via Alex Road and truck traffic access via Benet Road.

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

3116 Toopal Drive

			⏏	⏏
<small>name</small>	<small>signature</small>	<small>date</small>		

3120 Toopal Drive

Jose Sanchez		2-11-25	⏏	⏏
<small>name</small>	<small>signature</small>	<small>date</small>		

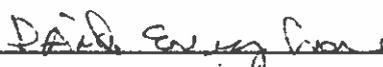
3124 Toopal Drive

Vy Ngo		2/15/25	⏏	☑
<small>name</small>	<small>signature</small>	<small>date</small>		

3128 Toopal Drive

Nicholas Arcantor		2/11/25	⏏	⏏
<small>name</small>	<small>signature</small>	<small>date</small>		

3132 Toopal Drive

David Evazohan		2-11-25	⏏	☑
<small>name</small>	<small>signature</small>	<small>date</small>		

3136 Toopal Drive

			⏏	⏏
<small>name</small>	<small>signature</small>	<small>date</small>		

3140 Toopal Drive

Bethany Storz		2/11/2025	⏏	☑
<small>name</small>	<small>signature</small>	<small>date</small>		

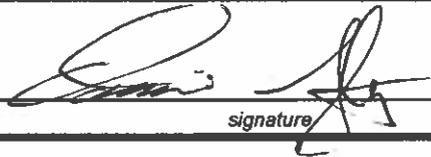
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Tenant
Homeowner

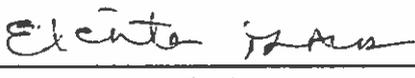
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3140 TOOPAL DRIVE

DAMIAN STORZ |  | 2/11/25

name | signature | date

address: 3132 TOOPAL DR

Elento Jhan |  | 2-11-25

name | signature | date

address: 3120 Toopal DR

 | 2-11-25

name | signature | date

address: 3120 Toopal Dr

 | 2-11-25

name | signature | date

address:

name | signature | date

address:

name | signature | date

address:

name | signature | date

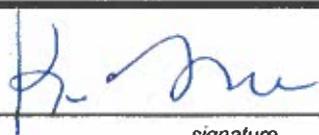
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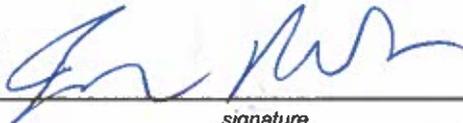
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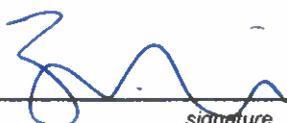
Tenant
Homeowner

3152
3144 Toopal Drive
 Kristina Dumbek |  | 2-15-25
name signature date

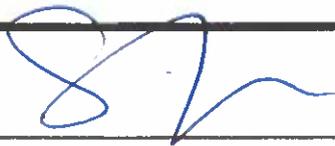
3152
3148 Toopal Drive
 Jason Dumbek |  | 2-15-25
name signature date

3148
3152 Toopal Drive
 Ki Gina Margiotta |  | 2-16-25
name signature date

3156 Toopal Drive
 Jim Kordeval |  |
name signature date

3160 Toopal Drive
 Erin Kleffisch |  | 2/16/25
name signature date

3164 Toopal Drive
 Eeva Plati |  | 2-15-25
name signature date

3168 Toopal Drive
 Shonell Hunter |  | 2/15/25
name signature date

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Tenant
Homeowner

address: 3164 Toopal Dr
Bailey Platt
name signature date 2/15/25

address: 3156 TOOPAL DR
CANDICE WALTER
name signature date 2/14/25

address: 3144 TOOPA DR
Tuyen Nguyen
name signature date 2/16/25

address: 3144 TOOPAL DR
CATHERINE NGUYEN
name signature date 2/16/25

address: 3148 Toopal Dr.
Vince Margiotta
name signature date 2/16/25

address: 3160 Toopal Dr.
Elisabeth Nickerson
name signature date 2/16/25

address: 3160 TOOPAL DR
JOE KLEEMER
name signature date 2/16/25

PG. 2
CONT.

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

3172 Toopal Drive

ERIK HANSEN

name

[Signature]

signature

11 FEB 25

date

3176 Toopal Drive

[Signature]

name

BRAD BURGESS

signature

11 Feb 25

date

3180 Toopal Drive

[Signature]

name

[Signature]

signature

2/14/25

date

3184 Toopal Drive

[Signature] Erin Garcia

name

signature

2/13/25

date

3188 Toopal Drive

ANTHONY PICCIOLA

name

[Signature]

signature

2/11/25

date

3192 Toopal Drive

DAVID KERTENIAN

name

[Signature]

signature

2/11/25

date

3196 Toopal Drive

PATRICIA FRAZIER

name

[Signature]

signature

2/11/25

date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

address: 3172 Toopal Dr.
 Carol McConnell | *Carol McConnell* | 2/1/25

name signature date

address: 3176 TOOPAL DR.
 SOPHIE BURGNER | *[Signature]* | 2/11/25

name signature date

address: 3192 Toopal Dr.
[Signature] | *[Signature]* | 2-11-25

name signature date

address: 3180 Toopal Drive
 VIRGINIA name MAYFIELD | *[Signature]* | 2/14/25

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

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Tenant
Homeowner

3135 Toopal Drive

Joshua Ong
name

[Signature]
signature

2/16/25
date

3139 Toopal Drive

name

signature

date

3143 Toopal Drive

Kim Chambers
name

[Signature]
signature

2/15/25
date

3147 Toopal Drive

Dana Martinez
name

[Signature]
signature

2/12/25
date

479 Tishmal Court

Chris Eichner
name

[Signature]
signature

02/12/25
date

475 Tishmal Court

Dawn C. Noakes
name

[Signature]
signature

2/12/25
date

471 Tishmal Court

BRETCHEN GARY
name

[Signature]
signature

02/12/25
date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 479 TISHMAL CT
 Juliet Eichner |  | 02/12/25

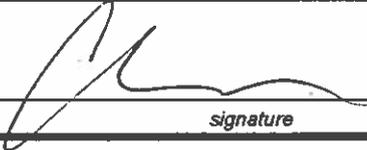
name signature date

address: 471 TISHMAL COURT
 JOHN Bucalo |  | 02/12/25

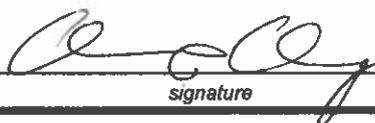
name signature date

address: 475 TISHMAL CT
 MARK NORRES |  | 2/15/25

name signature date

address: 3143 Toopal Dr.
 Andy Chambers |  | 2/15/25

name signature date

address: 3135 Toopal Dr
 Ann Ong |  | 2/15/25

name signature date

address: 471 TISHMAL COURT
 Daniela Bucalo | Daniela Bucalo | 2/10/25

name signature date

address: _____
 _____ | _____ | _____

name signature date

Page 4
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

470 Tishmal Court

LARRY D. MORROW | Larry D. Morrow | 2-12-2025

name signature date

474 Tishmal Court

Ryan Reyes | Ryan Reyes | 2/8/25

name signature date

478 Tishmal Court

Gene R. O'NEAL JR | [Signature] | 2-15-25

name signature date

3173 Toopal Drive

Daniela Hernandez | Daniela Hgz | 2.12.25

name signature date

3177 Toopal Drive

Cindi Endsley | Cindi Endsley | 2/17/25

name signature date

3181 Toopal Drive

Renee Puckett | Renee Puckett | 02-15-25

name signature date

3185 Toopal Drive

Jeanette Simon | Jeanette Simon | 2-15-25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: Cindy Morrow
470 Tishmal Ct. | Cindy Morrow | 2/12/25

name signature date

address: Daisy Thonnard
3173 Topal Drive | Daisy Thonnard | 2/12/25

name signature date

address: Daisy
3173 Topal dr | [Signature] | 2/14/25

name signature date

address: 3185 Topal Dr
James Simon | [Signature] | 2-15-25

name signature date

address: Candice Neal
478 Tishmal court
Carrero Neal | Candice Neal | 2-15-25

name signature date

address: 474 Tishmal Ct
Saron Reyes | [Signature] | 2/15/25

name signature date

address: 3177 Topal Dr.
Ray Endsley | [Signature] | 2/17/25

name signature date

Pg 5
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3177 Toopal Drive 92058

Kathryn Endsley | *Kathryn Endsley* | 2/16/25

name signature date

address: 3177 Toopal Drive 92058

Kaycee Endsley | *Kaycee Endsley* | 2/16/25

name signature date

address:

name signature date

Page 5
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

3189 Toopal Drive
 name: Julia Caballero | signature: [Signature] | date: 2/15/25

3193 Toopal Drive
 name: Minh Pham | signature: [Signature] | date: 2/11/25

3195 Toopal Drive
 name: MABEL ARANGO | signature: Mabel Arango | date: 2-11-25

3199 Toopal Drive
 name: _____ | signature: _____ | date: _____

3203 Toopal Drive
 name: ROBERTA SCHILD | signature: Roberta Schild | date: 2/11/25

3207 Toopal Drive
 name: TRANG TRAN THUY LE | signature: [Signature] | date: 2/15/25

3211 Toopal Drive
 name: Tam Nguyen | signature: Tam Nguyen | date: 2/16/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

↓ ↓

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: Brnh Pham | [Signature] | 2/11/25

name signature date

address: 3203 Toopal Drive
Richard Schiltz ← → Richard Schildt | 2/11/25

name signature date

address: _____ | _____ | _____

name signature date

address: _____ | _____ | _____

name signature date

address: _____ | _____ | _____

name signature date

address: _____ | _____ | _____

name signature date

address: _____ | _____ | _____

name signature date

Page 6
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

3215 Toopal Drive

JANICE OLIVER | *[Signature]* | 2/16/25

name signature date

3219 Toopal Drive

Charles Braithwaite | *[Signature]* | 2/16/25

name signature date

3223 Toopal Drive

name signature date

3227 Toopal Drive

Gregory Bushnell | *[Signature]* | 2/17/25

name signature date

3231 Toopal Drive

Thai' Chow | *[Signature]* | 2/16/25

name signature date

3235 Toopal Drive

Apolonio Palomares | *[Signature]* | 2/16/25

name signature date

3239 Toopal Drive

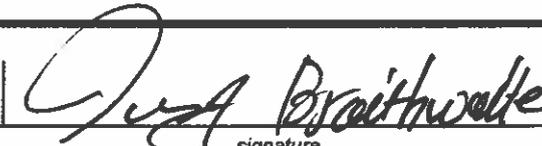
name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

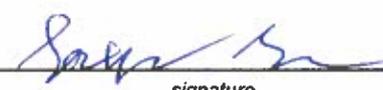
We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

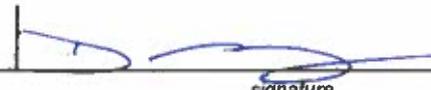
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3219 Toopal Dr.
 Jung Braithwaite |  | 2/16/25

name signature date

address: 3231 Toopal Dr.
 Soryann Sam |  | 2/16/25

name signature date

address: 3215 Toopal Dr.
 David P Oliver Jr |  | 2/17/25

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

PG 7 cont.

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

address: 3235 TOOPAL DR
OCEANSIDE CA 92058

Apolonia Palomares | *Apolonia Palomares* | 2/15/25

name signature date

address: 3235 Toopal

MARIA TERESA PALOMARES | *Maria Teresa Palomares* | 2/15/25

name signature date

address: 3235 Toopal

Adriano G. Palomares Jr. | *Adriano G. Palomares Jr.* | 2/15/25

name signature date

address: 3235 Toopal

Juan Rodriguez | *Juan Rodriguez* | 2/15/25

name signature date

address: 3235 Toopal

Maria Palomares | *Maria Palomares* | 2/15/25

name signature date

address:

name signature date

address:

name signature date

PAGE 7 CONTINUED

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

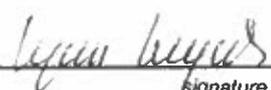
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

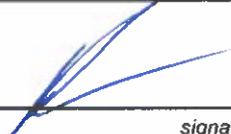
3243 Toopal Drive

Alyce Bunde |  | 2/11/25

3247 Toopal Drive

LYNN HUYNH |  | 2/11/25

3251 Toopal Drive

Richard Johnson |  | 2/11/25

3255 Toopal Drive

Mark McIntyre |  | 2/16/25

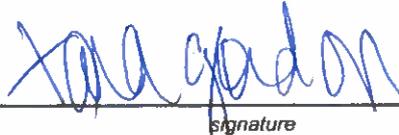
3259 Toopal Drive

Linda Jones-Zack |  | 2/14/2025

3263 Toopal Drive

John Chu |  | 2/15/25

3267 Toopal Drive

Tara Gordon |  | 2/16/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

3243 TOOPAL DR

address: 3243 Toopal Dr

TERENCE L. PATTON	<i>[Signature]</i> Resident	2/11/25	<input checked="" type="checkbox"/>
name	signature	date	

3247 TOOPAL DR

address: 3247 Toopal Dr

Jamie Hayash	<i>[Signature]</i> Resident	2/11/25	<input checked="" type="checkbox"/>
name	signature	date	

3247 TOOPAL DR

address: 3247 Toopal Dr

THE-NGUYEN	<i>[Signature]</i> Resident	2/11/25	<input checked="" type="checkbox"/>
name	signature	date	

3247 TOOPAL DR

address: 3247 Toopal Dr

VU HUANH	<i>[Signature]</i> Resident	2/11/25	<input checked="" type="checkbox"/>
name	signature	date	

3275 TOOPAL DR

address: 3275 Toopal Dr

KEVIN CORNELL	<i>[Signature]</i> Resident	2/15/25	<input checked="" type="checkbox"/>
name	signature	date	

3263 Toopal Dr

address: 3263 Toopal Dr

Justin Brown-Chu	<i>[Signature]</i> Resident	20250215	<input checked="" type="checkbox"/>
name	signature	date	

3275 Toopal Dr

address: 3275 Toopal Dr

Kelly Cornell	<i>[Signature]</i> Resident	2-15	<input checked="" type="checkbox"/>
name	signature	date	

PG 8
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

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3271 Toopal Drive

^{SUSAN}
SUSAN J. MORA
name

signature

2-15-25

3275 Toopal Drive

Kelly Cornell
name

signature

2-15-25

3281 Toopal Drive

Randall J Hanson
name

signature

2/15/25

3287 Toopal Drive

Jill Montgomery
name

signature

2-17-25

3293 Toopal Drive

Cuby Temperini
name

signature

2-15-25

3299 Toopal Drive

Valerie Bennett
name

signature

2-15-25

3303 Toopal Drive

ERNEST LINCOLN
name

signature

2/17/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

address: 3281 Toopal Drive
 Mary Hanson |  | 2/15/25

name signature date

address: 3281 Toopal Drive
 Johnathan Peterson |  | 2/15/25

name signature date

address: 3281 Toopal Drive
 Robert Hanson |  | 2/15/25

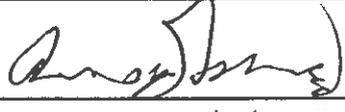
name signature date

address: 3275 Toopal Dr
 KEVIN CORNELL |  | 2/15/25

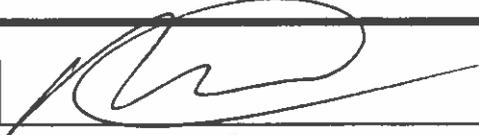
name signature date

address: 3311 Toopal Drive
 Dan + Laura Davis |  | 2/15/25

name signature date

address: 3271 Toopal Drive
 ANTHONY MORA |  | 2/13/25

name signature date

address: 3275 Toopal Drive
 MATT BOURNE |  | 2/15/25

name signature date

P6 9
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

address: 3275 Toopal dr.

3275 Kirstie Bourne	<i>[Signature]</i>	2-15-25 <input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date

address: 3303 Toopal Dr.

CHRISTINA LINDER	<i>[Signature]</i>	2/17/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address: 3207 Toopal Dr.

Nick Montgomery	<i>[Signature]</i>	2/17/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

Page 9
Cont.

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

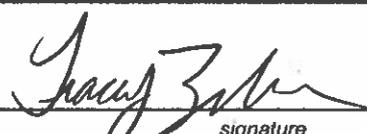
3307 Toopal Drive

			□	□
<small>name</small>	<small>signature</small>	<small>date</small>		

3311 Toopal Drive

			□	□
<small>name</small>	<small>signature</small>	<small>date</small>		

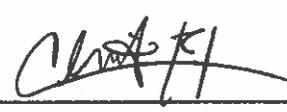
3321 Toopal Drive

³ Tracy Zuber <small>name</small>	 <small>signature</small>	2/15/25 <small>date</small>	□	✗
--	--	--------------------------------	---	---

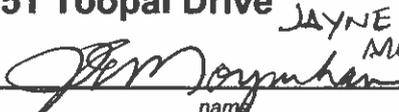
3331 Toopal Drive

Nancy Albert <small>name</small>	 <small>signature</small>	2/15/25 <small>date</small>	□	✗
-------------------------------------	--	--------------------------------	---	---

3341 Toopal Drive

Clinton e Heckman <small>name</small>	 <small>signature</small>	2/25/25 <small>date</small>	□	✗
--	---	--------------------------------	---	---

3351 Toopal Drive

JAYNE E MURPHY  <small>name</small>	 <small>signature</small>	2/16/25 <small>date</small>	□	✗
---	--	--------------------------------	---	---

3361 Toopal Drive

Richard A Gomez <small>name</small>	 <small>signature</small>	16 Feb 2025 <small>date</small>	□	□
--	--	------------------------------------	---	---



APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
 Homeowner

address: 3331 Toopal Dr Oceanside CA 92058

Art Alber name | [Signature] signature | 2/15/25 date

address: 3341 TOOPAL DRIVE OCEANSIDE, CA 92058

Courtney V Heckman name | [Signature] signature | 2/15/2025 date

address: 3321 TOOPAL DR OCEANSIDE, CA 92058

Patrick Zuber name | [Signature] signature | 2/15/2025 date

address: 3381 Toopal Dr Oceanside CA 92058

Grant Gagne name | [Signature] signature | 2/16/25 date

address: 3357 Toopal Dr 92058

PAUL TERWILLIGER name | [Signature] signature | 2-16-25 date

address: 3361 Toopal Drive Oceanside CA 92058

Trish Gomez name | [Signature] signature | 2-17-25 date

address: 3341 Toopal Drive Oceanside, CA 92058

Jenna McKay name | [Signature] signature | 2-17-25 date

Pg 10 Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

↓ ↓

3360 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3350 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Holly Bacci		2/16/25		
name	signature	date		

3340 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Terisa Sowul		2/16/25		
name	signature	date		

3330 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3320 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Margarita Garcia		2-17-25		
name	signature	date		

3310 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

484 Big Sky Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Erik Vega		2/17/25		
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3299 Toopal Drive		
David Bennett		2/16/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address: 3340 TOOPAL DR.		
CHRIS LEVENTIS		2/16/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address: 3320 Toopal DR		
Wendy P. Tapia		2/16/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address: 3320 Toopal DR		
Tita A Gomez		2/16/25 <input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date

address: 3320 Toopal DR.		
Sergio Gomez		2/16/25 <input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date

address: 3350 TOOPAL DR.		
ADAM LOCKWOOD		2/16/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address:		
		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

PG 11 cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

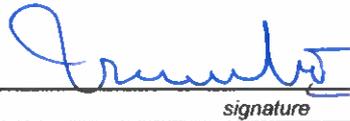
We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

488 Big Sky Drive

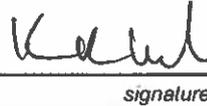
Tri Vo
name


signature

2/11/25

492 Big Sky Drive

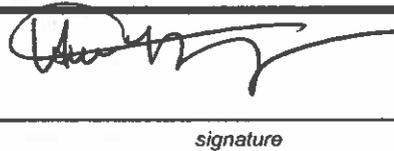
Karin McKeown
name


signature

2/11/25

496 Big Sky Drive

Anna Kasperowicz-Floyd
name


signature

2/11/25

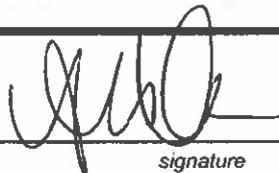
497 Big Sky Drive

Jean Zhang
name


signature

2/11/25

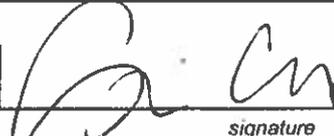
493 Big Sky Drive Alison Drummond
name


signature

2/11/25

489 Big Sky Drive

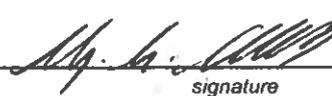
Gina Cuevas
name


signature

2/11/25

485 Big Sky Drive

Henry Light
name


signature

2/11/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

↓ ↓

3280 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3276 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
Linda Jones		2/15/25		
name	signature	date		

3272 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3268 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
		2/15/25		
name	signature	date		

3264 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3260 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
KURT BRADY		2/15		
name	signature	date		

3256 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
	THU NGUYEN	2/17/25		
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

3256 Toopal Dr Oceanside, CA 92058

address: _____

HANH NGUYEN		2/17/25	<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

address: _____

			<input type="checkbox"/>	<input type="checkbox"/>
<i>name</i>	<i>signature</i>	<i>date</i>		

Pg 13
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

3252 Toopal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3248 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Amber Sweet-Smith	<i>[Signature]</i>	2/11/25		
name	signature	date		

3244 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
THANH G PHAM	<i>[Signature]</i>	2/12/25		
name	signature	date		

3240 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
RICHARD L. EMPSON	<i>[Signature]</i>	2/11/2025		
name	signature	date		

3236 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Kimberly Romero	<i>[Signature]</i>	2/14/25		
name	signature	date		

3232 Toopal Drive			<input checked="" type="checkbox"/>	<input type="checkbox"/>
Robert Ramirez	<i>[Signature]</i>	02/12/25		
name	signature	date		

3228 Toopal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
William R. Pritchard	<i>[Signature]</i>	2/11/26		
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 3240 Toopal Dr.
 Terri Empson | Terri Empson | 2/11/25

name signature date

address: 3228 Toopal Dr
 Maria Pritchard | Maria Pritchard | 2/11/25

name signature date

address: 3248 Toopal Dr
 Oceanside CA 92058 | Amber Sweet Smith | 2/11/25

name signature date

address: 3248 Toopal Dr
 Oceanside CA 92058 | Theresa Smith | 2/11/25

name signature date

address: 3248 Toopal Dr
 Oceanside CA 92058 | Shaun Smith | 2/11/25

name signature date

address: 3248 Toopal Dr. Oceanside CA 92058
 Patrice Smith | Patrice Smith | 2/11/25

name signature date

address: 3248 Toopal Dr. Oceanside CA 92058
 Clarence Smith | Clarence Smith | 2/11/25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

address: 3244 TOOPAL DR | HARRY NGUYEN *[Signature]* | 2/12/25

name signature date

address: 3236 Toopal Dr | Jose Romero *[Signature]* | 2/12/25

name signature date

address: 3232 Toopal Dr | Gabriel Ramirez *[Signature]* | 02/17/2025

name signature date

address: | | |

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

3224 Toopal Drive
 LUDMILA LYNN | Ludmila Lynn | 2/15/25
name signature date

3220 Toopal Drive
 _____ | _____ | _____
name signature date

3216 Toopal Drive
 STUART CANNES | [Signature] | 02.15.25
name signature date

3212 Toopal Drive
 Vincent Alessi | [Signature] | 2/15/25
name signature date

3208 Toopal Drive
 _____ | _____ | _____
name signature date

3204 Toopal Drive
 _____ | _____ | _____
name signature date

3200 Toopal Drive
 Susan M Bumgardner | Susan M Bumgardner | 2-15-25
name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

↓ ↓

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3200 Toopal Drive

Todd Bumgardner		2-15-25 <input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date

address: 3216 Toopal Drive

Pam Veiga Cannes		2/15/25 <input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date

address: 3212 Toopal Dr.

Ana Alessi		2/17/25 <input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

address:

		<input type="checkbox"/> <input type="checkbox"/>
name	signature	date

Pg. 15
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

3276 Canyon View Drive

<p><i>Samuel J. Lee</i></p> <p>name</p>	<p><i>Samuel J. Lee</i></p> <p>signature</p>	<p>2/16/25</p> <p>date</p>
---	--	----------------------------

<p>520 Blue Jay Court</p>		
<p>CASSIDY FLATT</p> <p>name</p>	<p><i>Cassidy Flatt</i></p> <p>signature</p>	<p>2/15/25</p> <p>date</p>

<p>516 Blue Jay Court</p>		
<p>SALMA DAYIN GAYAT</p> <p>name</p>	<p><i>Salma Dayin Gayat</i></p> <p>signature</p>	<p>2/11/25</p> <p>date</p>

<p>3288 Canyon View Drive</p>		
<p>PAUL ZENTAI</p> <p>name</p>	<p><i>Paul Zentai</i></p> <p>signature</p>	<p>2/15/25</p> <p>date</p>

<p>3284 Canyon View Drive</p>		
<p>DOUGLAS KIDD</p> <p>name</p>	<p><i>Douglas Kidd</i></p> <p>signature</p>	<p>2-15-25</p> <p>date</p>

<p>3280 Canyon View Drive</p>		
<p></p> <p>name</p>	<p></p> <p>signature</p>	<p></p> <p>date</p>

<p>3276 Canyon View Drive</p>		
<p>Jacob Cole</p> <p>name</p>	<p><i>Jacob Cole</i></p> <p>signature</p>	<p>2/15/25</p> <p>date</p>

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

3289 CANYON VIEW DRIVE
Sylina KIDD | Sylina Kidd | 2/15/25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
 Homeowner

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3272 Canyon View Drive

MARTINA RAKOWSKI | *[Signature]* | 2-15-2025

name signature date

3268 Canyon View Drive

Sean P. O'Donnell | *[Signature]* | 2-13-2025

name signature date

3264 Canyon View Drive

DAVID M. BAURAC | *[Signature]* | 13 FEB 2025

name signature date

3260 Canyon View Drive

name signature date

3256 Canyon View Drive

Darren Parsons | *[Signature]* | 13 Feb 2025

name signature date

3252 Canyon View Drive

Carlos Peralde | *[Signature]* | 2/16/25

name signature date

3248 Canyon View Drive

Andrew Meyers | *[Signature]* | 13 Feb 2025

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 3252 Canyon View Dr
 Tina Recalde |  | 2/16/2025

name signature date

address: 3256 Canyon View
 Alise Parsons |  | 2/16/25

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

Pg 17
 Con't (lot 2)

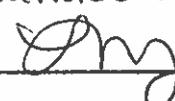
APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

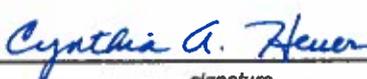
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3248 Canyon View Dr Oceanside CA 92058

Lindsay Meyers |  | 2.13.2020

name | signature | date

address: 3264 Canyon View Dr., Oceanside CA 92058

CYNTHIA A. HEUER |  | 2-13-2020

name | signature | date

address:

name | signature | date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

↓ ↓

3244 Canyon View Drive

Matt Livingston
name

[Signature]
signature

2/16/25

3240 Canyon View Drive

Tamara Livingston
name

[Signature]
signature

2/15/25

3236 Canyon View Drive

AARON BAGGETT
name

[Signature]
signature

2/15/25

3232 Canyon View Drive

SHEPARD WILLIAMS
name

[Signature]
signature

2/16/25

3228 Canyon View

name

signature

date

3224 Canyon View

Steve Scardel
name

[Signature]
signature

2/14/25

name

signature

date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3236 Canyon View Dr.
 Lindsay Baggett | *Lindsay Baggett* | 2/15/25

address: 3244 Canyon View Dr.
 Luanne Livingston | *Luanne Livingston* | 2/16/25

address: 3232 Canyon View Dr Oceanside CA
 Joan Kwasiborski | *Joan Kwasiborski* | 2/16/25

address: _____
 _____ | _____ | _____

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

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3219 Big Sky Drive / Canyon View Dr

Cassandra Maldonado | C. Maldonado | 02-14-25

name | signature | date

3223 Canyon View Drive

name | signature | date

3227 Canyon View Drive

Anne Sheldon | [Signature] | 2-15-25

name | signature | date

3231 Canyon View Drive

Jeff McKinney | [Signature] | 2-13-25

name | signature | date

3235 Canyon View Drive

Ashley Snyder | [Signature] | 2-13-25

name | signature | date

3239 Canyon View Drive

August B. Valentine | [Signature] | 2-13-25

name | signature | date

3243 Canyon View Drive

ERNEST SCHREFFER | [Signature] | 15 FEB 2025

name | signature | date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: Elizabeth Sheldon
3227 Canyon View Dr

name signature date 2/15/25

address: 3227 Canyon View Dr
Scott Sheldon

name signature date 2/15/25

address: AKA CR42
3219 Canyon View Dr.

name signature date 2/17/25

address: Leo Cruz
3219 Canyon View Dr

name signature date 2/17/25

address:

name signature date

address:

name signature date

address:

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

3247 Canyon View Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

3251 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Julie Francisco		2/12/25		
name	signature	date		

3255 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Warren Given		2-15-25		
name	signature	date		

3259 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
THOMAS LEVINE		2/12/25		
name	signature	date		

3263 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Terry V. Toyar		2/12/25		
name	signature	date		

3267 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Synda Lynn		2/18/25		
name	signature	date		

3271 Canyon View Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Barnes Peacock		2/15/25		
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3251 Canyon View Drive Oceanside, CA. 92058

David Francisco	<i>David Francisco</i>	2/12/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 3251 Canyon View Drive Oceanside, CA 92058

Emma Francisco	<i>Emma Francisco</i>	2/12/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 3251 Canyon View Drive Oceanside, CA. 92058

Andrew Francisco	<i>Andrew Francisco</i>	2/12/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 3259 Canyon View Dr Oceanside, CA 92058

Renee Levine	<i>Renee Levine</i>	2/12/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 3259 Canyon View Dr Oceanside, CA 92058

Dawn Levine	<i>Dawn Levine</i>	2/12/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 3255 Canyon View Dr

Zhenhua Li	<i>Zhenhua Li</i>	2/15/25	<input checked="" type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 3271 Canyon View

Colezen Cerey	<i>Colezen Cerey</i>	2/15/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

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Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

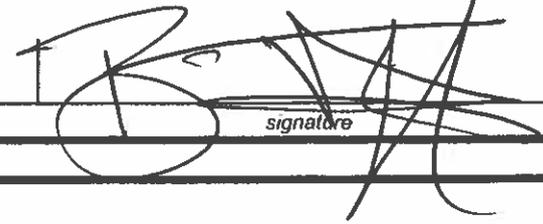
559 Big Sky Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

563 Big Sky Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

567 Big Sky Drive			<input type="checkbox"/>	<input type="checkbox"/>
Elijah Shepherd		2/15/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

571 Big Sky Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
JUDY SANDERS		2/13/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

575 Big Sky Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Shawn Danielsen		2-15-25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

579 Big Sky Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Bill Young		2-15-25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

583 Big Sky Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
RHONDA THOMSON	Rhonda Thomson	2/13/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 583 Big Sky Dr, Oceanside, CA 92058

Madeleine Thomson	Madeleine Thomson	2/15/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 583 Big Sky Dr, Oceanside CA 92058

Brian Thomson	B Thomson	2/15/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 583 Big Sky Dr, Oceanside CA 92058

Miranda Thomson	Miranda Thomson	2/15/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: Sean Danielsen
579 Big Sky Dr.
Oceanside, CA 92058

Sean Danielsen	[Signature]	2/15/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address:

			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address:

			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address:

			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

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587 Big Sky Drive

Yusuf Mohamed | *Yusuf Mohamed* | 13-2-25

name signature date

3215 Big Sky Drive

Douglas P. Smith | *Douglas P. Smith* | 12/2/25

name signature date

3218 Canyon View Drive

Douglas P. Smith | *Douglas P. Smith* | 12/2/25

name signature date

560 Crestwood Drive

S. Mohamed | | 13-2-25

name signature date

556 Crestwood Drive

Cynthia Gladstone Keenema | *Cynthia Keenema* | 14-2-25

name signature date

552 Crestwood Drive

| | |

name signature date

548 Crestwood Drive

Miguel Antonio Salano | *Miguel Salano* | 14/2/25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

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address: 3215 Canyon View Drive

Marilyn K. Smith	Marilyn K. Smith	12/2/25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: ~~S. Mohammed~~

S. Mohammed			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 587 Big Key Drive

SALAH Mohamed	S. Mohammed	13-2-25	<input type="checkbox"/>	<input checked="" type="checkbox"/>
name	signature	date		

address: 560 Crestwood Dr.
Oceanside CA 92058

Durkhanai Tahmas	Tahmas	02-12-25	<input checked="" type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 548 Crestwood Dr
Broken Machine
Oceanside CA 92058

Broken Machine	Broken Machine	02/15/25	<input checked="" type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address:

			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address:

			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

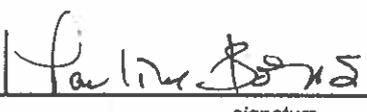
We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

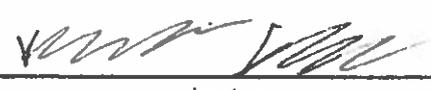
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

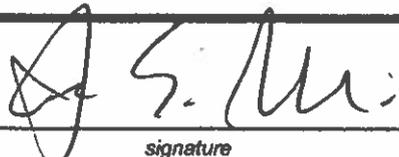
544 Crestwood Drive
 Khushbu Pate |  | 2/12/25
name signature date

540 Crestwood Drive
 Cassandra Diego |  | 2/12/25
name signature date

536 Crestwood Drive
 PAULINE TSORIS |  | 2-11-25
name signature date

_____ Drive
 _____ | _____ | _____
name signature date

616 Tukmal Drive
 Binky Hollingsworth |  | 2/12/25
name signature date

606 Tukmal Drive
 Luis MURCUIA |  | 2/12/25
name signature date

598 Tukmal Drive
 Greg Allen |  | 2-11-25
name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 536 Crestwood Dr
Oceanside, CA 92058

George Tsoris

2-11-25

name signature date

address: 598 Tukmal Dr.
Katie Allen

Katie Allen

2-11-25

name signature date

address: 5410 Crestwood Dr.
Jose David Diego

Jose David Diego

2/12/25

name signature date

address: 544 Crestwood Pr.
Hitesh Patel

Hitesh Patel

2/12/25

name signature date

address: 606 Tukmal Dr
Tierra Murguia

Tierra Murguia

2/12/25

name signature date

address: 616 Tukmal Dr
Brady Hillingworth

Brady Hillingworth

2/12/25

name signature date

address:

name signature date

Pg 03
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

596 Tukmal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

576 Tukmal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
RAYMOND HO	<i>Raymond Ho</i>	15 FEB 2025		
name	signature	date		

556 Tukmal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
ED & SHARI SAYERS	<i>Ed & Shari Sayers</i>	15 FEB 25		
name	signature	date		

596

596 Tukmal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
WILLIAM WATERBURY	<i>William Waterbury</i>	15 FEB 25		
name	signature	date		

526 Tukmal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
ANTHONY SERRATO	<i>Anthony Serrato</i>	2/15/25		
name	signature	date		

523 Tukmal Drive			<input type="checkbox"/>	<input checked="" type="checkbox"/>
Jonathan Freeman	<i>Jonathan Freeman</i>	2/15/25		
name	signature	date		

533 Tukmal Drive			<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

↓ ↓

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 523 TUKMAL DR

Robi Freeman | Robi Free | 2/16/25

name signature date

address:

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

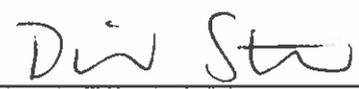
553 Tukmal Drive			
Timothy Cribbs		2/11/25	<input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date	

573 Tukmal Drive			
Brian Robinson		2-12-25	<input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date	

593 Tukmal Drive			
GREEN PERRAULT		2/15/25	<input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date	

601 Tukmal Drive			
Linda Middleton		2-15-2025	<input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date	

611 Tukmal Drive			
JEFF KILGORE		2 15 25	<input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date	

621 Tukmal Drive			
Diana Stephens		2/16/25	<input checked="" type="checkbox"/> <input type="checkbox"/>
name	signature	date	

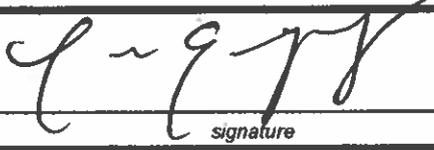
631 Tukmal Drive			
Andrew Dion		2-15-25	<input type="checkbox"/> <input checked="" type="checkbox"/>
name	signature	date	

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 553 Tukmal Dr
 Calixta R. Espinoza |  | 2/12/25

name signature date

address: 553 Tukmal Dr
 Ivone R. Cribbs |  | 2/12/25

name signature date

address: 573 Tukmal Drive
 Wendy Robinson |  | 2-12-25

name signature date

address: 601 Tukmal Dr.
 Les Middleton |  | 2/15/25

name signature date

address: 631 Tukmal Drive
 Brenda Dion |  | 2/16/25

name signature date

address: _____
 _____ | _____ | _____

name signature date

address: _____
 _____ | _____ | _____

name signature date

PG 25 cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

622 Wala Drive

ROBERT FLORES |  | 2/11/25

name signature date

612 Wala Drive

Anthony Bowling |  | 2/11/25

name signature date

602 Wala Drive

Julia Miller |  | 2.11.25

name signature date

590 Wala Drive

Sally Zuckowich |  | 2/11/25

name signature date

570 Wala Drive

Clarence Lou |  | 02/11/2025

name signature date

550 Wala Drive

WADE THOMAS |  | 02/15/2025

name signature date

530 Wala Drive

| | |

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

↓ ↓

address: 602 WALK Drive

John Miller |  | 2/15/25

name signature date

address: 550 WALK Drive

Edna Y. Thomas |  | 15 Feb 25

name signature date

address:

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

510 Wala Drive

Chad Bernardo

name



signature

2-11-2025

date

511 Wala Drive

THOMAS SHAW

name



signature

2-11-2025

date

531 Wala Drive

Nancy Burns

name



signature

2-11-25

date

551 Wala Drive

Tim Sebreros

name



signature

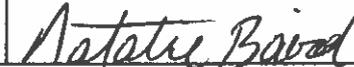
2-14-25

date

571 Wala Drive

Natalie Baird

name



signature

2/11/25

date

591 Wala Drive

Mary Jane Stacy

name



signature

2-11-25

date

603 Wala Drive

name

signature

date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

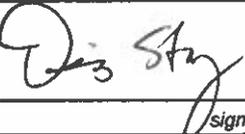
We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

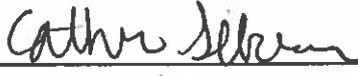
* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

↓ ↓

address: 531 Wals
 BRANDON BAIRD |  | 2-11-25
 name signature date

address: DENNIS Stacy
 591 Wals Dr. Oceanside Ca |  | 2/14/25
 name signature date

address: Catherine Sebens
 551 Wals Dr. Oceanside, CA 92058 |  | 02/14/25
 name signature date

address: | | |
 name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant

 Homeowner

 v

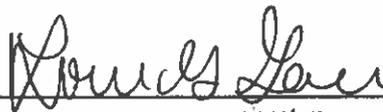
609 Wala Drive		
<i>Jeremy Ramsey</i>		2/14/25 <input checked="" type="checkbox"/>
<small>name</small>	<small>signature</small>	<small>date</small>

617 Wala Drive		
<small>name</small>	<small>signature</small>	<small>date</small>

592 Moyla Court		
<i>EDUARDO V. ROSA</i>		
<small>name</small>	<small>signature</small>	<small>date</small>

572 Moyla Court		
<i>Theresa Nguyen</i>		2/11/25 <input type="checkbox"/> <input type="checkbox"/>
<small>name</small>	<small>signature</small>	<small>date</small>

552 Moyla Court		
<i>Lin Gallagher</i>		2/13/25 <input type="checkbox"/> <input type="checkbox"/>
<small>name</small>	<small>signature</small>	<small>date</small>

532 Moyla Court		
<i>Louise Garcia</i>		2/13/25 <input type="checkbox"/> <input type="checkbox"/>
<small>name</small>	<small>signature</small>	<small>date</small>

512 Moyla Court		
<i>PG. 29</i>		
<small>name</small>	<small>signature</small>	<small>date</small>

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 525 TOMA CT OCEANSIDE CA 92058

MARY WESLOH  2-11-2025

name signature date

address: 525 TOMA CT OCEANSIDE, CA 92058

JADA MORALES  2-11-2025

name signature date

address: 525 TOMA CT OCEANSIDE CA 92058

LUKE WESLOH  2-11-2025

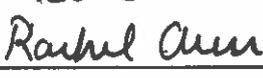
name signature date

address: 525 TOMA CT OCEANSIDE CA 92058

JACK WESLOH  2-11-2025

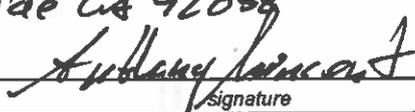
name signature date

address: 595 Moyla Ct Oceanside CA 92058

Rachel Auen  2-11-2025

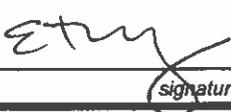
name signature date

address: 603 Wala Dr, Oceanside CA 92058

Anthony Vincent  2-11-2025

name signature date

address: 609 WALA DR

Emily Ramsey  2/14/2025

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

515 Moyla Court

Tracyn • Kate Marples | *[Signature]* | 2/14/25

name signature date

535 Moyla Court

Georgia Smith | *[Signature]* | 2-16-25

name signature date

555 Moyla Court

Nicole Holderman | *[Signature]* | 2/14/25

name signature date

575 Moyla Court

Martin Urbach | *[Signature]* | 2/14/25

name signature date

595 Moyla Court

CHRIS ADEN | *[Signature]* | 2/14/25

name signature date

590 Toota Court

Kelly Frey | *[Signature]* | 2/14/25

name signature date

584 Toota Court

Beau Peters | *[Signature]* | 2/15/25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 575 Moyla Ct.
 Lisa Urbach | Lisa Urbach | 2-14-25

name signature date

address: 512 MOYLA Ct.
 Paul Hanna | [Signature] | 2/16/25

name signature date

address: | | |

name signature date

PG-29
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

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Tenant
Homeowner

↓ ↓

580 Toota Court

Thomas Birk |  | 2-16-25

name signature date

570 Toota Court

CELERINA L-CORNETT | Celerina Lopez Cornett | 2/14/25

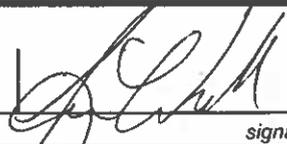
name signature date

560 Toota Court

_____ | _____ | _____

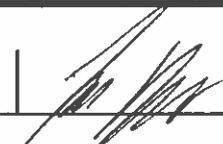
name signature date

550 Toota Court

Angelica Woodrell |  | 2-16-25

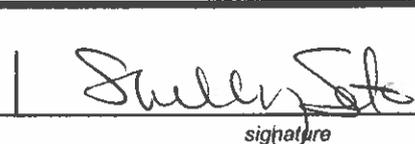
name signature date

540 Toota Court

TAYLOR PRINCE |  | 2/16/25

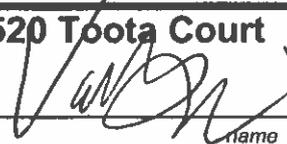
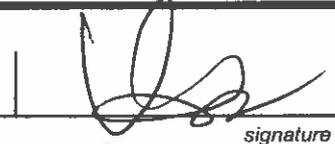
name signature date

530 Toota Court

Shelley Seto |  | 2-16-25

name signature date

520 Toota Court

 Valerie Blake |  | 2-16-25

name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: BILL E. CORNETT
570 TOOTA CT, OCEANSIDE | [Signature] | 2/14/25

name signature date

address: 580 TOOTA CT, OCEANSIDE
Juliette Birk | [Signature] | 2/16/25

name signature date

address: 530 TOOTA CT OCEANSIDE
PAUL SETO | [Signature] | 2-16-25

name signature date

address: 540 Toota Ct, Oceanside
Clayton Perce | [Signature] | 2-16-25

name signature date

address: 550 TOOTA CT OCEANSIDE
Nolan Woodnon | [Signature] | 2/17/25

name signature date

address: _____ | _____ | _____

name signature date

address: _____ | _____ | _____

name signature date

Pg 30
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

510 Toota Court
 Lisa Mulkern-Caney | *Lisa Mulkern-Caney* | 2/16/25

500 Toota Court
 Ethan Rueda | *Ethan Rueda* | 2/17/25

525 Toota Court
 Jerry Wesslow | *Jerry Wesslow* | 2/17/25

535 Toota Court
 Mike Lambert | *Mike Lambert* | 2/16/25

545 Toota Court
~~Fab~~ Fab Lebbre | *Fab Lebbre* | 2/16/25

555 Toota Court
 No | | |

565 Toota Court
 MICHAEL Brown | *Michael Brown* | 2/16/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 510 TOOTA CT
DAVID CANBY | [Signature] | 16 FEB 25

name signature date

address: 545 TOOTA CT
Sabrina Jester | [Signature] | 16 Feb 25

name signature date

address: 565 TOOTA CT
Brandi Brown | [Signature] | 2/16/25

name signature date

address: 504 TOOTA CT
Tianne Peters | [Signature] | 2/16/25

name signature date

address: | | |

name signature date

address: | | |

name signature date

address: | | |

name signature date

Pg 31
Cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

3159 Huula Drive
 James Burrell | *[Signature]* | 2/11 |
name signature date

3139 Huula Drive
 Jacek Maslowski | *[Signature]* | |
name signature date

3131 Huula Drive
 Julie Urbina | *[Signature]* | 2/14/25 |
name signature date

3121 Huula Drive
 Jodie Christensen | *[Signature]* | 2/11/25 |
name signature date

3111 Huula Drive
 Kirsten Sacher | *[Signature]* | 2/11/25 |
name signature date

3105 Huula Drive
 | | |
name signature date

3106 Huula Drive
 LOUISE SACHER | *[Signature]* | 2/11 |
name signature date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: David Sacher | [Signature] | 2/11/25

name signature date

address: Priscilla Encinas | [Signature] | 2/11/25

name signature date

address: Hailey Young | [Signature] | 2/11/25

name signature date

address: 3131 Huula Drive
Brett Urbina | [Signature] | 2/14/25

name signature date

address: | | |

name signature date

address: | | |

name signature date

address: | | |

name signature date

PG. 32
cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

3116 Huula Drive
KELLY POWELL | [Signature] | 2/11/25

3126 Huula Drive
Thomas [Signature] | [Signature] | 2/11/25

3136 Huula Drive
Sandra Kuhn | Sandra Kuhn | 2/11/25

3146 Huula Drive
[Signature] | Kimberly Hoagland | 2/11/25

3156 Huula Drive
| | |

3166 Huula Drive
MICHAEL RINSEA | Michael Rinsea | 2/11/25

3170 Huula Drive
CHRISTOPHER FREE | Christopher Free | 2/11/25

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

Tenant
Homeowner

address: 3126 Huula Dr. | *Ember Thorne* | 2-11-25

name signature date

address: 3146 Huula Dr. | *[Signature]* | 2/11/2025

name signature date

address: 3170 HUULA DR | *[Signature]* | 2/11/2025

name signature date

address: 3170 HUULA DR | *Judy Volino* | 2/11/2025

name signature date

address: 3194 Huula Dr | *[Signature]* | 2/11/25

name signature date

address: 609 Wala Dr. | *[Signature]* | 2/11/25

name signature date

address: 609 Wala Dr | *[Signature]* | 2/11/25

name signature date

PG. 33
Cont

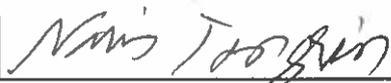
APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

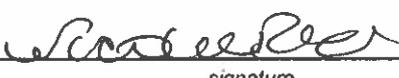
We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

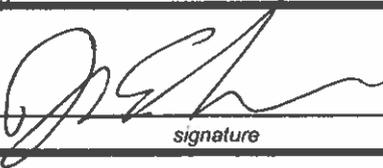
3174 Huula Drive
 Tyler Elmore |  | 2/10/25

3178 Huula Drive
 NARIS TONGRIN |  | 2-11-25

3182 Huula Drive
 Nicole Flores |  | 2-14-25

3186 Huula Drive
 Mary Gibbs |  | 2-11-25

3194 Huula Drive
 PHAM, Dannie |  | 2-11-25

3174 Huula Dr
 Jessica Elmore |  | 2/10/25

 _____ | _____ | _____

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

We all agree and knowingly support the attached "APPEAL OF PLANNING COMMISSION ACTION" Form and accompanying letter detailing our concerns to specific items in City of Oceanside Planning Commission's decision.

Tenant
Homeowner

* Original signatures below represent residents and businesses with 1500 feet of the proposed Eddie Jones Warehouse located at 250 Eddie Jones Way in Oceanside.

address: 3111 Huula Dr

JORDAN SAUER | JORDAN SAUER | 2/11/25

name signature date

address: 3122 Huula Dr, Oceanside
WY

LORIE CAZAM | LORIE CAZAM | 2-14-25

name signature date

address:

name signature date

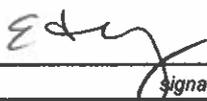
Pg 34 cont

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

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address: 609 Wala		
Emily Ramsey		2/11/25
name	signature	date

address:		
name	signature	date

address:		
name	signature	date

address:		
name	signature	date

address:		
name	signature	date

address:		
name	signature	date

address:		
name	signature	date

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

Saint Charles Priory (AKA Prince of Peace Abbey)

address: 650 Benet Hill Rd Oceanside, CA 92058

GARY D. EWEN	<i>Gary D Ewen</i>	2/12/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

Thomas Meyer	<i>Thomas Meyer</i>	2/12/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

David Koch	<i>David Koch</i>	2/12/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

Christopher Sokol	<i>Christopher Sokol</i>	2/13/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

MANUEL WINSTON QUIZON	<i>Manuel Winston Quizon</i>	2/13/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

John Davidson	<i>John Davidson</i>	2/13/25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

address: 650 Benet Hill Rd Oceanside, CA 92058

WALTER GRAHAM	<i>Walter Graham</i>	2-14-25	<input type="checkbox"/>	<input type="checkbox"/>
name	signature	date		

Residents of:

1 of 3

Saint Charles Priory (AKA Prince of Peace Abbey)

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

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Saint Charles Priory (AKA Prince of Peace Abbey)

address: 650 Benet Hill Rd Oceanside, CA 92058
CW ~~Charles~~ CHARLES WRIGHT | Charles Wright | 2/12/25

address: 650 Benet Hill Rd Oceanside, CA 92058
SON LE | Son | 2/12/25

address: 650 Benet Hill Rd Oceanside, CA 92058
John L. Poutous | John L. Poutous | 2/12/25

address: 650 Benet Hill Rd Oceanside, CA 92058
Chien Tran CHIEN-TRAN | Chien Tran | 2/12/25

address: 650 Benet Hill Rd Oceanside, CA 92058
Robert J. Vaillancourt | Robert J. Vaillancourt | 2/12/25

address: 650 Benet Hill Rd Oceanside, CA 92058
Joshua M. DeLaRosa | [Signature] | 2/13/25

address: 650 Benet Hill Rd Oceanside, CA 92058
DALE V BLACK | Dale V Black | 2/18/25

Residents of:
Saint Charles Priory (AKA Prince of Peace Abbey)

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

Saint Charles Priory (AKA Prince of Peace Abbey)

address: 650 Benet Hill Rd Oceanside, CA 92058
 ROAD CLARK | Ronald G Clark | 2/12/25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2-12-25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2/12/25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2/12/25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2-12-25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2-12-25
 name signature date

address: 650 Benet Hill Rd Oceanside, CA 92058
 | | 2-12-25
 name signature date

Residents of: Saint Charles Priory (AKA Prince of Peace Abbey) 3 of 3

APPEAL IN ACCORDANCE WITH ZONING ORDINANCE (Section 4604)

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Tenant
Homeowner

BUSINESS

address: EYNON LEASING LLC, 600 Airport Rd Oceanside 92058

STEPHEN EYNON | *[Signature]* | 3/13/25

name signature date



Eynon Management
 O: 760.529.9931
 F: 760.547.7519
 600 Airport Road
 Oceanside, CA 92058
 eynonweed@eynonweed.com
 www.eynonweed.com

signature date

signature date

address: 3207 Raymar Rd Ste A
 Josh SOTO | 42058 | *[Signature]* | 2/15/25

name signature date



BOARSNEST CHOPPERS
 info@BoarsNestChoppers.com
 760-757-HAWG 4294
 3207 Roymar Rd. Suite A
 Oceanside, CA 92058
 BoarsNestChoppers.com

signature date

signature date

address: _____

_____ | _____ | _____

name signature date

Businesses



DATE: February 10, 2025

TO: Chairperson and Members of the Planning Commission

FROM: Development Services Department/Planning Division

SUBJECT: **CONSIDERATION OF DEVELOPMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001), AND VARIANCE (V22-00001) TO ALLOW THE CONSTRUCTION OF A WAREHOUSE, MANUFACTURING, AND OFFICE FACILITY WITH FOUR SEPARATE BUILDINGS AT 250 EDDIE JONES WAY – EDDIE JONES WAREHOUSE, MANUFACTURING, AND OFFICE FACILITY PROJECT – APPLICANT: RPG OCEANSIDE EDDY JONES WAY OWNER, LLC**

RECOMMENDATION

Staff recommends that the Planning Commission by motion:

- (1) Certify the Environmental Impact Report (EIR) and adopt the associated findings of fact and Mitigation Monitoring and Reporting Program (MMRP) by adopting Planning Commission Resolution No. 2025-P05; and
- (2) Approve a Development Plan, Conditional Use Permit, and Variance for the Multi-Building and Truck Bay Reduction Alternative (MBTRA) project by adopting Planning Commission Resolution No. 2025-P04 with findings and conditions approval attached herein.

LOCATION AND BACKGROUND

The 31.79-acre project site is located at 250 Eddie Jones Way, immediately north of the Oceanside Municipal Airport. Situated in the Airport Neighborhood Planning Area, the site has a General Plan Land Use Designation of Light Industrial (LI) and a corresponding zoning designation of Limited Industrial (IL). The project site is located within the Airport Influence Area (AIA) for the Oceanside Municipal Airport – Airport Land Use Compatibility Plan (ALUCP). Surrounding land use includes the San Luis Rey River and recreational trail to the north, the airport to the south, and vacant industrial land to the east and west. Single-family residential subdivisions, the Prince of Peace Abbey, and a variety of general industrial uses that include a concrete batching plant, scrap yard, and vehicle storage

facility, are located on the north side of the river. Additional industrial uses and Highway 76 are located further south of the site.

The Airport Neighborhood Planning Area is centered around the Oceanside Municipal Airport and bisected by the San Luis Rey River and Highway 76. Covered in thick vegetation, the river spans between 500 to 600 feet wide and divides the planning area into two distinct sections. Single-family residential subdivisions and open space are situated north of the river, and a variety of industrial, commercial, and public/semi-public uses are located to the south of the river adjacent to the Highway 76 corridor.



The project site is currently vacant and was previously developed with a 172,300 square-foot industrial facility, owned and operated by TE Connectivity, that specialized in industrial plating and electrical connector manufacturing for the defense, aerospace, and marine industry. The plant was originally constructed in 1967 by the Deutsch Company and sold to TE Connectivity in 2012. The facility was vacated and sold to the current property owner in 2021 and demolished a year later. Historical use of large quantities of hazardous substances and petroleum products in the manufacturing process has resulted in known soil contamination at the site. Soil remediation efforts have been initiated with the CA Department of Toxic Substances Control (DTSC) in accordance with the CA Land Reuse and Revitalization Act (CLRRRA).



Pursuant to the California Environmental Quality Act (CEQA), an Environmental Impact Report (EIR) has been prepared to evaluate the environmental impacts associated with implementation of the proposed development project. The "Proposed Project" described and fully analyzed in the Draft EIR (DEIR) included the development of a new 566,905 square-foot warehouse and distribution facility on the 31.79-acre project site with 590 parking spaces for employees/visitors, 60 truck trailer parking stalls, and 114 truck terminals. Pursuant to Section 15126.6 of

the CEQA Guidelines, an "EIR shall describe a range of reasonable alternatives to the project, or to the location of the project, which would feasibly attain most of the basic

objectives of the project but would also avoid or substantially lessen any of the significant effects of the project, and evaluate the comparative merits of the alternatives”.

The DEIR included an analysis of three project alternatives: No Project/No Development Alternative, Multi-Building Alternative, and Reduced Building Footprint Alternative. In response to public comments received on the DEIR, a Multi-Building and Truck Bay Reduction Alternative has been included in the Final EIR (FEIR), which is a variation on the project and Multi-Building Alternative previously evaluated in the DEIR. The City, as Lead Agency, recommends approval of the Multi-Building and Truck Bay Reduction Alternative which is considered the preferred alternative that meets all but one of the objectives of the project while lessening potentially significant impacts associated with implementation the project.

The project description, analysis, and recommendation provided below exclusively focuses on the Multi-Building and Truck Bay Reduction Alternative, which will be further referenced as the "MBTRA" project. The original project, as described in the FEIR, will be referenced as "Proposed Project."

PROJECT DESCRIPTION

The proposed MBTRA project is comprised of three entitlement requests:

Development Plan (D22-00001):

A request for a development plan to construct an industrial warehouse, manufacturing, and office facility consisting of four separate buildings totaling 497,822 square feet. Each building would be designed to include mezzanines and truck bays to accommodate a variety of warehouse, manufacturing, distribution, and office uses. The applicant has not identified a future tenant(s) at this time. All future uses would be subject to compliance with the IL District and conditions of approval associated with this entitlement.

Site Design: The MBTRA would include the construction of four shell industrial buildings on the 31.79-acre site. This alternative would orient the proposed truck bays (dock-high doors) on the east/west side of each building and reduce the total number of truck bays to 56 (originally 114). In comparison to the initial Proposed Project, the MBTRA would also reduce the overall square footage of the proposed facility to a total of 497,822 square feet with each building allocated space for warehouse, manufacturing, and office uses as provided in Table 1.

Building	Warehouse	Manufacturing	Office	Total Building Area
1	73,508	27,025	9,127	109,660
2	89,107	32,760	10,733	132,600
3	81,591	29,997	9,959	121,547
4	90,069	33,114	10,832	134,015
TOTAL	334,275 SF	122,896 SF	40,651 SF	497,822 SF

In an effort to provide additional buffering between the project site and surrounding uses, the MBTRA would include 19 to 20-foot high screen walls adjacent to truck terminals. In addition, increased tree plantings would be incorporated along the northern perimeter of site.

Access and Circulation: Access to the site would be provided from both Benet Road and Alex Road via Highway 76 and Foussat Road. Heavy trucks would be limited to the entry point on Benet Road and would access the site from Highway 76. Passenger vehicles could access the site from either entrance. A dedicated right-hand turn lane would be constructed at the project entrance on Benet Road to allow for deceleration of trucks entering the site. This feature would ensure the north-bound travel lane remains clear for vehicles crossing the river. Onsite drive aisles would be 35-feet wide to provide adequate circulation for heavy trucks and emergency vehicles.

Pedestrian walkways would be provided throughout the site and new pedestrian connections would be provided through the construction of 600 feet of sidewalk along the Benet Road frontage that would connect to the San Luis Rey River trail. A new sidewalk would also connect to the river trail on the Alex Road frontage.

Parking: The MBTRA would provide a total of 593 parking spaces for employees and customers based on standard parking ratios for office, warehouse, and manufacturing uses. 90 stalls would be dedicated for electric vehicle charging and 30 bike racks would also be provided onsite. Other than the proposed 56 truck bays, no additional truck trailer parking spaces would be provided onsite.

Landscaping: The proposed landscape plan would provide a total landscape area of 316,366 square feet (23-percent lot coverage). A variety of trees would be planted along the perimeter including California Sycamore, Coast Live Oak, and Bay Laurel. Interior landscape areas would include Desert Willow, Chilean Mesquite, California Laurel and Willow Acacia. Tree plantings along the northern perimeter would be increased with the MBTRA to provide additional screening for the residential neighborhood located north of the river. In addition, the MBTRA would maintain the 100-foot biological buffer from the edge of the San Luis Rey River riparian habitat.

Architecture: All four buildings would be designed in a modern light industrial style incorporating concrete tilt-up panels with horizontal reveals, offset wall planes, and various window elements and façade details to create visual interest. The buildings would be painted with neutral colors, including gray and off-white, with color variations in wall panels to offset wall expanses. Office and warehouse components would be clearly distinguished with the use of large, tinted windows, metal canopies, and aluminum storefront window systems. Additionally, concrete screen walls would be located adjacent to loading bays to reduce visual impacts of the loading areas.

Grading and Drainage: The project site is located adjacent to the San Luis Rey River levee embankment and the Benet Road right-of-way, which are elevated above the site and act as a berm on the western and northern perimeter of the site. The proposed development would generally maintain the grades of the existing site. Since the development would be located below the Base Flood Elevation (BFE), a proposed flood wall would be constructed

around the perimeter to provide additional flood mitigation.

A stormwater drainage system would be designed to collect on-site runoff and convey it through the project site into existing drainage facilities. Stormwater treatment would occur onsite through the installation of inlets, storm drains, biofiltration basins, and an underground stormwater detention tank.

Conditional Use Permit (CUP20-00023):

A request for a conditional use permit to allow (A) four industrial buildings for distribution and storage purposes with a floor area greater than 50,000 square-feet; and (B) truck terminals with more than six heavy trucks on the premises at one time per Article 13 of the Zoning Ordinance. The MBTRA project would include four separate industrial buildings with a total warehouse area of 334,275 square feet and a total of 56 truck bays. Each building would have warehouse space ranging from 73,508 square feet to 90,069 square feet. As previously noted, the applicant has not identified a potential tenant or use at this time. The shell design and warehouse allocation would allow flexibility to accommodate a variety of industrial uses permitted in the IL District.

Building	Total Warehouse Area (SF)	Number of Truck Bays
1	73,508	12
2	89,107	12
3	81,591	14
4	90,069	18
SITE TOTAL	334,275 SF	56

The MBTRA project would orient the truck terminals in an east-west direction. Each building would include 12-18 truck bays situated on interior facing elevations to fully screen the truck terminals. No truck bays would be visible from the perimeter of the site. In addition, 19 to 20-foot-high concrete screen walls would be constructed at the north and south end caps of each building to further reduce visual impacts of the truck terminals on residential properties north of the river.

Based on the proposed number of truck bays, the site has the capacity to accommodate a maximum of 56 trucks and/or trailers at one time. The demand for trucking terminals would ultimately be determined by future tenants occupying the buildings.

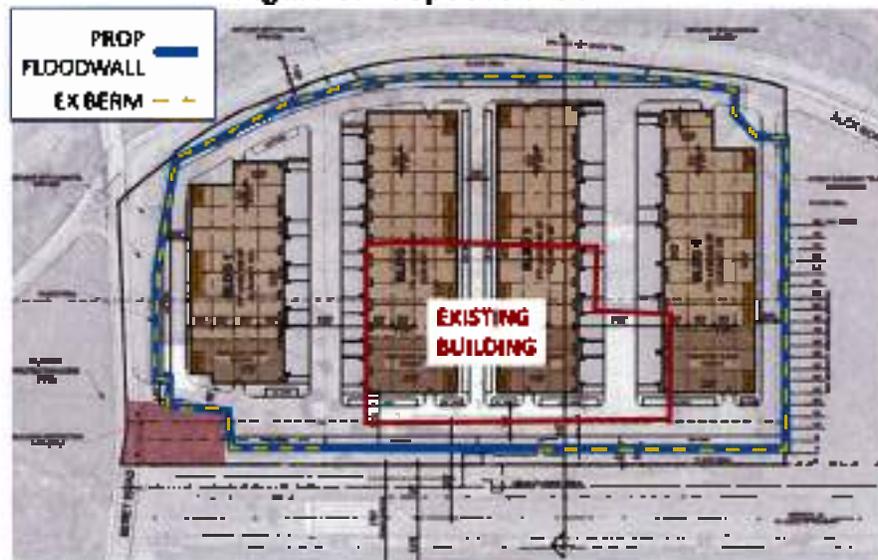
Variance (V21-00003)

A variance request to construct a flood wall around the perimeter of the site that would exceed the maximum allowable height of eight (8) feet per Article 30 of the Zoning Ordinance. As proposed, exterior facing wall elevations would range from 7.9 feet to 9.9 feet above the exterior grade. Interior facing wall elevations would extend up to approximately 9.5 feet in height. The flood wall would be constructed as a decorative masonry block wall system.

The project site is located adjacent to the San Luis Rey River and is within a Special Flood Hazard Area (Zone A99 per the FEMA Flood Insurance Map). An existing earthen flood levee was previously constructed to protect the site from flooding from the San Luis Rey River. Zone A99 designates areas within the 100-year floodplain that will be protected by a federal flood protection system under construction. However, the existing levee and other levees along the SLR have yet to be certified by FEMA. The City is currently participating in a program with the US Army Corp of Engineers aimed at evaluating and improving the levee system, as necessary, to achieve FEMA certification.

Since elevating the site above the Base Flood Elevation (BFE) would not be feasible, the applicant coordinated with the City and FEMA to provide alternative floodplain mitigation. The proposed flood wall system would extend around the perimeter of the site to protect the facility from potential flood events.

Figure 3: Proposed Floodwall



ANALYSIS

Project Comparison: In response to public input and comments received on the DEIR, the MBTRA was incorporated into the FEIR as a variation of the "Multi-building Alternative" presented in the DEIR. The MBTRA represents a reduced project intended to further mitigate potential environmental impacts and enhance compatibility with the surrounding community.

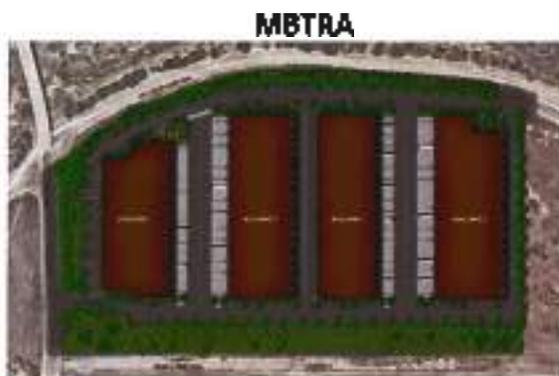
Key features that are represented in the MBTRA project include:

- Reduction in overall building square footage from 566,905 to 497,882 square feet
- Four multi-tenant buildings rather than one large building
- Reduction of truck bays from 114 to 56 and divided among all four buildings
- Re-orientation of the truck bays from north-south to east-west
- Removal of the 60 truck trailer parking spaces

- Proposed 19 to 20-foot high screen walls adjacent to truck terminals
- Increased tree plantings along the northern perimeter of site

The following Table is a comparison of the Proposed Project versus the MBTRA:

TABLE 3 – Project Comparison		
Development Standard	Proposed Project (original)	Multi-Building & Reduced Truck Bay Alternative
Number of Buildings	1	4
Total Building SF	566,905	497,882
Total Truck Bays	114	56
Truck Trailer Parking	60	0
Standard Parking	590	593
Building Height	45 feet	24-45 feet
Lot Coverage	41%	36%
Landscaping	21%	23%
Riparian Buffer	100 feet	100 feet



Traffic Analysis: A Local Transportation Study (LTS) was prepared for the proposed project in accordance with the City's Traffic Impact Analysis Guidelines for Vehicle Miles travelled (VMT) and Level of Service Assessment. For comparison purposes, an Alternative Memorandum for LTS was prepared for the MBTRA as Appendix I-1 of the FEIR. The memorandum compares Average Daily Trips between the proposed project consisting of a single 566,905 square-foot industrial building and the MBTRA consisting of four separate buildings totaling 497,882 square feet.

Average Daily Trips

The traffic analysis for the project included an analysis of AM peak hour, PM peak hour, and daily traffic volumes for all proposed land uses, including both car trip rates and truck trip rates. To account for the additional length of trucks, the traffic analysis includes a Passenger Car Equivalent (PCE) multiplier of two (2) applied to average daily trips (ADT) for trucks.

TABLE 4 - Trip Generation Comparison			
Proposed Project			
Trip Generation	Daily	AM Peak	PM Peak
Manufacturing			
Cars	809	116	126
Trucks	77	5	5
Warehouse			
Cars	557	32	40
Trucks	87	8	4
Total Car Trips	1,366	148	166
Total Trucks Trips PCE* (Only Daily Trips)	328		
PROJECT TOTAL	1,694	161	175
Multi-Building and Truck Bay Reduction Alternative			
Trip Generation	Daily	AM Peak	PM Peak
Manufacturing			
Cars	636	91	99
Trucks	60	4	4
Warehouse			
Cars	510	29	36
Trucks	80	7	4
Total Car Trips	1,145	120	135
Total Trucks Trips PCE* (Only Daily Trips)	281		
PROJECT TOTAL	1,286	131	143

***Passenger Car Equivalent (PCE)** is a metric to assess traffic flow rate that represents a specific mode of transport (heavy trucks) compared to a single car. For example, five (5) truck trips would be the equivalent of ten (10) passenger car trips.

As provided in Table 4, the originally Proposed Project would generate a total of 1,695 ADT compared to the MBTRA at 1,286 ADT. The proposed alternative would reduce daily trips by 409 ADT representing a 24-percent reduction. Truck trips are calculated based on the proposed square footage of warehouse area utilizing the Institute of Transportation Engineers (ITE) land use code for High-Cube Transload and Short-Term Storage Warehouse (Code 154).

Based on the City's significance criteria, the LTS concluded that the project would have a transportation impact at the intersection of SR-76/Benet Road resulting from cumulative traffic being added to the intersection. Since the project would contribute traffic to a failing intersection, the project would be subject to a fair share monetary contribution towards a future improvement based on the project's proportional share of future traffic added to roadway network.

The LTS was reviewed and approved by the Transportation Section of the Public Works Department. A copy of the LTS is provided as Attachment 6.

Community Outreach: In accordance with the City's Enhanced Notification Policy (300-14), the applicant conducted community outreach throughout the entitlement process. Outreach efforts included hosting two in-person meetings and two virtual meetings, establishing a project website, sending mail and virtual notifications regarding meetings and project updates, and hosting one on one and small group meetings. A copy of the community outreach report is provided as Attachment No. 5:

The project has received a significant level of opposition from the residential community north of the San Luis Rey River. Many residents submitted comment letters and have voiced concern regarding potential impacts of the project. The following is an overview of key concerns and how those concerns have been addressed.

Traffic Impacts

Community members expressed concern that truck traffic from the project would impede traffic flow on Benet Road and lead to additional congestion on Foussal Road and Hwy 76 when combined with the Ocean Kamp Project.

As previously noted, an LTS was prepared for the Proposed Project and analyzed the ADT for both cars and trucks. The study concluded that the Proposed Project would have a traffic impact at the intersection of SR-76/Benet Road. This impact would be mitigated through a fair share payment to the City's Thoroughfare and Signal Account.

The MBTRA would reduce the overall ADT of the proposed facility and would include the construction of a dedicated right hand turn into the project site on Benet Road. Coupled with the existing through lane, the proposed turn lane would allow trucks to move out of the northbound travel lane and decelerate before entering the site. This improvement would help maintain the flow of traffic entering the neighborhood north of the river.

Future Tenant(s)

Community members commented that the applicant should have disclosed the future tenant of the project to allow the community to fully assess potential impacts of the project.

While it is common for a business to be identified with a development application, the City does not require an applicant to declare a tenant during the entitlement review process. Development plan and conditional use permit applications are reviewed based on the proposed land use in accordance with zoning regulations. All proposed uses must be consistent with use classifications defined in Article 4 of the Zoning Ordinance and permitted in the underlying zoning district. Development requirements, such as parking ratios, vary by land use type to ensure that a development plan is adequately designed to accommodate a proposed use.

According to the applicant, the MBTRA is designed to accommodate manufacturing, warehouse, and office uses based on the demand for this type of facility in the Oceanside area. Regardless, future tenants and land use would be limited to the IL use classifications authorized by this entitlement. Permitted and conditionally permitted uses would be subject to compliance with all conditions of approval provided in the attached resolution.

Any requested modifications to the conditions of approval would require consideration by the Planning Commission at a public hearing.

Pollution

Concern was expressed regarding potential air and noise pollution generated by the project from semi-trucks, service trucks, forklifts, and employee vehicles that would impact the community and San Luis Rey River.

An air quality study was prepared with the EIR to analyze potential impacts associated with the Proposed Project. The study concluded that daily operational emissions, including passenger vehicle and truck traffic associated with warehouse and manufacturing operations, would not exceed the San Diego APCD's significance thresholds for any criteria air pollutant.

Per Section 3.2.5 of the FEIR, the Proposed Project would also implement both construction-related and operational project design features (PDFs) intended to reduce emissions of air pollutants and toxic air contaminants. The applicant agreed to include applicable measures found in the California Department of Justice Warehouse Project Best Practices document (DOJ 2022). Such measures would reduce potential impacts of warehouse operations and have been incorporated as conditions of approval for the MBTRA project. As noted in the air quality memorandum (Appendix B-1 of the FEIR), the MBTRA would be subject to the same mitigation measures and PDFs of the Proposed Project. The memorandum concluded the MBTRA would further decrease emissions compared to the Proposed Project based on the reduced building square footage and reduction in vehicle trip generation.

A Noise Technical Report was prepared with the EIR that evaluated various types of noise impacts, including potential impacts on residential areas north of the river. Field measurements of noise levels and acoustical modeling concluded that the operational noise levels of the Proposed Project would be below the decibel level limits established by the City's Noise Control Ordinance. The Noise Alternative Memorandum (Appendix H-1 of FEIR) concluded that the MBTRA would further reduce stationary operations noise based on fewer loading docks and a smaller project footprint. Noise impacts of the MBTRA would be considered less than significant.

Safety and Emergency Services

Community members expressed concern that truck traffic from the project would impede vehicle access on Benet Road and pose a safety hazard for pedestrians, bicyclists, and skateboarders. There is also concern that increased traffic from the project would hinder evacuation and emergency vehicle access during a fire or other emergency.

As previously noted, the MBTRA project would construct various road improvements including a dedicated right hand turn lane and new sidewalk along the project frontage on Benet Road connecting to the river trail. Heavy trucks would be prohibited from entering

or exiting the site on Alex Road to avoid conflicts with the existing skate park and future roundabouts planned for construction with the proposed Ocean Kamp project.

In response to concerns about emergency evacuation, the applicant prepared a Wildfire Evacuation Study (WES) for the Proposed Project to evaluate the project's consistency with relevant emergency evacuation plans and emergency response plans, disclose the prevention and minimization regulations and measures applicable to the project, and determine evacuation times for the existing and post-project conditions, as well as provide emergency preparedness information and resources to increase occupant preparedness and facilitate efficient evacuation in the event of an emergency. The WES was prepared in consultation with both the Oceanside Fire Department and Oceanside Police Department.

The key finding of the WES concluded that the proposed project would not substantially impair an adopted emergency evacuation plan or emergency response plan, and impacts would be less than significant without mitigation. Further, the available roadway capacity was found sufficient to safely accommodate the number of evacuating vehicles within the relevant areas with and without the project development. Project impacts would be less than significant as the study demonstrated that evacuation times would be reasonable and in line with typical evacuation times and would allow for safe evacuations to be conducted under all evaluated scenarios.

The Proposed Project and MBTRA would not eliminate any existing evacuation routes. In an evacuation or emergency scenario, adequate emergency access would be maintained. Considering these facts and others discussed in the study, neither the Proposed Project nor the less intense MBTRA project would interfere with evacuation response planning, and would not result in inadequate emergency access.

Habitat

Concern was expressed that the project would have an impact on wildlife along the San Luis Rey River corridor.

The project site was previously developed with IL uses and consists of disturbed habitat and graded land. A biological technical report was prepared for the Proposed Project and appended to the EIR. No special-status plants were observed on the property. The study evaluated potential biological impacts of the development and included recommended mitigation measures as provided in the FEIR. Given the proximity to riparian habitat along the San Luis Rey River, a 100-foot biological buffer would be established from the edge of the riparian habitat adjacent to the site. The establishment of the buffer would avoid a substantial adverse effect on any riparian habitat or other sensitive natural community.

1. General Plan Conformance

The project site has a General Plan land use designation of LI and would be consistent with the following General Plan policies:

Land Use Element - Section 2.1 Industrial Development

Objective: To promote industries which are consistent with community enhancement and provide stable tax bases and a balance of employment opportunities.

- Policy A: Industrially designated lands shall be devoted to industrial uses for the preservation of the City's economic future, employment opportunities, and general welfare.

The MBTRA would be considered a light industrial development designed for warehousing, manufacturing, distribution, and office uses. The shell building design and multi-tenant configuration would accommodate a variety of industrial businesses that would help preserve the City's economic future and generate employment opportunities. The applicant forecasts the project generating as many as 499 jobs.

- Policy B: Industrial uses shall be compatible with surrounding land uses and available community facilities

The MBTRA was designed in compliance with the regulations of the Zoning Ordinance and is consistent with the design and scale of adjacent industrial uses in the Airport Neighborhood. The project was also designed in compliance with the Oceanside Municipal Airport – Airport Land Use Compatibility Plan (ALUCP) as documented by the determination of consistency from Airport Land Use Commission for San Diego County. Light industrial uses are compatible with the airport and align with the ALUCP objective to protect the airport from incompatible uses that may hinder airport operations.

The MBTRA would include four buildings with truck terminals oriented in an east-west direction to provide adequate screening from adjacent uses. A significant buffer would also be provided between the project site and the residential area to the north by the San Luis Rey River and perimeter landscaping. The project would be served by existing water and sewer infrastructure as well as the existing roadway network.

Land Use Element - Section 2.12 Light Industrial

Objective: To provide and protect industrial lands that can accommodate a wide range of moderate to low intensity industrial uses capable of being located adjacent to residential areas with minimal buffering and attenuation measures.

- Policy D: Light industrial developments shall place its emphasis on presenting an efficient, clean and visually appealing industrial environment. Architectural, landscape, signage and development standards shall be coordinated to provide a unified site design.

The MBTRA was designed in a modern industrial style incorporating concrete tilt-up panels and various design details to create visual interest. Proposed

landscaping would complement the building design and be dispersed throughout the site to enhance the visual quality of the project and provide additional buffering of loading and parking areas.

- **Policy C:** Light industrial areas shall be primarily developed as industrial parks and commerce centers providing both single-use and multi-tenant structures. Independent development for single-use projects on larger sites may also be permitted.

The MBTRA would introduce four multi-tenant structures on the 31.79-acre site to accommodate a variety of light industrial uses.

- **Policy E:** Each industrial use shall provide attenuating structures, devices and procedures to ensure that noise, vibration, glare, odors, heat and other emissions are not perceptible outside its boundaries by the natural senses.

The applicant prepared a Noise Impact Analysis to evaluate long-term noise impacts of the project on adjacent residential properties to the north of the site and ensure compliance with the City's Noise Control Ordinance. Site features, including enhanced landscaping and 19 to 20-foot high screen walls, would further attenuate noise, glare and other emissions beyond the project boundary. Site lighting would be fully shielded and subject to compliance with the City's light pollution regulations.

Economic Development Element

Goal: Underutilized employment land will be efficiently and profitably repurposed.

- **Policy EDE-2d-1:** Identify underutilized and obsolete commercial and industrial properties with the greatest potential for redeveloping into more productive use to enhance the City's competitive position in the regional economy.

The 31.79-acre project site was previously developed with a manufacturing facility that was demolished in 2022. The property is one of the last remaining vacant industrial zoned sites in the City's industrial land inventory. Due to the existing soil contamination and remediation requirements, redevelopment of the site would be subject to all terms of the CLRRA agreement, including restrictions on future land use. Development of the site into a multi-building industrial project, in accordance with the CLLRA agreement, would optimize the redevelopment potential of the site and provide opportunities for a variety of industrial uses.

The MBTRA would also make a significant contribution towards addressing the City's jobs-to-housing ratio by introducing up to 499 permanent jobs as forecasted by the applicant and noted in the EIR.

Goal: The City will maintain a broad based local economy

- **Policy EDE-3b-2:** Promote base industries that export goods and services, as well

as businesses that provide goods and services to the community.

The MBTRA project would provide four shell industrial buildings suited for a variety of businesses that provide goods and services to the community.

Energy and Climate Action Element

- Policy ECAE-1a-2: Require that new development supply a portion of its energy demand through renewable sources, to the extent practical and financially feasible.

The MBTRA would include rooftop mounted solar PV systems to supply at least 50 percent of forecasted electricity demand in accordance with Article 30, Section 3047 of the Zoning Ordinance.

- Policy ECAE-5a-7: Encourage new development to incorporate shade trees, to the extent practical and financially feasible.

The proposed landscape plan would comply with the urban forestry standards outlined Article 30, Section 3049 of the Zoning Ordinance. The MBTRA project would provide a tree canopy of 14 percent, which would exceed the required 12 percent for a project site area greater than one acre.

2. Airport Land Use Compatibility Plan (ALUCP) Compliance

The project site is located within the Airport Influence Area (AIA) for the Oceanside Municipal Airport – Airport Land Use Compatibility Plan (ALUCP). The ALUCP has the basic function of promoting compatibility between the airport and surrounding land uses with the objective of protecting against constraints on airport operations and minimizing the public's exposure to excessive noise and safety hazards.

New development within the AIA is subject to consistency review by the San Diego County Regional Airport Authority (SDCRAA) which acts as the Airport Land Use Commission (ALUC) of San Diego County. The MBTRA project was submitted to the SDCRAA for consistency review and was found to be conditionally consistent with the Oceanside Municipal Airport ALUCP as provided in the ALUC Consistency Determination dated October 8, 2024.

ALUC review focused on the project's compatibility in terms of safety, noise, airspace protection, and overflight. The ALUC determined that the proposed manufacturing and warehouse uses are compatible with the airport, including the maximum intensity of people per acre. The MBTRA was also found consistent with the ALUCP in terms of maximum height, lot coverage, setbacks, and noise compatibility. A copy of the ALUC Consistency Determination is provided as Attachment No. 7.

3. Zoning Ordinance Compliance

Development Plan

The project site is subject to Article 13 IL zoning regulations of the Zoning Ordinance.

Development Standards	Required	Proposed
Minimum Lot Size	20,000 sf.	31.79 acres
Setbacks		
Front Yard	10 feet	132 feet
Side Yard (south)	0 feet	179 feet
Side Yard (north)	0 feet	80-151 feet
Rear Yard	0 feet	150 feet
Maximum Height	80 feet	24-45 feet
Maximum Lot Coverage	75%	36%
Landscaping	12%	23%
Parking	Office 1/300 sf = 136 spaces Warehouse 1/2,000 sf = 223 spaces Manufacturing 1/750 sf = 164 spaces Total = 523 spaces	593 spaces

The MBTRA project complies with all requirements of the Zoning Ordinance, except as requested through a variance. Additionally, the MBTRA has been conditioned to comply with the recently adopted Climate Action Ordinances that address renewable energy, electric-vehicle charging and tree canopy.

Conditional Use Permit

In accordance with Section 1320 of the Zoning Ordinance, a CUP is required for both storage and distribution facilities exceeding 50,000 square feet of floor area and trucking terminals accessory to storage and distribution facilities having more than six heavy trucks on the premises at one time. Trucking terminals are generally described as an area of a building where trucks load and unload cargo. Each building would have a specific number of truck bays with rollup doors and concrete ramps. The MBTRA would include a total of 56 truck bays divided among all four buildings.

The shell building design would allow flexibility for a variety of light industrial uses, including wholesaling, distribution, and storage facilities. A proposed condition of approval would limit the number of truck bays to a maximum of 56 to ensure larger scale distribution uses would not occupy the facility. No additional truck bays or truck/trailer parking spaces

would be authorized. Any proposed modifications to the number of truck bays would require a formal amendment to the CUP.

To avoid potential conflicts with surrounding land uses, the MBTRA would be subject to conditions of approval regarding trucking operations, including the following requirements:

- The applicant shall file a Traffic Management Plan for approval by the City Traffic Engineer to regulate truck routes.
- The property owner shall prepare a Facilities Management Plan (FMP) to ensure future tenants comply with all conditions of approval. The FMP shall be provided to all tenants prior to building occupancy.
- The applicant shall prepare a Good Neighbor Policy as part of the FMP to prevent and enforce nuisance issues.
- Applicable measures from the CA Department of Justice's Warehouse Project Best Management Practices have been incorporated as conditions of approval to reduce potential impacts of warehouse operations.

As previously noted, the MBTRA would incorporate various design features to reduce potential impacts of the proposed trucking terminals on adjacent properties. These features would include enhanced tree canopy along the northern perimeter and the construction of 19 to 20-foot high screen walls adjacent to the truck maneuvering areas. The trucking terminals would also be oriented in an east-west direction away from residential uses to the north.

Variance

In accordance with Article 41 of the Zoning Ordinance, a variance is intended to resolve practical difficulties or unnecessary physical hardships that may result from the size, shape, or dimensions of a site or the location of existing structures thereon; from geographic, topographic, or other physical conditions on the site or in the immediate vicinity. The applicant is requesting to construct a flood wall around the perimeter of the site that would exceed the maximum allowable height of eight (8) feet per Article 30 of the Zoning Ordinance. As proposed, exterior facing wall elevations would range from 7.9 feet to 9.9 feet above the exterior grade.

According to the applicant, raising the site above the Base Flood Elevation (BFE) would not be feasible. The requested variance would help resolve practical difficulties of redeveloping the site by allowing increased height of the flood wall for additional flood protection and allow for proper grading. Compliance with the height limitation would reduce the effectiveness of the flood wall. As such, a variance is warranted subject to the findings for approval outlined in the resolution.

Findings

Per Article 43 of the Zoning Ordinance, the purpose of a Development Plan is to ensure that the architectural design of structures, and plans for the landscaping of open spaces conform with the requirements of the Zoning Ordinance, and with any prior conditions of

approval, as well as ensure that new development would not create public service and facility demands exceeding the capacity of existing and planned infrastructure.

Article 41 establishes review approval and disapproval procedures for conditional use permits (CUP) and variances. CUPs are typically required for use classifications having unusual site development features or operating characteristics requiring special consideration so that they may be designed, located, and operated compatibly with uses on adjoining properties and in the surrounding area. Variances are intended to resolve practical difficulties of unnecessary physical hardships that may result from size, shape, dimensions or from geographic, topographic, or other physical design characteristics.

The Planning Commission may approve an application for a development plan, conditional use permit, and variance subject to making the required findings provided in Articles 41 and 43 on the basis of the application, plans, materials, and testimony submitted. Staff finds that the MBTRA would be consistent with the requisite findings as provided in the attached resolution, and therefore recommends approval of the MBTRA project.

ENVIRONMENTAL DETERMINATION

In accordance with CEQA, an EIR has been prepared for the proposed project due to potentially significant impacts to air quality, biological resources, cultural resources, traffic and circulation, and tribal cultural resources. As described in the DEIR, implementation of mitigation measures would reduce potential impacts to a less-than-significant level for all identified environmental topic areas.

Pursuant to CEQA Guidelines Section 15087, the DEIR was circulated for public review beginning on October 26, 2023 and ending on December 29, 2023. A total of 80 comment letters were received during the public comment period. Per CEQA Guidelines Section 15088, all comments received on the DEIR during the public review period were evaluated and responded to and are incorporated into the FEIR.

The DEIR included an analysis of three project alternatives: No Project/No Development Alternative, Multi-Building Alternative, and Reduced Building Footprint Alternative. In response to public comments received on the DEIR, a proposed MBTRA has been included in Chapter 8, Section 8.4.4 of the FEIR, which is a variation on the Proposed Project and Multi-Building Alternative previously evaluated in the DEIR. The City, as Lead Agency, recommends approval of the MBTRA which is considered the preferred alternative that meets most of the objectives of the project while lessening potentially significant impacts of the project.

The MBTRA would meet all proposed project objectives, with the exception of objective 3 to *“Maximize the allowable use of an existing industrial zoned site that is compatible with the adjacent light industrial zoned sites and Oceanside Municipal Airport”*. As provided in the EIR, the MBTRA would reduce the overall square footage of the proposed project and not maximize the allowable use of an existing industrial zoned site. However, the proposed four-building design would maintain compatibility with adjacent light industrial zoned sites and the airport.

The FEIR, including the Mitigation Monitoring and Reporting Program (MMRP), findings of fact, and responses to public comments, are available on the City's website at:

<https://www.ci.oceanside.ca.us/government/development-services/planning/ceqa/eddie-jones-warehouse-manufacturing-and-distribution-facility-project-eir>

Staff has reviewed the MBTRA and determined that with the implementation of project conditions and mitigation measures contained within the MMRP, no significant impacts are anticipated. Therefore, staff recommends that the Planning Commission certify the FEIR with the Findings of Fact and MMRP for the MBTRA project.

PUBLIC NOTIFICATION

Legal notice was published in the newspaper and notices were sent to property owners within 1,500 feet and occupants within 100 feet of the subject property, individuals/organizations requesting notification, the applicant, and interested parties. A significant amount of correspondence has been received regarding the proposed project. Letters and emails addressed to staff, the Planning Commission, and City Council are included as Attachment No. 9. Primary concerns raised by opponents regarding the proposed project include traffic, air pollution, light pollution, noise, fire evacuation, pedestrian and cyclist safety, impacts to wildlife, compatibility, and hours of operation.

SUMMARY

The proposed Development Plan (D22-00001), Conditional Use Permit (CUP22-00001), and Variance (V22-00001) for the MBTRA project are consistent with the policies of the General Plan and the requirements of the Zoning Ordinance. The proposed land use and design of the project is consistent and compatible with existing industrial development in the vicinity of Oceanside Municipal Airport and the surrounding area. As such, staff recommends that the Planning Commission approve the MBTRA project based on the findings and subject to the conditions contained in the attached Resolution.

Staff recommends that the Planning Commission by motion:

- (1) Certify the Environmental Impact Report (EIR) and adopt the associated findings of fact and Mitigation Monitoring and Reporting Program (MMRP) by adopting Planning Commission Resolution No. 2025-P05; and
- (2) Approve Development Plan, Conditional Use Permit, and Variance for the Multi-Building and Truck Bay Reduction Alternative (MBTRA) project by adopting Planning Commission Resolution No. 2025-P04 with finding and conditions approval attached herein.

PREPARED BY:



Rob Dmohowski
Principal Planner

SUBMITTED BY:



Sergio Madera
City Planner

Attachments:

1. Planning Commission Resolution No. 2025-P04 for MBTRA project
2. Planning Commission Resolution 2025-P05 for FEIR with Findings of Fact and MMRP
3. Plans/Site Map (online)
4. FEIR (online)
5. Community Outreach Report (online)
6. Local Transportation Study (LTS)(online)
7. Airport Land Use Commission Determination of Consistency
8. Other Attachments – Application Page, Description and Justification, Legal Description (online)
9. Public Correspondence

ABBREVIATIONS

Table of abbreviations for architectural and construction terms, including symbols for materials, dimensions, and components.

Multi-Building & Truck Bay Reduction Alternative

EDDIE JONES INDUSTRIAL PROJECT - 4 BLDG ALTERNATE

260 EDDIE JONES WAY OCEANSIDE, CA 92058

SHEET INDEX

Table listing sheet titles and issue dates for various building components and site details.

OWNER

RPG 315 S. COAST HIGHWAY 101, SUITE U-12 ENCINITAS, CA 92024

PROJECT SCOPE

PROJECT DESCRIPTION: (4) NEW SPECULATIVE CONCRETE TILT MULTI-TENANT INDUSTRIAL BUILDINGS... SPECIAL CONDITIONS: OCEANSIDE AIRPORT SOUTH OF PROPERTY...

ARCHITECT

WARE MALCOMB

3911 SORRENTO VALLEY BLVD, SUITE #120 SAN DIEGO, CA 92121

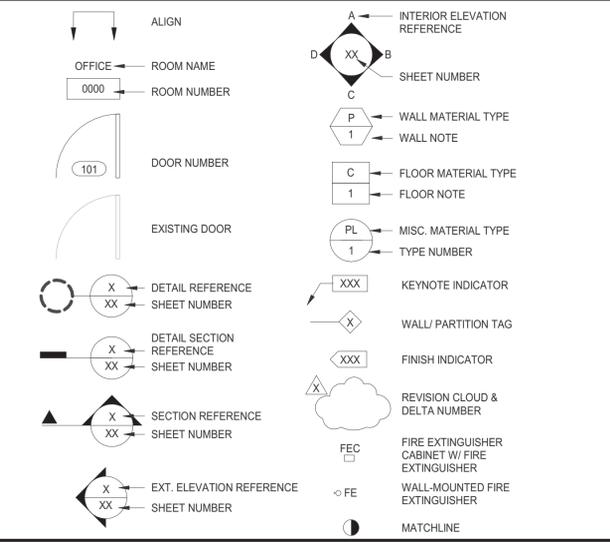
CONSULTANTS

ELECTRICAL ENGINEER: MPE CONSULTING... LANDSCAPE ARCHITECT: MLA-SD... CIVIL ENGINEER: PASCO LARET SUITER & ASSOCIATES

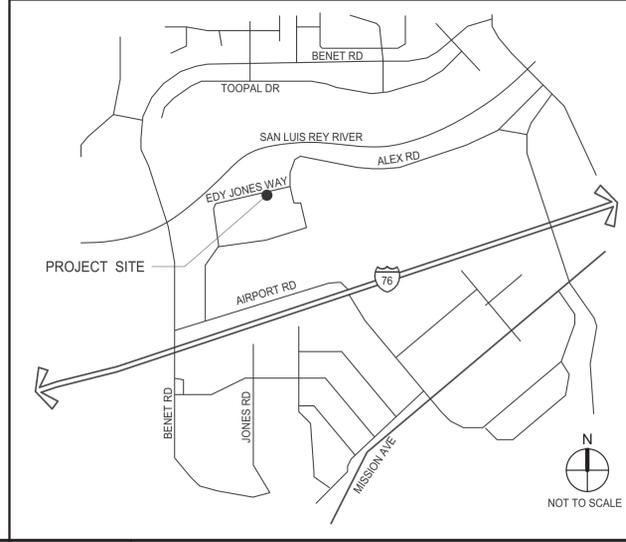
LEGAL DESCRIPTION

ACCESSOR'S PARCEL NO. 145-021-32-00, 145-021-29-00, 145-021-30-00... LEGAL DESCRIPTION: PARCEL 4A: (APN:145-021-32-00), PARCEL 4B: (145-021-29-00), PARCEL 4C: (145-021-30-00)

SYMBOLS



VICINITY MAP



WARE MALCOMB ARCHITECTURE CIVIL ENGINEERING PLANNING INTERIORS

EDDIE JONES INDUSTRIAL PROJECT - 4 BLDG ALT 260 EDDIE JONES WAY OCEANSIDE, CA 92058

Table with columns for DATE, REMARKS, and PLANNING SUBMITTAL.

PA/PM: S. NAGARAJ DRAWN BY: H.K. JOB NO.: SDG21-0057-01

SHEET A0.1

PROJECT DATA

SITE DATA		
LOT(GROSS)	1,384,746 S.F.	(31.79 ACRES)
EASEMENTS	43,309 S.F.	
RIPARIAN BUFFER	38,734 S.F.	
LOT(NET)	1,302,703 S.F.	(29.91 ACRES)
OVERALL DEVELOPMENT SUMMARY:		
BUILDING AREA:	ANCILLARY MEZZ:	FOOTPRINT:
BUILDING 1 AREA	1,560 SF	108,100 SF
BUILDING 2 AREA	1,560 SF	138,600 SF
BUILDING 3 AREA	1,560 SF	119,988 SF
BUILDING 4 AREA	1,560 SF	132,454 SF
TOTAL FOOTPRINT	6,240 SF	491,582 SF
OVERALL USE SUMMARY:		
BUILDING 1:		
OFFICE	7%	9,127 SQ FT
WAREHOUSE	68%	73,508 SQ FT
MANUFACTURING	25%	27,025 SQ FT
TOTAL GFA(INCLUDES MEZZANINE):		109,660 SQ FT
BUILDING 2:		
OFFICE	7%	10,733 SQ FT
WAREHOUSE	68%	89,107 SQ FT
MANUFACTURING	25%	32,760 SQ FT
TOTAL GFA (INCLUDES MEZZANINE):		132,600 SQ FT
BUILDING 3:		
OFFICE	7%	9,959 SQ FT
WAREHOUSE	68%	81,591 SQ FT
MANUFACTURING	25%	29,997 SQ FT
TOTAL GFA (INCLUDES MEZZANINE):		121,547 SQ FT
BUILDING 4:		
OFFICE	7%	10,832 SQ FT
WAREHOUSE	68%	90,069 SQ FT
MANUFACTURING	25%	33,114 SQ FT
TOTAL:		134,015 SQ FT
	TOTAL BUILDING AREA:	497,882 SQ FT
FAR:		@1.11/100 SF
GROSS		0.36
NET		0.40
COVERAGE:		
GROSS		36 %
NET		40 %
PARKING SUMMARY:		
BUILDING 1:		
PARKING FOR PROPOSED AND FUTURE OFFICE: 1/300		31 STALLS
PARKING FOR WAREHOUSE: 1/1500		49 STALLS
PARKING FOR MANUFACTURING: 1/750		36 STALLS
TOTAL PARKING REQUIRED		116 STALLS
PARKING PROVIDED		110 STALLS
TOTAL DOCK DOORS		12 DOORS
TOTAL GRADE DOORS		10 DOORS
BUILDING 2:		
PARKING FOR PROPOSED AND FUTURE OFFICE: 1/300		36 STALLS
PARKING FOR WAREHOUSE: 1/1500		60 STALLS
PARKING FOR MANUFACTURING: 1/750		44 STALLS
TOTAL PARKING REQUIRED		140 STALLS
PARKING PROVIDED		168 STALLS
TOTAL DOCK DOORS		12 DOORS
TOTAL GRADE DOORS		14 DOORS
BUILDING 3:		
PARKING FOR PROPOSED AND FUTURE OFFICE: 1/300		34 STALLS
PARKING FOR WAREHOUSE: 1/1500		55 STALLS
PARKING FOR MANUFACTURING: 1/750		40 STALLS
TOTAL PARKING REQUIRED		129 STALLS
PARKING PROVIDED		174 STALLS
TOTAL DOCK DOORS		14 DOORS
TOTAL GRADE DOORS		14 DOORS
BUILDING 4:		
PARKING FOR PROPOSED AND FUTURE OFFICE: 1/300		36 STALLS
PARKING FOR WAREHOUSE: 1/1500		61 STALLS
PARKING FOR MANUFACTURING: 1/750		45 STALLS
TOTAL PARKING REQUIRED		142 STALLS
PARKING PROVIDED		141 STALLS
TOTAL DOCK DOORS		18 DOORS
TOTAL GRADE DOORS		7 DOORS
OVERALL COUNTS:		
TOTAL PARKING REQUIRED		527 STALLS
TOTAL PARKING PROVIDED		593 STALLS
ADA PARKING REQUIRED		15 STALLS
ADA PARKING PROVIDED		22 STALLS
EV PARKING PROVIDED		90 STALLS
BIKE RACKS PROVIDED		30 RACKS
DEVELOPMENT STANDARDS:		
ZONING		IL
MAX FAR		1.00
MAX COVERAGE		75
MAX HEIGHT		80 FT
BUILDING SETBACKS:		
FRONT		10 FT
SIDE		0 FT
REAR		0 FT
LANDSCAPE SETBACKS:		
FRONT		10 FT
SIDE		0 FT
REAR		0 FT
LANDSCAPE REQUIRED		12 %

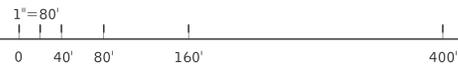
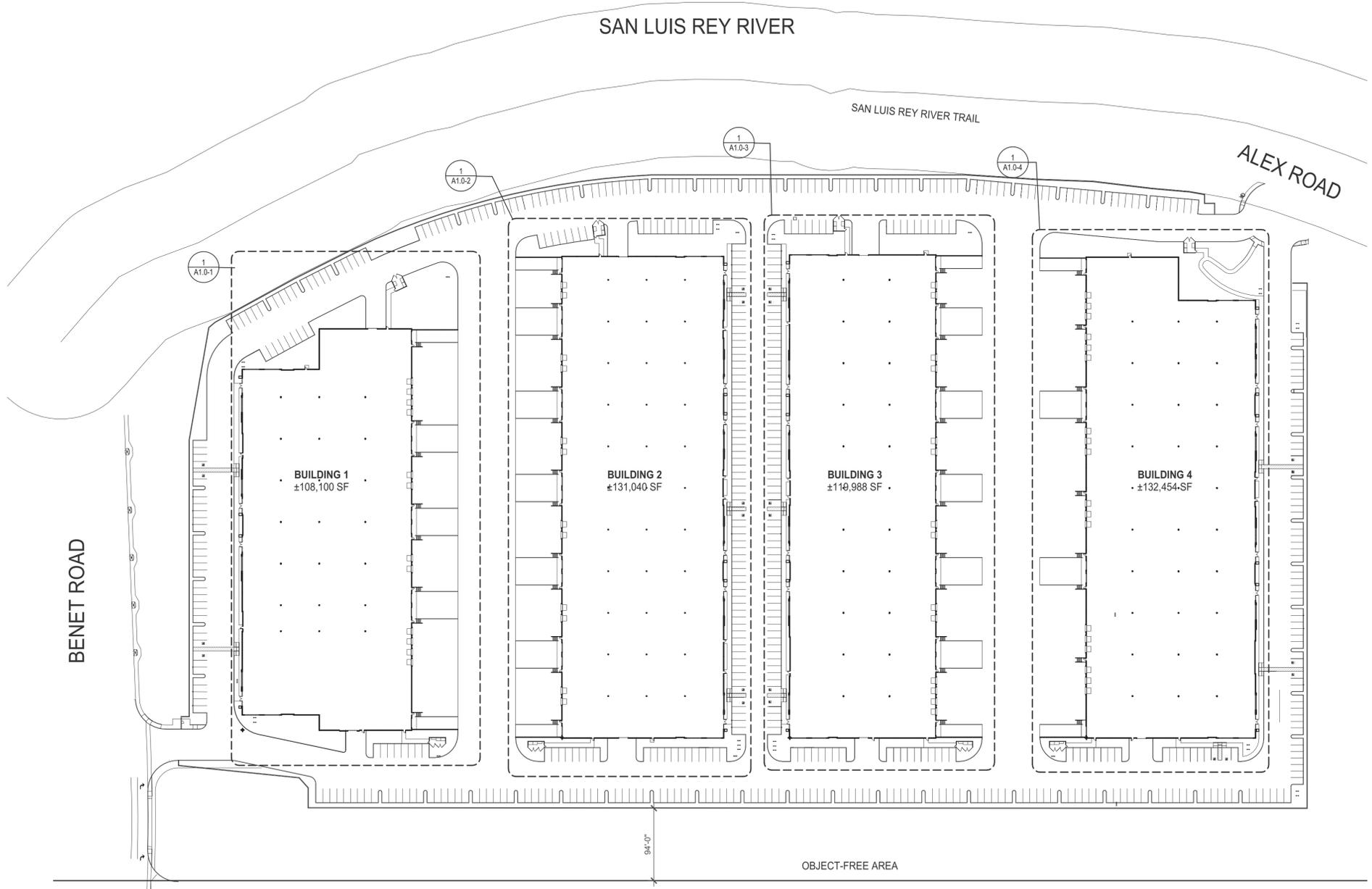
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 INTERIORS BUILDING MEASUREMENT
 3911 Sorrento Valley Blvd, Suite #120
 San Diego, CA 92121
 P 858.638.7277

**EDDIE JONES INDUSTRIAL
 PROJECT - 4 BLDG ALT
 260 EDDIE JONES WAY
 OCEANSIDE, CA 92058**

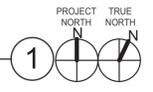
OVERALL - SITE PLAN	
DATE	REMARKS
05/31/2024	PLANNING SUBMITTAL
1	

PA/PM:	S. NAGARAJ
DRAWN BY.:	H.K.
JOB NO.:	SDG21-0057-01

SHEET
A1.0



OVERALL - SITE PLAN
 SCALE: 1" = 80'-0"

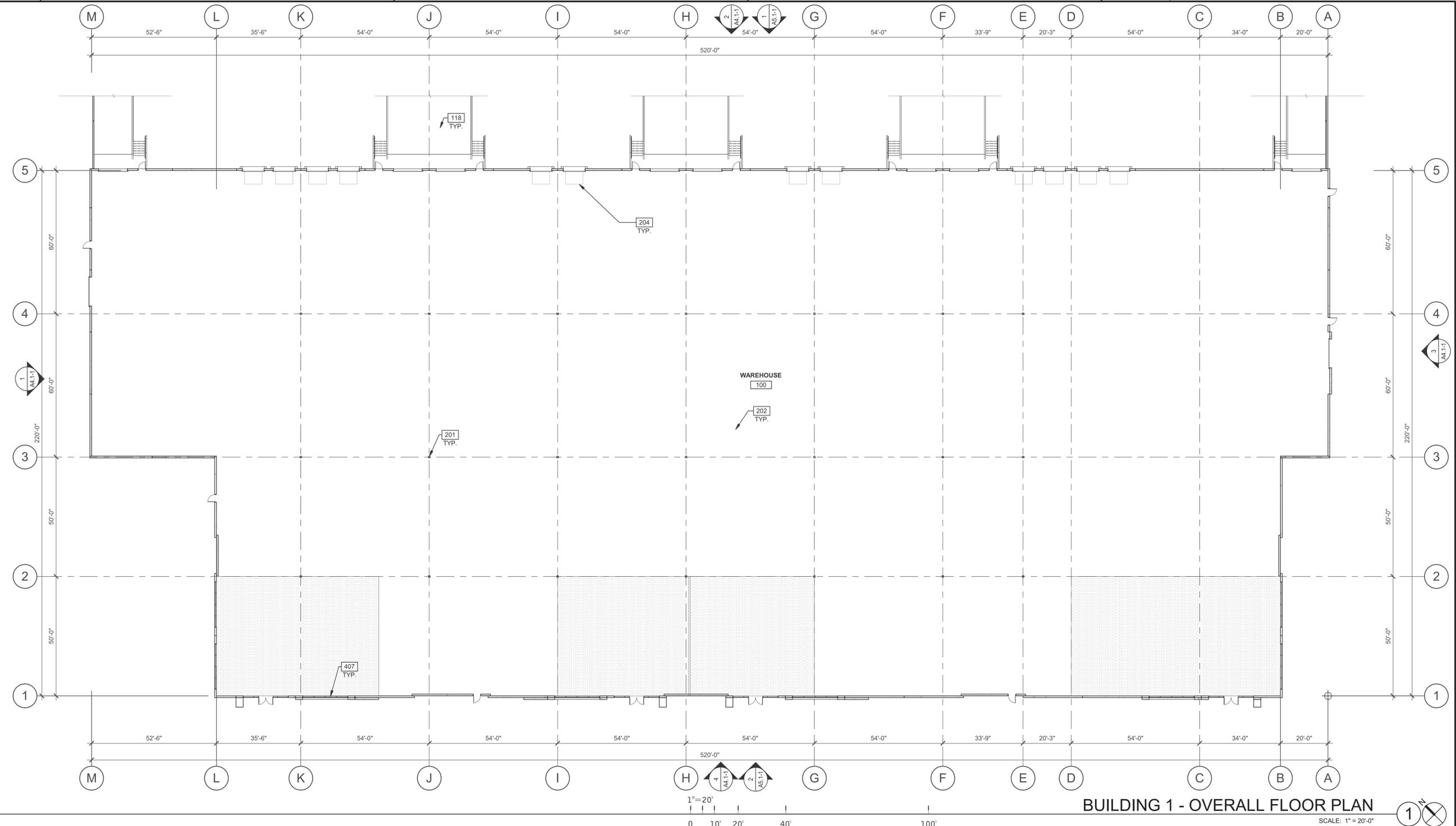


LEGEND

- ACCESSIBLE PATH OF TRAVEL: 1:20 MAX. SLOPE, 2% MAX. CROSS SLOPE.
- - - PROPERTY LINE.
- ☐ POLE MOUNTED LIGHT FIXTURE.
- ☐ WALLPACK LIGHT FIXTURE.
- T TRANSFORMER WITH CONCRETE PAD. (PROVIDE PROTECTION BOLLARDS PER LOCAL UTILITY OR PUBLIC WORK STANDARDS)
- ☐ LANDSCAPE AND IRRIGATION AREA.
- ▨ FIRE LANE (HATCHED)
- ① PARKING STALL COUNT TOTAL
- ☐ DOCK HIGH TRUCK DOOR
- ☐ GRADE LEVEL TRUCK DOOR
- ☐ FUTURE DOCK HIGH TRUCK DOOR
- FIRE HYDRANT
- P.I.V. WITH TAMPER

BUILDING 1 - OVERALL FLOOR PLAN	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY:	H.M.
JOB NO.:	SDG21-0057-01



LEGEND

PROPOSED FUTURE OFFICE AREA

WALL/ PARTITION LEGEND

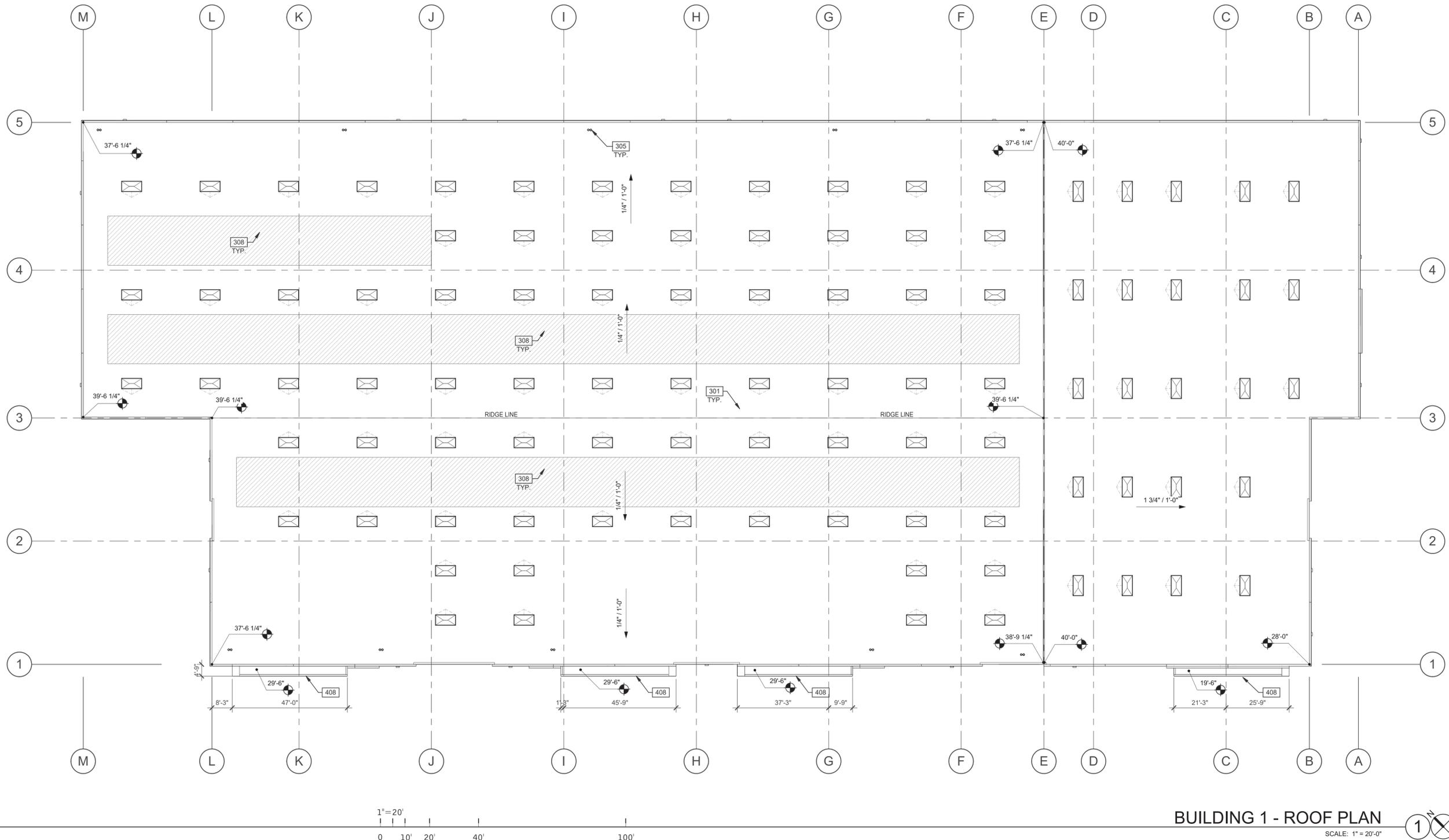
CONCRETE WALL

KEYNOTES:

- SEE SHEET A010 FOR GENERAL NOTES
- 118 CONCRETE RAMP.
 - 201 STRUCTURAL STEEL COLUMN.
 - 202 CONCRETE SLAB. PROVIDE VAPOR RETARDER OVER SAND BASE AT OFFICE AREA PER SOILS REPORT. PROVIDE SEALER FOR CONCRETE FLOOR AREA IN WAREHOUSE.
 - 204 6'X 8' HYDRAULIC DOCK LEVELER.
 - 407 ALUMINUM STOREFRONT SYSTEM WITH 1" TINTED INSULATED GLASS.

GENERAL NOTES

- 1 DOCUMENTATION TO TENANT CONSTRUCTION COORDINATOR OR ARCHITECT UPON COMPLETION.
- 2 CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS (IF ANY) AND NOTIFY ARCHITECT OF ANY DISCREPANCIES.
- 3 CONTRACTOR TO INCLUDE ALL FLOOR PREPARATIONS IN BASE BID.
- 4 ALL EXTERIOR WALLS TO BE TILT-UP CONCRETE PANEL, UNLESS NOTED OTHERWISE.
- 5 DETAIL REFERENCES SHOULD BE APPLIED TO ALL INSTANCES WHERE THE SAME CONDITIONS OCCUR, UNLESS NOTED OTHERWISE.
- 6 DIMENSIONS ARE GIVEN TO THE COLUMN GRID, FACE OF STUD, CENTERLINE OF OPENING, OR ROUGH OPENING UNLESS NOTED OTHERWISE, ALIGN FINISHES WHERE INDICATED.
- 7 REFER TO DESIGN CRITERIA DOCUMENT FOR ADDITIONAL INFORMATION.



BUILDING 1 - ROOF PLAN
 SCALE: 1" = 20'-0"

CALCULATIONS

SKYLIGHTS:
 SKYLIGHT SIZE: 48"x96"=32 S.F.
 (WAREHOUSE AREA S.F.) x 3.0% = $\frac{94,881 \times .03}{32}$
 REQUIRED: 88 SKYLIGHTS
 PROVIDED: 95 SKYLIGHTS

FUTURE SOLAR:
 ROOF AREA: 108,100 S.F.
 SKYLIGHT AREA: 3,040 S.F.
 ROOF AREA LESS SKYLIGHT: 105,060 S.F.
 15% OF ROOF AREA (LESS SKYLIGHTS): 15,759 S.F.
 TOTAL AREA FOR FUTURE SOLAR REQUIRED: 15,859 S.F.*
 TOTAL AREA FOR FUTURE SOLAR PROVIDED: 16,460 S.F.

*CEC SECTION 110.10(B)(4) DOES NOT REQUIRE THE INCLUSION OF ANY COLLATERAL LOADS FOR FUTURE SOLAR ENERGY SYSTEMS.

LEGEND

- SKYLIGHT: (2%) COORDINATE EXACT LOCATION TO PREVENT CONFLICT WITH FRAMING, FIRE SPRINKLER LINES, ELECTRICAL CONDUITS AND LIGHTING. SEE STRUCTURAL DRAWINGS.
- 301 BUILT-UP ROOF OVER WOOD DECK.
- 305 PRIMARY AND SECONDARY DRAINS.
- 308 FUTURE SOLAR.
- 408 METAL CANOPY, PAINTED.

KEYNOTES:

- SEE SHEET A010 FOR GENERAL NOTES
- 301 BUILT-UP ROOF OVER WOOD DECK.
 - 305 PRIMARY AND SECONDARY DRAINS.
 - 308 FUTURE SOLAR.
 - 408 METAL CANOPY, PAINTED.

BUILDING 1 - ROOF PLAN

DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL
1	

PA/PM:	S. NAGARAJ
DRAWN BY.:	H.M.
JOB NO.:	SDG21-0057-01

SHEET
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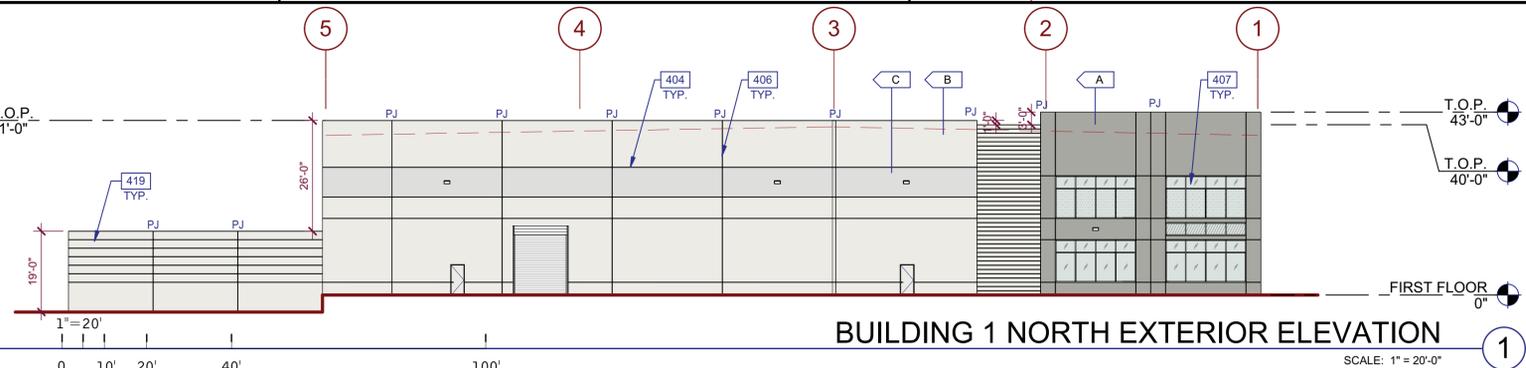
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**EDDIE JONES INDUSTRIAL
 PROJECT (4 BLDG ALT) BLDG 1**
 260 EDDY JONES WAY
 OCEANSIDE, CA 92058

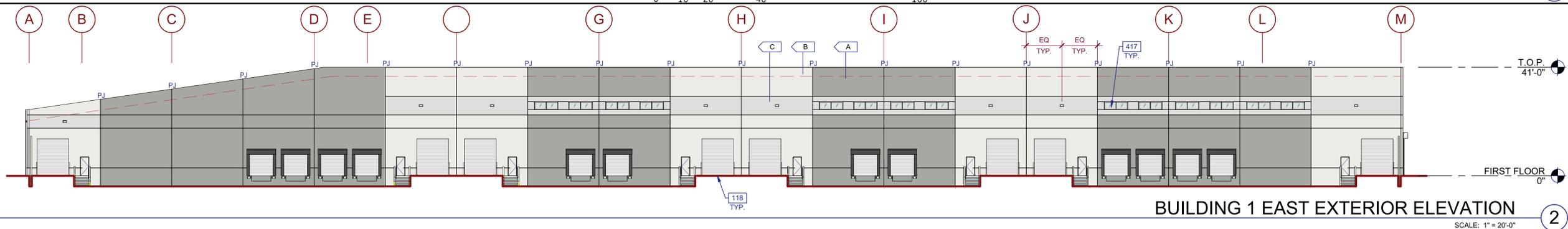
BUILDING 1 - EXTERIOR ELEVATION	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL
1	

PA/PM:	S. NAGARAJ
DRAWN BY.:	H.M.
JOB NO.:	SDG21-0057-01

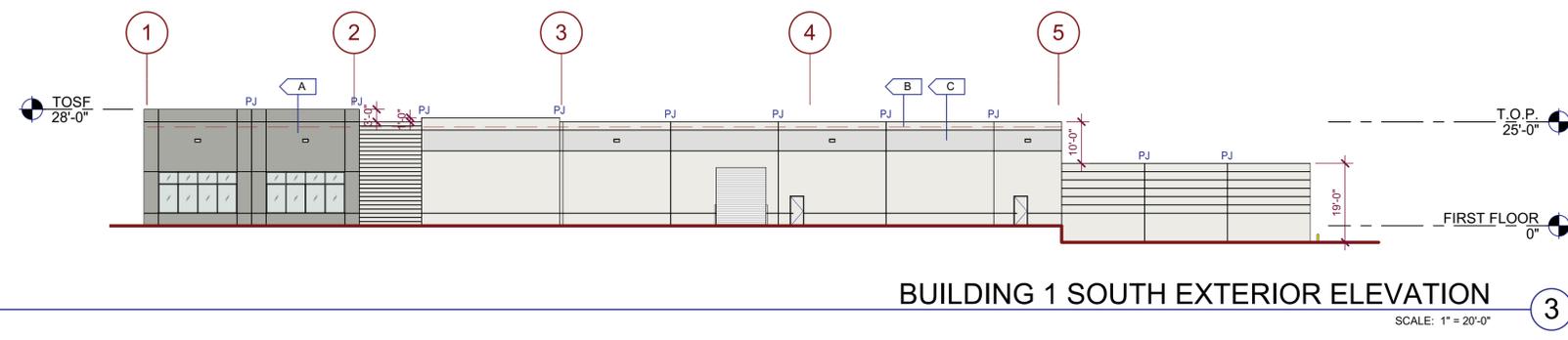
SHEET
A4.1-1



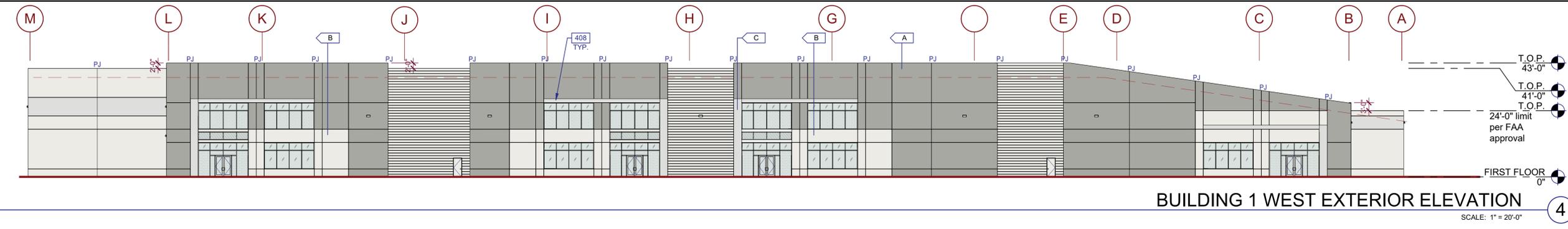
BUILDING 1 NORTH EXTERIOR ELEVATION



BUILDING 1 EAST EXTERIOR ELEVATION



BUILDING 1 SOUTH EXTERIOR ELEVATION



BUILDING 1 WEST EXTERIOR ELEVATION

LEGEND

- MATERIALS:**
- METAL CANOPY, PAINTED
 - VISION GLASS
 - SPANDREL GLASS
 - TEMPERED GLASS

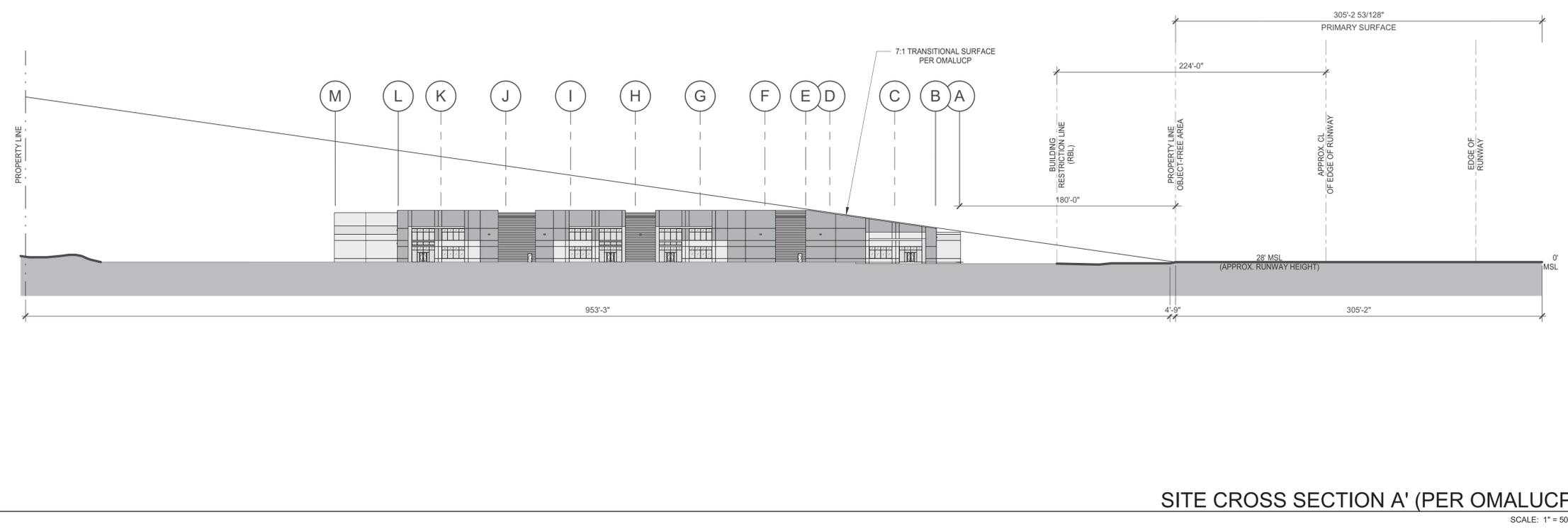
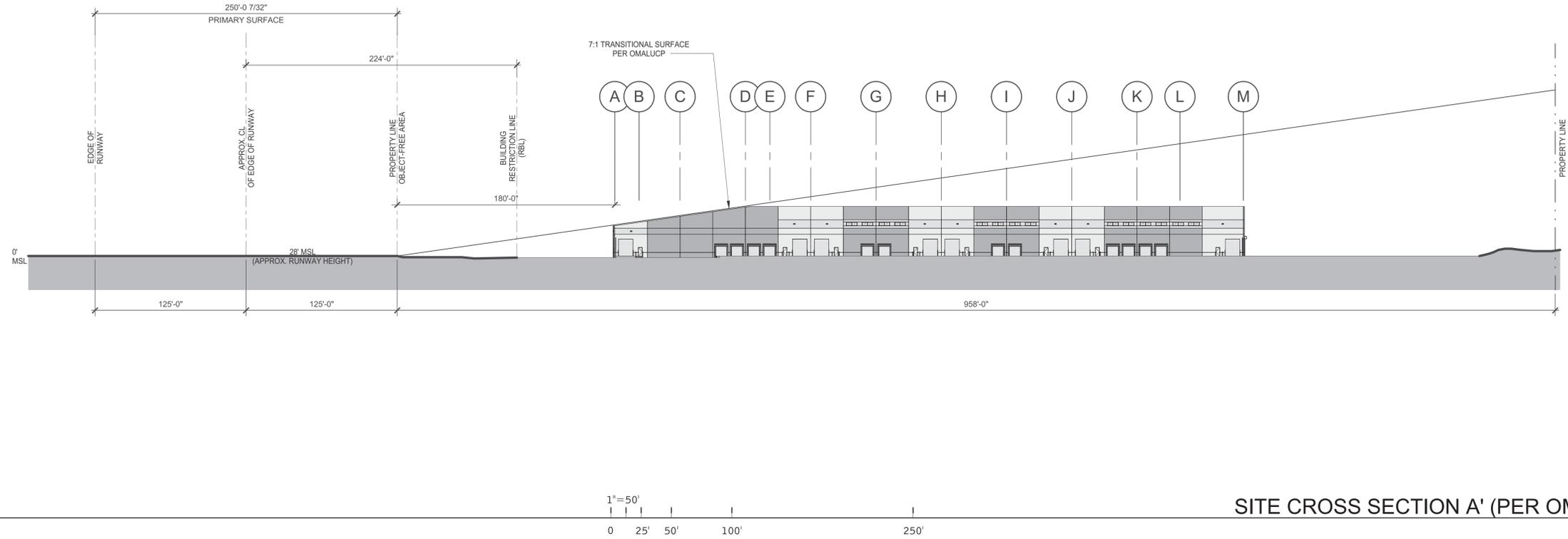
- COLORS:**
- PROVIDE 6'-0" WIDE PAINT COLOR MOCK-UP FULL HEIGHT OF BUILDING FOR OWNER/ARCHITECT REVIEW.
- ALL WALLS TO BE PAINTED WITH BASE COLOR U.O.N.
- BASE COLOR: SHERWIN WILLIAMS - SW 7066 GRAY MATTERS
 - SECONDARY COLOR: SHERWIN WILLIAMS - SW 7005 PURE WHITE
 - ACCENT COLOR: SHERWIN WILLIAMS - SW 7063 NEBULOUS WHITE

GLAZING NOTES

- EACH LIGHT SHALL BEAR THE MANUFACTURER'S LABEL DESIGNATING THE TYPE AND THICKNESS OF THE GLASS.
- GLASS SHALL BE FIRMLY SUPPORTED ON ALL FOUR EDGES, U.O.N.
- FIELD MEASURE ALL OPENINGS PRIOR TO FABRICATION.
- FIXED OR OPERABLE GLAZING IN HAZARDOUS LOCATIONS AS DEFINED IN THE BUILDING CODE SHALL BE TEMPERED GLASS (SAFETY GLASS).
- GLAZING IN SWINGING, SLIDING AND BIFOLD DOORS SHALL BE TEMPERED.
- ALL GLAZING WITHIN A 24" ARC OF EITHER EDGE OF A DOOR AND WITHIN 60" OF THE FLOOR SHALL BE TEMPERED.
- GLAZING IN WINDOWS: ALL GLAZING THAT MEETS ALL OF THE FOLLOWING CONDITIONS SHALL BE TEMPERED:
 - A. THE EXPOSED AREA OF AN INDIVIDUAL PANE IS GREATER THAN 9 SF.
 - B. THE BOTTOM EDGE IS LESS THAN 18" ABOVE THE FLOOR.
 - C. THE TOP EDGE IS GREATER THAN 36" ABOVE THE FLOOR.
 - D. WALKING SURFACE IS WITHIN 36".
- ALL GLASS SHALL COMPLY WITH THE REQUIREMENTS OF THE STATE AND LOCAL CODES AND THE U.S. PRODUCT SAFETY COMMISSIONS: SAFETY STANDARDS FOR ARCHITECTURAL GLAZING MATERIALS.
- WHERE JOINTS ARE REQUIRED IN MULTIPLE LITES, CLEAR SILICONE IS TO BE PROVIDED, U.O.N.

KEYNOTES:

- 118 CONCRETE RAMP.
- 404 3/4" V-REVEAL.
- 406 CONCRETE WALL JOINT.
- 407 ALUMINUM STOREFRONT SYSTEM WITH 1" TINTED INSULATED GLASS.
- 408 METAL CANOPY, PAINTED.
- 417 CLERESTORY WINDOW.
- 419 CONCRETE SCREEN WALL, PAINTED.



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**EDDIE JONES INDUSTRIAL
PROJECT (4 BLDG ALT) BLDG 1**

260 EDDY JONES WAY
OCEANSIDE, CA 92058

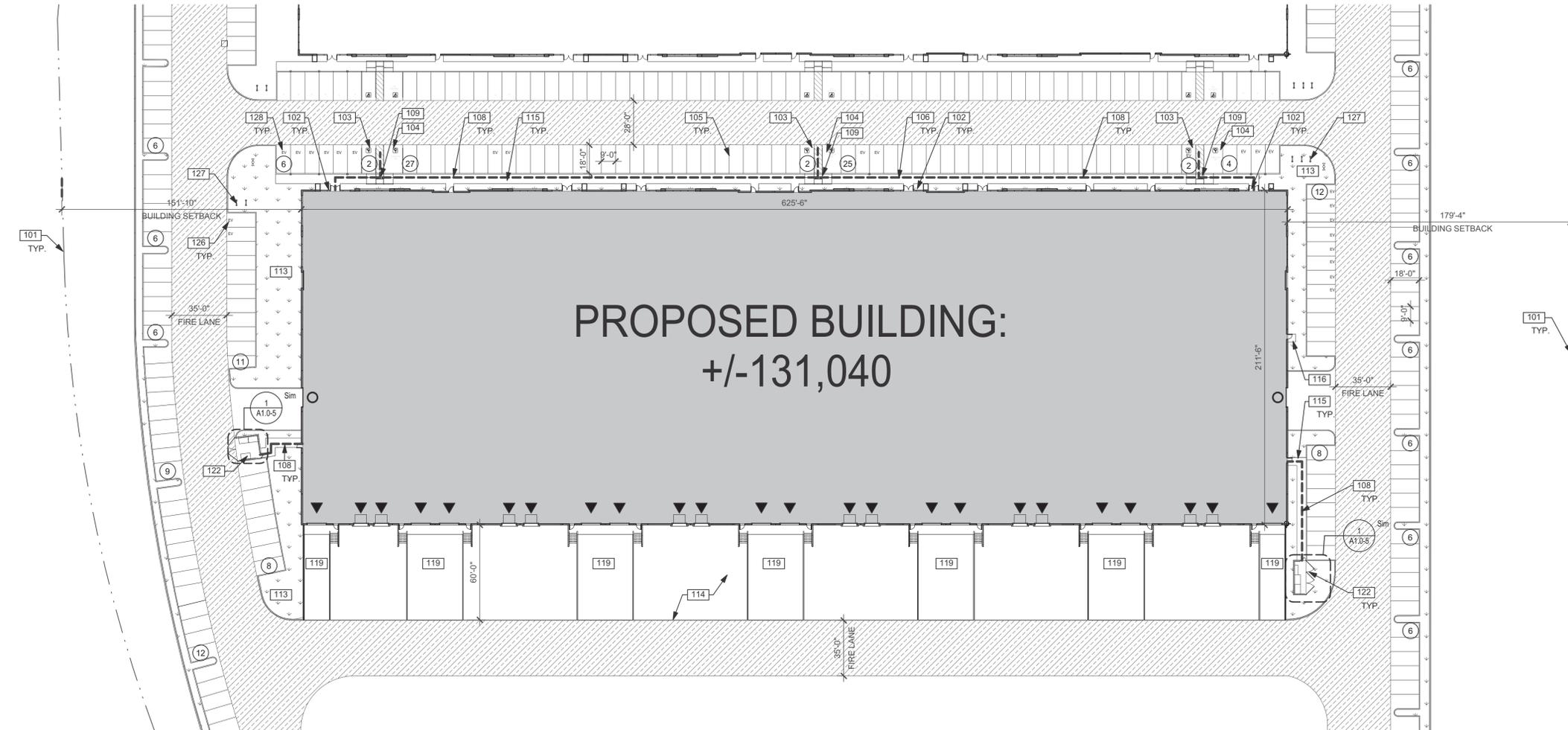
BUILDING 1 - BUILDING SECTIONS

DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL
1	

PA/PM:	S. NAGARAJ
DRAWN BY.:	H.M.
JOB NO.:	SDG21-0057-01

SHEET

A5.1-1



BUILDING 2 - ENLARGED SITE PLAN
 SCALE: 1" = 40'-0"

LEGEND

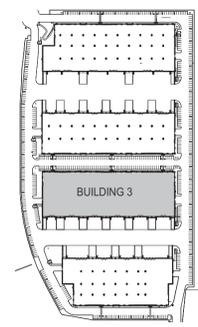
- ACCESSIBLE PATH OF TRAVEL, 1:20 MAX. SLOPE, 2% MAX. CROSS SLOPE.
- - - PROPERTY LINE.
- POLE MOUNTED LIGHT FIXTURE.
- WALLPACK LIGHT FIXTURE.
- T TRANSFORMER WITH CONCRETE PAD. (PROVIDE PROTECTION BOLLARDS PER LOCAL UTILITY OR PUBLIC WORK STANDARDS)
- LANDSCAPE AND IRRIGATION AREA.
- ▨ FIRE LANE (HATCHED)
- ① PARKING STALL COUNT TOTAL
- ▲ DOCK HIGH TRUCK DOOR
- GRADE LEVEL TRUCK DOOR
- FIRE HYDRANT
- P.I.V. WITH TAMPER

KEYNOTES: □

SEE SHEET A010 FOR GENERAL NOTES

- | | | | |
|-----|---|-----|--|
| 101 | PROPERTY LINE. | 115 | CONCRETE SIDEWALK. |
| 102 | ACCESSIBLE ENTRY SIGNAGE. | 116 | CONCRETE STOOP. |
| 103 | ACCESSIBLE PARKING STALL WITH SIGNAGE. | 117 | FLOOD GATE. |
| 104 | VAN ACCESSIBLE PARKING STALL WITH SIGNAGE. | 118 | FLOOD WALL. |
| 105 | PAINTED PARKING STRIPING PER CITY STANDARDS. | 119 | CONCRETE RAMP. |
| 106 | PRECAST CONCRETE WHEELSTOP. | 120 | CONCRETE RETAINING WALL. |
| 106 | 2'-0" PARKING OVERHANG. | 121 | POURED IN PLACE CONCRETE MONUMENT SIGN. |
| 108 | ACCESSIBLE PATH OF TRAVEL, 1:20 MAX. SLOPE, 2% MAX. CROSS SLOPE. | 122 | TRASH ENCLOSURE WITH RECYCLE BIN. |
| 109 | ACCESSIBLE CURB RAMP WITH TRUNCATED DOMES. | 122 | EASEMENT LINE. |
| 110 | FIRELANE ENTRY SIGNAGE. | 124 | STRIPES 5' X 20' MINIMUM AREA FOR PASSENGER DROP-OFF AND LOADING ZONE WITH SIGNAGE. |
| 111 | FIRELANE SIGNAGE. | 125 | RECESSED KNOX-BOX, INSTALL AT 5'-0" ABOVE FINISH FLOOR. COORDINATE FINAL LOCATION WITH FIRE INSPECTOR. |
| 112 | FIRELANE CURB, DASHED LINE INDICATES EXTENT OF CONTINUOUS CURB TO BE PAINTED RED. | 126 | CLEAN AIR/VANPOOL/EV DESIGNATED PARKING STALL. |
| 113 | LANDSCAPE AND IRRIGATION AREA. | 127 | SHORT-TERM AND LONG-TERM BIKE PARKING FOR THE PUBLIC AND EMPLOYEES. |
| 114 | CONCRETE PAVEMENT. | | |

KEY PLAN

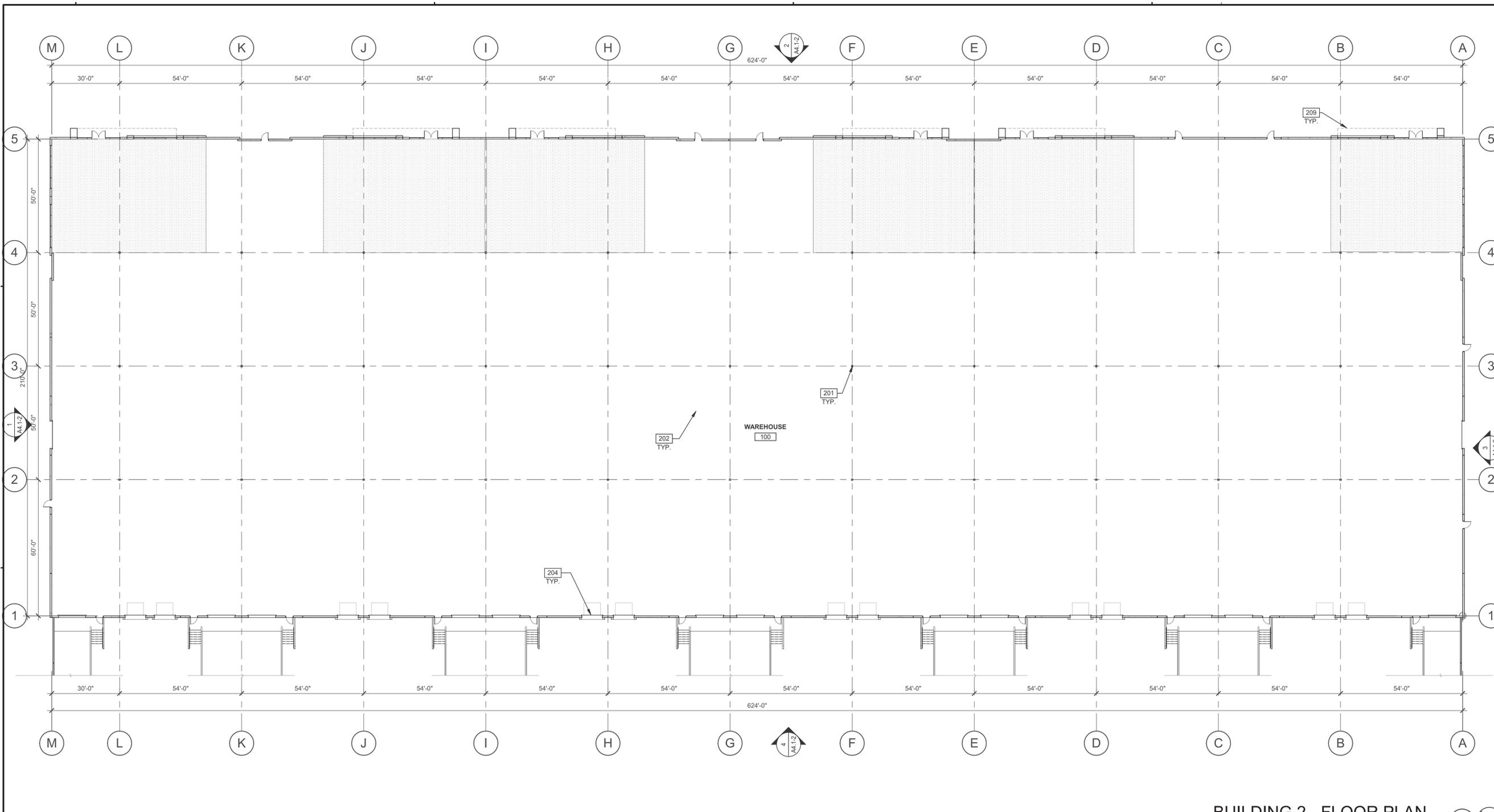


EDDIE JONES INDUSTRIAL PROJECT (4 BLDG ALT) BLDG 2
 260 EDDY JONES WAY
 OCEANSIDE, CA 92058

BUILDING 2 - ENLARGED SITE PLAN	REMARKS
DATE	PLANNING SUBMITTAL
05-31-2024	

PA/PM:	S. NAGARAJ
DRAWN BY:	HM
JOB NO.:	SDG21-0057-01

SHEET
A1.0-2



BUILDING 2 - FLOOR PLAN
SCALE: 3/64" = 1'-0"

LEGEND

PROPOSED FUTURE OFFICE AREA

WALL/ PARTITION LEGEND

CONCRETE WALL

KEYNOTES:

- SEE SHEET A010 FOR GENERAL NOTES
- 201 STRUCTURAL STEEL COLUMN. COMPLETION.
- 202 CONCRETE SLAB. PROVIDE VAPOR RETARDER OVER SAND BASE AT OFFICE AREA PER SOILS REPORT. PROVIDE SEALER FOR CONCRETE FLOOR AREA IN WAREHOUSE.
- 204 6'X 8' HYDRAULIC DOCK LEVELER.
- 209 OUTLINE OF CANOPY ABOVE.

GENERAL NOTES

- 1 DOCUMENTATION TO TENANT CONSTRUCTION COORDINATOR OR ARCHITECT UPON COMPLETION.
- 2 CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS (IF ANY) AND NOTIFY ARCHITECT OF ANY DISCREPANCIES.
- 3 CONTRACTOR TO INCLUDE ALL FLOOR PREPARATIONS IN BASE BID.
- 4 ALL EXTERIOR WALLS TO BE TILT-UP CONCRETE PANEL, UNLESS NOTED OTHERWISE. DETAIL REFERENCES SHOULD BE APPLIED TO ALL INSTANCES WHERE THE SAME CONDITIONS OCCUR, UNLESS NOTED OTHERWISE.
- 5 DIMENSIONS ARE GIVEN TO THE COLUMN GRID, FACE OF STUD, CENTERLINE OF OPENING, OR ROUGH OPENING UNLESS NOTED OTHERWISE, ALIGN FINISHES WHERE INDICATED.
- 6 REFER TO DESIGN CRITERIA DOCUMENT FOR ADDITIONAL INFORMATION.

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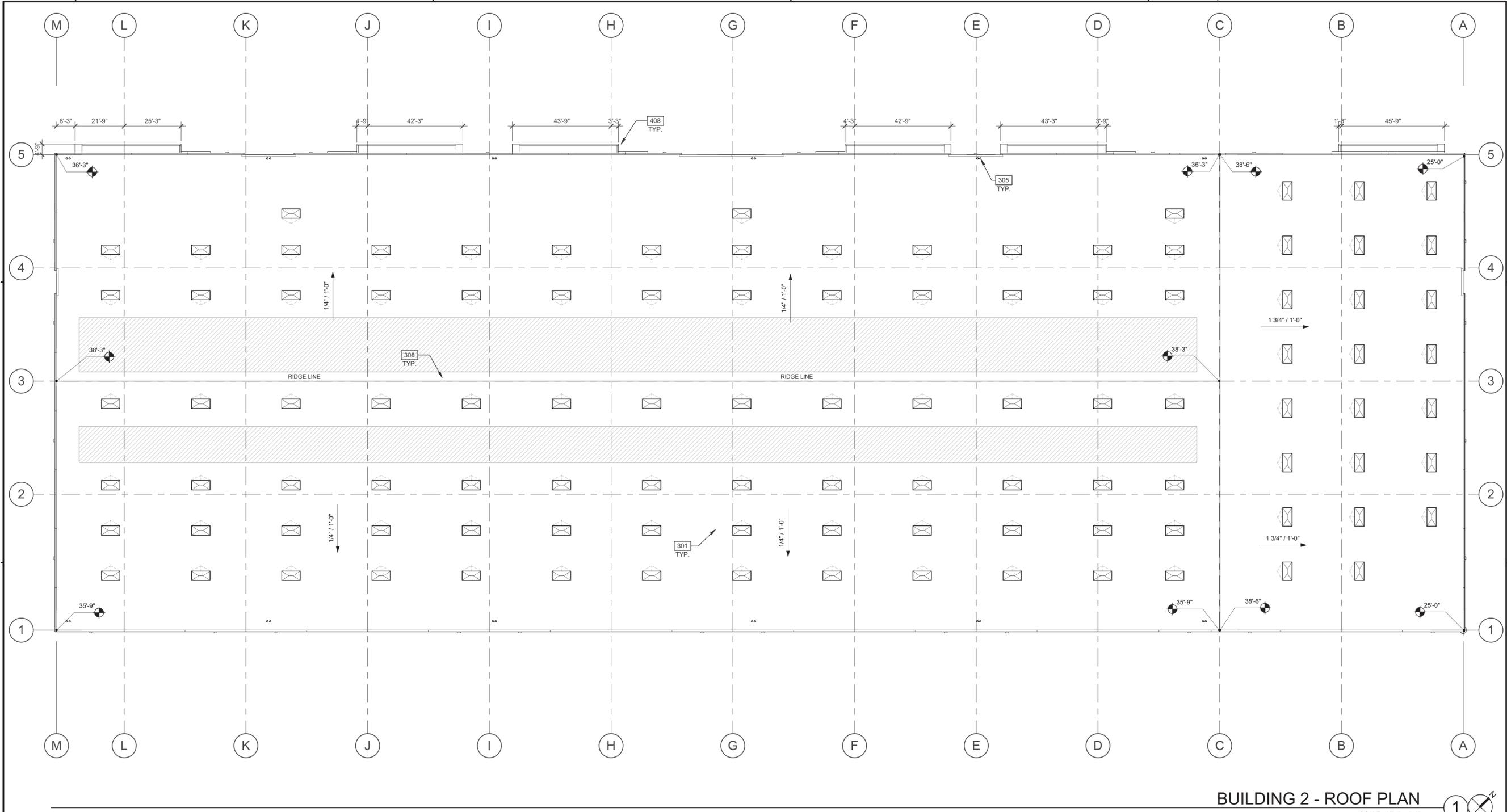
**EDDIE JONES INDUSTRIAL
 PROJECT (4 BLDG ALT) BLDG 2**
 260 EDDY JONES WAY
 OCEANSIDE, CA 92058

BUILDING 2 - FLOOR PLAN	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY:	Author
JOB NO.:	SDG21-0057-01

SHEET
A2.1-2

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BUILDING 2 - ROOF PLAN
SCALE: 3/64" = 1'-0"

CALCULATIONS

SKYLIGHTS:
SKYLIGHT SIZE: 48"x96"=32 S.F.
$$\frac{(\text{WAREHOUSE AREA S.F.}) \times 3.0\%}{(\text{SKYLIGHT SIZE})} = \frac{110,541 \times 0.03}{32}$$

REQUIRED: 103 SKYLIGHTS
PROVIDED: 104 SKYLIGHTS

FUTURE SOLAR:
ROOF AREA: 131,040 S.F.
SKYLIGHT AREA: 3,328 S.F.
ROOF AREA LESS SKYLIGHT: 127,712 S.F.
15% OF ROOF AREA (LESS SKYLIGHTS): 19,157 S.F.
TOTAL AREA FOR FUTURE SOLAR REQUIRED: 19,157 S.F.
TOTAL AREA FOR FUTURE SOLAR PROVIDED: 19,831 S.F.

* CEC SECTION 110.10(B)4 DOES NOT REQUIRE THE INCLUSION OF ANY COLLATERAL LOADS FOR FUTURE SOLAR ENERGY SYSTEMS.

LEGEND

SKYLIGHT: (2%)
COORDINATE EXACT LOCATION TO PREVENT CONFLICT WITH FRAMING, FIRE SPRINKLER LINES, ELECTRICAL CONDUITS AND LIGHTING. SEE STRUCTURAL DRAWINGS.

KEYNOTES:

SEE SHEET A010 FOR GENERAL NOTES

301 BUILT-UP ROOF OVER WOOD DECK.
305 PRIMARY AND SECONDARY DRAINS.
308 FUTURE SOLAR.
408 METAL CANOPY, PAINTED.

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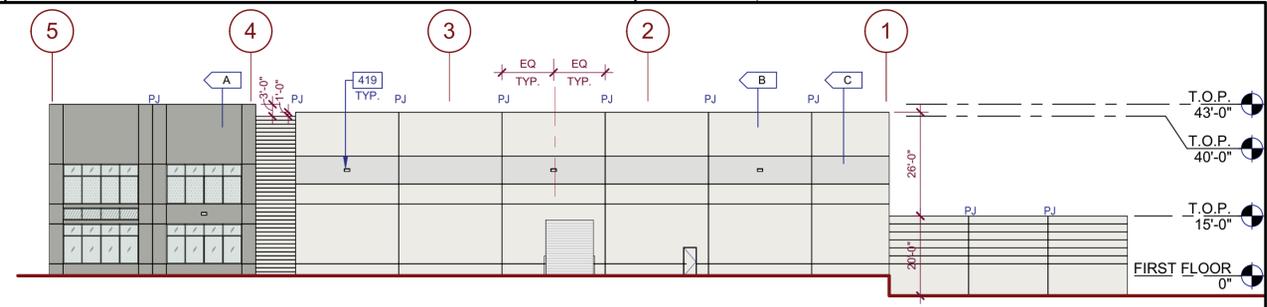
EDDIE JONES INDUSTRIAL PROJECT (4 BLDG ALT) BLDG 2
260 EDDY JONES WAY
OCEANSIDE, CA 92058

BUILDING 2 - ROOF PLAN

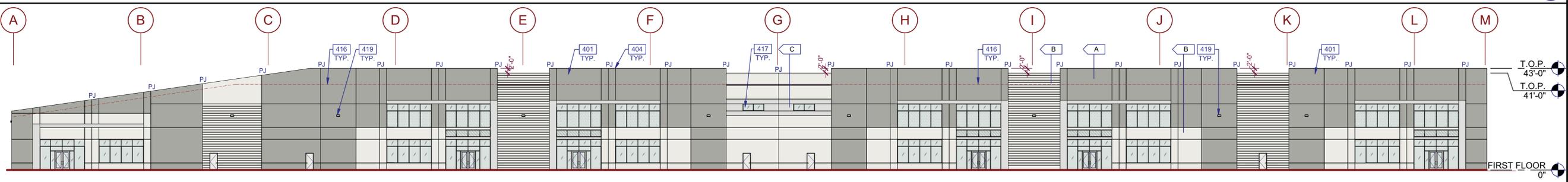
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

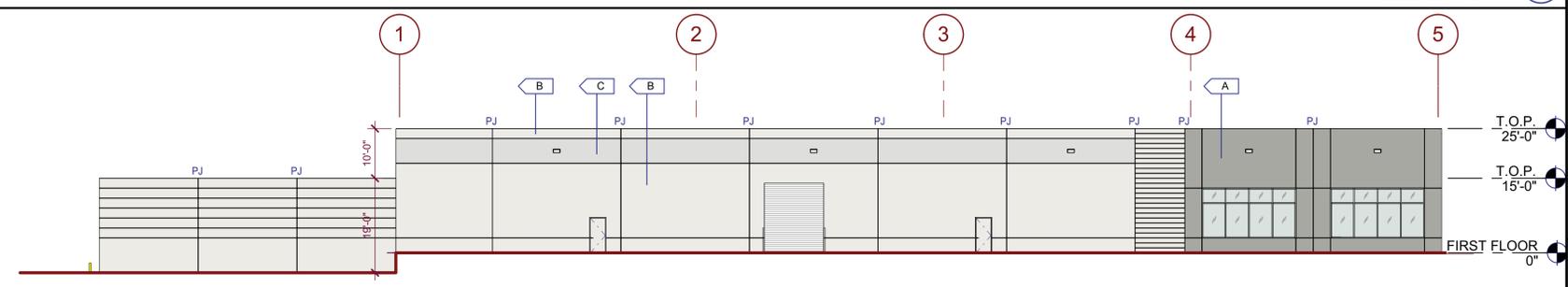
SHEET
A3.1-2



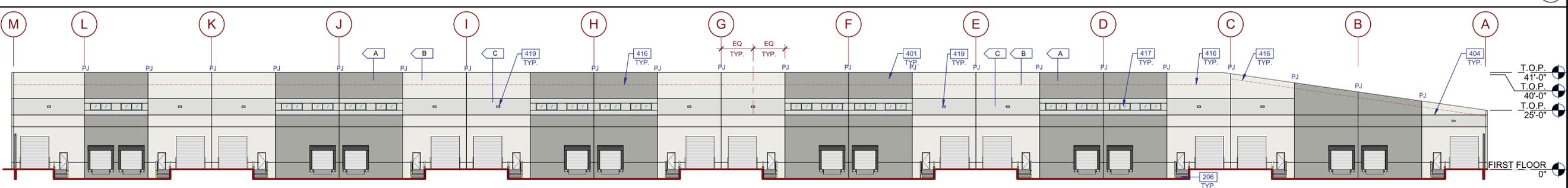
BUILDING 2 - NORTH EXTERIOR ELEVATION
 SCALE: 3/64" = 1'-0" 1



BUILDING 2 - EAST ELEVATION
 SCALE: 3/64" = 1'-0" 2



BUILDING 2 - SOUTH EXTERIOR ELEVATION
 SCALE: 1/16" = 1'-0" 3



BUILDING 2 - WEST EXTERIOR ELEVATION
 SCALE: 3/64" = 1'-0" 4

LEGEND

- MATERIALS:**
- METAL CANOPY, PAINTED
 - GLASS:
 - VISION GLASS
 - SPANDREL GLASS
 - TEMPERED GLASS

- COLORS:**
- PROVIDE 6'-0" WIDE PAINT COLOR MOCK-UP FULL HEIGHT OF BUILDING FOR OWNER/ARCHITECT REVIEW.
- ALL WALLS TO BE PAINTED WITH BASE COLOR U.O.N.
- BASE COLOR: SHERWIN WILLIAMS - SW 7066 GRAY MATTERS
 - SECONDARY COLOR: SHERWIN WILLIAMS - SW 7005 PURE WHITE
 - ACCENT COLOR: SHERWIN WILLIAMS - SW 7063 NEBULOUS WHITE

GLAZING NOTES

1. EACH LIGHT SHALL BEAR THE MANUFACTURER'S LABEL DESIGNATING THE TYPE AND THICKNESS OF THE GLASS.
2. GLASS SHALL BE FIRMLY SUPPORTED ON ALL FOUR EDGES, U.O.N.
3. FIELD MEASURE ALL OPENINGS PRIOR TO FABRICATION.
4. FIXED OR OPERABLE GLAZING IN HAZARDOUS LOCATIONS AS DEFINED IN THE BUILDING CODE SHALL BE TEMPERED GLASS (SAFETY GLASS).
5. GLAZING IN SWINGING, SLIDING AND BIFOLD DOORS SHALL BE TEMPERED.
6. ALL GLAZING WITHIN A 24" ARC OF EITHER EDGE OF A DOOR AND WITHIN 60" OF THE FLOOR SHALL BE TEMPERED.
7. GLAZING IN WINDOWS: ALL GLAZING THAT MEETS ALL OF THE FOLLOWING CONDITIONS SHALL BE TEMPERED:
 - A. THE EXPOSED AREA OF AN INDIVIDUAL PANE IS GREATER THAN 9 SF.
 - B. THE BOTTOM EDGE IS LESS THAN 18" ABOVE THE FLOOR.
 - C. THE TOP EDGE IS GREATER THAN 36" ABOVE THE FLOOR.
 - D. WALKING SURFACE IS WITHIN 36".
8. ALL GLASS SHALL COMPLY WITH THE REQUIREMENTS OF THE STATE AND LOCAL CODES AND THE U.S. PRODUCT SAFETY COMMISSIONS: SAFETY STANDARDS FOR ARCHITECTURAL GLAZING MATERIALS.
9. WHERE JOINTS ARE REQUIRED IN MULTIPLE LITES, CLEAR SILICONE IS TO BE PROVIDED, U.O.N.

KEYNOTES:

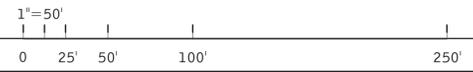
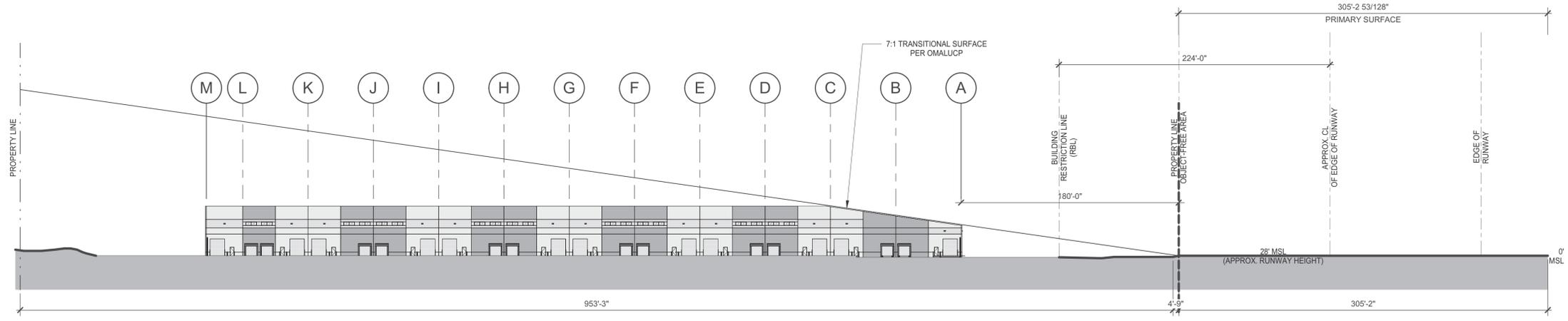
- 206 EXTERIOR CONCRETE STAIR, ALL COMPONENTS GALVANIZED AND PAINTED.
- 401 CONCRETE WALL, PAINTED.
- 404 3/4" V.REVEAL.
- 416 LINE OF ROOF BEYOND.
- 417 CLERESTORY WINDOW.
- 419 CONCRETE SCREEN WALL, PAINTED.

**EDDIE JONES INDUSTRIAL
 PROJECT (4 BLDG ALT) BLDG 2**
 260 EDDY JONES WAY
 OCEANSIDE, CA 92058

BUILDING 2 - EXTERIOR ELEVATIONS	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

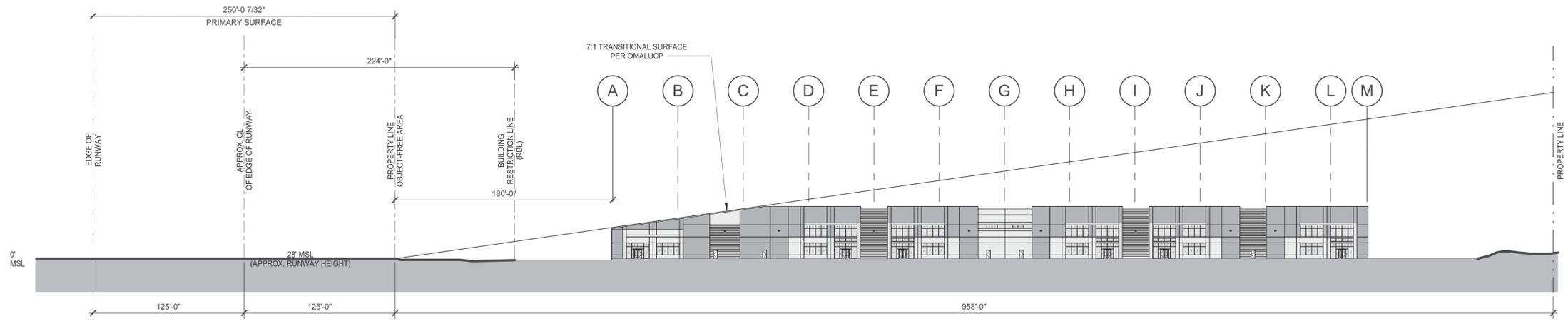
PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

SHEET
A4.1-2



SITE CROSS SECTION A' (PER OMalucp) ①

SCALE: 1" = 50'-0"



SITE CROSS SECTION A' (PER OMalucp) ②

SCALE: 1" = 50'-0"

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**EDDIE JONES INDUSTRIAL
 PROJECT (4 BLDG ALT) BLDG 2**
 260 EDDY JONES WAY
 OCEANSIDE, CA 92058

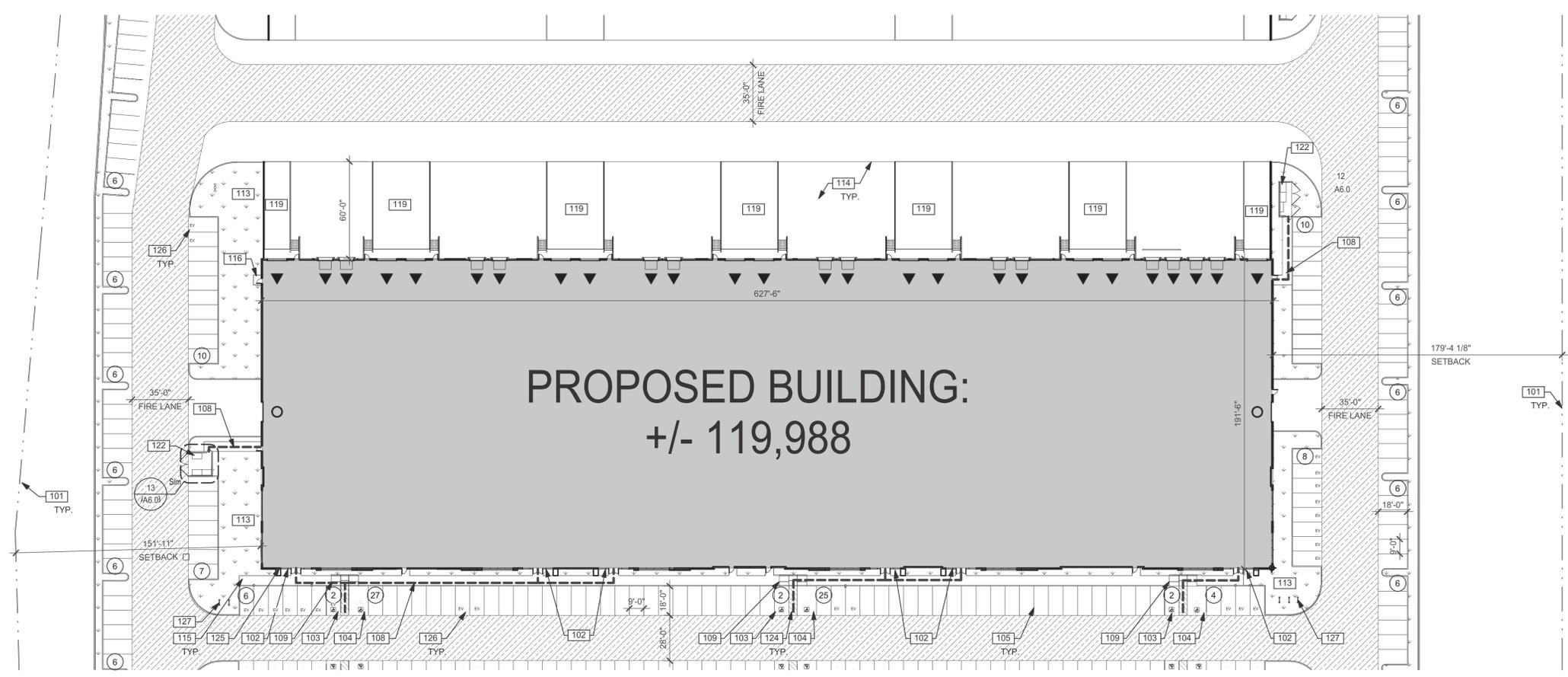
BUILDING 2 - BUILDING SECTIONS	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

SHEET
A5.1-2

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P 656.6572777



**PROPOSED BUILDING:
+/- 119,988**



BUILDING 3 - ENLARGED SITE PLAN
SCALE: 1" = 40'-0"



LEGEND

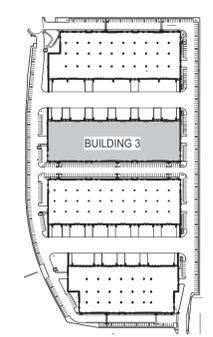
- ACCESSIBLE PATH OF TRAVEL, 1:20 MAX. SLOPE, 2% MAX. CROSS SLOPE.
- - - PROPERTY LINE.
- ☐ POLE MOUNTED LIGHT FIXTURE.
- ☐ WALLPACK LIGHT FIXTURE.
- T TRANSFORMER WITH CONCRETE PAD. (PROVIDE PROTECTION BOLLARDS PER LOCAL UTILITY OR PUBLIC WORK STANDARDS)
- ☐ LANDSCAPE AND IRRIGATION AREA.
- ▨ FIRE LANE (HATCHED)
- ① PARKING STALL COUNT TOTAL
- ▲ DOCK HIGH TRUCK DOOR
- GRADE LEVEL TRUCK DOOR
- FIRE HYDRANT
- P.I.V. WITH TAMPER

KEYNOTES: ☐

SEE SHEET A010 FOR GENERAL NOTES

- 101 PROPERTY LINE.
- 102 ACCESSIBLE ENTRY SIGNAGE.
- 103 ACCESSIBLE PARKING STALL WITH SIGNAGE.
- 104 VAN ACCESSIBLE PARKING STALL WITH SIGNAGE.
- 105 PAINTED PARKING STRIPING PER CITY STANDARDS.
- 106 2'-0" PARKING OVERHANG.
- 107 PRECAST CONCRETE WHEELSTOP.
- 108 ACCESSIBLE PATH OF TRAVEL, 1:20 MAX. SLOPE, 2% MAX. CROSS SLOPE.
- 109 ACCESSIBLE CURB RAMP WITH TRUNCATED DOMES.
- 110 FIRELANE ENTRY SIGNAGE.
- 111 FIRELANE SIGNAGE.
- 112 FIRELANE CURB, DASHED LINE INDICATES EXTENT OF CONTINUOUS CURB TO BE PAINTED RED.
- 113 LANDSCAPE AND IRRIGATION AREA.
- 114 CONCRETE PAVEMENT.
- 115 CONCRETE SIDEWALK.
- 116 CONCRETE STOOP.
- 117 FLOOD GATE.
- 118 FLOOD WALL.
- 119 CONCRETE RAMP.
- 120 CONCRETE RETAINING WALL.
- 121 POURED IN PLACE CONCRETE MONUMENT SIGN WITH GROUND MOUNTED ILLUMINATION.
- 122 TRASH ENCLOSURE WITH RECYCLE BIN.
- 123 EASEMENT LINE.
- 124 STRIPE 5'x20' MINIMUM AREA FOR PASSENGER DROP-OFF AND LOADING ZONE WITH SIGNAGE.
- 125 RECESSED KNOX-BOX, INSTALL AT 5'-0" ABOVE FINISH FLOOR. COORDINATE FINAL LOCATION WITH FIRE INSPECTOR.
- 126 CLEAN AIR/VAN/POOLEV DESIGNATED PARKING STALL.
- 127 SHORT-TERM AND LONG-TERM BIKE PARKING FOR THE PUBLIC AND EMPLOYEES.

KEY PLAN



**EDDIE JONES INDUSTRIAL
PROJECT (4 BLDG ALT) BLDG 3**
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

BUILDING 3 - ENLARGED SITE PLAN	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY:	HM
JOB NO.:	SDG21-0057-01

SHEET
A1.0-3

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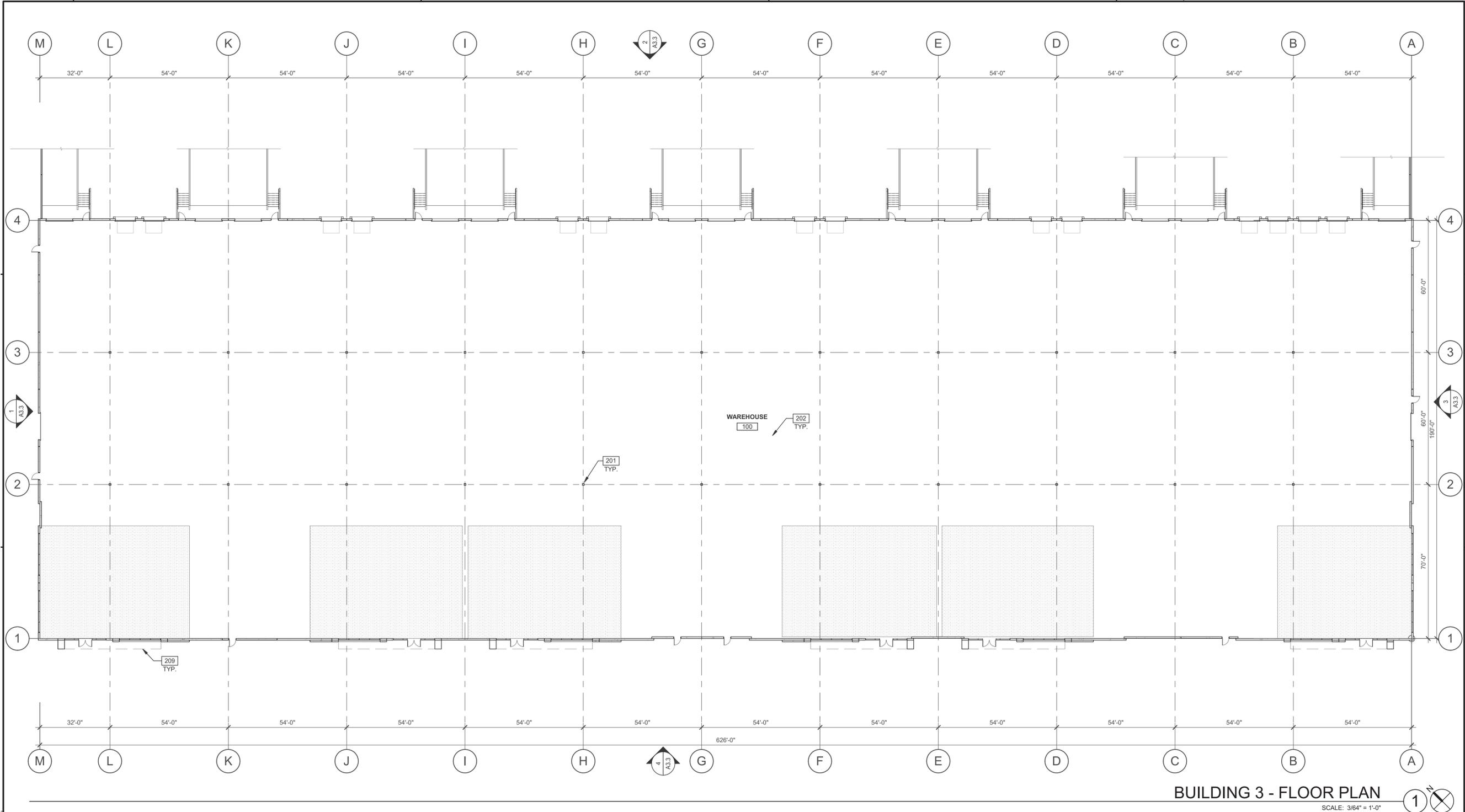
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ARCHITECTURE
PLANNING
INTERIORS
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San Diego, CA 92121
P 656.6372777

**EDDIE JONES INDUSTRIAL
PROJECT (4 BLDG ALT) BLDG 3**
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

BUILDING 3 - FLOOR PLAN	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

SHEET
A3.1



BUILDING 3 - FLOOR PLAN
SCALE: 3/64" = 1'-0"

LEGEND

PROPOSED FUTURE OFFICE AREA

WALL/ PARTITION LEGEND

CONCRETE WALL

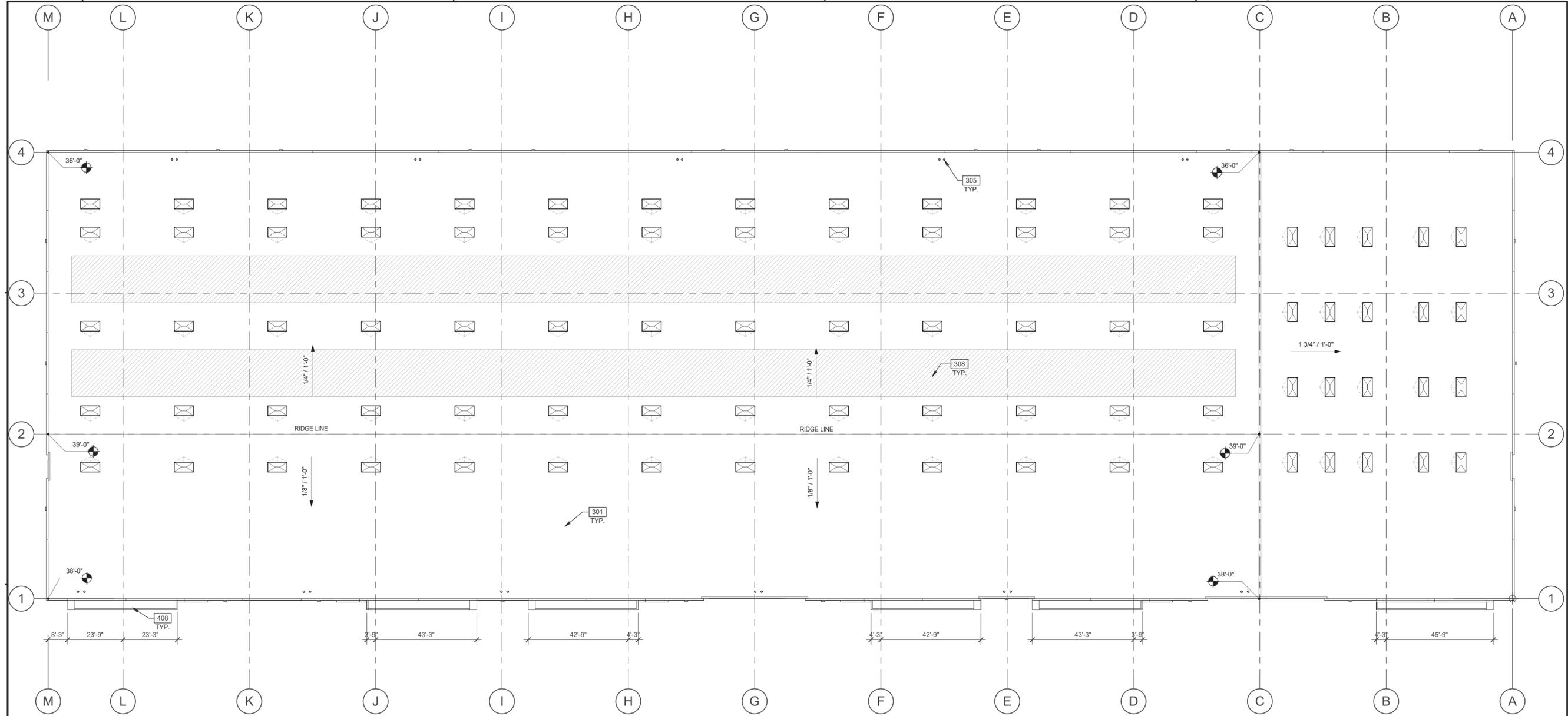
KEYNOTES:

SEE SHEET A010 FOR GENERAL NOTES

- 201 STRUCTURAL STEEL COLUMN.
- 202 CONCRETE SLAB. PROVIDE VAPOR RETARDER OVER SAND BASE AT OFFICE AREA PER SOILS REPORT. PROVIDE SEALER FOR CONCRETE FLOOR AREA IN WAREHOUSE.
- 209 OUTLINE OF CANOPY ABOVE.

GENERAL NOTES

- 1 DOCUMENTATION TO TENANT CONSTRUCTION COORDINATOR OR ARCHITECT UPON COMPLETION.
- 2 CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS (IF ANY) AND NOTIFY ARCHITECT OF ANY DISCREPANCIES.
- 3 CONTRACTOR TO INCLUDE ALL FLOOR PREPARATIONS IN BASE BID.
- 4 ALL EXTERIOR WALLS TO BE TILT-UP CONCRETE PANEL, UNLESS NOTED OTHERWISE.
- 5 DETAIL REFERENCES SHOULD BE APPLIED TO ALL INSTANCES WHERE THE SAME CONDITIONS OCCUR, UNLESS NOTED OTHERWISE.
- 6 DIMENSIONS ARE GIVEN TO THE COLUMN GRID, FACE OF STUD, CENTERLINE OF OPENING, OR ROUGH OPENING UNLESS NOTED OTHERWISE, ALIGN FINISHES WHERE INDICATED.
- 7 REFER TO DESIGN CRITERIA DOCUMENT FOR ADDITIONAL INFORMATION.



BUILDING 4 - ROOF PLAN
SCALE: 3/64" = 1'-0"

CALCULATIONS

SKYLIGHTS:
 SKYLIGHT SIZE: 48"x96"=32 S.F.
 (WAREHOUSE AREA S.F.) x 2.0% = $\frac{118,940 \times .02}{32}$
 (SKYLIGHT SIZE)
 DESIRED: 74 SKYLIGHTS
 PROVIDED: 85 SKYLIGHTS

FUTURE SOLAR:
 ROOF AREA: 118,940 S.F.
 SKYLIGHT AREA: 2,720 S.F.
 ROOF AREA LESS SKYLIGHT: 116,220 S.F.
 15% OF ROOF AREA (LESS SKYLIGHTS): 17,433 S.F.
 TOTAL AREA FOR FUTURE SOLAR REQUIRED: 17,433 S.F.*
 TOTAL AREA FOR FUTURE SOLAR PROVIDED: 19,904 S.F.

* CEC SECTION 110.10(B)(4) DOES NOT REQUIRE THE INCLUSION OF ANY COLLATERAL LOADS FOR FUTURE SOLAR ENERGY SYSTEMS.

LEGEND

 SKYLIGHT: (2%)
 COORDINATE EXACT LOCATION TO PREVENT CONFLICT WITH FRAMING, FIRE SPRINKLER LINES, ELECTRICAL CONDUITS AND LIGHTING. SEE STRUCTURAL DRAWINGS.

KEYNOTES:

SEE SHEET **A010** FOR GENERAL NOTES

301 SINGLE-PLY ROOFING OVER RIGID INSULATION OVER METAL DECK.
 305 PRIMARY AND SECONDARY DRAINS.
 308 FUTURE SOLAR.
 408 METAL CANOPY, PAINTED.

BUILDING 3 - ROOF PLAN

DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

SHEET
A3.2

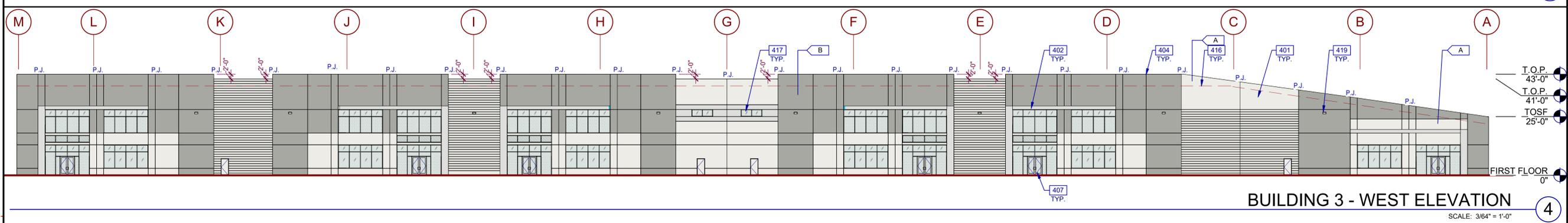
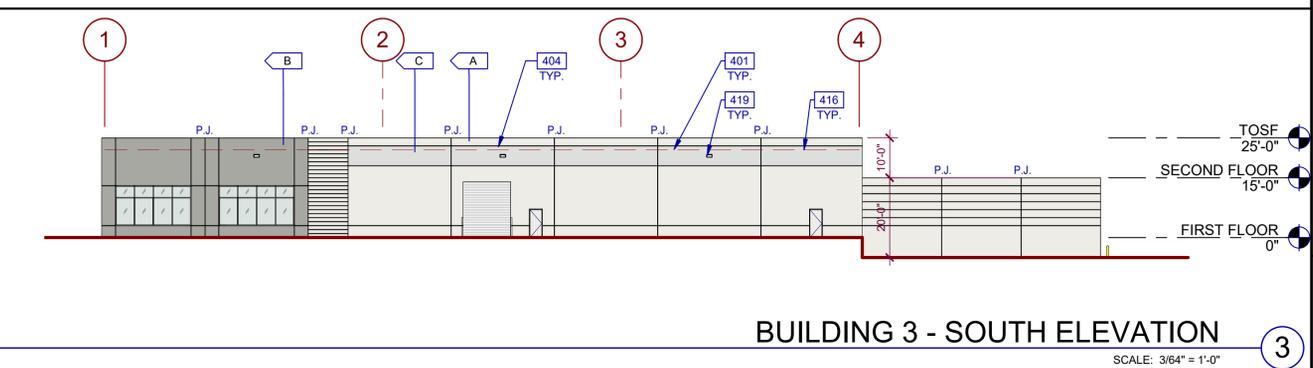
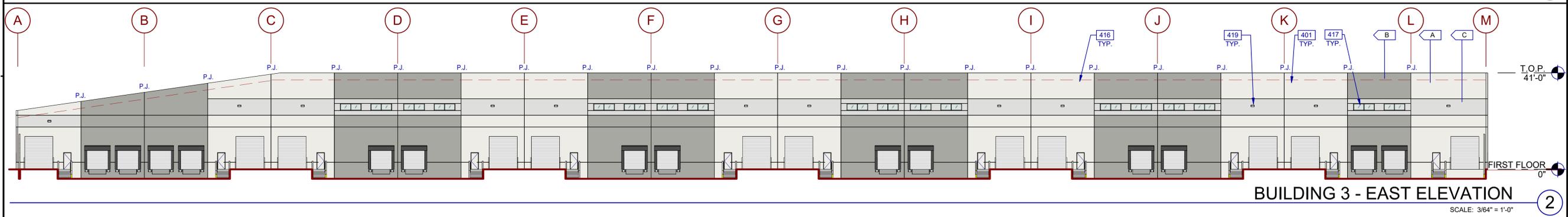
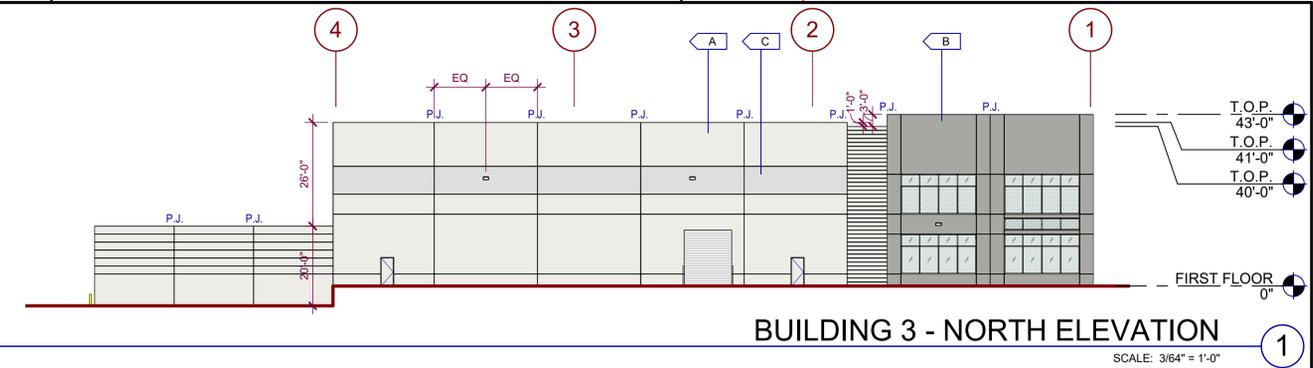
EDDIE JONES INDUSTRIAL PROJECT (4 BLDG ALT) BLDG 3
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

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LEGEND

- MATERIALS:**
- METAL CANOPY, PAINTED
- GLASS:**
- VISION GLASS
 - SPANDREL GLASS
 - TEMPERED GLASS

- COLORS:**
- PROVIDE 6'-0" WIDE PAINT COLOR MOCK-UP FULL HEIGHT OF BUILDING FOR OWNER/ARCHITECT REVIEW.
- BASE COLOR: PT-1 MANUFACTURER SHERWIN WILLIAMS COLORNO: PURE WHITE / SW7005
 - SECONDARY COLOR: PT-2 MANUFACTURER: SHERWIN WILLIAMS COLORNO: GRAY MATTERS / SW7066
 - ACCENT COLOR: PT-3 MANUFACTURED: SHERWIN WILLIAMS COLORNO: NEBULOUS WHITE / SW 7063

GLAZING NOTES

- EACH LIGHT SHALL BEAR THE MANUFACTURER'S LABEL DESIGNATING THE TYPE AND THICKNESS OF THE GLASS.
- GLASS SHALL BE FIRMLY SUPPORTED ON ALL FOUR EDGES, U.O.N.
- FIELD MEASURE ALL OPENINGS PRIOR TO FABRICATION.
- FIXED OR OPERABLE GLAZING IN HAZARDOUS LOCATIONS AS DEFINED IN THE BUILDING CODE SHALL BE TEMPERED GLASS (SAFETY GLASS).
- GLAZING IN SWINGING, SLIDING AND BIFOLD DOORS SHALL BE TEMPERED.
- ALL GLAZING WITHIN A 24" ARC OF EITHER EDGE OF A DOOR AND WITHIN 60" OF THE FLOOR SHALL BE TEMPERED.
- GLAZING IN WINDOWS: ALL GLAZING THAT MEETS ALL OF THE FOLLOWING CONDITIONS SHALL BE TEMPERED:
 - A. THE EXPOSED AREA OF AN INDIVIDUAL PANE IS GREATER THAN 9 SF.
 - B. THE BOTTOM EDGE IS LESS THAN 18" ABOVE THE FLOOR.
 - C. THE TOP EDGE IS GREATER THAN 36" ABOVE THE FLOOR.
 - D. WALKING SURFACE IS WITHIN 36".
- ALL GLASS SHALL COMPLY WITH THE REQUIREMENTS OF THE STATE AND LOCAL CODES AND THE U.S. PRODUCT SAFETY COMMISSIONS: SAFETY STANDARDS FOR ARCHITECTURAL GLAZING MATERIALS.
- WHERE JOINTS ARE REQUIRED IN MULTIPLE LITES, CLEAR SILICONE IS TO BE PROVIDED, U.O.N.

KEYNOTES:

- 401 CONCRETE WALL, PAINTED.
- 402 PRE-FINISHED ALUMINUM COMPOSITE METAL PANEL OVER METAL STUD FRAMING. CLEAR ANODIZED ALUMINUM MULLION.
- 404 3/4" V-REVEAL.
- 407 ALUMINUM STOREFRONT SYSTEM WITH 1" TINTED INSULATED GLASS.
- 416 LINE OF ROOF BEYOND.
- 417 CLERESTORY WINDOW.
- 419 WALL PACK.

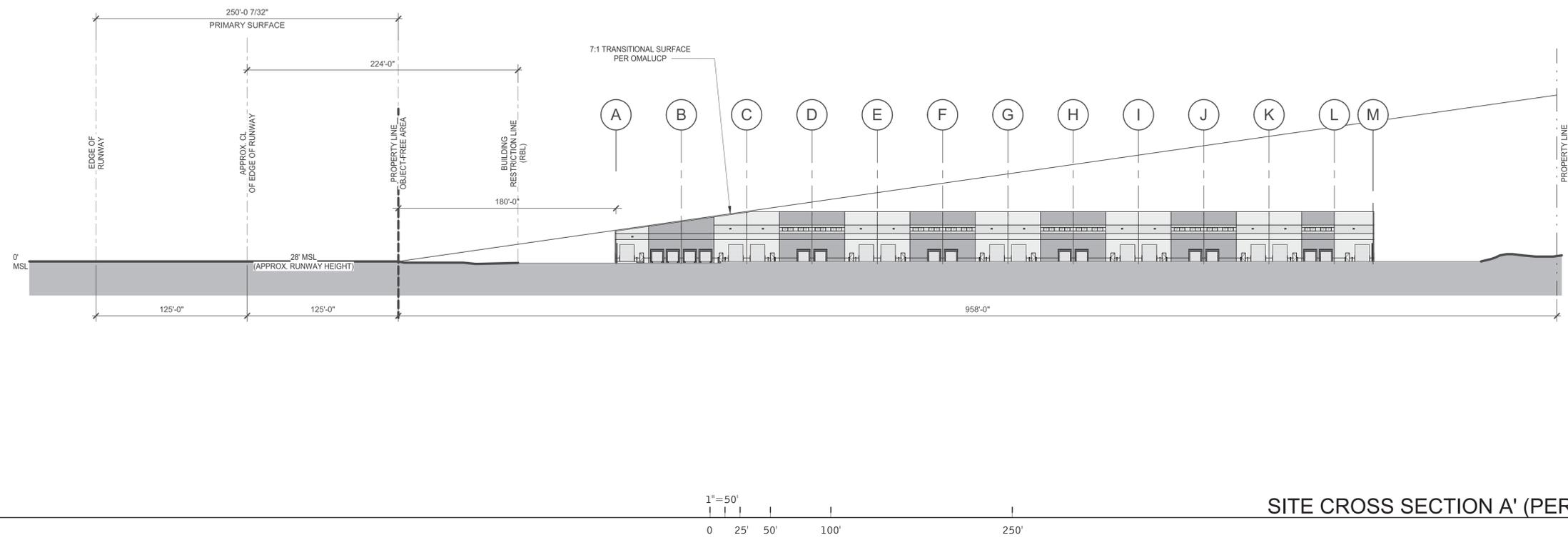
ALL GLASS USED ON THE ENVELOPE OF THE BUILDING TO BE: XXXXXX
GLASS WIND LOAD RESISTANCE CALCULATIONS ARE UNDER SEPARATE PERMIT

EDDIE JONES INDUSTRIAL PROJECT (4 BLDG ALT) BLDG 3
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

BUILDING 3 - EXTERIOR ELEVATIONS	
DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

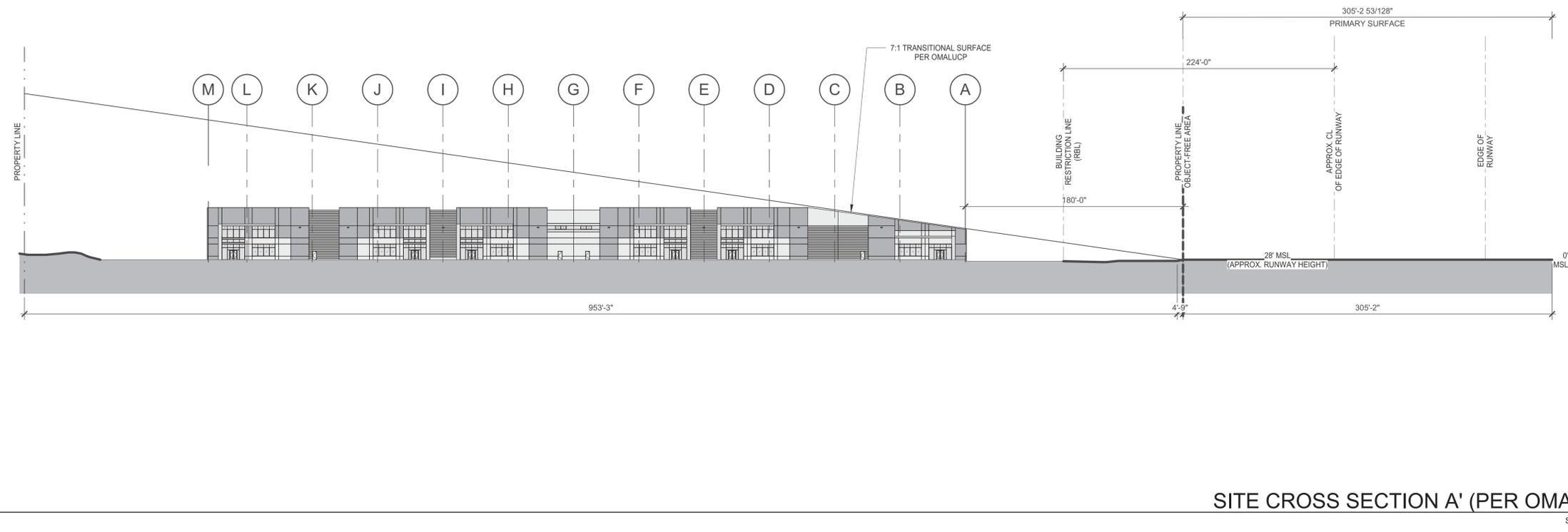
PA/PM:	S. NAGARAJ
DRAWN BY.:	HM
JOB NO.:	SDG21-0057-01

SHEET
A3.3



SITE CROSS SECTION A' (PER OMalucP) 1

SCALE: 1" = 50'-0"



SITE CROSS SECTION A' (PER OMalucP) 2

SCALE: 1" = 50'-0"

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BUILDING 3 - BUILDING SECTIONS

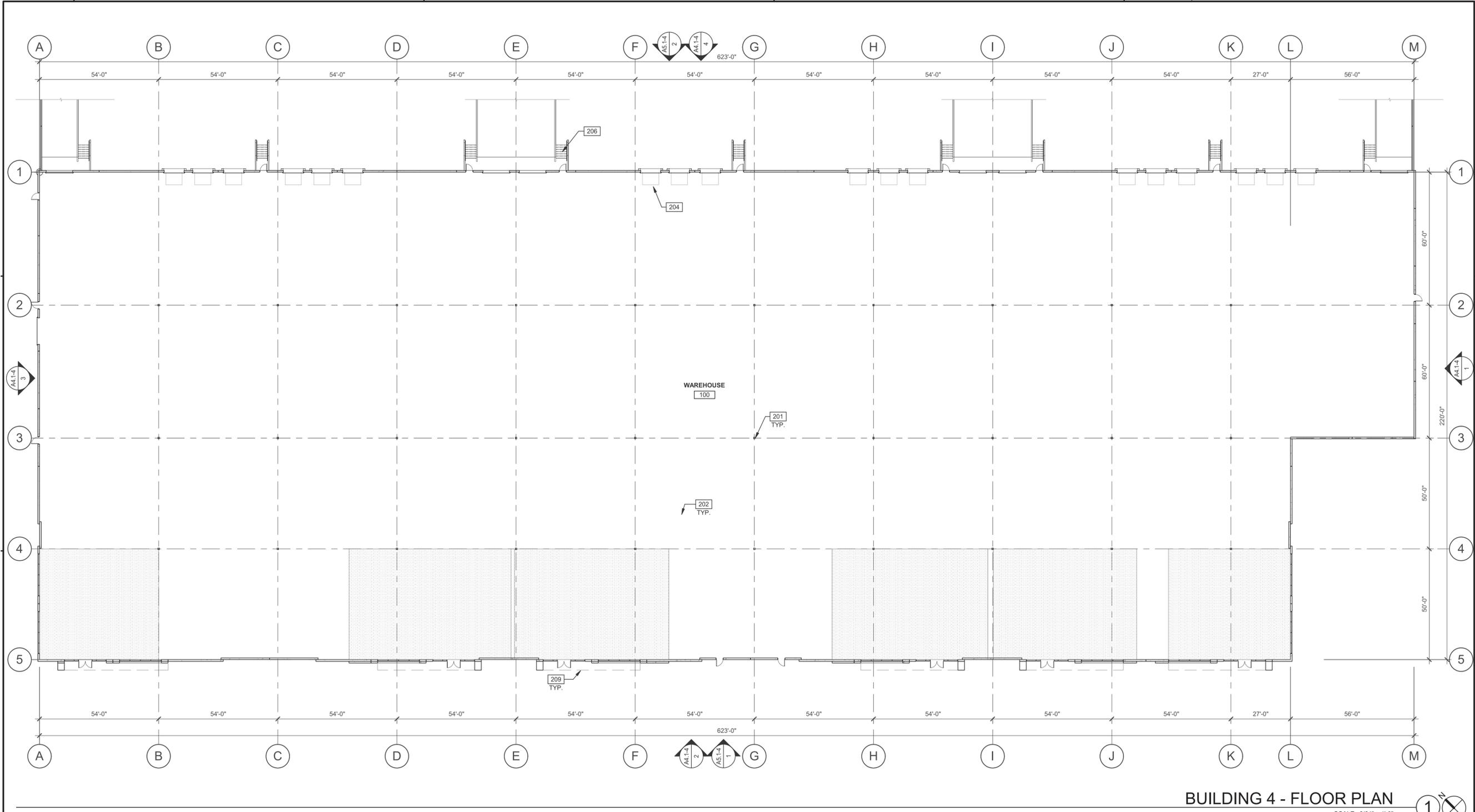
DATE	REMARKS
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PA/PM:	S. NAGARAJ
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SHEET
A3.4

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BUILDING 4 - FLOOR PLAN
SCALE: 3/64" = 1'-0"

LEGEND

-  PROPOSED FUTURE OFFICE AREA
-  CONCRETE WALL

WALL/ PARTITION LEGEND

KEYNOTES:

- SEE SHEET A010 FOR GENERAL NOTES
- 201 STRUCTURAL STEEL COLUMN.
 - 202 CONCRETE SLAB - PROVIDE VAPOR RETARDER OVER SAND BASE AT OFFICE AREA PER SOILS REPORT. PROVIDE SEALER FOR CONCRETE FLOOR AREA IN WAREHOUSE.
 - 204 6'X 8' HYDRAULIC DOCK LEVELER.
 - 206 EXTERIOR CONCRETE STAIR, ALL COMPONENTS GALVANIZED AND PAINTED.
 - 209 OUTLINE OF CANOPY ABOVE.

GENERAL NOTES

- 1 DOCUMENTATION TO TENANT CONSTRUCTION COORDINATOR OR ARCHITECT UPON COMPLETION.
- 2 CONTRACTOR TO FIELD VERIFY EXISTING CONDITIONS (IF ANY) AND NOTIFY ARCHITECT OF ANY DISCREPANCIES.
- 3 CONTRACTOR TO INCLUDE ALL FLOOR PREPARATIONS IN BASE BID.
- 4 ALL EXTERIOR WALLS TO BE TILT-UP CONCRETE PANEL, UNLESS NOTED OTHERWISE
- 5 DETAIL REFERENCES SHOULD BE APPLIED TO ALL INSTANCES WHERE THE SAME CONDITIONS OCCUR, UNLESS NOTED OTHERWISE.
- 6 DIMENSIONS ARE GIVEN TO THE COLUMN GRID, FACE OF STUD, CENTERLINE OF OPENING, OR ROUGH OPENING UNLESS NOTED OTHERWISE, ALIGN FINISHES WHERE INDICATED.
- 7 REFER TO DESIGN CRITERIA DOCUMENT FOR ADDITIONAL INFORMATION.

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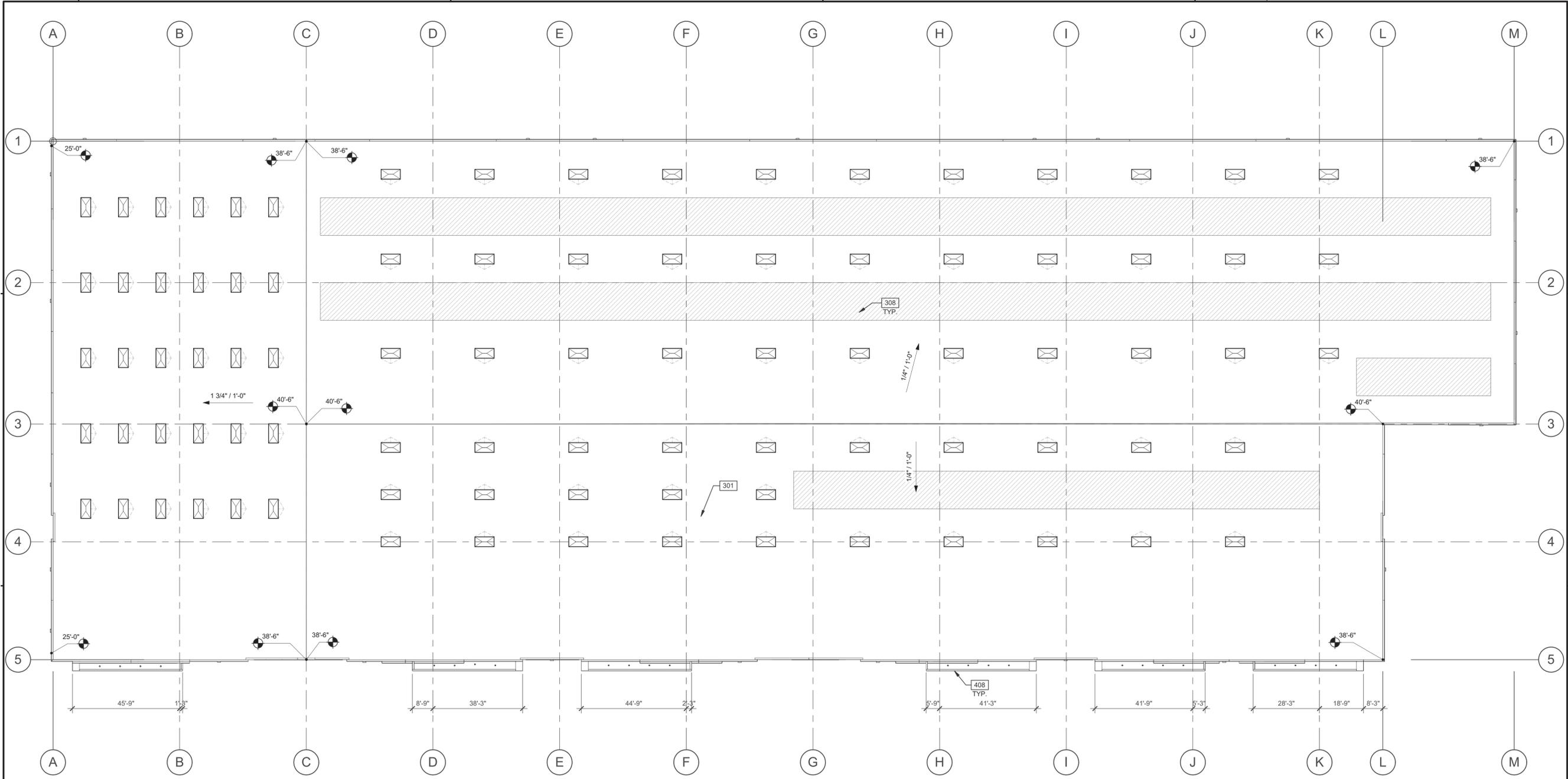
**EDDIE JONES INDUSTRIAL
 PROJECT (4 BLDG ALT) BLDG 4**
 260 EDDY JONES WAY
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BUILDING 4 - FLOOR PLAN

DATE	REMARKS
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PA/PM:	S. NARAGAJ
DRAWN BY:	H.K.
JOB NO.:	SDG21-0057-01

SHEET
A2.1-4



BUILDING 4 - ROOF PLAN
SCALE: 3/64" = 1'-0"

CALCULATIONS

SKYLIGHTS:
SKYLIGHT SIZE: 48"x96"=32 S.F.
(WAREHOUSE AREA S.F.) x 2.0% = $\frac{111,830 \times 0.02}{32}$
(SKYLIGHT SIZE)

DESIRED: 69 SKYLIGHTS
PROVIDED: 88 SKYLIGHTS

FUTURE SOLAR:

ROOF AREA:	131,439	S.F.
SKYLIGHT AREA:	2,816	S.F.
ROOF AREA LESS SKYLIGHT:	128,623	S.F.
15% OF ROOF AREA (LESS SKYLIGHTS):	19,294	S.F.
TOTAL AREA FOR FUTURE SOLAR REQUIRED:	19,294	S.F.*
TOTAL AREA FOR FUTURE SOLAR PROVIDED:	20,472	S.F.

* CEC SECTION 110.10(B)4 DOES NOT REQUIRE THE INCLUSION OF ANY COLLATERAL LOADS FOR FUTURE SOLAR ENERGY SYSTEMS.

LEGEND

 SKYLIGHT: (2%)
COORDINATE EXACT LOCATION TO PREVENT CONFLICT WITH FRAMING, FIRE SPRINKLER LINES, ELECTRICAL CONDUITS AND LIGHTING. SEE STRUCTURAL DRAWINGS.

KEYNOTES:

SEE SHEET **A010** FOR GENERAL NOTES

301	SINGLE-PLY ROOFING OVER RIGID INSULATION OVER METAL DECK.
308	FUTURE SOLAR.
408	METAL CANOPY, PAINTED.

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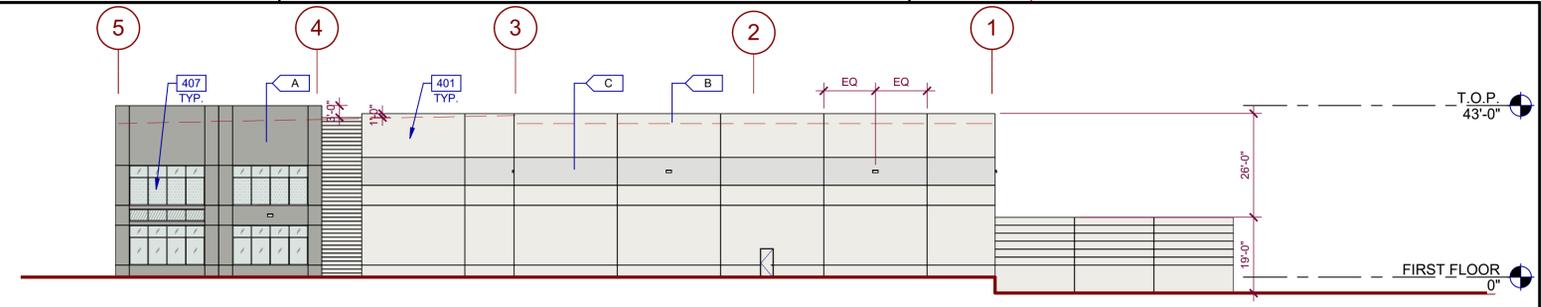
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OCEANSIDE, CA 92058

BUILDING 4 - ROOF PLAN

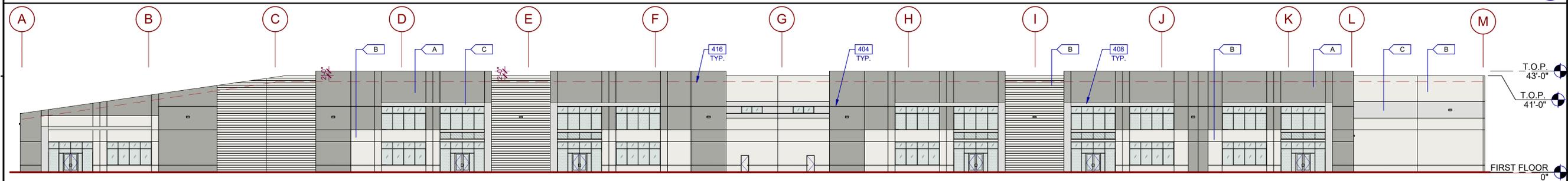
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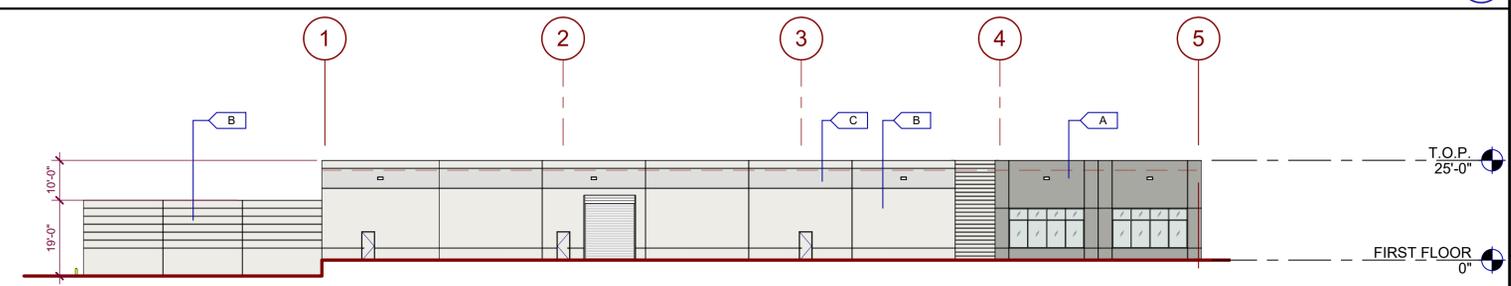
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A3.1-4



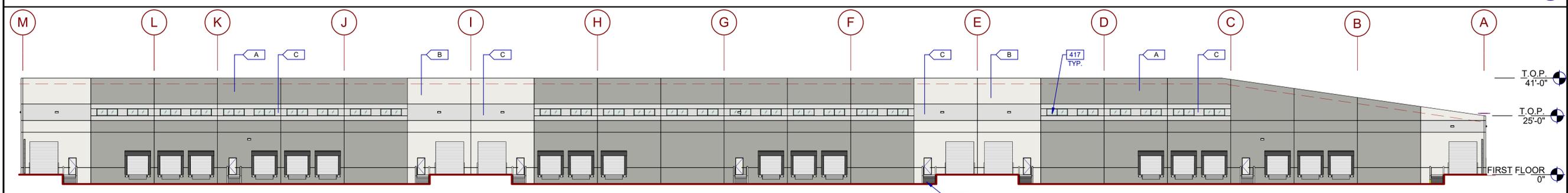
BUILDING 4 NORTH ELEVATION
 SCALE: 3/64" = 1'-0"



BUILDING 4 EAST ELEVATION
 SCALE: 3/64" = 1'-0"



BUILDING 4 SOUTH ELEVATION
 SCALE: 3/64" = 1'-0"



BUILDING 4 WEST ELEVATION
 SCALE: 3/64" = 1'-0"

LEGEND

- MATERIALS:**
- METAL CANOPY, PAINTED
 - VISION GLASS
 - SPANDREL GLASS
 - TEMPERED GLASS

- COLORS:**
- PROVIDE 6"-0" WIDE PAINT COLOR MOCK-UP FULL HEIGHT OF BUILDING FOR OWNER/ARCHITECT REVIEW.
- ALL WALLS TO BE PAINTED WITH BASE COLOR U.O.N.
- BASE COLOR: SHERWIN WILLIAMS - SW 7066 GRAY MATTERS
 - SECONDARY COLOR: SHERWIN WILLIAMS - SW 7005 PURE WHITE
 - ACCENT COLOR: SHERWIN WILLIAMS - SW 7063 NEBULOUS WHITE

ALL GLASS USED ON THE ENVELOPE OF THE BUILDING TO BE:
 XXXXXX
 GLASS WIND LOAD RESISTANCE CALCULATIONS ARE UNDER SEPARATE PERMIT

GLAZING NOTES

1. EACH LIGHT SHALL BEAR THE MANUFACTURER'S LABEL DESIGNATING THE TYPE AND THICKNESS OF THE GLASS.
2. GLASS SHALL BE FIRMLY SUPPORTED ON ALL FOUR EDGES, U.O.N.
3. FIELD MEASURE ALL OPENINGS PRIOR TO FABRICATION.
4. FIXED OR OPERABLE GLAZING IN HAZARDOUS LOCATIONS AS DEFINED IN THE BUILDING CODE SHALL BE TEMPERED GLASS (SAFETY GLASS).
5. GLAZING IN SWINGING, SLIDING AND BIFOLD DOORS SHALL BE TEMPERED.
6. ALL GLAZING WITHIN A 24" ARC OF EITHER EDGE OF A DOOR AND WITHIN 60" OF THE FLOOR SHALL BE TEMPERED.
7. GLAZING IN WINDOWS: ALL GLAZING THAT MEETS ALL OF THE FOLLOWING CONDITIONS SHALL BE TEMPERED:
 - A. THE EXPOSED AREA OF AN INDIVIDUAL PANE IS GREATER THAN 9 SF.
 - B. THE BOTTOM EDGE IS LESS THAN 18" ABOVE THE FLOOR.
 - C. THE TOP EDGE IS GREATER THAN 36" ABOVE THE FLOOR.
 - D. WALKING SURFACE IS WITHIN 36".
8. ALL GLASS SHALL COMPLY WITH THE REQUIREMENTS OF THE STATE AND LOCAL CODES AND THE U.S. PRODUCT SAFETY COMMISSIONS: SAFETY STANDARDS FOR ARCHITECTURAL GLAZING MATERIALS.
9. WHERE JOINTS ARE REQUIRED IN MULTIPLE LITES, CLEAR SILICONE IS TO BE PROVIDED, U.O.N.

KEYNOTES:

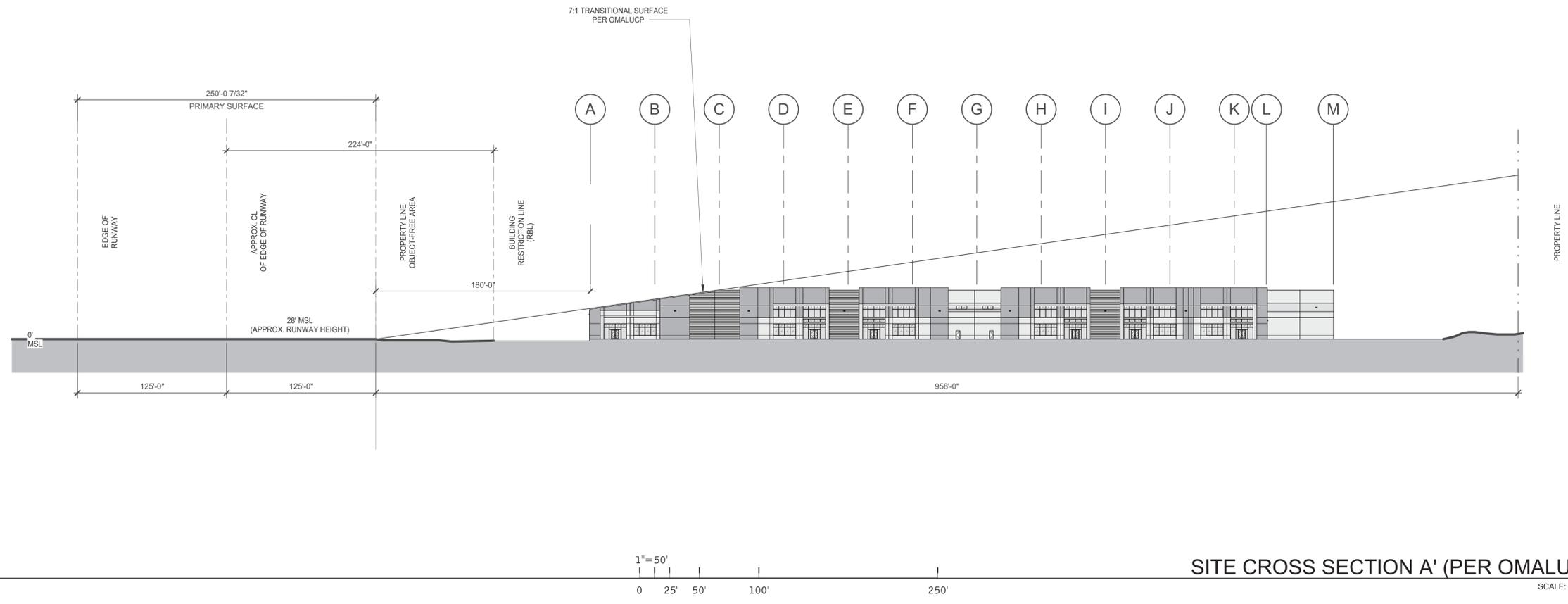
- 206 EXTERIOR CONCRETE STAIR, ALL COMPONENTS GALVANIZED AND PAINTED.
- 401 CONCRETE WALL, PAINTED.
- 404 3/4" V-REVEAL.
- 407 ALUMINUM STOREFRONT SYSTEM WITH 1" TINTED INSULATED GLASS.
- 408 METAL CANOPY, PAINTED.
- 416 LINE OF ROOF BEYOND.
- 417 CLERESTORY WINDOW.

BUILDING 4 - EXTERIOR ELEVATIONS
 PROJECT (4 BLDG ALT) BLDG 4
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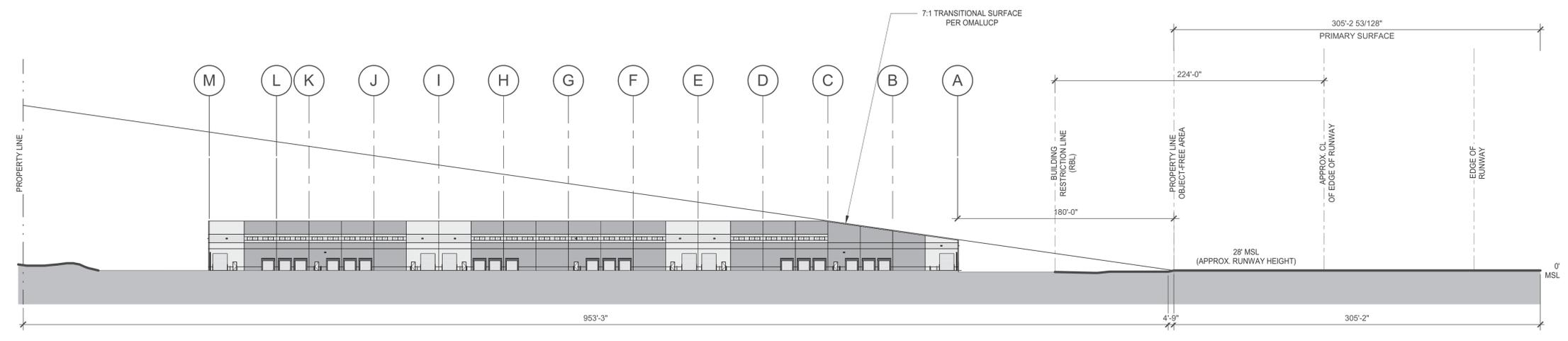
DATE	REVISIONS
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SHEET
A4.1-4



SITE CROSS SECTION A' (PER OMalucp) ①
SCALE: 1" = 50'-0"



SITE CROSS SECTION A' (PER OMalucp) ②
SCALE: 1" = 50'-0"

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**EDDIE JONES INDUSTRIAL
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**BUILDING 4 - BUILDING
SECTIONS**

DATE	REMARKS
05-31-2024	PLANNING SUBMITTAL

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A5.1-4

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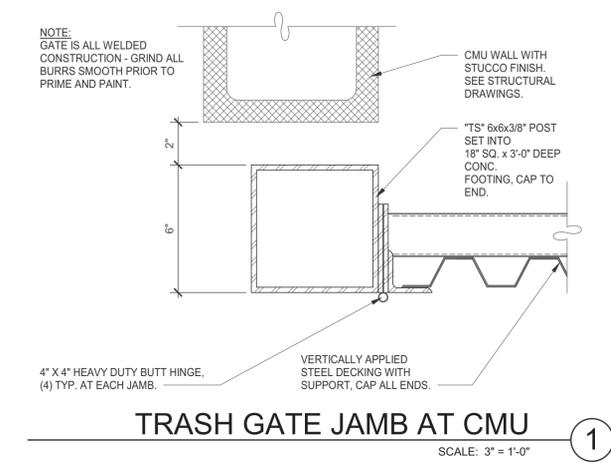
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SITE DETAILS

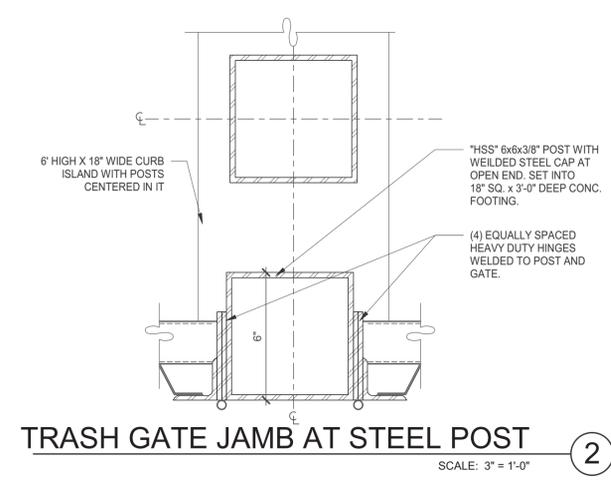
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JOB NO.:	SDG21-0057-01

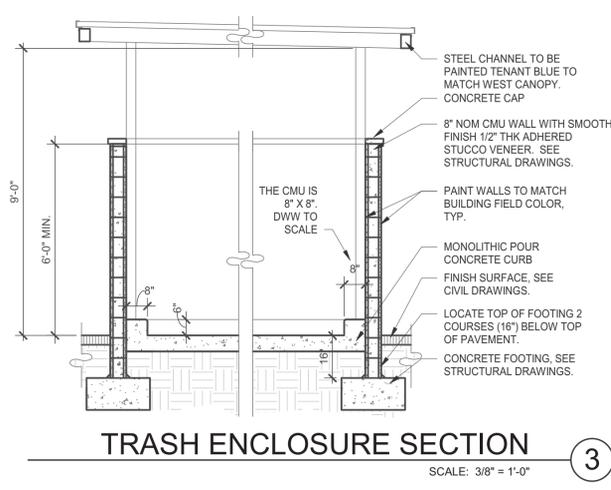
SHEET
A6.0



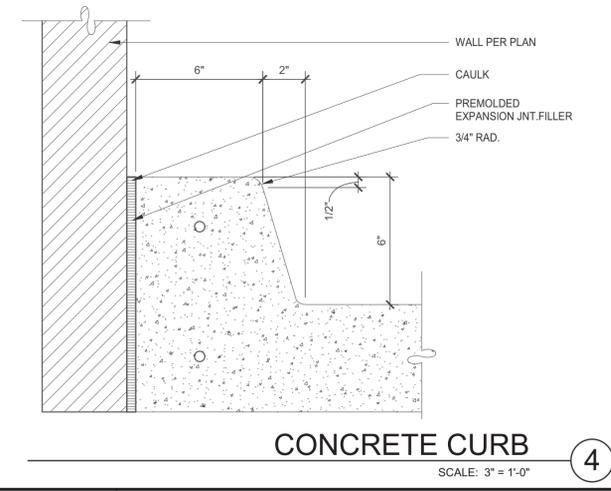
TRASH GATE JAMB AT CMU
 SCALE: 3" = 1'-0"



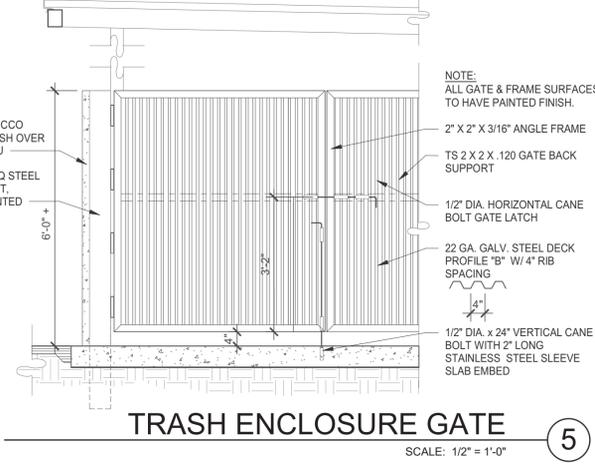
TRASH GATE JAMB AT STEEL POST
 SCALE: 3" = 1'-0"



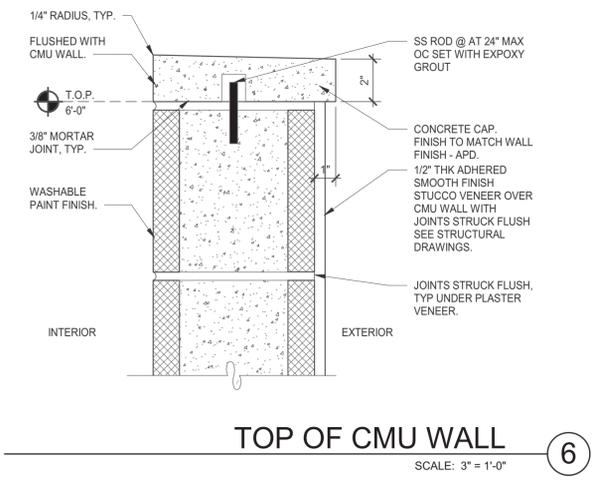
TRASH ENCLOSURE SECTION
 SCALE: 3/8" = 1'-0"



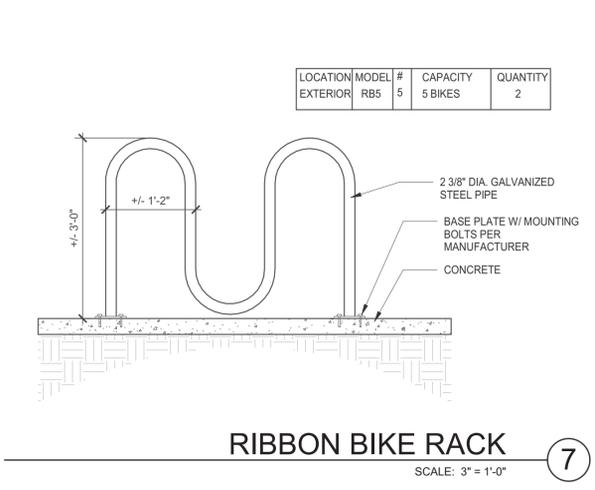
CONCRETE CURB
 SCALE: 3" = 1'-0"



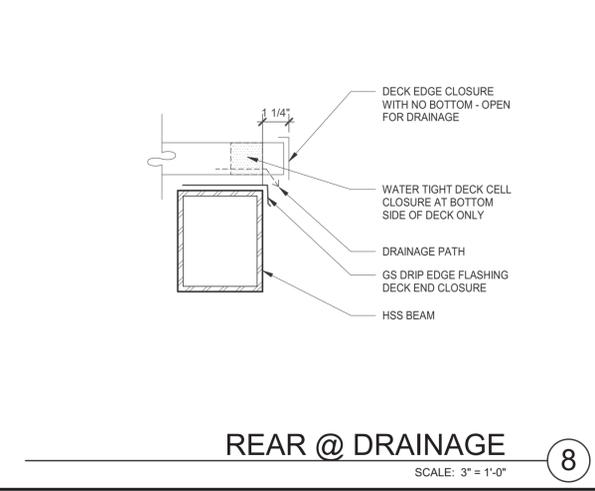
TRASH ENCLOSURE GATE
 SCALE: 1/2" = 1'-0"



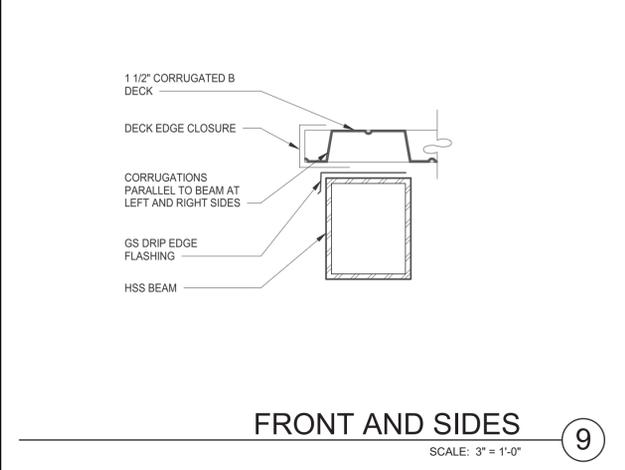
TOP OF CMU WALL
 SCALE: 3" = 1'-0"



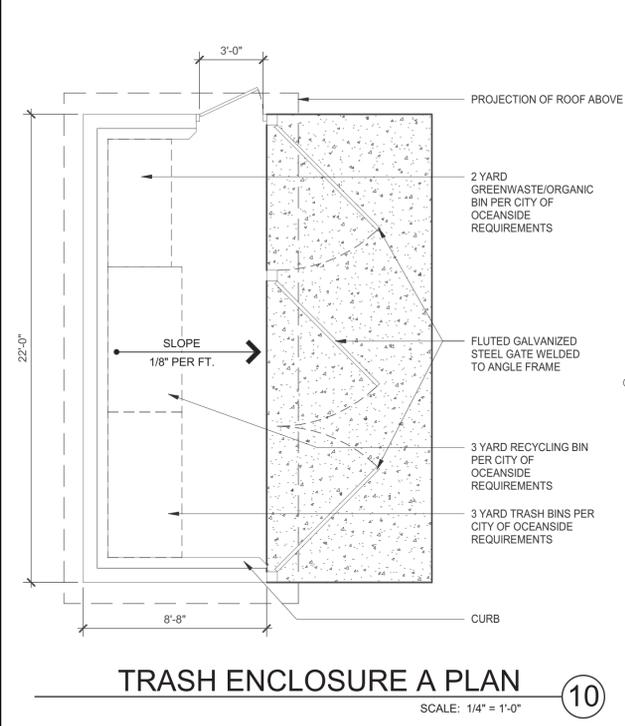
RIBBON BIKE RACK
 SCALE: 3" = 1'-0"



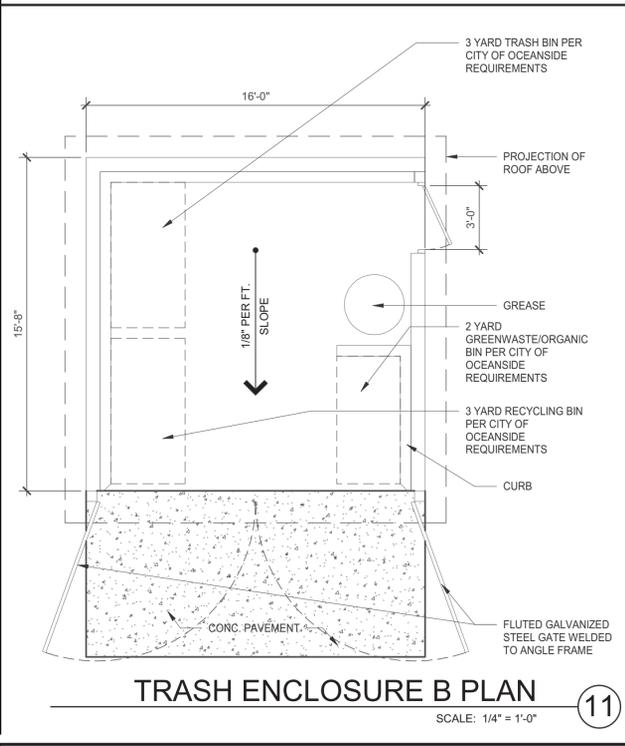
REAR @ DRAINAGE
 SCALE: 3" = 1'-0"



FRONT AND SIDES
 SCALE: 3" = 1'-0"



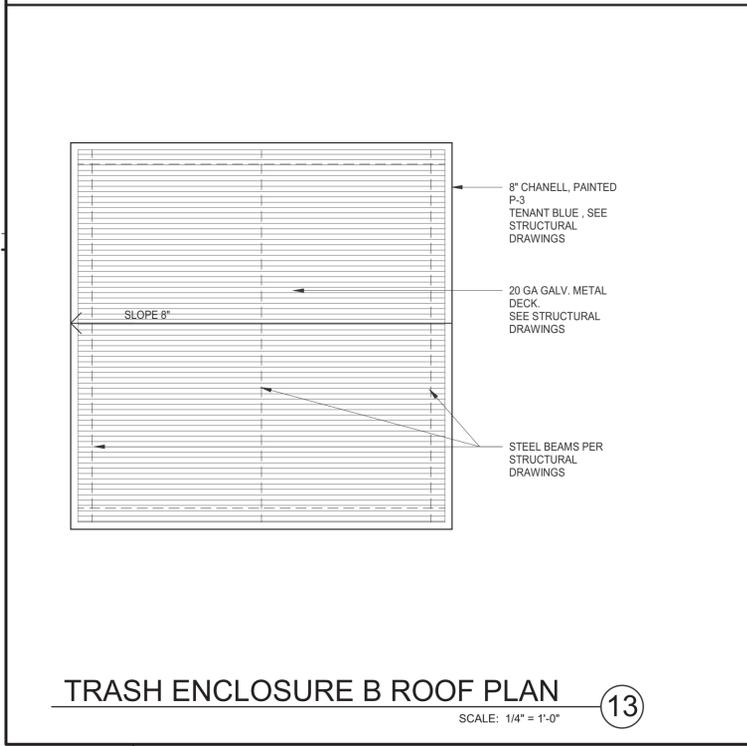
TRASH ENCLOSURE A PLAN
 SCALE: 1/4" = 1'-0"



TRASH ENCLOSURE B PLAN
 SCALE: 1/4" = 1'-0"



TRASH ENCLOSURE A ROOF PLAN
 SCALE: 1/4" = 1'-0"



TRASH ENCLOSURE B ROOF PLAN
 SCALE: 1/4" = 1'-0"



G
1

ITEM: EXTERIOR GLASS
MANUF.: VITRO GLAZING
COLORNO.: SOLAR GRAY

**NOTE: EXTERIOR GLASS SHALL BE 1" INSULATED, LOW-E COATED ON 3RD SURFACE, GRAY TINTED, FLOAT GLASS IN ACCORDANCE WITH LOCAL BUILDING CODES.

PROVIDE SIMULATED SANDBLAST ON THE #3 SURFACE EQUIVALENT TO VIRACON: CERAMIC FRIT, SIMULATED SANDBLAST (V1086)

PROVIDE TEMPERED UNITS AS REQUIRED BY CODE.



M
1

ITEM: FRAMES THROUGHOUT
MANUF.: ARCADIA
COLORNO.: CLEAR ANODIZED ALUMINUM



P
1

ITEM: PAINT - EXTERIOR FIELD COLOR
MANUF.: SHERWIN WILLIAMS
COLORNO.: PURE WHITE / SW 7005
FINISH: -



P
2

ITEM: PAINT - EXTERIOR FIELD COLOR
MANUF.: SHERWIN WILLIAMS
COLORNO.: NEBULOUS WHITE / SW 7063
FINISH: -



P
3

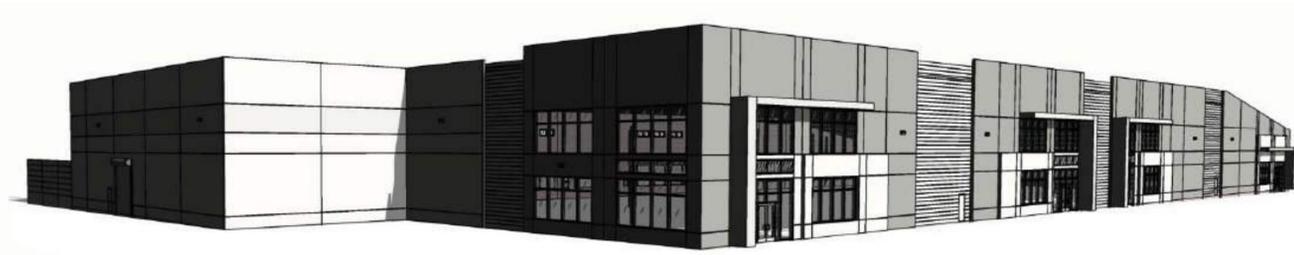
ITEM: PAINT - EXTERIOR FIELD COLOR
MANUF.: SHERWIN WILLIAMS
COLORNO.: GRAY MATTERS / SW 7066
FINISH: -



BLDG 1 - 3D VIEW

SCALE: 3/8" = 1'-0"

1



BLDG 2 - 3D VIEW

SCALE: 3/8" = 1'-0"

2



BLDG 3 - 3D VIEW

SCALE: 3/8" = 1'-0"

3



BLDG 4 - 3D VIEW

SCALE: 3/8" = 1'-0"

4

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260 EDDIE JONES WAY
OCEANSIDE, CA 92058

MATERIAL BOARD

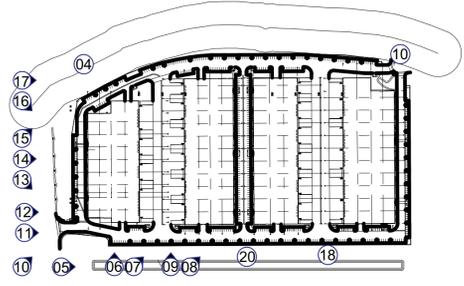
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1	

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JOB NO.:	SDG21-0057-01

SHEET
A7.0



SITE KEYPLAN



WARE MALCOMB
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 BUILDING MEASUREMENT
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 San Diego, CA 92121
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**EDDIE JONES INDUSTRIAL
 PROJECT - 4 BLDG ALT**
 260 EDDIE JONES WAY
 OCEANSIDE, CA 92058

PHOTOGRAPHIC SURVEY

DATE	REMARKS
05/31/2024	PLANNING SUBMITTAL
1	

PA/PM:	S. NAGARAJ
DRAWN BY.:	H.K.
JOB NO.:	SDG21-0057-01

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A8.0

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INDUSTRIAL PROJECT SHELL 260 EDDY JONES WAY OCEANSIDE, CA 92058

NOTE SHEET table with columns for DATE, REMARKS, and a grid for notes.

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SHEET E001

ABBREVIATIONS table listing symbols for AMPERES, ALTERNATING CURRENT, ABOVE FINISHED FLOOR, etc.

ABBREVIATIONS

Sheet List Table with columns for Sheet Number and Sheet Title, listing E001, E002, E010, E100, and E200.

SHEET INDEX

mpe consulting logo and professional engineer seal for Daniel W. Mayoreus, No. E018377, Exp. 6-30-26.

TEL/DATA SYMBOLS table listing symbols for DATA OUTLET WITH TRIM RING, TELEPHONE OUTLET WITH TRIM RING, etc.

TEL/DATA SYMBOLS

POWER SYMBOLS table listing symbols for FUSED SWITCH, CIRCUIT BREAKER, TRANSFORMER, SHIELDED TRANSFORMER, CURRENT TRANSFORMER, etc.

POWER SYMBOLS

NOTE SHEET

LIGHTING SYMBOLS table listing symbols for LIGHTING CONTROL PANEL, PUSH BUTTON LOW VOLTAGE OVERRIDE CONTROL SWITCH, etc.

LIGHTING SYMBOLS

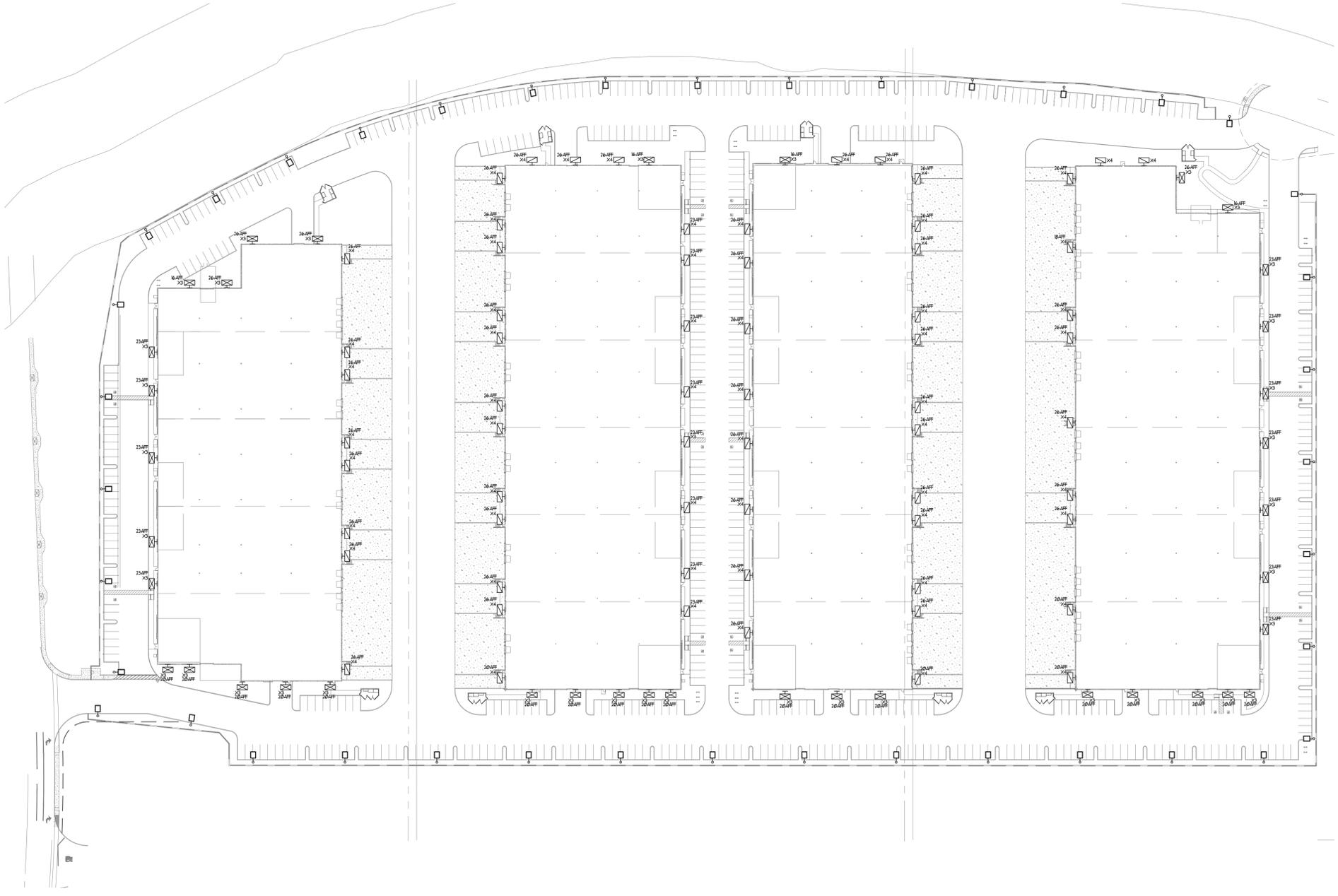
LAB AREA DEVICE TYPES table listing symbols for WALL MOUNTED MOTION CONTROLLED LIGHTING SWITCH, LED MODULE RELAY PACK, etc.

LIGHTING SYMBOLS

GENERAL NOTES table listing 17 numbered notes regarding conductor types, electrical contractor responsibilities, and installation requirements.

GENERAL NOTES

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GENERAL NOTES:

1. ARCHITECT VERIFICATION CONTRACTOR TO REVIEW ARCHITECTURAL PLANS AND CASEWORK ELEVATIONS FOR ALL FINAL DEVICE AND FIXTURE TYPE REQUIREMENTS, FINAL SWITCH LOCATIONS, AND FINAL OUTLET PLACEMENTS. ANY DISCREPANCIES SHOULD BE BROUGHT TO THE ATTENTION OF THE ARCHITECT AND ENGINEER PRIOR TO ROUGH-IN.
2. GROUP MULTIPLE SWITCHES WITHIN 1' MAX. USE HI-LOW VOLTAGE PARTITIONS AS REQUIRED.
3. LOCATION OF WALL MOUNTED LOCAL LIGHTING SWITCHES IS SUBJECT TO MODIFICATION AT OR NEAR DOORS. INSTALL SWITCHES ON SIDE OPPOSITE DOOR HINGE. VERIFY FINAL HINGE LOCATION IN FIELD PRIOR ANY WORK. REFER TO ARCHITECTURAL ELEVATIONS FOR ADDITIONAL DETAILS.
4. EMERGENCY LIGHT TEST SWITCH TO BE INTEGRAL TO ALL EMERGENCY FIXTURES, OTHER WISE LOCATION OF EMERGENCY LIGHT TEST SWITCH TO BE APPROVED BY ARCHITECT AND ELECTRICAL ENGINEER 30 DAYS PRIOR TO ROUGH IN.
5. ELECTRICAL CONTRACTOR TO OBTAIN APPROVAL ON ALL LIGHT SWITCH LOCATIONS 30 DAYS PRIOR TO ROUGH IN.
6. ELECTRICAL CONTRACTOR TO REVIEW ARCHITECTS EXITING PLAN AND PROVIDE EXIT SIGNS PER ARCHITECTS PLAN AND ELECTRICAL ENGINEERS LIGHTING PLANS. EXIT SIGNS MOUNTED ABOVE 10' AFF REQUIRE TEAM APPROVAL PRIOR TO ROUGH-IN.
7. ELECTRICAL CONTRACTOR TO REVIEW ARCHITECTS REFLECTED CEILING PLAN AND PROVIDE BID PER ARCHITECTS PLAN AND ELECTRICAL ENGINEERS PLAN.
8. ELECTRICAL CONTRACTOR TO NOTIFY ARCHITECT AND ELECTRICAL ENGINEER FOR ANY FIXTURE CONFLICTS THAT PROHIBIT CORRECT INSTALL WITH FRAMING OR CEILING T-BAR OR CEILING SYSTEMS. PROVIDE MINIMUM OF 30 DAYS NOTICE PRIOR TO ROUGH-IN.
9. ELECTRICAL CONTRACTOR TO VERIFY LIGHT FIXTURE AND T-GRID COMPATIBILITY PRIOR TO ORDER. PROVIDE MINIMUM OF 30 DAYS NOTICE TO ENGINEER AND ARCHITECT IN THE EVENT OF CONFLICT.

EGRESS LIGHTING NOTES:

1. FACES AND CHEVRONS ON EMERGENCY EXIT SIGNS TO MATCH ARCHITECTURAL PLANS.
2. FOR EMERGENCY LIGHTING SHOWN, PROVIDE EMERGENCY LIGHTING CONTROL UNIT (ELCU OR EQUAL) AND CONNECT BOTH LOCAL CONTROLLED NORMAL POWER AND CONSTANT HOT EMERGENCY CIRCUIT SO THAT FIXTURE MAY BE CONTROLLED DURING MANUAL OPERATION AND PROVIDE 100% OUTPUT EMERGENCY LIGHTING DURING POWER OUTAGE.
3. PROVIDE ALL COST FOR COMPLETE INSTALLATION OF FIVE ADDITIONAL EXIT SIGNS. LOCATION TO BE DETERMINED DURING FINAL INSPECTION BY FIRE MARSHAL. TURN OVER EXTRA STOCK TO OWNER.
4. THE MEANS OF EGRESS INCLUDING THE EXIT DISCHARGE SHALL BE ILLUMINATED TO A LEVEL OF NOT LESS THAN ONE FOOT CANDLE AT THE WALKING SURFACE AT ALL TIMES THE BUILDING SPACE SERVED BY MEANS OF EGRESS IS OCCUPIED.
5. EXIT SIGNS SHALL BE INTERNALLY OR EXTERNALLY ILLUMINATED AT ALL TIMES AND SHALL BE CONNECTED TO AN EMERGENCY POWER SYSTEM (BATTERIES UNIT EQUIPMENT OR AN ON-SITE GENERATOR) THAT WILL AUTOMATICALLY ILLUMINATE THE EXIT SIGNS FOR A DURATION OF NOT LESS THAN 90 MINUTES.

SHEET NOTES:

①

INDUSTRIAL PROJECT SHELL

**260 EDDY JONES WAY
 OCEANSIDE, CA 92058**

DATE	REMARKS

PA/PM:	Approver
DRAWN BY:.	Author
JOB NO.:	SDG21-0056-00

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 San Diego, CA 92127
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SHEET
E010

SITE LIGHTING PLAN

SCALE:
 1/10" = 1'-0"

C:\NewDraws_Arch\j_malcom\m4

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BUILDING MEASUREMENT

INDUSTRIAL PROJECT SHELL
260 EDDY JONES WAY
OCEANSIDE, CA 92058

DETAIL SHEET
DATE: _____
REMARKS: _____

PA/PM: Approver
DRAWN BY: Author
JOB NO.: SDG21-0056-00

SHEET
E200

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ADDITIONAL INFORMATION (CONTINUED)
FEATURES
CONTROL TECHNOLOGY
SPECIFICATIONS
CONSTRUCTION
ELECTRICAL (CONTINUED)
CERTIFICATIONS
WARRANTY
KEY DATA

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
DELIVERED LUMENS
ORDERING GUIDE (CONTINUED)
CONTROLS
DIMENSIONS
PHOTOMETRY

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
DELIVERED LUMENS
DIMENSIONS
PHOTOMETRY

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ELECTRICAL DATA
PROJECTED LUMEN MAINTENANCE
ADDITIONAL INFORMATION
DIMENSIONS
PHOTOMETRY

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ADDITIONAL INFORMATION (CONTINUED)
USE OF TRADEMARKS AND TRADE NAMES
DIMENSIONS
PHOTOMETRY

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ORDERING GUIDE
KEY DATA

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
DELIVERED LUMENS
ORDERING GUIDE
KEY DATA

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
PHOTOMETRY
KEY DATA

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ADDITIONAL INFORMATION (CONTINUED)
KEY DATA

BEACON VIPER L LARGE VIPER LUMINAIRE
DATE: _____ LOCATION: _____
TYPE: _____ PROJECT: _____
CATEGORY: _____
ADDITIONAL INFORMATION (CONTINUED)
KEY DATA

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EDDIE JONES INDUSTRIAL PROJECT

CONCEPT LANDSCAPE PLAN

260 EDDIE JONES WAY, OCEANSIDE, CA. 92058

CITY OF OCEANSIDE GENERAL NOTES:

1. FINAL LANDSCAPE PLANS SHALL ACCURATELY SHOW PLACEMENT OF TREES, SHRUBS, AND GROUNDCOVERS.

2. LANDSCAPE ARCHITECT SHALL VERIFY UTILITY, SEWER, STORM DRAIN EASEMENT AND PLACE PLANTING LOCATIONS ACCORDINGLY TO MEET CITY OF OCEANSIDE REQUIREMENTS.

3. ALL REQUIRED LANDSCAPE AREAS SHALL BE MAINTAINED BY OWNER. THE LANDSCAPE AREAS SHALL BE MAINTAINED PER CITY OF OCEANSIDE REQUIREMENTS.

4. IRRIGATION: AN AUTOMATIC IRRIGATION SYSTEM SHALL BE INSTALLED TO PROVIDE COVERAGE FOR ALL PLANTING AREAS SHOWN ON THE PLAN. LOW PRECIPITATION EQUIPMENT SHALL PROVIDE SUFFICIENT WATER FOR PLANT GROWTH WITH A MINIMUM WATER LOSS DUE TO WATER RUN - OFF. IRRIGATION SYSTEMS SHALL USE HIGH QUALITY, AUTOMATIC CONTROL VALVES, CONTROLLERS AND OTHER NECESSARY IRRIGATION EQUIPMENT. ALL COMPONENTS SHALL BE OF NON - CORROSIVE MATERIAL. ALL DRIP SYSTEMS SHALL BE ADEQUATELY FILTERED AND REGULATED PER THE MANUFACTURER'S RECOMMENDED DESIGN PARAMETERS. ALL IRRIGATION IMPROVEMENTS SHALL FOLLOW THE CITY OF OCEANSIDE GUIDELINES AND WATER CONSERVATION ORDINANCE.

5. PLANTING: THE SELECTION OF PLANT MATERIAL IS BASED ON CULTURAL, AESTHETIC, AND MAINTENANCE CONSIDERATIONS. ALL PLANTING AREAS SHALL BE PREPARED WITH APPROPRIATE SOIL AMENDMENTS, FERTILIZERS, AND APPROPRIATE SUPPLEMENTS BASED UPON A SOILS REPORT FROM AN AGRICULTURAL SUITABILITY SOIL SAMPLE TAKEN FROM THE SITE. GROUND COVERS OR BARK MULCH SHALL FILL IN BETWEEN THE SHRUBS TO SHIELD THE SOIL FROM THE SUN, EVAPOTRANSPIRATION AND RUN - OFF. ALL THE FLOWER AND SHRUB BEDS SHALL BE MULCHED TO A 3" DEPTH TO HELP CONSERVE WATER, LOWER THE SOIL TEMPERATURE AND REDUCE WEED GROWTH. THE SHRUBS SHALL BE ALLOWED TO GROW IN THEIR NATURAL FORMS. ALL LANDSCAPE IMPROVEMENTS SHALL FOLLOW THE CITY OF OCEANSIDE GUIDELINES.

LANDSCAPE NOTES:

ALL LANDSCAPE AND IRRIGATION PLANS SHALL CONFORM TO APPLICABLE MUNICIPAL AND REGIONAL DESIGN CODE, LANDSCAPE REGULATIONS, STREETScape MANUALS, OR LANDSCAPE STANDARDS.

1. ALL LANDSCAPE AREAS NOT CONTAINING TREES SHALL HAVE A MINIMUM FINISHED DIMENSION OF 3 FEET (WIDTH) MEASURED FROM THE INSIDE FACE OF PAVEMENT.

2. EACH TREE SHALL BE PLANTED IN AN AIR AND WATER-PERMEABLE PLANTING AREA OF AT LEAST 40

SQUARE FEET WITH A MINIMUM FINISHED DIMENSION (WIDTH) OF 5 FEET MEASURED FROM THE INSIDE FACE OF PAVEMENT. THE PLANTING AREA SHALL BE UNENCUMBERED BY UTILITIES.

3. ALL IRRIGATION DESIGN AND INSTALLATION SHALL CONFORM TO MUNICIPAL AND REGIONAL STANDARDS AS WELL AS INDUSTRY BEST PRACTICES.

4. ANY CHANGES TO THE STREETScape, SITE AND/OR LANDSCAPE PLANS SHALL BE SUBMITTED TO THE LANDSCAPE ARCHITECT PRIOR TO SUBMITTAL TO THE APPLICABLE MUNICIPAL OR REGIONAL REVIEW / APPROVAL AGENCY.

5. ANY DISCREPANCY OR CONFLICTS IN DIMENSIONS, LANDSCAPE AREAS, OR MATERIAL SHALL BE BROUGHT TO THE ATTENTION OF THE OWNERS, LANDSCAPE ARCHITECT, AND THE AFFECTED DISCIPLINES IMMEDIATELY.

6. TREES SHALL BE SELF-SUPPORTING, WOODY PLANTS WITH AT LEAST ONE WELL-DEFINED TRUNK AND SHALL NORMALLY ATTAIN A MATURE HEIGHT AND SPREAD OF AT LEAST 15 FEET WITHIN 8 YEARS OF PLANTING.

7. NO TREES OR SHRUBS EXCEEDING THREE FEET IN HEIGHT AT MATURITY TO BE INSTALLED WITHIN TEN FEET OF ANY PUBLIC SEWER FACILITIES OR IN THE PUBLIC EASEMENT.

STREET TREE SEPARATION DISTANCE:

IMPROVEMENT	MINIMUM DISTANCE TO STREET TREE
INTERSECTIONS	25 FEET
DRIVEWAYS	10 FEET
SEWER	10 FEET
WATER LINE	10 FEET
LIGHT POLE	15 FEET
UTILITY POLE	15 FEET
ABOVE GROUND UTILITY STRUCTURE	10 FEET

CONTRACTOR SHALL REPAIR AND REPLACE ALL EXISTING LANDSCAPE, IRRIGATION, AND ANY EXISTING IMPROVEMENTS DISPLACED OR DAMAGED AS A RESULT OF PERFORMING THE WORK OF THIS CONTRACT AT NO ADDITIONAL COST TO THE OWNER. EXISTING LANDSCAPE AND IRRIGATION SHALL BE EXTENDED IN-KIND INTO AREAS LEFT VACANT BY DEMOLITION AT NO ADDITIONAL COST TO THE OWNER.



LANDSCAPE SHEET INDEX	
Sheet Number	Sheet Title
L1.00	CONCEPTUAL LANDSCAPE COVER SHEET
L1.01	CONCEPTUAL LANDSCAPE ORIENTATION PLAN
L2.00	CONCEPTUAL LANDSCAPE NOTES AND LEGEND
L2.01	CONCEPTUAL LANDSCAPE PLAN
L2.02	CONCEPTUAL LANDSCAPE PLAN
L2.03	CONCEPTUAL LANDSCAPE PLAN
L2.04	CONCEPTUAL LANDSCAPE PLAN

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CONCEPTUAL
LANDSCAPE
COVER SHEET

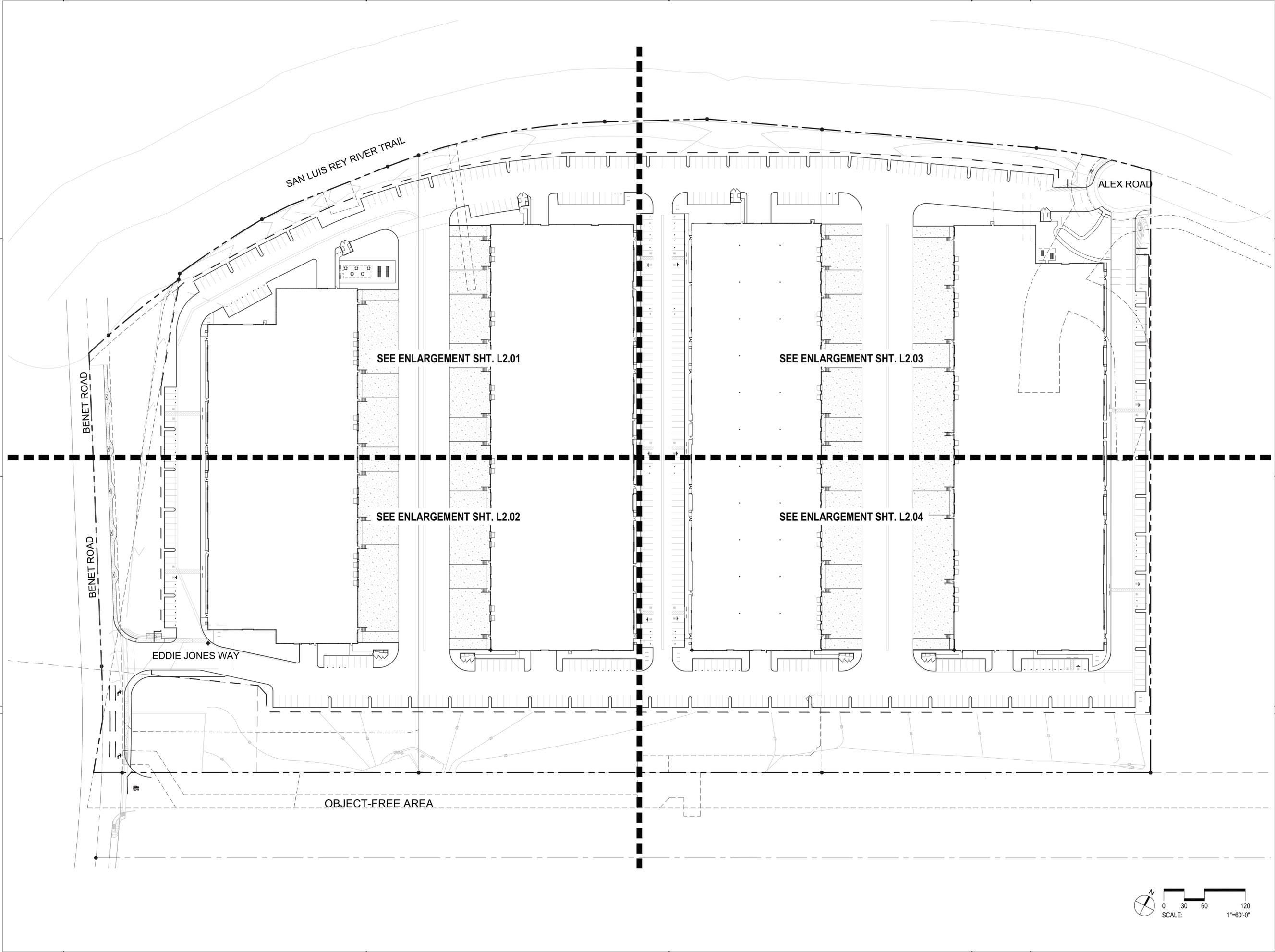
EDDIE JONES INDUSTRIAL
PROJECT - 4 BLDG ALT
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

DATE	REMARKS

PA/PM:	D. McCullough
DRAWN BY.:	MB/KB
JOB NO.:	22174

SHEET
L1.00

03/05/2024



SAN LUIS REY RIVER TRAIL

ALEX ROAD

BENET ROAD

BENET ROAD

EDDIE JONES WAY

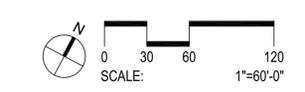
SEE ENLARGEMENT SHT. L2.01

SEE ENLARGEMENT SHT. L2.03

SEE ENLARGEMENT SHT. L2.02

SEE ENLARGEMENT SHT. L2.04

OBJECT-FREE AREA



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CONCEPTUAL
 LANDSCAPE
 ORIENTATION
 PLAN

EDDIE JONES INDUSTRIAL
 PROJECT - 4 BLDG ALT
 260 EDDIE JONES WAY
 OCEANSIDE, CALIFORNIA 92058

DATE	REMARKS

PA/PM:	D. McCullough
DRAWN BY.:	MB/KB
JOB NO.:	22174

SHEET
L1.01

PLANT SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME	CONT	WUCOLS	QTY	MATURE HEIGHT	MATURE WIDTH
TREES						
	PERIMETER TREE 50% - 24" BOX & 50% - 36" BOX			135		
	PLATANUS MEXICANA / MEXICAN SYCAMORE	24" BOX	M: 0.4 - 0.6		25 - 40ft. ht.	12 - 25ft. w.
	PLATANUS RACEMOSA / CALIFORNIA SYCAMORE	15 GAL	M: 0.4 - 0.6		40 - 65ft. ht.	25 - 40ft. w.
	POPULUS FREMONTII / FREMONT COTTONWOOD	15 GAL	M: 0.4 - 0.6			
	QUERCUS AGRIFOLIA / COAST LIVE OAK	15 GAL	VL: < 0.1		40 - 65ft. ht.	25 - 40ft. w.
	QUERCUS ENGELMANNII / ENGELMANN OAK	15 GAL	VL: < 0.1		25 - 65ft. ht.	25 - 40ft. w.
	SALIX LASIOLEPIS / ARROYO WILLOW	15 GAL	H: 0.7 - 0.9		25 - 40ft. ht.	10 - 15ft. w.
	UMBELLULARIA CALIFORNICA / BAY LAUREL	15 GAL	L: 0.1 - 0.3		25 - 40ft. ht.	25 - 40ft. w.
	ACCENT/SHADE TREE 50% - 24" BOX & 50% - 36" BOX			127		
	ALBIZIA JULIBRISSIN / SILK TREE	36" BOX	L: 0.1 - 0.3		15 - 65ft. ht.	15 - 25ft. w.
	CHILOPSIS LINEARIS / DESERT WILLOW	36" BOX	VL: < 0.1		15 - 25ft. ht.	15 - 25ft. w.
	CHITALPA TASHKENTENSIS / CHITALPA	36" BOX	L: 0.1 - 0.3		15 - 25ft. ht.	15 - 25ft. w.
	JACARANDA MIMOSIFOLIA / JACARANDA MULTI-TRUNK	36" BOX	M: 0.4 - 0.6		40 - 65ft. ht.	15 - 25ft. w.
	PROSOPIS CHILENSIS / CHILEAN MESQUITE	36" BOX	L: 0.1 - 0.3		25 - 40ft. ht.	25 - 40ft. w.
	QUERCUS AGRIFOLIA / COAST LIVE OAK MULTI-TRUNK	36" BOX	VL: < 0.1		40 - 65ft. ht.	25 - 40ft. w.
TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE	36" BOX	M: 0.4 - 0.6		25 - 40ft. ht.	15 - 25ft. w.	
ULMUS PARVIFOLIA / LACEBARK ELM	15 GAL	M: 0.4 - 0.6		25 - 40ft. ht.	25 - 40ft. w.	
UMBELLULARIA CALIFORNICA / CALIFORNIA LAUREL	36" BOX	L: 0.1 - 0.3		25 - 40ft. ht.	25 - 40ft. w.	
VERTICAL ACCENT TREE/SCREEN TREE 24" BOX			60			
ACACIA SALICINA / WILLOW ACACIA	24" BOX	L: 0.1 - 0.3		25 - 65ft. ht.	15 - 25ft. w.	
ACACIA STENOPHYLLA / SHOESTRING ACACIA	24" BOX	L: 0.1 - 0.3		25 - 40ft. ht.	10 - 15ft. w.	
CEDRUS DEODARA / DEODAR CEDAR	24" BOX	L: 0.1 - 0.3		40 - 65ft. ht.	25 - 40ft. w.	
CUPRESSUS SEMPERVIRENS / ITALIAN CYPRESS	24" BOX	L: 0.1 - 0.3		40 - 65ft. ht.	3 - 6ft. w.	
HYMENOSPORUM FLAVUM / SWEETSHADE	24" BOX	M: 0.4 - 0.6		40 - 65ft. ht.	15 - 20ft. w.	
LYONOTHAMNUS FLORIBUNDUS / CATALINA IRONWOOD	24" BOX	L: 0.1 - 0.3		25 - 40ft. ht.	15 - 25ft. w.	
PINUS ELДАРICA / AFGHAN PINE	24" BOX	VL: < 0.1		40 - 65ft. ht.	15 - 25ft. w.	
PINUS TORREYANA / TORREY PINE	24" BOX	L: 0.1 - 0.3				
TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE	24" BOX	M: 0.4 - 0.6		25 - 40ft. ht.	15 - 25ft. w.	
TRISTANIA CONFERTA / BRISBANE BOX	24" BOX	M: 0.4 - 0.6		25 - ' ht.	15 - 25ft. w.	
SMALL ACCENT TREES/VERTICAL ACCENT PALM 15 GAL - 24" BOX			120			
ARBUTUS X / ARBUTUS MULTI-TRUNK	15 GAL	L: 0.1 - 0.3		15 - 25ft. ht.	15 - 25ft. w.	
ARCTOSTAPHYLOS X / MANZANITA	15 GAL	L: 0.1 - 0.3		6 - 10ft. ht.		
CERCIS CANADENSIS 'FOREST PANSY' TM / FOREST PANSY REDBUD MULTI-TRUNK	15 GAL	M: 0.4 - 0.6		15 - 25ft. ht.	15 - 25ft. w.	
CERCIS OCCIDENTALIS / WESTERN REDBUD MULTI-TRUNK	15 GAL	L: 0.1 - 0.3		10 - 15ft. ht.	6 - 10ft. w.	
CORDYLINE X / CORDYLINE	15 GAL	M: 0.4 - 0.6				
CYATHEA COOPERI / AUSTRALIAN TREE FERN	15 GAL	M: 0.4 - 0.6		15 - 40ft. ht.	6 - 10ft. w.	
DRACAENA ALETRIFORMIS / DRAGON TREE	15 GAL	M: 0.4 - 0.6				
GINKGO BILOBA / MAIDENHAIR TREE	15 GAL	M: 0.4 - 0.6		40 - 65ft. ht.	25 - 40ft. w.	
LAGERSTROEMIA INDICA / CRAPE MYRTLE	15 GAL	M: 0.4 - 0.6		10 - 15ft. ht.	10 - 15ft. w.	
MELALEUCA ARMILLARIS / DROOPING MELALEUCA	15 GAL	L: 0.1 - 0.3		40 - 65ft. ht.		
OLEA EUROPAEA 'SWAN HILL' TM / SWAN HILL OLIVE	15 GAL	L: 0.1 - 0.3		15 - 25ft. ht.	15 - 25ft. w.	
SPECIMEN TREE 48"-60"BOX			8			
QUERCUS ENGELMANNII / ENGELMANN OAK	15 GAL	VL: < 0.1		25 - 65ft. ht.	25 - 40ft. w.	

SYMBOL	BOTANICAL / COMMON NAME	CONT	WUCOLS	MATURE HEIGHT	MATURE WIDTH
SHRUB AREAS					
	SLOPE PLANTING (1 GAL-5GAL.)				
	ACHILLEA MILLEFOLIUM / COMMON YARROW	-	L: 0.1 - 0.3	6 - 18in. ht.	1 - 3ft. w.
	ALOE SPP. / ALOE	-	VL: <0.1		
	BACCHARIS PILULARIS / COYOTE BRUSH	-	L: 0.1 - 0.3	6"-36in. ht.	1 - 3ft. w.
	BOUTELOUA GRACILIS / BLUE GRAMA GRASS	-	L: 0.1 - 0.3	18 - 36in. ht.	1 - 3ft. w.
	CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / KARL FOERSTER FEATHER REED GRASS	-	M: 0.4 - 0.6	3 - 6ft. ht.	1 - 3ft. w.
	CAREX MORROWII / JAPANESE SEDGE	-	M: 0.4 - 0.6	6 - 18in. ht.	1 - 3ft. w.
	CARISSA MACROCARPA / NATAL PLUM	LINER		6 - 1' ht.	
	CEANOTHUS CYANEUS / SAN DIEGO CEANOTHUS	-	L: 0.1 - 0.3		
	CHONDROPETALUM ELEPHANTINUM / LARGE CAPE RUSH	-	L: 0.1 - 0.3	3 - 6ft. ht.	3 - 6ft. w.
	CISTUS X SKANBERGII / CORAL ROCKROSE	-	L: 0.1 - 0.3	18 - 36in. ht.	1 - 3ft. w.
	EPILOBIUM CANUM / CALIFORNIA FUCHSIA	-	L: 0.1 - 0.3	6"-36in. ht.	1 - 3ft. w.
	ERIOGONUM FASCICULATUM / CALIFORNIA BUCKWHEAT	-	VL: <0.1	18 - 36in. ht.	1 - 3ft. w.
	FESTUCA IDAHOENSIS / IDAHO FESCUE	-	L: 0.1 - 0.3	6"-36in. ht.	< 1ft. w.
	GERANIUM SPP. / GERANIUM	-	M: 0.4 - 0.6	6"-36in. ht.	
HETEROMELES ARBUTIFOLIA / TOYON	-	L: 0.1 - 0.3	10 - 15ft. ht.	6 - 10ft. w.	
LEYMUS CONDENSATUS 'CANYON PRINCE' / CANYON PRINCE GIANT WILD RYE	-	L: 0.1 - 0.3	18 - 36in. ht.	1 - 3ft. w.	
MIMULUS AURANTIACUS / STICKY MONKEYFLOWER	-	VL: <0.1	3 - 6ft. ht.	1 - 3ft. w.	
MYRICA CALIFORNICA / PACIFIC WAX MYRTLE	-	L: 0.1 - 0.3	15 - 25ft. ht.	10 - ' w.	
RHAMNUS CALIFORNICA / CALIFORNIA COFFEEBERRY	-	VL: <0.1	6 - 10ft. ht.	6 - 10ft. w.	
ROSA CALIFORNICA / CALIFORNIA WILD ROSE	-	L: 0.1 - 0.3	6 - 10ft. ht.	6 - 10ft. w.	
SALVIA SPP. / SALVIA SPECIES	-	L: 0.1 - 0.3			
BUILDING FOUNDATION PLANTING 50% 1 GAL. & 50% 5 GAL.					
ACHILLEA MILLEFOLIUM / COMMON YARROW	-	L: 0.1 - 0.3	6 - 18in. ht.	1 - 3ft. w.	
AGAVE AMERICANA / CENTURY PLANT	-	VL: <0.1	3 - 6ft. ht.	6 - 10ft. w.	
AGAVE ATTENUATA / AGAVE	-	VL: <0.1	3 - 6ft. ht.	3 - 6ft. w.	
AGAVE DESMETTIANA / DWARF CENTURY PLANT	-	VL: <0.1	18 - 36in. ht.	3 - 6ft. w.	
ALOE SPP. / ALOE	-	VL: <0.1			
ASPARAGUS DENSIFLORUS / ASPARAGUS FERN	-	M: 0.4 - 0.6	18 - 36in. ht.	1 - 3ft. w.	
BACCHARIS SAROTHIODES / DESERTBROOM BACCHARIS	-	VL: <0.1	18 - 6ft. ht.	3 - 6ft. w.	
BOUTELOUA GRACILIS / BLUE GRAMA GRASS	-	L: 0.1 - 0.3	6 - 10ft. ht.	1 - 3ft. w.	
BROMELIA BALANSAE / HEART OF FLAME BROMELIAD	-	L: 0.1 - 0.3			
BUXUS SPP. / BOXWOOD	-	M: 0.4 - 0.6	6 - 10ft. ht.	6 - 10ft. w.	
CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS	-	M: 0.4 - 0.6	3 - 6ft. ht.	1 - 3ft. w.	
CHLOROPHYTUM COMOSUM / SPIDER PLANT	-	M: 0.4 - 0.6	6 - 36in. ht.		
CUPHEA IGNEA / CIGAR PLANT	-	M: 0.4 - 0.6	18 - 36in. ht.	1 - 3ft. w.	
DASYLIRION LONGISSIMUM / TOOTHLESS DESERT SPOON	-	VL: <0.1	6 - 10ft. ht.	3 - 6ft. w.	
DIANELLA SP. / FLAX LILY	-	L: 0.1 - 0.3			
DODONAEA VISCOSA / HOPSEED BUSH	-	L: 0.1 - 0.3	10 - 15ft. ht.		
FURCRAEA FOETIDA 'MEDIOPICTA' / MAURITIUS HEMP	-	L: 0.1 - 0.3	3 - 6ft. ht.	6 - 10ft. w.	
HESPERALOE PARVIFLORA / RED YUCCA	-	VL: <0.1	3 - 6ft. ht.	3 - 6ft. w.	

SYMBOL	BOTANICAL / COMMON NAME	CONT	WUCOLS	QTY	MATURE HEIGHT	MATURE WIDTH
	HEUCHERA SP. / CORAL BELLS	-	M: 0.4 - 0.6			
	LANTANA SP. / CHAPEL HILL YELLOW LANTANA	-	L: 0.1 - 0.3			
	LAVANDULA SP. / LAVENDER	-	L: 0.1 - 0.3			
	LEPTOSPERMUM SCOPARIUM / NEW ZEALAND TEA TREE	-	M: 0.4 - 0.6		10 - 15ft. ht.	6 - 10ft. w.
	LIGUSTRUM JAPONICUM / JAPANESE PRIVET	-	L: 0.1 - 0.3		6 - 15ft. ht.	3 - 6ft. w.
	LOMANDRA LONGIFOLIA / MAT RUSH	-	M: 0.4 - 0.6		18 - 36in. ht.	1 - 3ft. w.
	LONICERA JAPONICA / JAPANESE HONEYSUCKLE	-	L: 0.1 - 0.3		< 15ft. ht.	
	LOROPETALUM CHINENSE / CHINESE FRINGE FLOWER	-	L: 0.1 - 0.3		3 - 15ft. ht.	6 - 10ft. w.
	MIMULUS SPP. / MONKEYFLOWER	-	L: 0.1 - 0.3			
	MYRICA CALIFORNICA / PACIFIC WAX MYRTLE	-	L: 0.1 - 0.3		15 - 25ft. ht.	10 - ' w.
	PACHYCREUS MARGINATUS / TOTEM POLE CACTUS	-	VL: <0.1		3 - 1' ht.	1 - 3ft. w.
	PHILODENDRON XANADU / XANADU PHILODENDRON	-	M: 0.4 - 0.6		18 - 36in. ht.	3 - 6ft. w.
	PHORMIUM TENAX / NEW ZEALAND FLAX	-	L: 0.1 - 0.3		3 - 6ft. ht.	1 - 3ft. w.
	PITTIOSPORUM SPP. / PITTIOSPORUM	-	M: 0.4 - 0.6			
	POLYSTICHUM MUNITUM / WESTERN SWORD FERN	-	M: 0.4 - 0.6		18 - 36in. ht.	1 - 3ft. w.
	ROSA CALIFORNICA / CALIFORNIA WILD ROSE	-	L: 0.1 - 0.3		6 - 10ft. ht.	6 - 10ft. w.
	ROSMARINUS OFFICINALIS / ROSEMARY	-	VL: <0.1		6 - 6' ht.	1 - 6' w.
	RUSSELLIA EQUIREFORMIS / FIRECRACKER PLANT	-	L: 0.1 - 0.3		3 - 6ft. ht.	3 - 6ft. w.
	SALVIA SP. / SALVIA SPECIES	-	L: 0.1 - 0.3			
	PARKING LOT PLANTING (1 GAL.-5GAL.)					
TRACHELOSPERMUM JASMINOIDES / CHINESE STAR JASMINE	-	M: 0.4 - 0.6		3 - 1' ht.	3 - 1' w.	
CARISSA MACROCARPA / NATAL PLUM	LINER			6 - 1' ht.		
DIETES GRANDIFLORA / FORTNIGHT LILY	-	L: 0.1 - 0.3		18 - 36in. ht.	1 - 3ft. w.	
LIGUSTRUM JAPONICUM 'TEXANUM' / TEXAS JAPANESE PRIVET	-	L: 0.1 - 0.3		6 - 10ft. ht.	3 - 6ft. w.	
PHORMIUM TENAX / NEW ZEALAND FLAX	-	L: 0.1 - 0.3		3 - 6ft. ht.	1 - 3ft. w.	
SENECIO SERPENS / BLUE CHALKSTICKS	-	L: 0.1 - 0.3		6 - 18in. ht.	1 - 3ft. w.	
VERBENA PERUVIANA / PERUVIAN VERBENA	-	L: 0.1 - 0.3		< 6in. ht.		

EXISTING TREE SCHEDULE

EXISTING TREES (TO BE REMOVED)	BOTANICAL / COMMON NAME	CONT	CAL	QTY
EC3	CALLISTEMON CITRINUS LEMON BOTTLEBRUSH	EXISTING	6" CAL	4
EG	CASSIA LEPTOPHYLLA GOLD MEDALLION TREE	EXISTING	3" CAL	21
EE	EUCALYPTUS CAMALDULENSIS RIVER RED GUM	EXISTING	8" CAL	11
EM2	FRAXINUS VELUTINA 'MODESTO' MODESTO VELVET ASH	EXISTING	8" CAL	1
EJ	JUNIPERUS CHINENSIS 'KAIZUKA' HOLLYWOOD JUNIPER	EXISTING	6" CAL	26
EL	LAGERSTROEMIA INDICA CRAPE MYRTLE	EXISTING	5" CAL	9
EQ	MELALEUCA QUINQUENERVIA CAJEPUT TREE	EXISTING	10" CAL	9
EB	PINUS THUNBERGII JAPANESE BLACK PINE	EXISTING	1.0'	3
EC	POPULUS NIGRA COTTONWOOD	EXISTING	10" CAL	8
EP	SCHINUS MOLLE CALIFORNIA PEPPER	EXISTING	12" CAL	2
ES	SCHINUS TEREBINTHIFOLIA BRAZILIAN PEPPER TREE	EXISTING	14" CAL	7
ET	TIPUANA TIPU TIPU TREE	EXISTING	12" CAL	5

SCHEDULE OF TREE REPLACEMENT

EXISTING TREES = 772 CALIPER INCHES
 EXISTING TREES TO BE REMOVED = 772 CALIPER INCHES
 PROPOSED TREES = 1,495 CALIPER INCHES

REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION	QTY
1	PROPOSED FLOOD WALL OR FENCE PER CIVIL	
2	PROPOSED PCC SIDEWALK PER CIVIL	24,102 SF
3	PROPOSED TRASH ENCLOSURE PER ARCH.	
4	PROPOSED OUTDOOR SEATING AREA - STABILIZED DECOMPOSED GRANITE	1,192 SF
5	PROPOSED CONCRETE RAMP PER CIVIL	
6	BIKE RACK - PER ARCHITECT	

GENERAL NOTE:

ALL LANDSCAPE AND IRRIGATION SHALL CONFORM TO THE STANDARDS OF THE CITY-WIDE LANDSCAPE REGULATIONS AND THE LANDSCAPE TECHNICAL MANUAL AND ALL OTHER LANDSCAPE RELATED CITY AND REGIONAL STANDARDS.

MAINTENANCE:

ALL ON-SITE LANDSCAPE AREAS AND LANDSCAPE AREAS WITHIN RIGHT-OF-WAY TO BE MAINTAINED BY OWNER. THE LANDSCAPED AREAS SHALL BE MAINTAINED FREE OF DEBRIS AND LITTER AND ALL PLANT MATERIAL SHALL BE MAINTAINED IN A HEALTHY GROWING CONDITION. LANDSCAPE AS PART OF THE PROJECT SHALL BE MAINTAINED AND IRRIGATED FOR THE LIFE OF THE PERMIT.

STREET TREES:

ALL STREET TREES SHALL COMPLY WITH THE CITY OF OCEANSIDE STREET TREE STANDARD DETAIL 211A.

EXISTING TREES:

CARE SHALL BE USED DURING THE DEMOLITION AND CONSTRUCTION PROCESS TO ENSURE THAT EXISTING TREES TO REMAIN ARE PROPERLY MAINTAINED. CONTRACTOR SHALL INSTALL TEMPORARY CONSTRUCTION FENCING AROUND EACH EXISTING TREE TO REMAIN. TREES SHALL BE PROPERLY IRRIGATED AND KEPT IN A HEALTHY GROWING CONDITION.

DISTURBED AREA NOTE:

ALL GRADED, DISTURBED OR ERODED AREAS THAT WILL NOT BE PERMANENTLY PAVED OR COVERED BY STRUCTURES SHALL BE PERMANENTLY RE-VEGETATED AND IRRIGATED AS SHOWN IN TABLE 142-04F AND IN ACCORDANCE WITH THE STANDARDS IN THE LAND DEVELOPMENT MANUAL (142.0411(A)).

INVASIVE PLANT NOTE:

ALL EXISTING, INVASIVE PLANT SPECIES, INCLUDING VEGETATIVE PARTS AND ROOT SYSTEMS, SHALL BE COMPLETELY REMOVED FROM THE PREMISES WHEN THE COMBINATION OF SPECIES, TYPE, LOCATION, AND SURROUNDING ENVIRONMENTAL CONDITIONS PROVIDES A MEANS FOR THE SPECIES TO INVADE OTHER AREAS OF NATIVE PLANT MATERIAL THAT ARE ON OR OFF OF THE PREMISES.

UTILITY SCREENING:

ALL WATER, SEWER, AND GAS UTILITIES SHALL BE EFFECTIVELY SCREENED WITH PLANT MATERIAL AT THE TIME OF PLANT INSTALLATION.

ROOT BARRIER:

NON-BIODEGRADABLE ROOT BARRIERS SHALL BE INSTALLED AROUND ALL NEW STREET TREES WITHIN 5' OF HARDSCAPE SURFACES.

IRRIGATION NOTES:

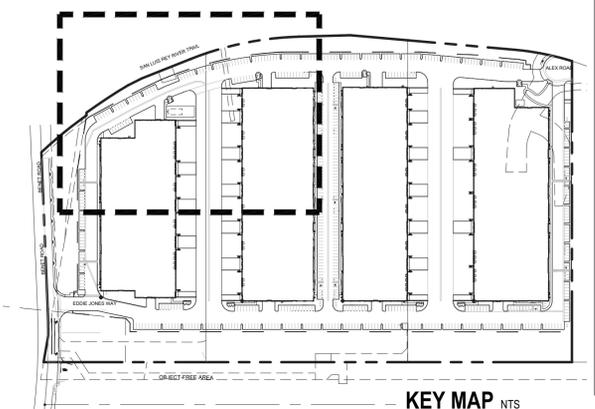
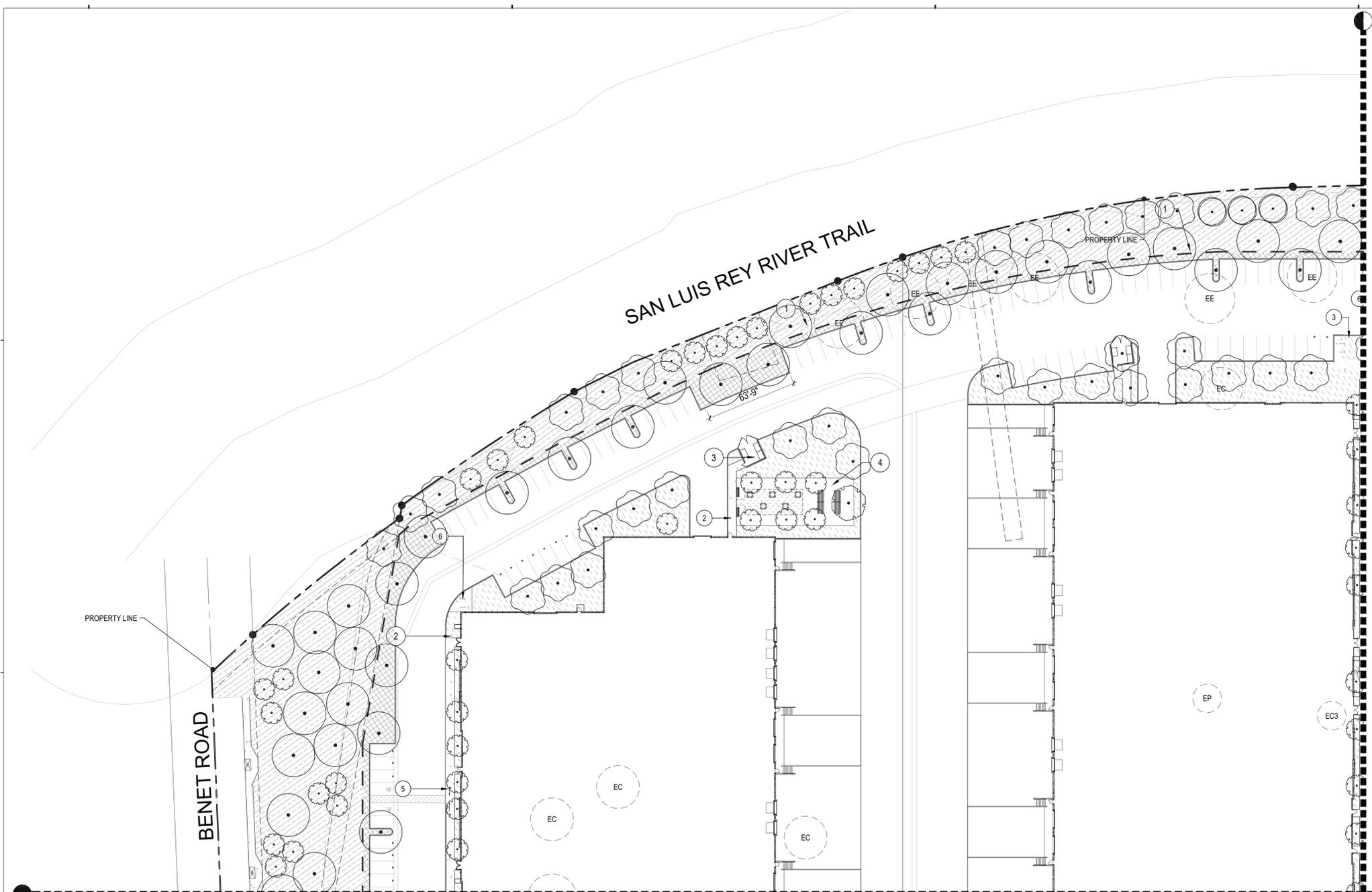
AN AUTOMATIC IRRIGATION SYSTEM SHALL BE INSTALLED TO PROVIDE COVERAGE FOR ALL PLANTING AREAS SHOWN ON THE PLAN. LOW PRECIPITATION EQUIPMENT SHALL PROVIDE SUFFICIENT WATER FOR PLANT GROWTH WITH A MINIMUM WATER LOSS DUE TO WATER RUNOFF. IRRIGATION SYSTEMS SHALL USE HIGH QUALITY, AUTOMATIC CONTROL VALVES, CONTROLLERS AND OTHER NECESSARY IRRIGATION EQUIPMENT. ALL COMPONENTS SHALL BE OF NON CORROSIVE MATERIAL. ALL DRIP SYSTEMS SHALL BE ADEQUATELY FILTERED AND REGULATED PER THE MANUFACTURER'S RECOMMENDED DESIGN PARAMETERS. ALL IRRIGATION IMPROVEMENTS SHALL FOLLOW THE CITY OF OCEANSIDE GUIDELINES AND WATER CONSERVATION ORDINANCE.

PLANTING NOTES:

THE SELECTION OF PLANT MATERIAL IS BASED ON CULTURAL, AESTHETIC, AND MAINTENANCE CONSIDERATIONS. ALL PLANTING AREAS SHALL BE PREPARED WITH APPROPRIATE SOIL AMENDMENTS, FERTILIZERS, AND APPROPRIATE SUPPLEMENTS BASED UPON A SOILS REPORT FROM AN AGRICULTURAL SUITABILITY SOIL SAMPLE TAKEN FROM THE SITE. GROUND COVERS OR BARK MULCH SHALL FILL IN BETWEEN THE SHRUBS TO SHIELD THE SOIL FROM THE SUN, EVAPOTRANSPIRATION AND RUN OFF. ALL THE FLOWER AND SHRUB BEDS SHALL BE MULCHED TO A 3" DEPTH TO HELP CONSERVE WATER, LOWER THE SOIL TEMPERATURE AND REDUCE WEED GROWTH. THE SHRUBS SHALL BE ALLOWED TO GROW IN THEIR NATURAL FORMS. ALL LANDSCAPE IMPROVEMENTS SHALL FOLLOW THE CITY OF OCEANSIDE GUIDELINES.

WATER CONSERVATION STATEMENT:

IN RECOGNITION OF WATER AS A LIMITED RESOURCE IN SOUTHERN CALIFORNIA, THE FOLLOWING MEASURES WILL BE UNDERTAKEN TO REDUCE THIS PROJECT'S DEMAND ON THE CITY'S AVAILABLE WATER SUPPLY: THE IRRIGATION SYSTEM WILL BE AUTOMATIC AND WILL INCORPORATE LOW VOLUME SPRAY EMITTERS AND CONVENTIONAL LOW ANGLE SPRAY HEADS. DRIP IRRIGATION SYSTEMS MAY BE EMPLOYED WHERE CONSIDERED TO BE EFFECTIVE AND FEASIBLE. IRRIGATION VALVES SHALL BE SEGREGATED TO ALLOW FOR THE SYSTEM OPERATION IN RESPONSE TO ORIENTATION AND EX



PLANT SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME
TREES	
(Symbol)	PERIMETER TREE 50% - 24" BOX & 50% - 36" BOX
(Symbol)	PLATANUS RACEMOSA / MEXICAN SYCAMORE PLATANUS RACEMOSA / CALIFORNIA SYCAMORE POPULUS FREMONTII / FREMONT COTTONWOOD QUERCUS AGRIFOLIA / COAST LIVE OAK QUERCUS ENGELMANNII / ENGELMANN OAK SALIX LASIOLEPIS / ARROYO WILLOW UMBELLULARIA CALIFORNICA / BAY LAUREL
(Symbol)	ACCENT/SHADE TREE 50% - 24" BOX & 50% - 36" BOX
(Symbol)	ALBIZIA JULIBRISSIN / SILK TREE CHILOPSIS LINEARIS / DESERT WILLOW CHITALPA TASHKENTENSIS / CHITALPA JACARANDA MIMOSIFOLIA / JACARANDA MULTI-TRUNK PROSOPIS CHILENSIS / CHILEAN MESQUITE QUERCUS AGRIFOLIA / COAST LIVE OAK MULTI-TRUNK TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE ULMUS PARVIFOLIA / LACEBARK ELM UMBELLULARIA CALIFORNICA / CALIFORNIA LAUREL
(Symbol)	VERTICAL ACCENT TREE/SCREEN TREE 24" BOX
(Symbol)	ACACIA SALICINA / WILLOW ACACIA ACACIA STENOPHYLLA / SHOESTRING ACACIA CEDRUS DEODARA / DEODAR CEDAR CUPRESSUS SEMPERVIRENS / ITALIAN CYPRESS HYMENOSPORUM FLAVUM / SWEETSHADE LYONOTHAMNUS FLORIBUNDUS / CATALINA IRONWOOD PINUS ELДАРICA / AFGHAN PINE PINUS TORREYANA / TORREY PINE TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE TRISTANIA CONFERTA / BRISBANE BOX SMALL ACCENT TREES/VERTICAL ACCENT PALM 15 GAL. - 24" BOX
(Symbol)	ARBUS X / ARBUS MULTI-TRUNK ARCTOSTAPHYLOS X / MANZANITA CERCIS CANADENSIS 'FOREST PANSY' TM / FOREST PANSY REDBUD MULTI-TRUNK CERCIS OCCIDENTALIS / WESTERN REDBUD MULTI-TRUNK CORDYLIN X / CORDYLIN
(Symbol)	CYATHEA COOPERI / AUSTRALIAN TREE FERN DRACAENA ALETRIFORMIS / DRAGON TREE GINKGO BILOBA / MAIDENHAIR TREE LAGERSTROEMIA INDICA / CRAPE MYRTLE MELALEUCA ARMILLARIS / DROOPING MELALEUCA OLEA EUROPAEA 'SWAN HILL' TM / SWAN HILL OLIVE
(Symbol)	SPECIMEN TREE 48"-60" BOX
(Symbol)	QUERCUS ENGELMANNII / ENGELMANN OAK
SHRUB AREAS	
(Symbol)	SLOPE PLANTING (1 GAL.-5GAL.) ACHILLEA MILLEFOLIUM / COMMON YARROW ALOE SPP. / ALOE BACCHARIS PILULARIS / COYOTE BRUSH BOUTELLOUA GRACILIS / BLUE GRAMA GRASS BACCHARIS SAROTHOIDES / DESERTBROOM BACCHARIS BOUTELOUA GRACILIS / BLUE GRAMA GRASS BROMELIA BALANSAE / HEART OF FLAME BROMELIAD BUXUS SPP. / BOXWOOD CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS CHLOROPHYTUM COMOSUM / SPIDER PLANT CUPHEA IGNEA / CIGAR PLANT DASYLIRION LONGISSIMUM / TOOTHLESS DESERT SPOON DIANELLA SP. / FLAX LILY DODONAEA VISCOZA / HOPSEED BUSH FURCRAEA FOETIDA 'MEDIOPICTA' / MAURITIUS HEMP HESPERALOE PARVIFLORA / RED YUCCA HEUCHERA SP. / CORAL BELLS LANTANA SP. / CHAPEL HILL YELLOW LANTANA LAVANDULA SP. / LAVENDER

EXISTING TREE SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME
EC3	CALLISTEMON CITRINUS LEMON BOTTLEBRUSH
EG	CASSIA LEPTOPHYLLA GOLD MEDALLION TREE
EE	EUCALYPTUS CAMALDULENSIS RIVER RED GUM
EM2	FRAXINUS VELUTINA 'MODESTO' MODESTO VELVET ASH
EJ	JUNIPERUS CHINENSIS 'KAIZUKA' HOLLYWOOD JUNIPER
EL	LAGERSTROEMIA INDICA CRAPE MYRTLE
EQ	MELALEUCA QUINQUENERVIA CAJEPUT TREE
EB	PINUS THUNBERGII JAPANESE BLACK PINE
EC	POPULUS NIGRA COTTONWOOD
EP	SCHINUS MOLLE CALIFORNIA PEPPER
ES	SCHINUS TEREBINTHIFOLIA BRAZILIAN PEPPER TREE
ET	TIPUANA TIPU TIPU TREE

REFERENCE NOTES SCHEDULE

SYMBOL	DESCRIPTION
1	PROPOSED FLOOD WALL OR FENCE PER CIVIL
2	PROPOSED PCC SIDEWALK PER CIVIL
3	PROPOSED TRASH ENCLOSURE PER ARCH.
4	PROPOSED OUTDOOR SEATING AREA - STABILIZED DECOMPOSED GRANITE
5	PROPOSED CONCRETE RAMP PER CIVIL
6	BIKE RACK - PER ARCHITECT

SHRUB AREAS

SYMBOL	BOTANICAL / COMMON NAME
(Symbol)	LEPTOSPERMUM SCOPARIUM / NEW ZEALAND TEA TREE LIGUSTRUM JAPONICUM / JAPANESE PRIVET LOMANDRA LONGIFOLIA / MAT RUSH LONICERA JAPONICA / JAPANESE HONEYSUCKLE LOROPETALUM CHINENSE / CHINESE FRINGE FLOWER MIMULUS SPP. / MONKEYFLOWER MYRICA CALIFORNICA / PACIFIC WAX MYRTLE PACHYCREUS MARGINATUS / TOTEM POLE CACTUS PHILODENDRON XANADU / XANADU PHILODENDRON PHORMIUM TENAX / NEW ZEALAND FLAX PITTOSPORUM SPP. / PITTOSPORUM POLYSTICHUM MUNITUM / WESTERN SWORD FERN ROSA CALIFORNICA / CALIFORNIA WILD ROSE ROSMARINUS OFFICINALIS / ROSEMARY RUSSELLIA EQUISETIFORMIS / FIRECRACKER PLANT SALVIA SP. / SALVIA SPECIES
(Symbol)	PARKING LOT PLANTING (1 GAL.-5GAL.) TRACHELOSPERMUM JASMINOIDES / CHINESE STAR JASMINE CARISSA MACROCARPA / NATAL PLUM DIETES GRANDIFLORA / FORTNIGHT LILY LIGUSTRUM JAPONICUM 'TEXANUM' / TEXAS JAPANESE PRIVET PHORMIUM TENAX / NEW ZEALAND FLAX SENECIO SERPENS / BLUE CHALKSTICKS VERBENA PERUVIANA / PERUVIAN VERBENA

SHRUB AREAS

SYMBOL	BOTANICAL / COMMON NAME
(Symbol)	MIMULUS AURANTIACUS / STICKY MONKEYFLOWER MYRICA CALIFORNICA / PACIFIC WAX MYRTLE RHAMNUS CALIFORNICA / CALIFORNIA COFFEEBERRY ROSA CALIFORNICA / CALIFORNIA WILD ROSE SALVIA SPP. / SALVIA SPECIES BUILDING FOUNDATION PLANTING 50% 1 GAL. & 50% 5 GAL. ACHILLEA MILLEFOLIUM / COMMON YARROW AGAVE AMERICANA / CENTURY PLANT AGAVE ATTENUATA / AGAVE AGAVE DESMETTIANA / DWARF CENTURY PLANT ALOE SPP. / ALOE ASPARAGUS DENSIFLORUS / ASPARAGUS FERN BACCHARIS SAROTHOIDES / DESERTBROOM BACCHARIS BOUTELLOUA GRACILIS / BLUE GRAMA GRASS BROMELIA BALANSAE / HEART OF FLAME BROMELIAD BUXUS SPP. / BOXWOOD CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS CHLOROPHYTUM COMOSUM / SPIDER PLANT CUPHEA IGNEA / CIGAR PLANT DASYLIRION LONGISSIMUM / TOOTHLESS DESERT SPOON DIANELLA SP. / FLAX LILY DODONAEA VISCOZA / HOPSEED BUSH FURCRAEA FOETIDA 'MEDIOPICTA' / MAURITIUS HEMP HESPERALOE PARVIFLORA / RED YUCCA HEUCHERA SP. / CORAL BELLS LANTANA SP. / CHAPEL HILL YELLOW LANTANA LAVANDULA SP. / LAVENDER

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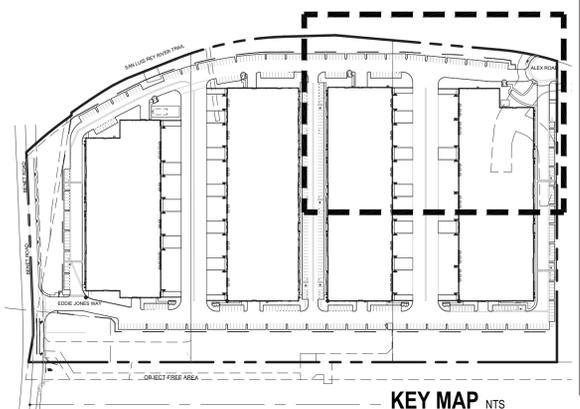
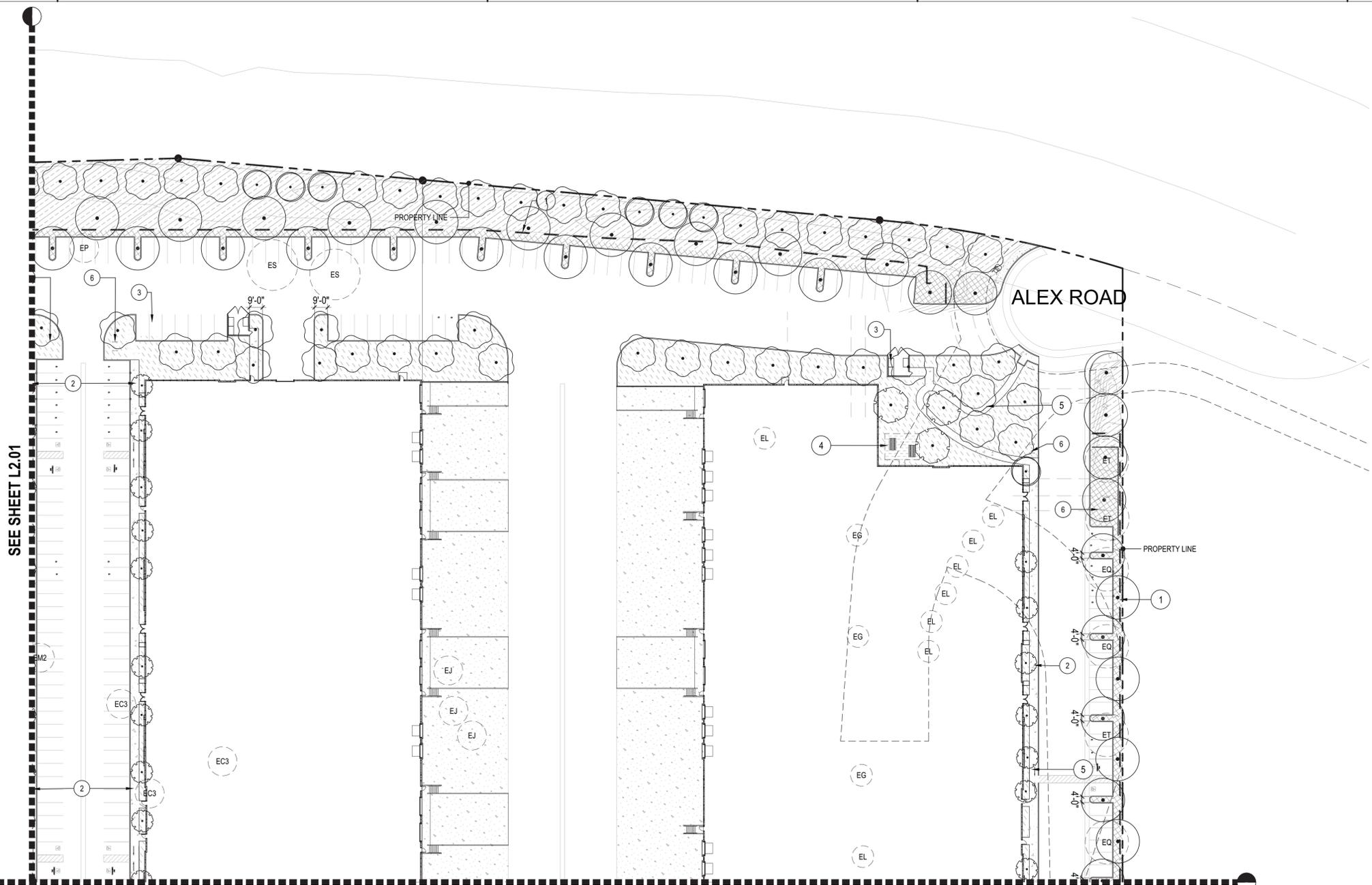
**CONCEPTUAL
LANDSCAPE PLAN**

**EDDIE JONES INDUSTRIAL
PROJECT - 4 BLDG ALT
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058**

DATE	REVISIONS

PA/PM:	D. McCullough
DRAWN BY:	MB/KB
JOB NO.:	22174

SHEET
L2.01



PLANT SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME
TREES	
(Symbol: Circle with dot)	PERIMETER TREE 50% - 24" BOX & 50% - 36" BOX
(Symbol: Circle with dot)	PLATANUS MEXICANA / MEXICAN SYCAMORE PLATANUS RACEMOSA / CALIFORNIA SYCAMORE POPULUS FREMONTII / FREMONT COTTONWOOD QUERCUS AGRIFOLIA / COAST LIVE OAK QUERCUS ENGELMANNII / ENGELMANN OAK SALIX LASIOLEPIS / ARROYO WILLOW UMBELLULARIA CALIFORNICA / BAY LAUREL
(Symbol: Circle with dot)	ACCENT/SHADE TREE 50% - 24" BOX & 50% - 36" BOX
(Symbol: Circle with dot)	ALBIZIA JULIBRISSIN / SILK TREE CHILOPSIS LINEARIS / DESERT WILLOW CHITALPA TASHKENTENSIS / CHITALPA JACARANDA MIMOSIFOLIA / JACARANDA MULTI-TRUNK PROSOPIS CHILENSIS / CHILEAN MESQUITE QUERCUS AGRIFOLIA / COAST LIVE OAK MULTI-TRUNK TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE ULMUS PARVIFOLIA / LACEBARK ELM UMBELLULARIA CALIFORNICA / CALIFORNIA LAUREL
(Symbol: Circle with dot)	VERTICAL ACCENT TREE/SCREEN TREE 24" BOX
(Symbol: Circle with dot)	ACACIA SALICINA / WILLOW ACACIA ACACIA STENOPHYLLA / SHOESTRING ACACIA CEDRUS DEODARA / DEODAR CEDAR CUPRESSUS SEMPERVIRENS / ITALIAN CYPRESS HYMENOSPORUM FLAVUM / SWEETSHADE LYONOTHAMNUS FLORIBUNDUS / CATALINA IRONWOOD PINUS ELДАРICA / AFGHAN PINE PINUS TORREYANA / TORREY PINE TABEBUIA IMPETIGINOSA / PINK TRUMPET TREE TRISTANIA CONFERTA / BRISBANE BOX SMALL ACCENT TREES/VERTICAL ACCENT PALM 15 GAL. - 24" BOX
(Symbol: Circle with dot)	ARBUTUS X / ARBUTUS MULTI-TRUNK ARCTOSTAPHYLOS X / MANZANITA CERCIS CANADENSIS 'FOREST PANSY' TM / FOREST PANSY REDBUD MULTI-TRUNK CERCIS OCCIDENTALIS / WESTERN REDBUD MULTI-TRUNK CORDYLINA X / CORDYLINA CYATHEA COOPERI / AUSTRALIAN TREE FERN DRACAENA ALETRIFORMIS / DRAGON TREE GINKGO BILOBA / MAIDENHAIR TREE LAGERSTROEMIA INDICA / CRAPE MYRTLE MELALEUCA ARMILLARIS / DROOPING MELALEUCA OLEA EUROPAEA 'SWAN HILL' TM / SWAN HILL OLIVE
(Symbol: Circle with dot)	SPECIMEN TREE 48"-60" BOX
(Symbol: Circle with dot)	QUERCUS ENGELMANNII / ENGELMANN OAK
SHRUB AREAS	
(Symbol: Hatched box)	SLOPE PLANTING (1 GAL.-5GAL.) ACHILLEA MILLEFOLIUM / COMMON YARROW ALOE SPP. / ALOE BACCHARIS PILULARIS / COYOTE BRUSH BOUTELLOUA GRACILIS / BLUE GRAMA GRASS CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / KARL FOERSTER FEATHER REED CAREX MORROWII / JAPANESE SEDGE CARISSA MACROCARPA / NATAL PLUM CEANOTHUS CYANEUS / SAN DIEGO CEANOTHUS CHONDRPETALUM ELEPHANTINUM / LARGE CAPE RUSH CISTUS X SKANBERGII / CORAL ROCKROSE EPHORBIA CANUM / CALIFORNIA FUCHSIA ERIOGONUM FASCICULATUM / CALIFORNIA BUCKWHEAT FESTUCA IDAHOENSIS / IDAHO FESCUE GERANIUM SPP. / GERANIUM HETEROMELES ARBUTIFOLIA / TOYON LEYMUS CONDENSATUS 'CANYON PRINCE' / CANYON PRINCE GIANT WILD RYE

SEE SHEET L2.01

SEE SHEET L2.04

EXISTING TREE SCHEDULE

SYMBOL	BOTANICAL / COMMON NAME
EC3	CALLISTEMON CITRINUS LEMON BOTTLEBRUSH
EG	CASSIA LEPTOPHYLLA GOLD MEDALLION TREE
EE	EUCALYPTUS CAMALDULENSIS RIVER RED GUM
EM2	FRAXINUS VELUTINA 'MODESTO' MODESTO VELVET ASH
EJ	JUNIPERUS CHINENSIS 'KAIZUKA' HOLLYWOOD JUNIPER
EL	LAGERSTROEMIA INDICA CRAPE MYRTLE
EQ	MELALEUCA QUINQUENERVIA CAJEPUT TREE
EB	PINUS THUNBERGII JAPANESE BLACK PINE
EC	POPULUS NIGRA COTTONWOOD
EP	SCHINUS MOLLE CALIFORNIA PEPPER
ES	SCHINUS TEREBINTHIFOLIA BRAZILIAN PEPPER TREE
ET	TIPUANA TIPU TIPU TREE

REFERENCE NOTES SCHEDULE

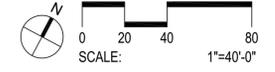
SYMBOL	DESCRIPTION
1	PROPOSED FLOOD WALL OR FENCE PER CIVIL
2	PROPOSED PCC SIDEWALK PER CIVIL
3	PROPOSED TRASH ENCLOSURE PER ARCH.
4	PROPOSED OUTDOOR SEATING AREA - STABILIZED DECOMPOSED GRANITE
5	PROPOSED CONCRETE RAMP PER CIVIL
6	BIKE RACK - PER ARCHITECT

SHRUB AREAS

SYMBOL	BOTANICAL / COMMON NAME
(Symbol: Dotted box)	LEPTOSPERMUM SCOPARIUM / NEW ZEALAND TEA TREE LIGUSTRUM JAPONICUM / JAPANESE PRIVET LOMANDRA LONGIFOLIA / MAT RUSH LONICERA JAPONICA / JAPANESE HONEYSUCKLE LOROPETALUM CHINENSE / CHINESE FRINGE FLOWER MIMULUS SPP. / MONKEYFLOWER MYRICA CALIFORNICA / PACIFIC WAX MYRTLE PACHYCEERUS MARGINATUS / TOTEM POLE CACTUS PHILODENDRON XANADU / XANADU PHILODENDRON PHORMIUM TENAX / NEW ZEALAND FLAX PITTIOSPORUM SPP. / PITTIOSPORUM POLYSTICHUM MUNITUM / WESTERN SWORD FERN ROSA CALIFORNICA / CALIFORNIA WILD ROSE ROSMARINUS OFFICINALIS / ROSEMARY RUSSELLIA EQUISETIFORMIS / FIRECRACKER PLANT SALVIA SP. / SALVIA SPECIES
(Symbol: Horizontal hatched box)	PARKING LOT PLANTING (1 GAL.-5GAL.) TRACHELOSPERMUM JASMINOIDES / CHINESE STAR JASMINE CARISSA MACROCARPA / NATAL PLUM DIETES GRANDIFLORA / FORTNIGHT LILY LIGUSTRUM JAPONICUM 'TEXANUM' / TEXAS JAPANESE PRIVET PHORMIUM TENAX / NEW ZEALAND FLAX SENECIO SERPENS / BLUE CHALKSTICKS VERBENA PERUVIANA / PERUVIAN VERBENA

SHRUB AREAS

SYMBOL	BOTANICAL / COMMON NAME
(Symbol: Vertical hatched box)	MIMULUS AURANTIACUS / STICKY MONKEYFLOWER MYRICA CALIFORNICA / PACIFIC WAX MYRTLE RHAMNUS CALIFORNICA / CALIFORNIA COFFEEBERRY ROSA CALIFORNICA / CALIFORNIA WILD ROSE SALVIA SPP. / SALVIA SPECIES BUILDING FOUNDATION PLANTING 50% 1 GAL. & 50% 5 GAL. ACHILLEA MILLEFOLIUM / COMMON YARROW AGAVE AMERICANA / CENTURY PLANT AGAVE ATTENUATA / AGAVE AGAVE DESMETTIANA / DWARF CENTURY PLANT ALOE SPP. / ALOE ASPARAGUS DENSIFLORUS / ASPARAGUS FERN BACCHARIS SAROTHOIDES / DESERTBROOM BACCHARIS BOUTELLOUA GRACILIS / BLUE GRAMA GRASS BROMELIA BALANSAE / HEART OF FLAME BROMELIAD BUXUS SPP. / BOXWOOD CALAMAGROSTIS X ACUTIFLORA 'KARL FOERSTER' / FEATHER REED GRASS CHLOROPHYTUM COMOSUM / SPIDER PLANT CUPHEA IGNEA / CIGAR PLANT DASYLIRION LONGISSIMUM / TOOTHLESS DESERT SPOON DIANELLA SP. / FLAX LILY DODONAEA VISCOZA / HOPSEED BUSH FURCRAEA FOETIDA 'MEDIOPICTA' / MAURITIUS HEMP HESPERALOE PARVIFLORA / RED YUCCA HEUCHERA SP. / CORAL BELLS LANTANA SP. / CHAPEL HILL YELLOW LANTANA LAVANDULA SP. / LAVENDER



McCulloughLA.com
703 16th Street
Suite 500
San Diego, California 92101.100
619.296.3150

McCullough
landscape architecture
environmental planning
urban design

CONCEPTUAL
LANDSCAPE PLAN

EDDIE JONES INDUSTRIAL
PROJECT - 4 BLDG ALT
260 EDDIE JONES WAY
OCEANSIDE, CALIFORNIA 92058

DATE	REMARKS

PA/PM:	D. McCullough
DRAWN BY:	MB/KB
JOB NO.:	22174

SHEET
L2.03

LEGEND

SUBJECT PROPERTY BOUNDARY	---
RIGHT-OF-WAY	---
CENTERLINE OF ROAD	---
EXISTING EASEMENT	---
EXISTING INTERIOR LEGAL LOT	---
ADJACENT LOT LINE	---

D22-00001 / CUP22-00001

DEVELOPMENT PLAN / CONDITIONAL USE PERMIT

EDDIE JONES INDUSTRIAL - 250 EDDIE JONES WAY

OWNER INFORMATION

WE HEREBY CERTIFY THAT WE ARE THE RECORDED OWNERS OF THE PROPERTY SHOWN ON THE ATTACHED TENTATIVE PARCEL MAP AND THAT SAID MAP SHOWS THE ENTIRE CONTIGUOUS OWNERSHIP. I UNDERSTAND THAT PROPERTY IS CONSIDERED CONTIGUOUS EVEN IF IT IS SEPARATED BY ROADS, STREETS, UTILITY EASEMENTS, OR RAILROAD RIGHTS OF WAY.

OWNER: TYCO ELECTRONICS CORPORATION, A PENNSYLVANIA CORPORATION
400 SOUTH HOPE STREET
LOS ANGELES, CA 90071

DEVELOPER INFORMATION

WE HEREBY CERTIFY THAT WE ARE THE RECORDED OWNERS OF THE PROPERTY SHOWN ON THE ATTACHED TENTATIVE PARCEL MAP AND THAT SAID MAP SHOWS THE ENTIRE CONTIGUOUS OWNERSHIP. I UNDERSTAND THAT PROPERTY IS CONSIDERED CONTIGUOUS EVEN IF IT IS SEPARATED BY ROADS, STREETS, UTILITY EASEMENTS, OR RAILROAD RIGHTS OF WAY.

DEVELOPER: ADAM ROBINSON FOR: RAF PACIFICA, LLC
315 S. COAST HWY 101, SUITE U-12
ENCINITAS, CA 92024
PH: (858) 314-3116

LEGAL DESCRIPTION

PARCEL 4A: (APN: 145-021-32-00)
PARCEL 4B: (APN: 145-021-29-00)
PARCEL 4C: (APN: 145-021-30-00)

**SEE SHEET 2 FOR FULL LEGAL DESCRIPTION OF UNDERLYING LEGAL PARCELS

SITE ADDRESS

250 EDDIE JONES WAY
OCEANSIDE, CA 92058
APN: 145-021-29, -30, & -32-00

TOPOGRAPHY

TOPOGRAPHY OBTAINED BY AERIAL MAPPING METHODS FLOWN ON SEPTEMBER 27, 2021

PREPARED BY: PASCO, LARET, SUITER & ASSOCIATES
119 ABERDEEN DRIVE
CARDIFF-BY-THE-SEA, CA 92007

BASIS OF BEARINGS

THE BASIS OF BEARINGS FOR THIS SURVEY IS THE CALIFORNIA COORDINATE SYSTEM, NAD 83 (CCS83) EPOCH 2011, ZONE 8, AS DETERMINED LOCALLY BY A LINE BETWEEN FIRST ORDER CONTROL STATIONS 1015 AND 1018 BEING A GRID BEARING OF N 38°47'28" E AS DERIVED FROM GEODETIC VALUES SHOWN ON RECORD OF SURVEY 21787, CITY OF OCEANSIDE SURVEY CONTROL, FILED ON AUGUST 21, 2014 AS FILE NUMBER 2014-0361129 IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY.

BENCHMARK

ELEVATIONS SHOWN HEREON ARE BASED ON 3.5" DISK, "LS 5282", FOUND IN STANDARD M10 MONUMENT AT THE INTERSECTION OF WALA DR. AND TOWKISH DR., AS SHOWN ON ROS 21787 AS PT NO. 1018.

EL = 54.70'

VERTICAL DATUM = NAVD 88

EARTHWORK / PROJECT GRADING

CUT:	60,000 CY	MAX CUT HEIGHT:	11.5 FT
FILL:	40,000 CY	MAX FILL HEIGHT:	6 FT
EXPORT:	20,000 CY		
REMEDIAL:	___ CY		

*ESTIMATE DOES NOT INCLUDE STRIPPINGS OR UTILITY TRENCH VOLUMES, IF REQUIRED BY SITE CONDITIONS

CONTRACTOR SHALL SATISFY SELF THAT ESTIMATES ARE CORRECT PRIOR TO COMMENCEMENT OF WORK.

EARTHWORK QUANTITIES ARE ESTIMATED FOR PERMIT PURPOSES ONLY. (CALCULATED ON A THEORETICAL BASIS. ACTUAL QUANTITIES MAY VARY DUE TO SHRINKAGE OR SWELL FACTORS).

DEPTH & QUANTITY OF REMEDIAL GRADING IS SUBJECT TO FIELD VERIFICATION BY PROJECT SOILS ENGINEER IN FIELD DURING EXCAVATION.

ENGINEER OF WORK

Tyler Lawson
TYLER LAWSON, PE #80356



PASCO LARET SUITER & ASSOCIATES
San Diego | Solana Beach | Orange County
Phone 858.259.8212 | www.plsaengineering.com

EXISTING EASEMENT INFORMATION

REFER TO SHEET 2 FOR PLOTTING OF EXISTING EASEMENTS

*EXISTING EASEMENTS SHOWN IN ACCORDANCE WITH PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY, ORDER NO. 00126233-987-0C1-K27, DATED EFFECTIVELY NOVEMBER 2, 2021

PROPOSED EASEMENT INFORMATION

NO PROPOSED EASEMENTS

UTILITIES

WATER	OCEANSIDE WATER DEPARTMENT
FIRE	OCEANSIDE FIRE DEPARTMENT
SEWER	OCEANSIDE WASTEWATER DIVISION
ELEMENTARY SCHOOL	MISSION ELEMENTARY SCHOOL DISTRICT
HIGH SCHOOL	OCEANSIDE HIGH SCHOOL DISTRICT

SETBACKS

FRONT YARD	(FYSB)	10' PER IL
REAR YARD	(RYSB)	0' PER IL
SIDE YARD SETBACK	(SYSB)	0' PER IL
INTERIOR SIDE YARD	(SYSB)	10' PER IL

AREA CALCULATIONS

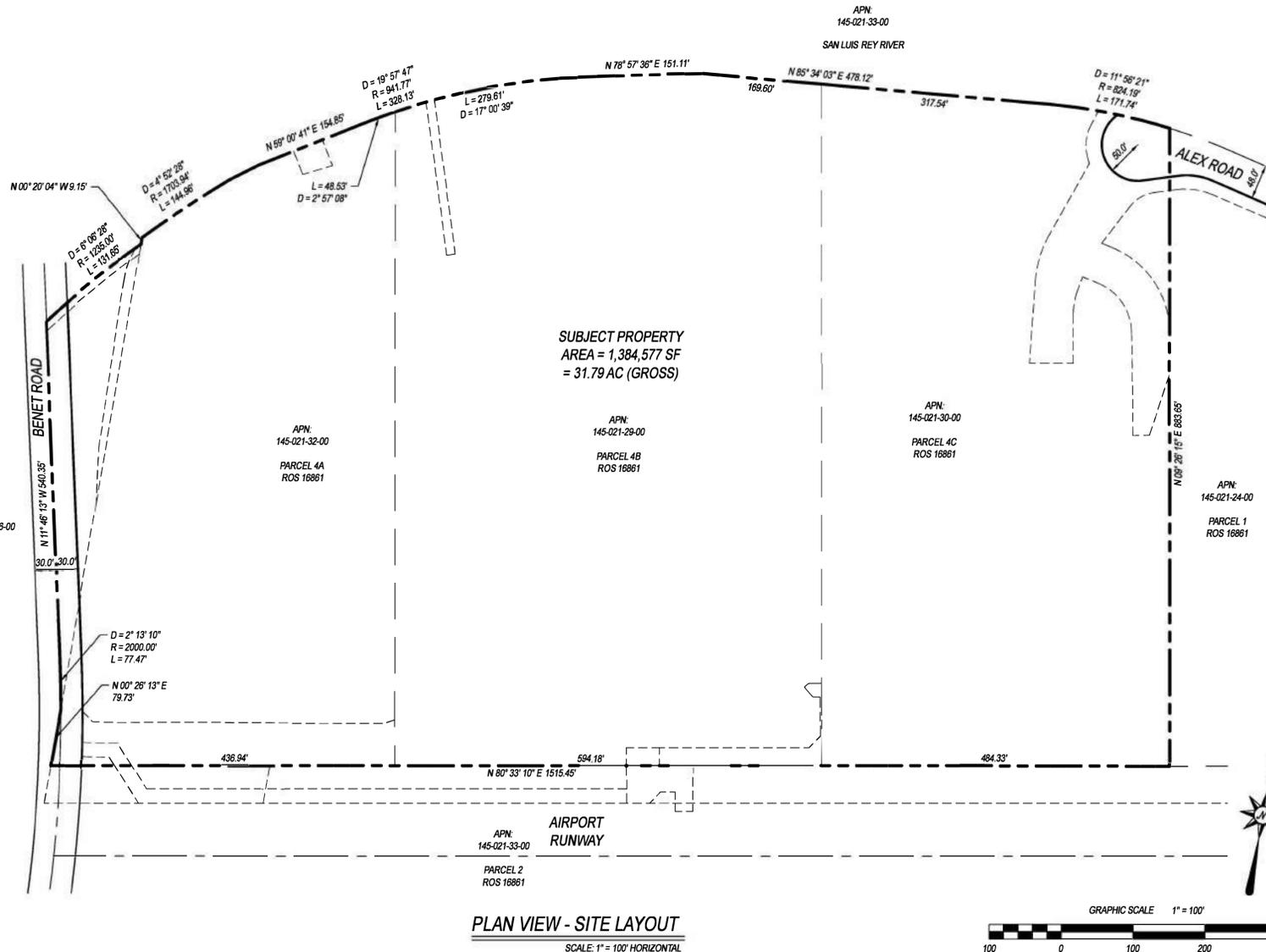
EXISTING IMPERVIOUS AREA (ONSITE):	591,152 SF
PROPOSED IMPERVIOUS AREA (ONSITE):	1,034,986 SF
INCREASE IMPERVIOUS AREA (ONSITE):	443,834 SF

ZONING INFORMATION

GENERAL PLAN DESIGNATION:	IL	EXISTING USE:	INDUSTRIAL
PRESENT ZONING REQUIREMENTS:	IL	PROPOSED USE:	INDUSTRIAL
HEIGHT:	80' FOR IL	TOTAL UNITS:	1
MAXIMUM LOT COVERAGE (PER ZONE):	75% FOR IL		

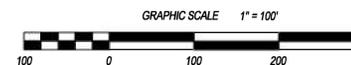
FEMA INFORMATION

FEMA ZONING DESIGNATION:	A99
FEMA FIRM PANEL NUMBER:	08073C0751H
DATE:	12/20/2019
BASE FLOOD ELEVATION:	BFE = 34.0
DATUM:	NAVD88



PLAN VIEW - SITE LAYOUT

SCALE: 1" = 100' HORIZONTAL



SITE AREA CALCULATIONS

TOTAL GROSS SITE AREA:	1,384,577 SF (31.79 AC)	EXISTING IMPERVIOUS AREA:	591,152 SF (13.58 AC)
AREA DISTURBED BY PROJECT:	1,316,779 SF (30.23 AC)	EXISTING PERVIOUS AREA:	793,405 SF (18.21 AC)
PROPOSED IMPERVIOUS AREA:	1,034,986 SF / 23.76 AC (75% OF SITE)		
PROPOSED PERVIOUS AREA:	281,793 SF / 6.47 AC (20% OF SITE)		
UNDISTURBED PERVIOUS AREA:	67,798 SF / 1.56 AC (5% OF SITE)		
*ADDTL. TREE CANOPY:	196,025 SF / 4.50 AC (14% OF SITE)		

*PER URBAN FORESTRY PROGRAM (ARTICLE 30, SECTION 3049), ADDITIONAL TREE CANOPY PERCENTAGE USED TO BE INCLUDED WITH TOTAL SITE PERMEABLE AREA AT A RATIO OF 1:1 TO MEET 22% PERMEABLE SURFACE REQUIREMENT; SEE LANDSCAPE SHEETS

SCOPE OF WORK

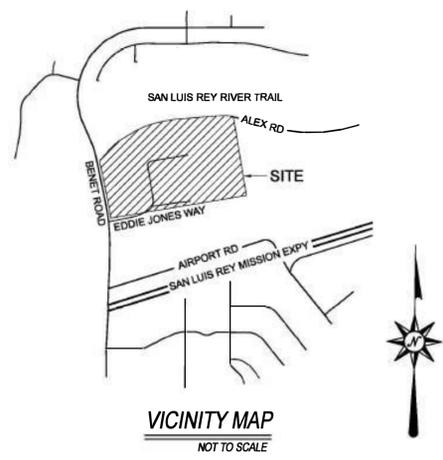
THE PROJECT PROPOSES TO SEEK APPROVAL TO DEMOLISH ALL EXISTING ONSITE STRUCTURES AND CONSTRUCT FOUR INDUSTRIAL BUILDINGS, TOTALING APPROXIMATELY 492,000 SF, AND ASSOCIATED IMPROVEMENTS. A FLOOD WALL IS PROPOSED AROUND THE PERIMETER OF THE SITE WRAPPING THE PROPOSED PARKING AREA TO FLOOD PROOF THE PROPERTY. EARTHWORK QUANTITIES GENERATED BY THE PROPOSED PROJECT ARE ANTICIPATED TO EXCEED 4-FT OF FILL, 8-FT OF CUT. PROJECT SEEKS APPROVAL OF A DEVELOPMENT PLAN PURSUANT TO ARTICLE 43 OF THE ZONING ORDINANCE AND A CONDITIONAL USE PERMIT PURSUANT TO ARTICLE 41 OF THE ZONING ORDINANCE.

ACCESS

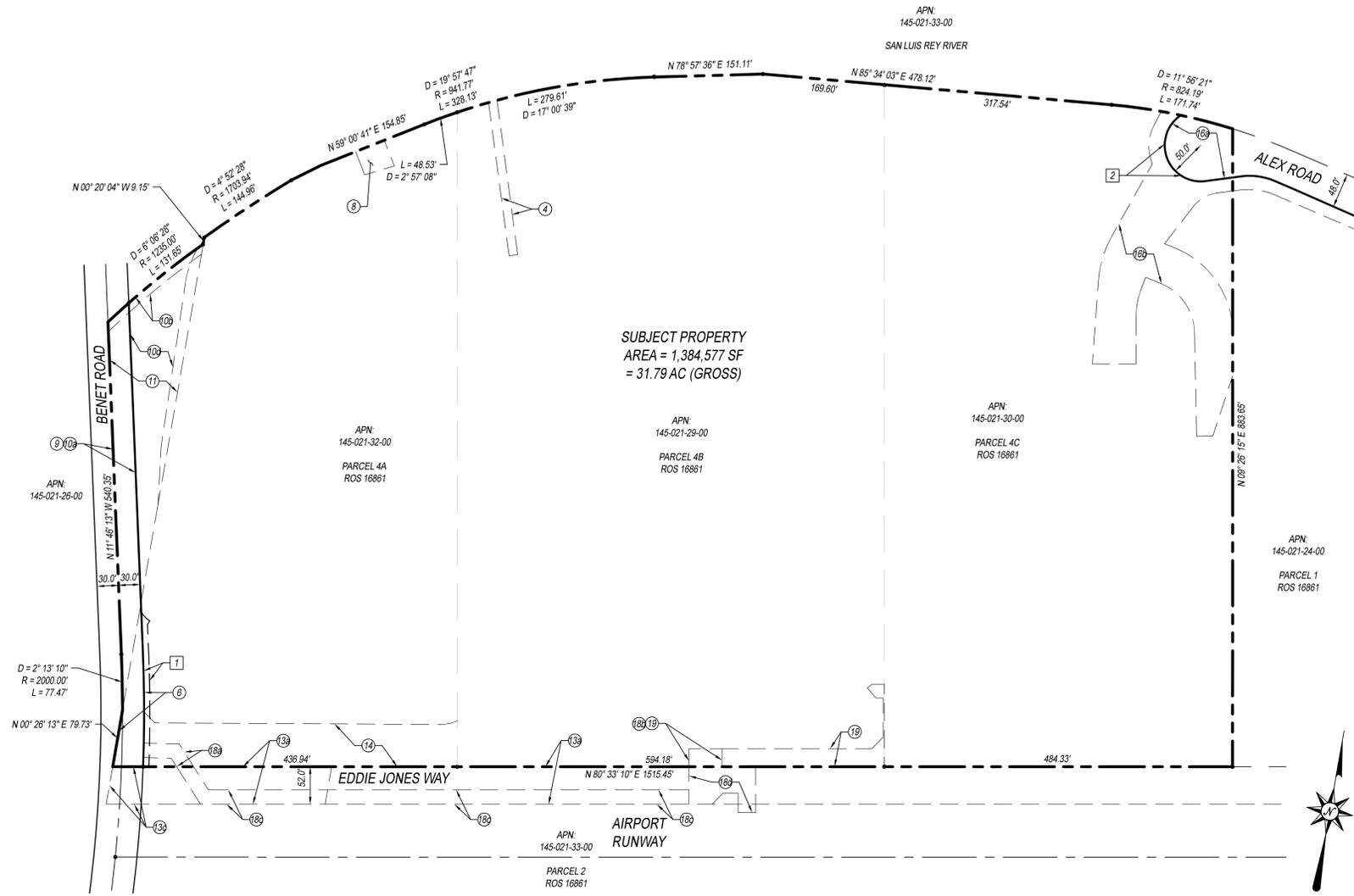
VEHICULAR ACCESS FROM BENET ROAD AND ALEX ROAD, PUBLIC ROADS

SHEET INDEX

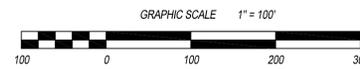
- SHEET 1 - CIVIL TITLE SHEET
- SHEET 2 - EXISTING EASEMENTS AND LOT CONFIGURATION
- SHEET 3 - PRELIMINARY GRADING PLAN
- SHEET 4 - PRELIMINARY GRADING PLAN
- SHEET 5 - PRELIMINARY GRADING PLAN
- SHEET 6 - PRELIMINARY GRADING PLAN
- SHEET 7 - PRELIMINARY UTILITY PLAN
- SHEET 8 - SECTIONS AND DETAILS
- SHEET 9 - SECTIONS AND DETAILS
- SHEET 10 - SECTIONS AND DETAILS
- SHEET 11 - TRUCK / VEHICLE TURNING EXHIBIT
- SHEET 12 - SITE WALL EXHIBIT



VICINITY MAP
NOT TO SCALE



PLAN VIEW - SITE LAYOUT
SCALE: 1" = 100' HORIZONTAL



FULL LEGAL DESCRIPTION

PARCEL 1 (APN: 145-021-32-00) THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHWEST QUARTER, TOGETHER WITH A PORTION OF THE NORTH HALF OF THE SOUTHWEST QUARTER DESCRIBED AS PARCEL 1 IN A GRANT DEED RECORDED DECEMBER 28, 1995 AS INSTRUMENT NO. 1995-0592152 OF OFFICIAL RECORDS, ALL WITHIN SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4A OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS INSTRUMENT NO. 1999-0796385 OF OFFICIAL RECORDS, DESCRIBED AS A WHOLE AS FOLLOWS: BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHWEST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF SURVEY NO. 13494, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 478.25 FEET; THENCE NORTH 09°26'35" WEST 906.49 FEET TO THE SOUTHERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS INSTRUMENT NO. 1994-0604672 OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 941.77 FEET TO WHICH A RADIAL LINE BEARS NORTH 28°02'18" WEST; THENCE SOUTHWESTERLY 45.73 FEET ALONG SAID CURVE AND SOUTHERLY LINE THROUGH A CENTRAL ANGLE OF 02°57'51"; THENCE SOUTH 58°50'51" WEST 155.19 FEET; THENCE SOUTH 54°01'29" WEST 48.25 FEET TO THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 1703.94 FEET TO WHICH A RADIAL LINE BEARS NORTH 40°18'45" WEST; THENCE SOUTHWESTERLY 144.55 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 04°51'38" TO SAID WEST LINE OF THE SOUTHWEST QUARTER OF SECTION 13; THENCE SOUTH 00°23'25" WEST 9.15 FEET ALONG SAID WEST LINE TO THE MOST NORTHERLY CORNER OF SAID PARCEL 1 DESCRIBED IN DOCUMENT RECORDED DECEMBER 28, 1995 AS INSTRUMENT NO. 1995-0592152 OF OFFICIAL RECORDS; AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 1235.00 FEET TO WHICH A RADIAL LINE BEARS NORTH 44°43'52" WEST; THENCE LEAVING SAID WEST LINE AND FOLLOWING ALONG THE NORTHWESTERLY LINE OF SAID PARCEL 1, SOUTHWESTERLY 170.73 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 07°55'15" TO THE CENTERLINE OF THE BENET ROAD, 60 FEET WIDE, DESCRIBED AS PARCEL C IN RESOLUTION NO. R94-189 OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE RECORDED MAY 08, 1995 AS INSTRUMENT NO. 1995-0192658 OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE LEAVING SAID NORTHWESTERLY LINE OF PARCEL 1 AND FOLLOWING ALONG SAID CENTERLINE OF BENET ROAD, SOUTH 11°45'56" EAST 480.14 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE WESTERLY HAVING A RADIUS OF 200.00 FEET; THENCE SOUTHERLY 77.47 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 02°13'10"; THENCE SOUTH 00°23'25" WEST 79.75 FEET TO THE POINT OF BEGINNING.

PARCEL 2 (APN: 145-021-29-00) THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHWEST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4B OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS INSTRUMENT NO. 1999-0796385 OF OFFICIAL RECORDS, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHWEST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF SURVEY NO. 13494, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 479.25 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 80°33'25" EAST 594.02 FEET; THENCE NORTH 09°26'35" WEST 944.68 FEET TO THE SOUTHERLY LINE OF PARCEL 1.

1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS INSTRUMENT NO. 1994-0604672 OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE, FOLLOWING ALONG SAID SOUTHERLY LINE, SOUTH 85°34'12" WEST 169.80 FEET; THENCE SOUTH 78°57'38" WEST 161.05 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHERLY HAVING A RADIUS OF 841.77 FEET; THENCE WESTERLY 279.41 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 16°59'59" TO A POINT WHICH BEARS NORTH 09°26'35" WEST FROM THE TRUE POINT OF BEGINNING; THENCE LEAVING SAID SOUTHERLY LINE, SOUTH 09°26'35" EAST 906.49 FEET TO THE TRUE POINT OF BEGINNING.

PARCEL 3 (APN: 145-021-30-00) THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHWEST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4C OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS INSTRUMENT NO. 1999-0796385 OF OFFICIAL RECORDS, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHWEST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF SURVEY NO. 13494, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 1073.27 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 80°33'25" EAST 484.51 FEET; THENCE NORTH 09°26'35" WEST 883.65 FEET TO THE SOUTHERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS INSTRUMENT NO. 1994-0604672 OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHERLY HAVING A RADIUS OF 824.19 FEET TO WHICH A RADIAL LINE BEARS NORTH 07°43'15" EAST; THENCE WESTERLY 171.75 FEET ALONG SAID CURVE AND SOUTHERLY LINE THROUGH A CENTRAL ANGLE OF 11°56'24"; THENCE SOUTH 85°34'12" WEST 317.54 FEET TO A POINT WHICH BEARS NORTH 09°26'35" WEST FROM THE TRUE POINT OF BEGINNING; THENCE LEAVING SAID SOUTHERLY LINE, SOUTH 09°26'35" EAST 944.68 FEET TO THE TRUE POINT OF BEGINNING.

PARCEL 4: EXCLUSIVE EASEMENTS RESERVED IN GRANT DEED RECORDED OCTOBER 14, 1999 AS INSTRUMENT NO. 1999-0894332 OF OFFICIAL RECORDS, UPON AND SUBJECT TO ALL THE PROVISIONS CONTAINED THEREIN, OVER THE FOLLOWING DESCRIBED PROPERTY: THAT PORTION OF THE NORTH HALF OF THE SOUTHWEST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, DESCRIBED AS FOLLOWS: BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHWEST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF SURVEY NO. 13494, FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 2384.40 FEET TO THE WESTERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS INSTRUMENT NO. 1994-0604672 OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE SOUTH 02°37'51" WEST 53.18 FEET ALONG SAID WESTERLY LINE TO THE SOUTHERLY LINE OF THAT CERTAIN PARCEL OF LAND DESCRIBED IN A GRANT DEED RECORDED SEPTEMBER 01, 1966 AS INSTRUMENT NO. 142881 OF OFFICIAL RECORDS; THENCE SOUTH 80°33'25" WEST 2382.29 FEET ALONG SAID SOUTHERLY LINE AND THE WESTERLY PROLONGATION THEREOF, AS DESCRIBED IN A DEED TO THE CITY OF OCEANSIDE RECORDED JUNE 04, 1982 AS INSTRUMENT NO. 84349 OF OFFICIAL RECORDS, TO SAID WEST LINE OF THE SOUTHWEST QUARTER OF SECTION 13; THENCE NORTH 00°23'25" EAST 52.78 FEET TO THE POINT OF BEGINNING.

APN: 145-021-29-00 (AFFECTS PARCEL 4B)
145-021-30-00 (AFFECTS PARCEL 4C)
145-021-32-00 (AFFECTS PARCEL 4A)

LEGEND

SUBJECT PROPERTY BOUNDARY	—————
RIGHT-OF-WAY	—————
CENTERLINE OF ROAD	—————
EXISTING EASEMENT	—————
PROPOSED EASEMENT	—————
EXISTING INTERIOR LEGAL LOT	—————
ADJACENT LOT LINE	—————

EXISTING EASEMENT INFORMATION

- ITEMS I, II, III, IV, V, VI, VII, A, B, C, D, AND 1 ARE NOW MAPPING ITEMS AND THEREFORE ARE NOT SHOWN HEREON.
- EASEMENT FOR ROAD PURPOSES RECORDED DECEMBER 30, 1965 PER DOCUMENT NO 170314 OF OFFICIAL RECORDS. DOC ILLEGIBLE.
- INTENTIONALLY DELETED.
- SAN DIEGO GAS & ELECTRIC COMPANY HOLDER OF AN EASEMENT FOR PUBLIC UTILITIES, INGRESS AND EGRESS PURPOSES RECORDED FEBRUARY 14, 1969 PER DOCUMENT NO 27630 OF OFFICIAL RECORDS TO BE QUITCLAIMED.
- LEO VITELLO HOLDER OF AN EASEMENT FOR INGRESS AND EGRESS RECORDED JULY 31, 1981 PER DOCUMENT NO 81-24280 OF OFFICIAL RECORDS. THE LOCATION OF SAID EASEMENT CANNOT BE DETERMINED FROM RECORD INFORMATION.
- AN IRREVOCABLE OFFER TO DEDICATE AND EASEMENT FOR PUBLIC HIGHWAY PURPOSES RECORDED OCTOBER 4, 1994 PER DOCUMENT NO 1994-0586824 OF OFFICIAL RECORDS.
- CITY OF OCEANSIDE HOLDER OF AN EASEMENT FOR TEMPORARY CONSTRUCTION PURPOSES RECORDED OCTOBER 4, 1994 PER DOCUMENT NO 1994-0586824 OF OFFICIAL RECORD. EASEMENT HAS EXPIRED AND THEREFORE IS NOT SHOWN HEREON.
- CITY OF OCEANSIDE HOLDER OF AN EASEMENT FOR STORM DRAIN PURPOSES RECORDED OCTOBER 14, 1994 PER DOCUMENT NO 1994-0604672 OF OFFICIAL RECORDS TO REMAIN.
- CITY OF OCEANSIDE HOLDER OF AN EASEMENT FOR PUBLIC STREET AND APPURTENANT USE PURPOSES RECORDED MAY 8, 1995 PER DOCUMENT NO 1995-0192658 OF OFFICIAL RECORDS TO REMAIN.
- CITY OF OCEANSIDE HOLDER OF AN EASEMENT FOR PUBLIC ROAD, UTILITIES, DRAINAGE, STORM DRAIN, TEMPORARY CONSTRUCTION, CONSTRUCTION AND MAINTENANCE OF SLOPES, AND PUBLIC INGRESS AND EGRESS PURPOSES RECORDED DECEMBER 28, 1995 PER DOCUMENT NO 1995-0592152 OF OFFICIAL RECORDS.
- PARCEL 2 OF SAID DOC IS AN EASEMENT FOR PUBLIC ROAD, UTILITIES AND DRAINAGE PURPOSES.
- PARCEL 3 OF SAID DOC IS AN EASEMENT FOR STORM DRAINAGE PURPOSES.
- PARCEL 4 OF SAID DOC IS AN EASEMENT FOR TEMPORARY CONSTRUCTION PURPOSES EASEMENT HAS EXPIRED AND THEREFOR IS NOT SHOWN HEREON.
- PARCEL 5 OF SAID DOC IS AN EASEMENT FOR CONSTRUCTION AND MAINTENANCE OF SLOPE PURPOSES.
- PARCEL 6 OF SAID DOC IS AN EASEMENT FOR PUBLIC INGRESS AND EGRESS PURPOSES. DOES NOT AFFECT SUBJECT PROPERTY AND THEREFOR IS NOT SHOWN HEREON.
- SAID LAND IS SUBJECT TO THE OCEANSIDE MUNICIPAL AIRPORT CLEAR ZONE AS CONTAINED IN DEED RECORDED DECEMBER 28, 1995 PER DOCUMENT NO 1995-0592152 OF OFFICIAL RECORDS. DESCRIBED IN PARCEL 1.
- INTENTIONALLY DELETED.
- VARIOUS CONDITIONS AFFECTING THE EASEMENTS RESERVED IN GRANT DEED RECORDED OCTOBER 14, 1999 AS INSTRUMENT NO 1999-0894332 OF OFFICIAL RECORDS.
- (1) OF SAID DOC IS AN EASEMENT FOR ROAD PURPOSES.
- (2) OF SAID DOC IS AN EASEMENT TO RETAIN IN PLACE, MAINTAIN, REPAIR, AND REPLACE THE ENGINEERED BLOCK WALL, TOGETHER WITH THE TREES AND LANDSCAPING.
- (3) OF SAID DOC IS AN EASEMENT FOR DIRECT DRIVEWAY ACCESS PURPOSES.
- A PROPOSED EASEMENT FOR PRIVATE ROAD PURPOSES OVER PARCEL 4A THAT WILL PROVIDE ACCESS TO PARCEL 4B IN THE EVENT THAT EASEMENT AA REFERRED TO HEREIN IS QUITCLAIMED AS PROVIDED FOR IN PARAGRAPH (1) OF DOCUMENT RECORDED OCTOBER 14, 1999 PER DOCUMENT NO 1999-0694332 OF OFFICIAL RECORDS TO BE QUITCLAIMED.
- DISCREPANCIES, CONFLICTS IN BOUNDARY LINES, SHORTAGE IN AREA, ENCROACHMENTS, OR ANY OTHER MATTER SHOWN ON RECORD OF SURVEY MAP NO 16861.
- CITY OF OCEANSIDE HOLDER OF AN EASEMENT FOR PUBLIC HIGHWAY AND GENERAL MAINTENANCE PURPOSES RECORDED AUGUST 2, 2002 PER DOCUMENT NO 2002-0652871 OF OFFICIAL RECORDS.
- PARCEL A AN EASEMENT FOR PUBLIC HIGHWAY PURPOSES DESCRIBED AS ALEX ROAD.
- PARCEL B AN EASEMENT FOR GENERAL MAINTENANCE PURPOSES. PORTION EASEMENT LOCATED ON SUBJECT PROPERTY TO BE QUITCLAIMED.
- PARCEL C AN EASEMENT FOR TEMPORARY CONSTRUCTION PURPOSES. EASEMENT HAS EXPIRED AND THEREFORE IS NOT SHOWN HEREON.
- COVENANTS, CONDITIONS AND RESTRICTIONS RECORDED DECEMBER 28, 2004 PER DOCUMENT NO 2004-1218146 OF OFFICIAL RECORDS.
- A COVENANT AND AGREEMENT AND ESTABLISHMENT OF EASEMENTS FOR WATER PIPELINE PURPOSES RECORDED APRIL 25, 2006 PER DOCUMENT NO 2006-0287115 OF OFFICIAL RECORDS.
- EXHIBIT C PARCEL 1 AN EASEMENT FOR WATER PIPELINE PURPOSES.
- EXHIBIT C PARCEL 2 AN EASEMENT FOR WATER PIPELINE PURPOSES.
- EXHIBIT D PARCEL 1 AN EASEMENT FOR WATER PIPELINE PURPOSES TO REMAIN.
- EXHIBIT D PARCEL 2 AN EASEMENT FOR WATER PIPELINE PURPOSES.
- A COVENANT AND AGREEMENT AND ESTABLISHMENT OF EASEMENTS FOR PRIVATE SANITARY SEWER AND WATER LINE IMPROVEMENT PURPOSES RECORDED APRIL 25, 2006 PER DOCUMENT NO 2006-0287116 OF OFFICIAL RECORDS.
- STORM WATER FACILITIES MAINTENANCE AGREEMENT WITH EASEMENT AND COVENANT PURPOSES RECORDED MAY 23, 2006 PER DOCUMENT NO 2006-0362810 OF OFFICIAL RECORDS. EASEMENT IS BLANKET IN NATURE AND THEREFORE IS NOT SHOWN HEREON.

ITEMS 21 AND 22 ARE NOW MAPPING ITEMS AND THEREFOR ARE NOT SHOWN HEREON.

*EXISTING EASEMENTS SHOWN IN ACCORDANCE WITH PRELIMINARY TITLE REPORT PREPARED BY CHICAGO TITLE INSURANCE COMPANY, ORDER NO. 00126233-987-0C1-K27, DATED EFFECTIVELY NOVEMBER 2, 2021

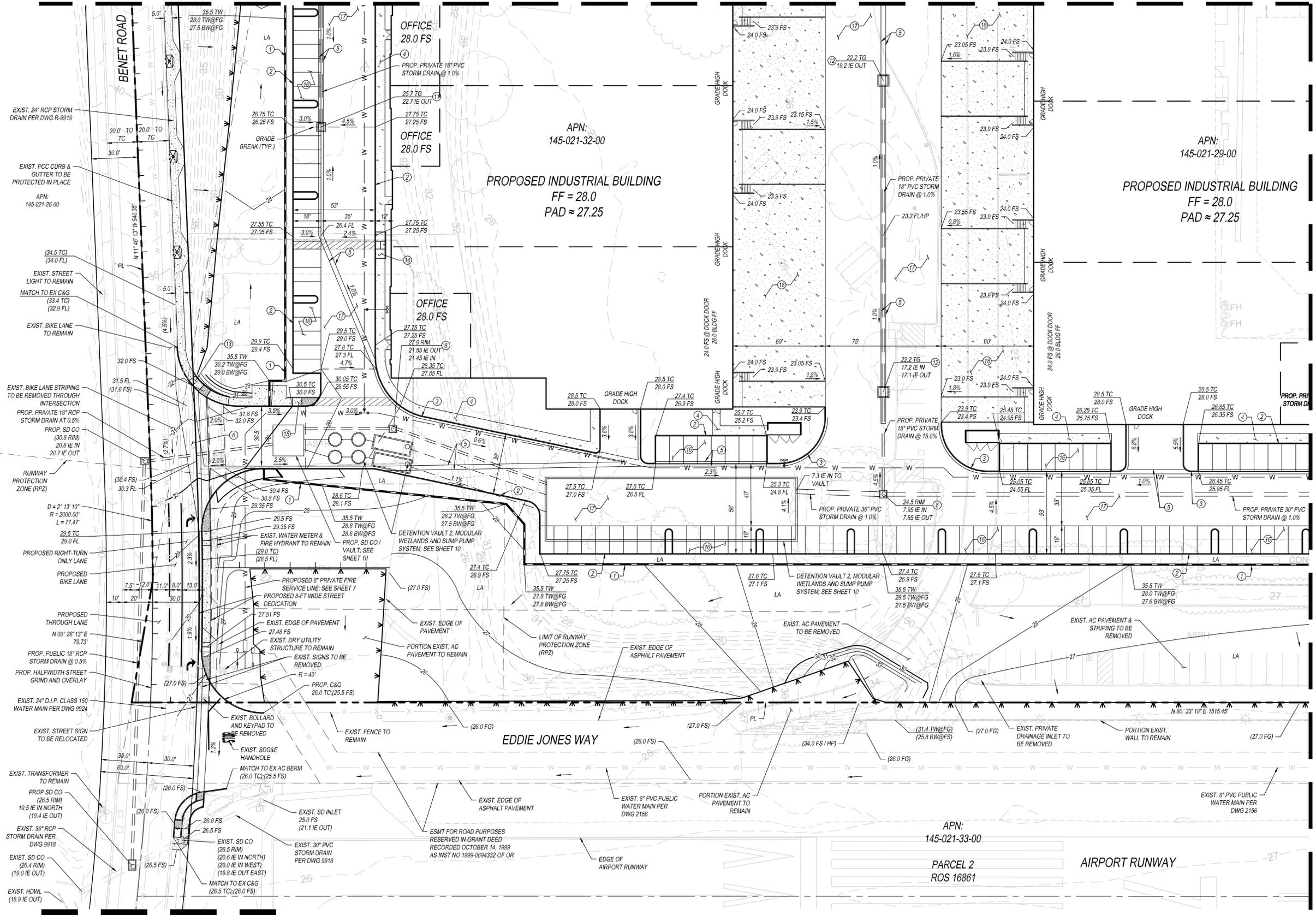
PROPOSED EASEMENT INFORMATION

- PUBLIC 8-FT WIDE PUBLIC STREET DEDICATION GRANTED TO THE CITY OF OCEANSIDE.
- PUBLIC STREET DEDICATION OF THE CUL-DE-SAC OF ALEX ROAD GRANTED TO THE CITY OF OCEANSIDE.



PASCO LARET SUITER & ASSOCIATES
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MATCHLINE: SEE SHEET 6

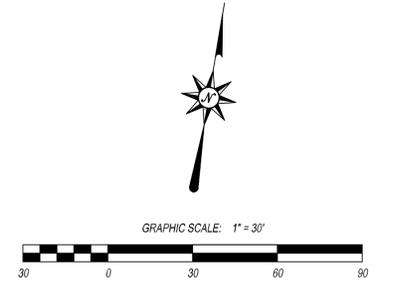


LEGEND

PROPERTY BOUNDARY	---
PROPERTY BOUNDARY (PROPOSED)	- - -
PROPERTY LINE / RIGHT-OF-WAY	---
ADJACENT PROPERTY LINE	---
CENTERLINE OF ROAD	---
EXISTING EASEMENT	---
EXISTING CONTOUR LINE	---
PROPOSED CONTOUR LINE	---
PROPOSED LIMIT OF GRADING	---
PROPOSED FLOOD WALL	---
EXISTING FENCE	X X
EXISTING WATER MAIN (SIZE PER PLAN)	W W
EXISTING SEWER MAIN (SIZE PER PLAN)	S S
EXISTING GAS MAIN (SIZE PER PLAN)	G G
EXISTING OVERHEAD ELECTRIC UTILITIES	OE OE
EXISTING STORM DRAIN (SIZE PER PLAN)	---
PROPOSED STORM DRAIN (SIZE PER PLAN)	---

- CONSTRUCTION NOTES**
- PROPOSED STRUCTURAL FLOOD WALL PER SPECIAL DESIGN TO FLOOD PROOF PROPERTY
 - PROPOSED 6" PCC CURB PER SDRSD G-1
 - PROPOSED 6" PCC CURB & GUTTER PER SDRSD G-2
 - PROPOSED 5" WIDE, 4" THICK PCC SIDEWALK PER SDRSD G-7
 - PROPOSED 3-FIT WIDE RIBBON GUTTER PER DETAIL SHEET 9
 - PROPOSED PCC CROSS GUTTER PER SDRSD G-12
 - INSTALL TYPE-F STORM DRAIN CATCH BASIN PER SDRSD D-7
 - INSTALL TYPE-A STORM DRAIN CLEANOUT PER SDRSD D-9
 - INSTALL TYPE-A CURB INLET PER SDRSD D-1
 - INSTALL 12"x12" BROOKS BOX STORM DRAIN INLET OR APPROVED EQUAL
 - INSTALL 24"x24" BROOKS BOX STORM DRAIN INLET OR APPROVED EQUAL
 - INSTALL 36"x36" BROOKS BOX STORM DRAIN INLET OR APPROVED EQUAL
 - PROPOSED ADA COMPLIANT PEDESTRIAN CURB RAMP PER SDRSD G-27, G-30 TYPE A
 - PROPOSED ADA COMPLIANT PEDESTRIAN CURB RAMP PER SDRSD G-27, G-30 TYPE B
 - PROPOSED ADA COMPLIANT PEDESTRIAN CURB RAMP PER SDRSD G-30, G-31 TYPE D
 - PROPOSED 3" AC PAVEMENT OVER 4" CLASS II AB PER GEOTECH RECOMMENDATION
 - PROPOSED 5" AC PAVEMENT OVER 7" CLASS II AB PER GEOTECH RECOMMENDATION
 - PROPOSED 6.5" THICK PCC PAVEMENT OVER 12" COMPACTED SUBGRADE PER GEOTECH RECOMMENDATION

MATCHLINE: SEE SHEET 4



SEE SHEET 7 FOR CONTINUATION OF OFFSITE UTILITIES

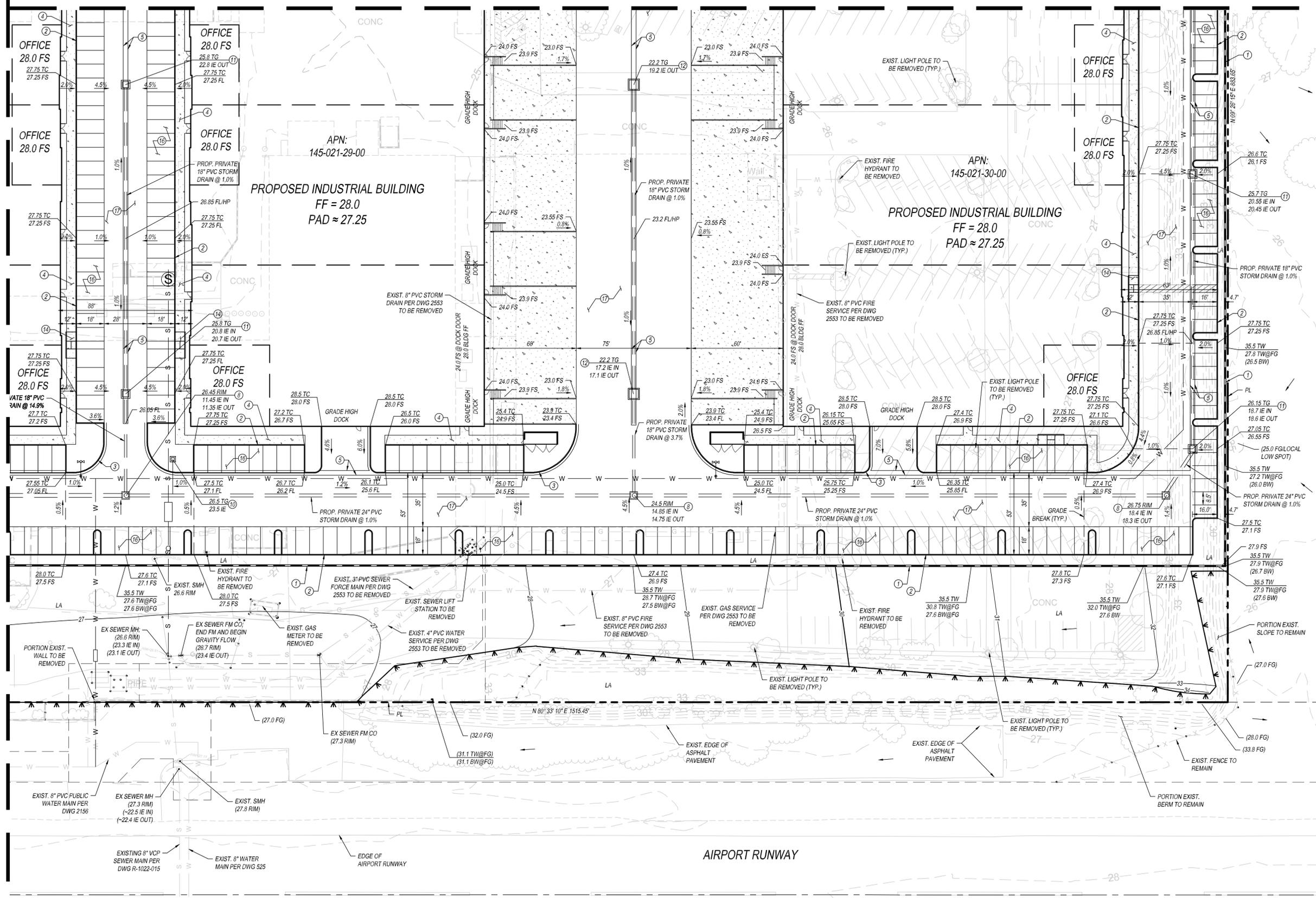
PLAN VIEW - PRELIMINARY GRADING PLAN

SCALE: 1" = 30' HORIZONTAL

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MATCHLINE: SEE SHEET 5

MATCHLINE: SEE SHEET 3



CONSTRUCTION NOTES

- 1 PROPOSED STRUCTURAL FLOOD WALL PER SPECIAL DESIGN TO FLOOD PROOF PROPERTY
- 2 PROPOSED 6" PCC CURB PER SDRSD G-1
- 3 PROPOSED 6" PCC CURB & GUTTER PER SDRSD G-2
- 4 PROPOSED 5' WIDE, 4" THICK PCC SIDEWALK PER SDRSD G-7
- 5 PROPOSED 3-F' WIDE RIBBON GUTTER PER DETAIL SHEET 9
- 6 PROPOSED PCC CROSS GUTTER PER SDRSD G-12
- 7 INSTALL TYPE-F STORM DRAIN CATCH BASIN PER SDRSD D-7
- 8 INSTALL TYPE-A STORM DRAIN CLEANOUT PER SDRSD D-9
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- 14 PROPOSED ADA COMPLIANT PEDESTRIAN CURB RAMP PER SDRSD G-27, G-30 TYPE B
- 15 PROPOSED ADA COMPLIANT PEDESTRIAN CURB RAMP PER SDRSD G-30, G-31 TYPE D
- 16 PROPOSED 3" AC PAVEMENT OVER 4" CLASS II AB PER GEOTECH RECOMMENDATION
- 17 PROPOSED 5" AC PAVEMENT OVER 7" CLASS II AB PER GEOTECH RECOMMENDATION
- 18 PROPOSED 6.5" THICK PCC PAVEMENT OVER 12" COMPACTED SUBGRADE PER GEOTECH RECOMMENDATION

PLAN VIEW - PRELIMINARY GRADING PLAN

SCALE: 1" = 30' HORIZONTAL

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MATCHLINE: SEE SHEET 6

MATCHLINE: SEE SHEET 4

PLAN VIEW - PRELIMINARY GRADING PLAN

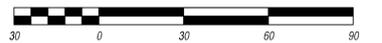
SCALE: 1" = 30' HORIZONTAL

CONSTRUCTION NOTES

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GRAPHIC SCALE: 1" = 30'



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APN:
145-021-33-00

SAN LUIS REY RIVER

BENET ROAD

PROPOSED INDUSTRIAL BUILDING
FF = 28.0
PAD ≈ 27.25

APN:
145-021-32-00

PROPOSED INDUSTRIAL BUILDING
FF = 28.0
PAD ≈ 27.25

APN:
145-021-29-00

CONSTRUCTION NOTES

- 1 PROPOSED STRUCTURAL FLOOD WALL PER SPECIAL DESIGN TO FLOOD PROOF PROPERTY
- 2 PROPOSED 6" PCC CURB PER SDRSD G-1
- 3 PROPOSED 6" PCC CURB & GUTTER PER SDRSD G-2
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MATCHLINE: SEE SHEET 5

MATCHLINE: SEE SHEET 3

PLAN VIEW - PRELIMINARY GRADING PLAN

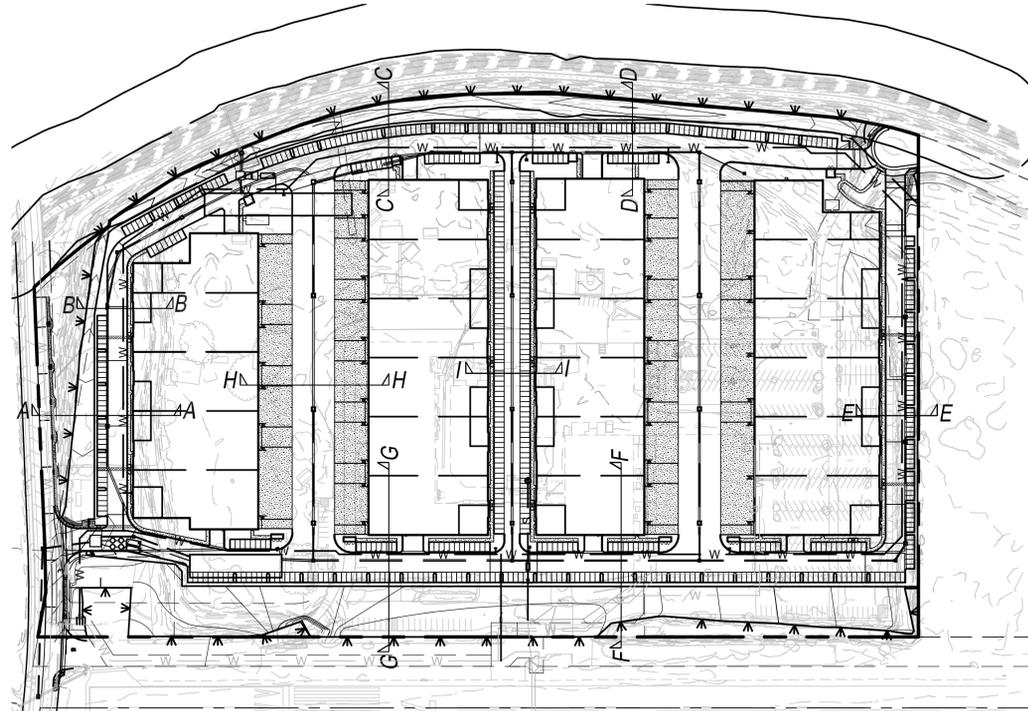
SCALE: 1" = 30' HORIZONTAL



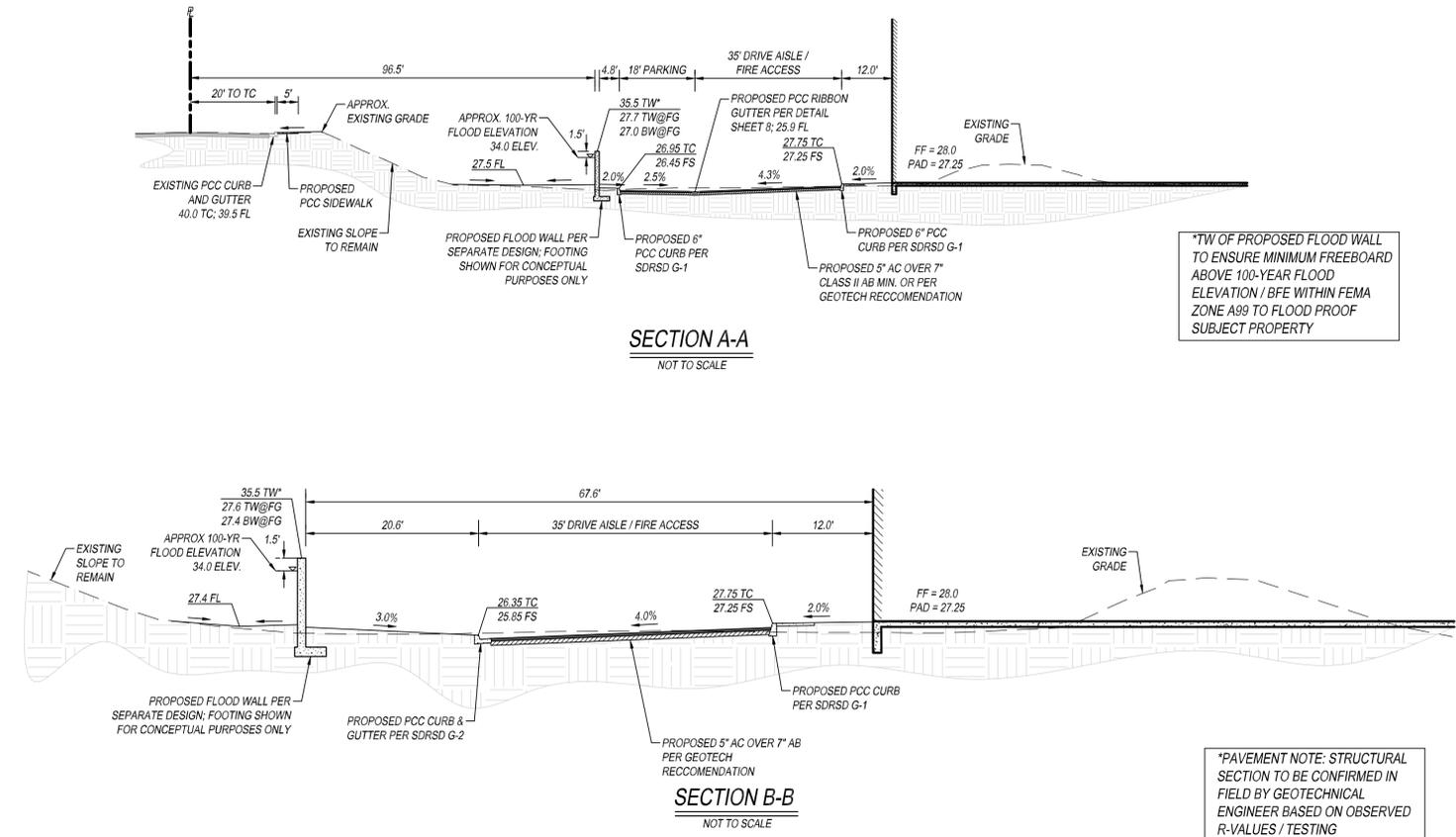
GRAPHIC SCALE: 1" = 30'



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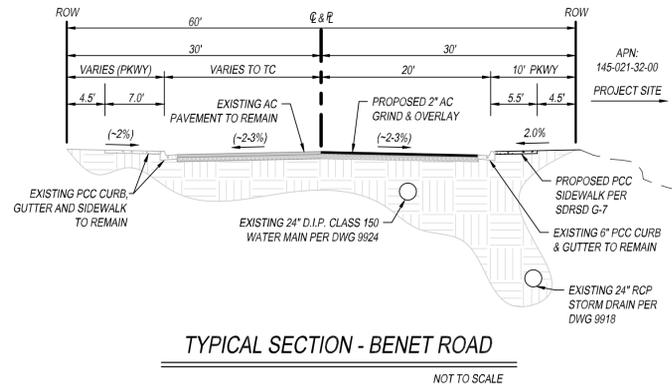


PLAN VIEW - SITE SECTIONS KEY MAP
SCALE: NTS

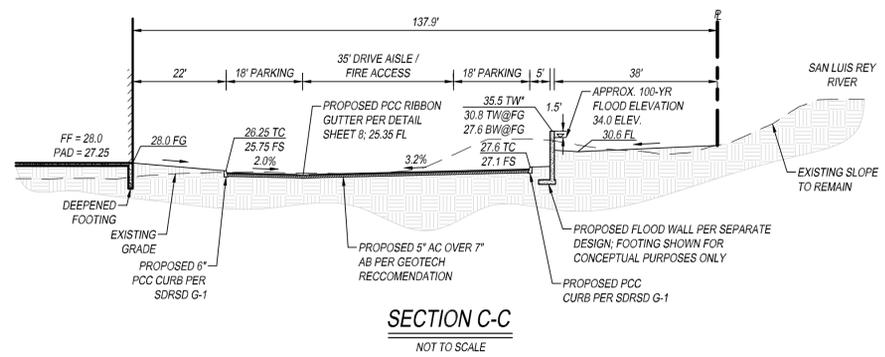


*TW OF PROPOSED FLOOD WALL TO ENSURE MINIMUM FREEBOARD ABOVE 100-YEAR FLOOD ELEVATION / BFE WITHIN FEMA ZONE A99 TO FLOOD PROOF SUBJECT PROPERTY

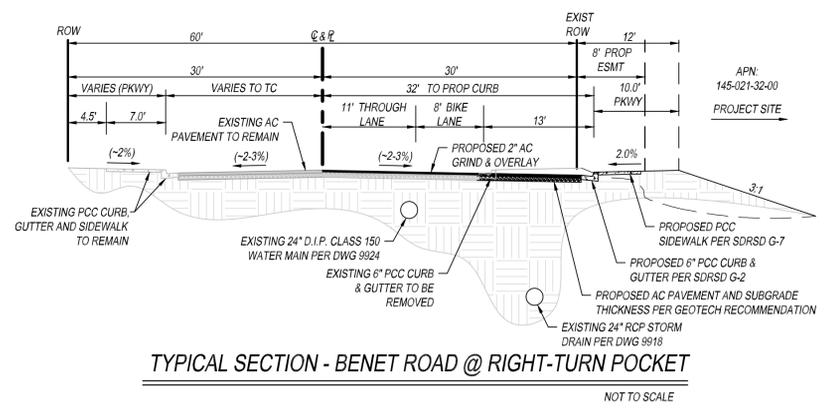
*PAVEMENT NOTE: STRUCTURAL SECTION TO BE CONFIRMED IN FIELD BY GEOTECHNICAL ENGINEER BASED ON OBSERVED R-VALUES / TESTING



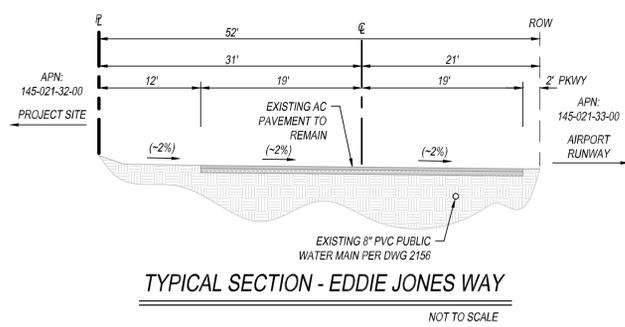
TYPICAL SECTION - BENET ROAD
NOT TO SCALE



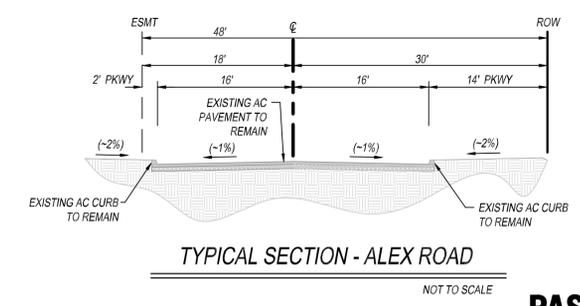
SECTION C-C
NOT TO SCALE



TYPICAL SECTION - BENET ROAD @ RIGHT-TURN POCKET
NOT TO SCALE

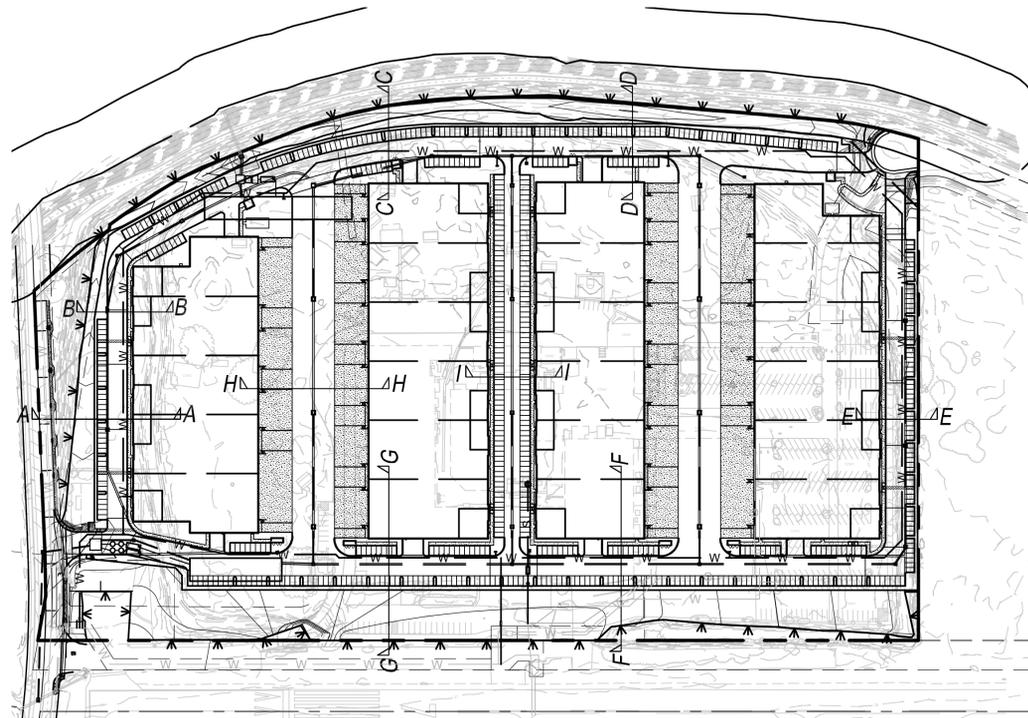


TYPICAL SECTION - EDDIE JONES WAY
NOT TO SCALE



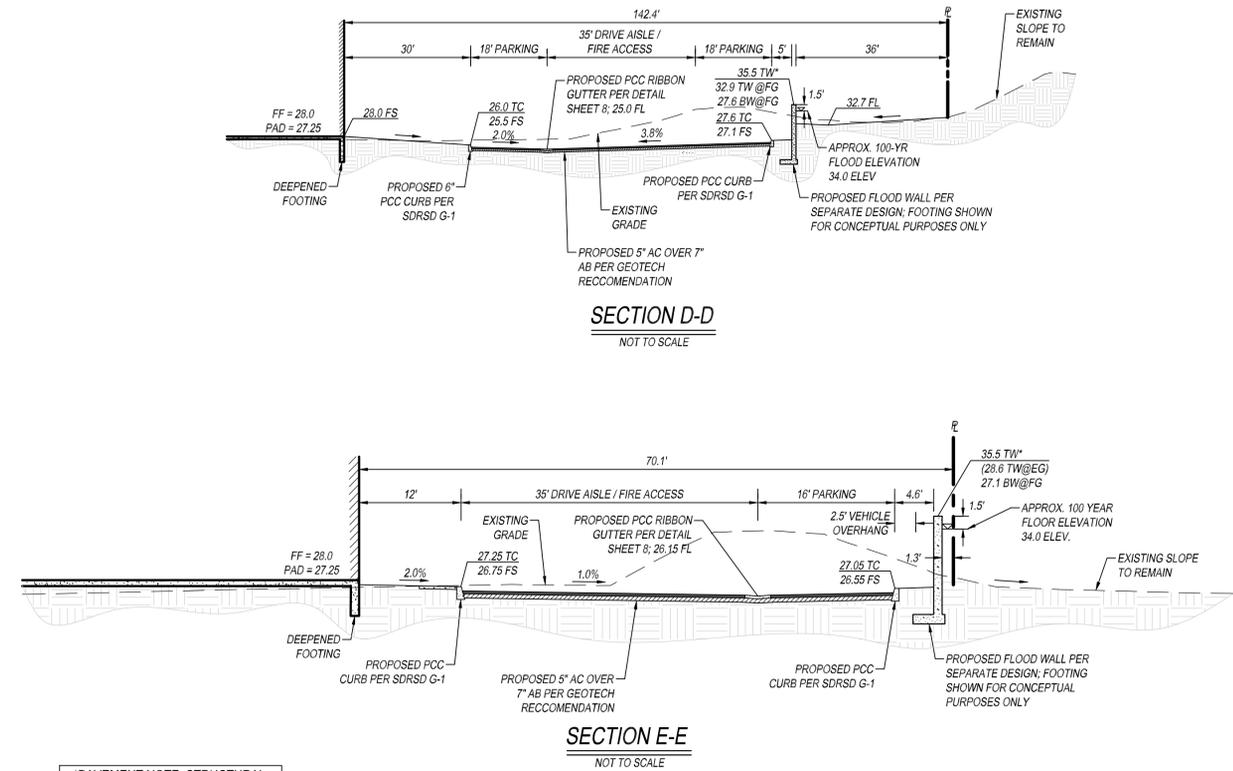
TYPICAL SECTION - ALEX ROAD
NOT TO SCALE

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PLAN VIEW - SITE SECTIONS KEY MAP

SCALE: NTS

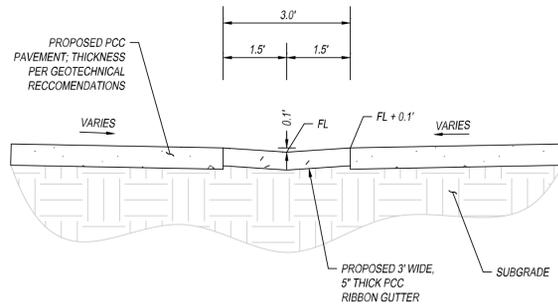


SECTION D-D
NOT TO SCALE

SECTION E-E
NOT TO SCALE

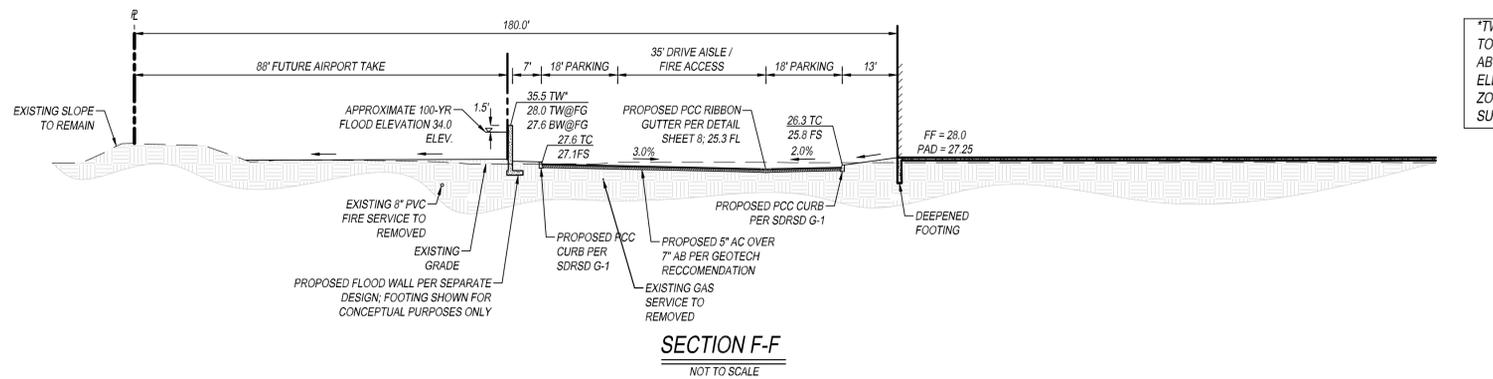
*PAVEMENT NOTE: STRUCTURAL SECTION TO BE CONFIRMED IN FIELD BY GEOTECHNICAL ENGINEER BASED ON OBSERVED R-VALUES / TESTING

*TW OF PROPOSED FLOOD WALL TO ENSURE MINIMUM FREEBOARD ABOVE 100-YEAR FLOOD ELEVATION / BFE WITHIN FEMA ZONE A99 TO FLOOD PROOF SUBJECT PROPERTY

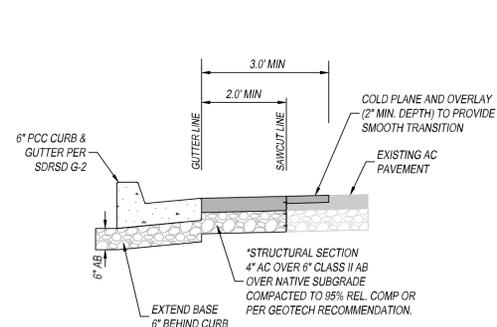


TYPICAL DETAIL - RIBBON GUTTER

NOT TO SCALE

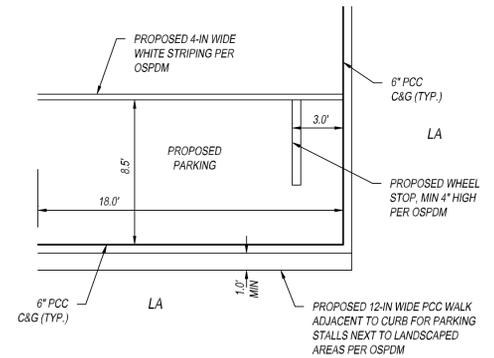


SECTION F-F
NOT TO SCALE



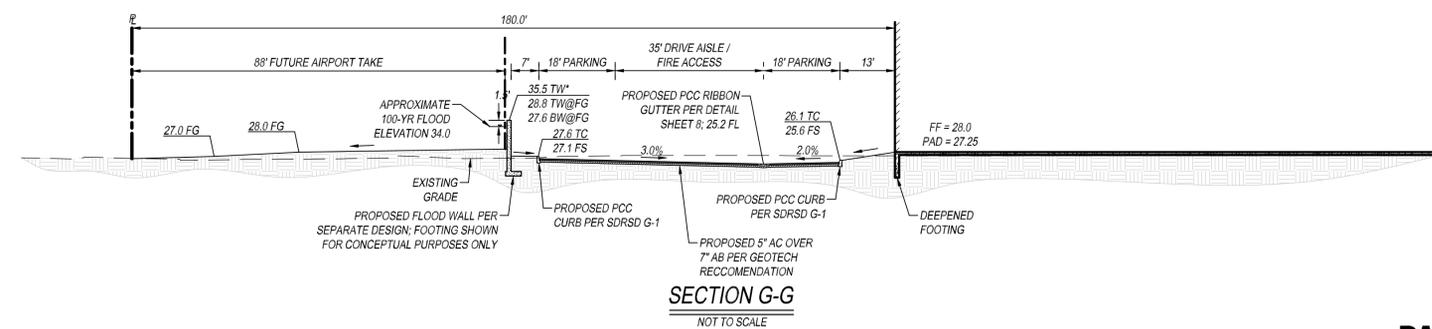
TYPICAL DETAIL - SAWCUT AC PAVEMENT

NOT TO SCALE

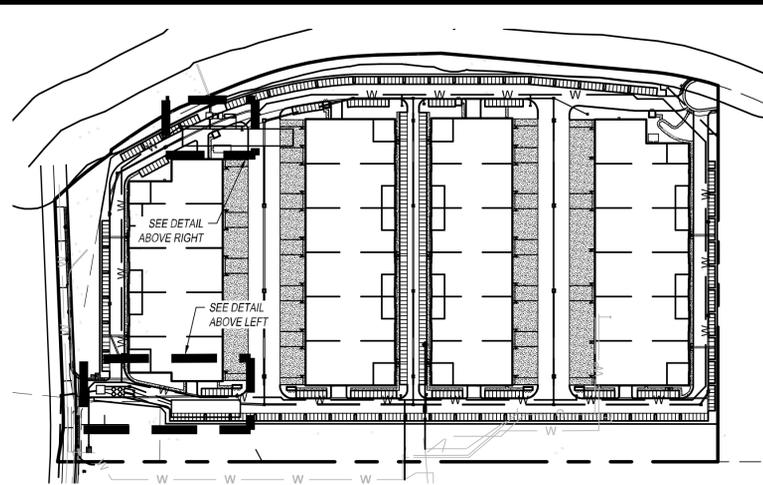


TYPICAL DETAIL - WHEELSTOP AND PARKING STALL

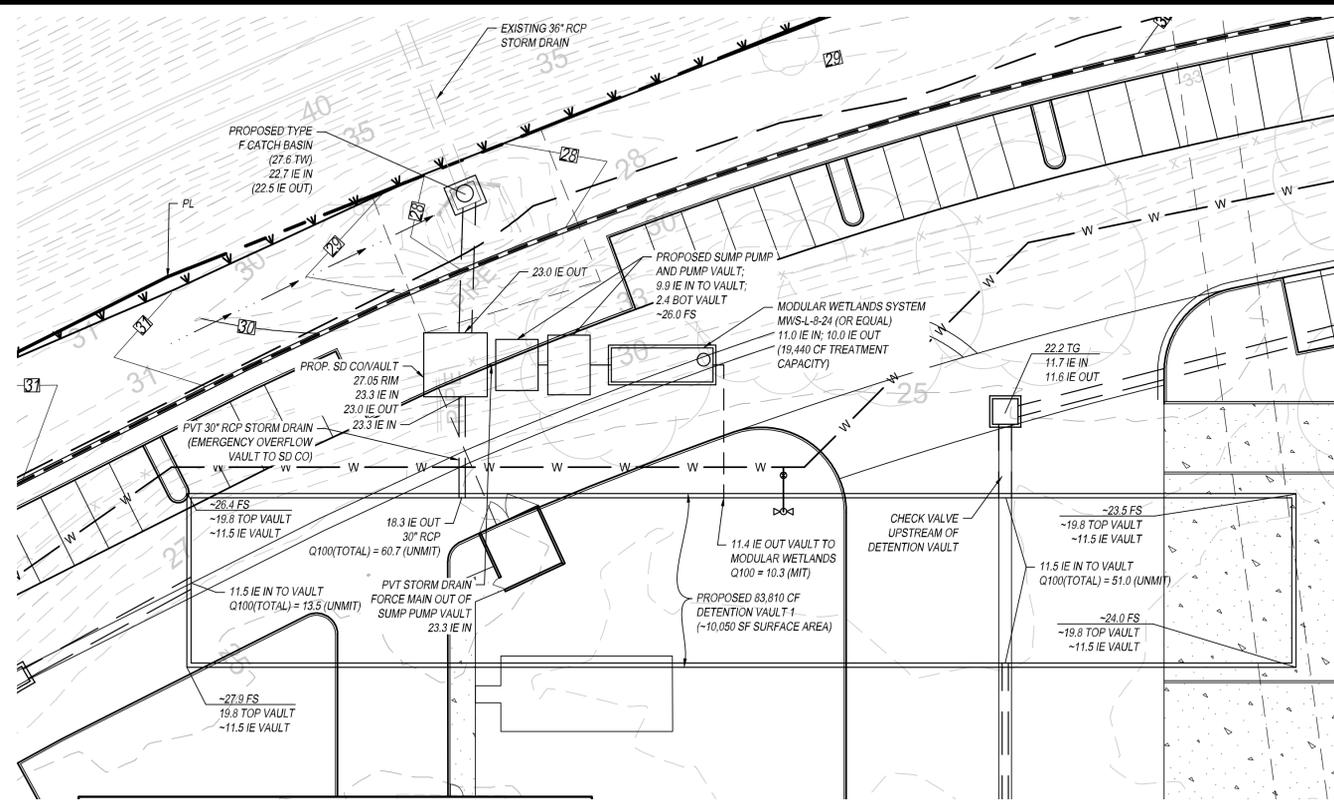
NOT TO SCALE



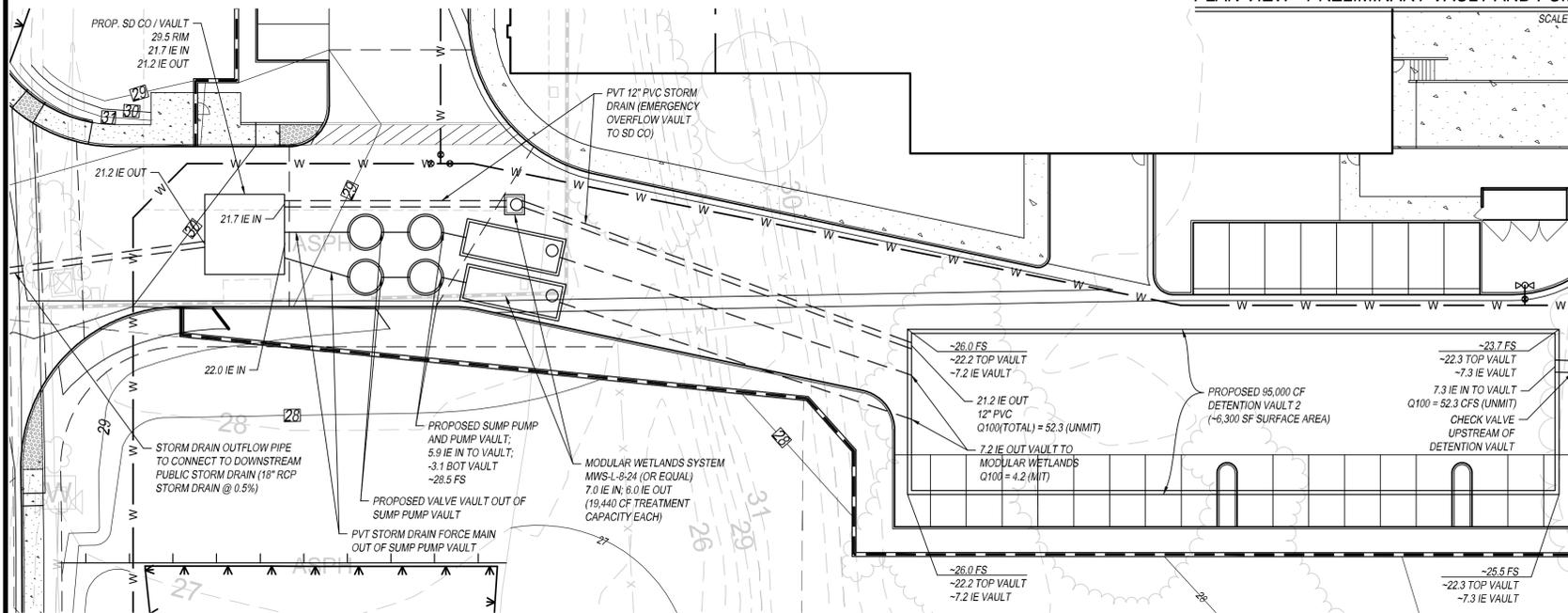
SECTION G-G
NOT TO SCALE



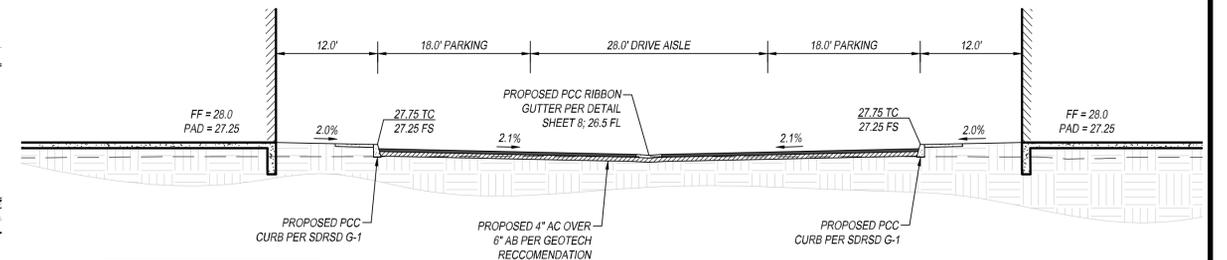
PLAN VIEW - DETENTION KEY MAP
SCALE: NTS



PLAN VIEW - PRELIMINARY VAULT AND PUMP DESIGN
SCALE: 1" = 20' HORIZONTAL

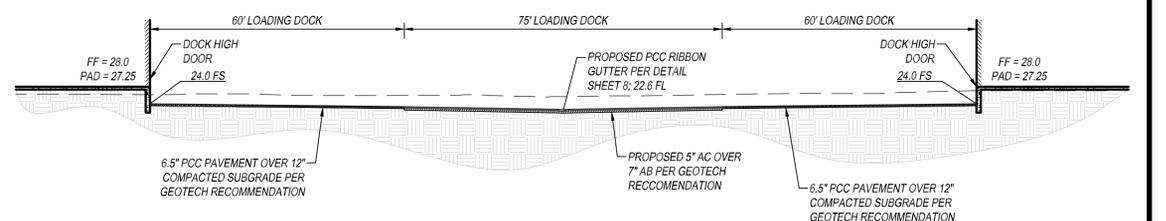


PLAN VIEW - PRELIMINARY VAULT AND PUMP DESIGN
SCALE: 1" = 20' HORIZONTAL



SECTION I-I
NOT TO SCALE

*PAVEMENT NOTE: STRUCTURAL SECTION TO BE CONFIRMED IN FIELD BY GEOTECHNICAL ENGINEER BASED ON OBSERVED R-VALUES / TESTING



SECTION H-H
NOT TO SCALE

APN:
145-021-33-00

SAN LUIS REY RIVER

N 78° 57' 36" E 151.11'

ALEX ROAD

BENET ROAD

EDDIE JONES WAY

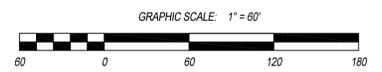
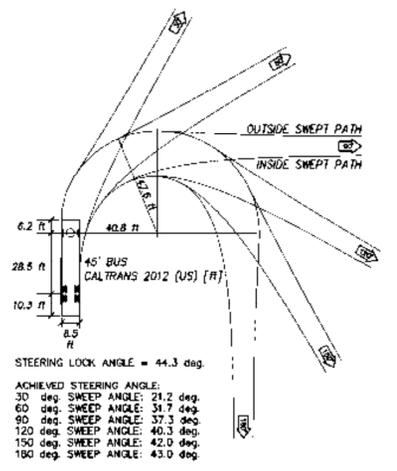
AIRPORT RUNWAY

APN:
145-021-33-00

PARCEL 2
ROS 16861

PLAN VIEW - TRUCK TURNING EXHIBIT

SCALE: 1" = 60' HORIZONTAL

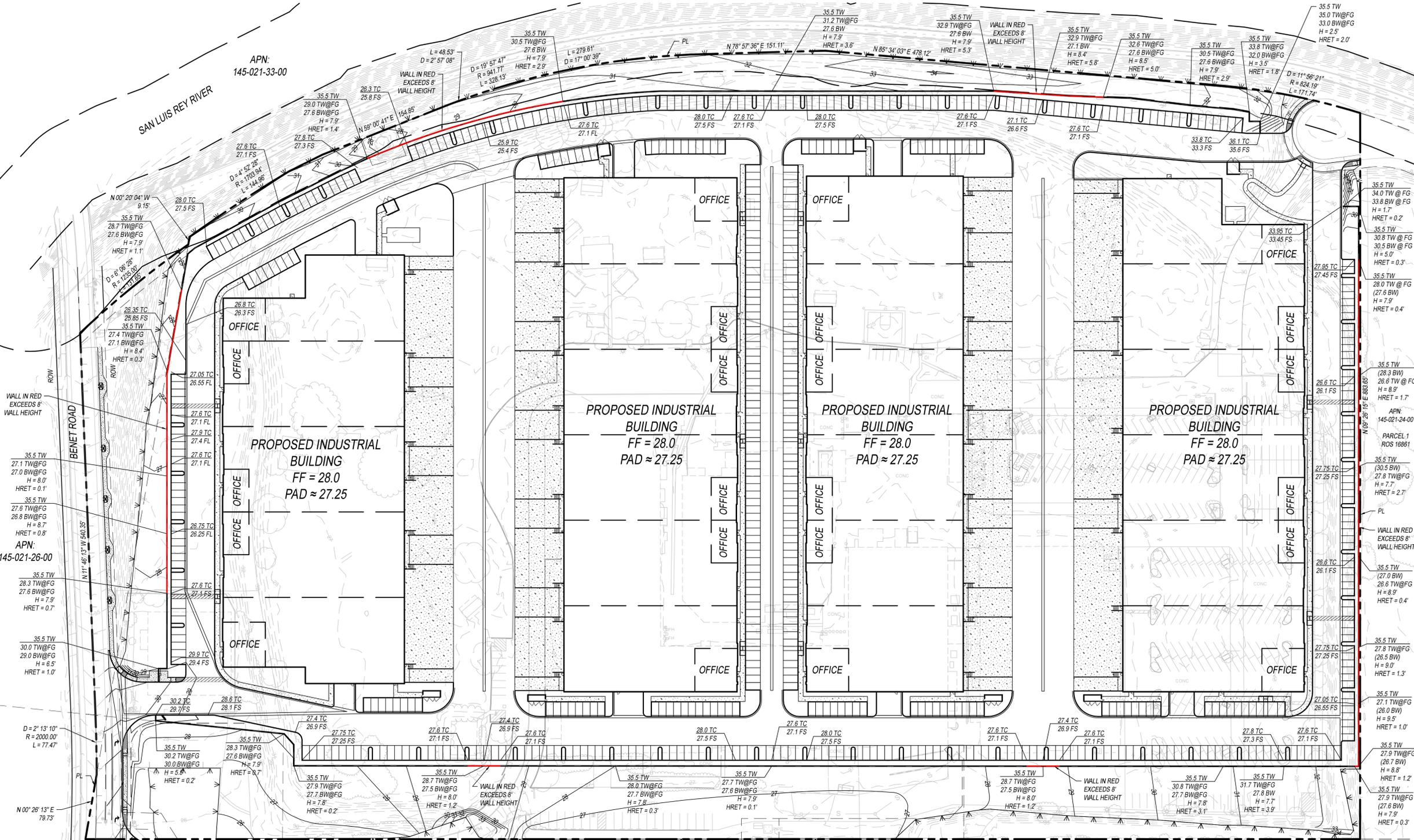


TRUCK TURNING EXHIBIT
250 EDDIE JONES WAY - CITY OF OCEANSIDE
SCALE: 1" = 60' HORIZONTAL

PASCO LARET SUITER
& ASSOCIATES
San Diego | Solana Beach | Orange County
Phone 858.259.8212 | www.plsaengineering.com

APN: 145-021-33-00

SAN LUIS REY RIVER



LEGEND

PROPERTY BOUNDARY	---
PROPERTY BOUNDARY (PROPOSED)	- - -
PROPERTY LINE / RIGHT-OF-WAY	---
ADJACENT PROPERTY LINE	---
CENTERLINE OF ROAD	---
PROPOSED EASEMENT	- - -
EXISTING EASEMENT	- - -
EXISTING CONTOUR LINE	---
PROPOSED CONTOUR LINE	---
PROPOSED LIMIT OF GRADING	---
PROPOSED FLOOD WALL	---
PROPOSED FLOOD WALL (EXCEEDING 8' IN EXPOSED WALL HEIGHT)	---
EXISTING FENCE	X - X
EXISTING WATER MAIN (SIZE PER PLAN)	W - W
EXISTING SEWER MAIN (SIZE PER PLAN)	S - S
EXISTING GAS MAIN (SIZE PER PLAN)	G - G
EXISTING OVERHEAD ELECTRIC UTILITIES	OE - OE
EXISTING STORM DRAIN (SIZE PER PLAN)	---

FLOOD WALL NOTES

ALL WALL HEIGHTS "H" SHOWN HEREON ARE THE GIVEN MAXIMUM EXPOSED WALL HEIGHT. ALL HEIGHTS "HRET" DELINEATE THE MAXIMUM RETAINED WALL HEIGHT.

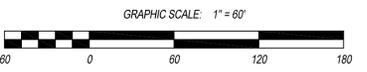
APN: 145-021-26-00

APN: 145-021-33-00

PARCEL 2 ROS 16861

AIRPORT RUNWAY

PLAN VIEW - FLOOD WALL EXHIBIT
SCALE: 1" = 60' HORIZONTAL



PRELIMINARY WALL EXHIBIT
250 EDDIE JONES WAY - CITY OF OCEANSIDE
SCALE: 1" = 60' HORIZONTAL

PASCO LARET SUITER & ASSOCIATES
San Diego | Solana Beach | Orange County
Phone 858.259.8212 | www.plsaengineering.com

MEMORANDUM

DATE: February 10, 2025
TO: Planning Commission
FROM: Rob Dmohowski, Development Services Department
SUBJECT: **Eddie Jones WH – D22-00001**

Due to size, attachment 4.04 Attachment 4 – “FEIR-Final Environmental Impact Report” has not been included in the packet. To view this document electronically, please go to:

<https://www.ci.oceanside.ca.us/government/development-services/planning/ceqa/eddie-jones-warehouse-manufacturing-and-distribution-facility-project-eir>

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

**City Council Policy 300-14 Enhanced Notification and Community Outreach Report
for Eddie Jones Warehouse, Manufacturing and Distribution Facility by RPG**

The proposed project is located on 30.79 acres on Eddie Jones Way at Benet Road within the city of Oceanside's industrial district, immediately adjacent to the Oceanside Municipal Airport (figure A). The vision for this site is to provide a modern yet functional industrial space that is consistent with the site's existing zoning (figure B) and long-time industrial use, yet sensitive to the surrounding uses.



Figure A: Project Location



Figure B: Existing Zoning (Project site outlined for reference)

In compliance with City Council Enhanced Notification Policy 300-14, a variety of opportunities have been available for interested parties to learn more about the project, ask questions and share their feedback (exhibit 1 includes a complete timeline of community engagement and links to virtual communication).

This report summarizes outreach and engagement conducted to date and provides responses to the community feedback received.

Community Responsive Planning

In response to community feedback, the following project modifications have been implemented:

- Added dedicated right turn lane into project from Benet Road
- Reduced truck bays from 114 to 57
- Reduced building footprint from 547k SF to 491k SF (gross square footage was decreased from 566k SF to 497k SF)

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

- Less intensive project that still falls within analysis of larger project
- Four smaller buildings instead of one larger structure
- All truck bays are screened and face inward toward other project buildings
- Truck trailer parking removed from perimeter parking areas
- Increases onsite landscaping

Key Themes and Responses

Topic	Response
Transparency	<p>In response to concerns over transparency, the applicant expanded the notification area beyond the required limits. Instead of just the 1,500-foot radius (figure C), all residences and businesses north of the project site were mailed the two project postcards (figure D). A project website was established and all materials are posted. Interested parties were also encouraged to register for the project interest list to be kept informed. Lastly, four public meetings were hosted for the project.</p>
Compatibility	<p>This 31-acre site is immediately adjacent to the Oceanside Municipal Airport. The San Luis Rey Bike Trail/levee and the river itself create a distinct separation between the city's industrial core and the residential uses to the north. This site has been used for industrial purposes since 1962 (which predates the construction of housing in the area):</p> <ul style="list-style-type: none">- 1962 – 1966 Swan Electrics- 1966 – 2012 Deutsch Company- 2012 – 2018 TE Connectivity- 2021 – RPG purchases site <p>Project design features like building orientation and landscaping buffers will also help shield neighboring property owners.</p> <p>The proposed use is consistent with the existing General Plan designation and zoning for the site (IL – Light Industrial). The General Plan is currently being updated, and the IL zone is being maintained. Further, the City's Economic Development Element estimates that the city will have a deficit in industrial land during this planning period and recommends the preservation of Industrial land.</p> <p>Lastly, the site design complies with the Airport Overlay Zone safety zones and height requirements and has received the FAA "No Hazard Determinations" and approval from San Diego County ALUC. It is consistent with current Airport Land Use Plan requirements and future expansion plans. (figure E).</p>
Emergency Evacuation	<p>Oceanside Fire is one of the many disciplines that reviews development applications. Availability of public facilities is also evaluated during the CEQA process. A Wildfire Evacuation Study was prepared for the project, reviewed and accepted by City of Oceanside Fire Staff, and is included in the Final EIR for the project.</p> <p>The building is designed to be ignition resistant (e.g. concrete, sprinklered), making it less vulnerable in the event of a wildfire. That is important because there would most likely be a different evacuation protocol for the facility than there would be for the residential uses.</p> <p>Evacuation is ultimately determined by first responders based on the nature and location of the emergency.</p>
Circulation	<p>A dedicated right turn lane was added to the project following the January 2023 public meeting. This turn lane was in response to public concern over possible delays for those turning left onto Benet Road from Highway 76 due to project queuing.</p>

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

	<p>The project also provides the missing link of sidewalk on Benet to improve pedestrian access.</p> <p>There are two ingress and egress points for the site: Benet and Foussat Roads. All truck traffic will be directed to the Benet entrance. Employees will be able to choose between Benet or Foussat.</p>
Traffic	<p>The traffic study was finalized in 2023 and is consistent with city standards. Base traffic levels were based on the highest level of traffic volumes measured over a seven-year period starting in 2015 from a variety of like uses. These standards provide an objective framework for analyzing anticipated trip generation. 13 trucks are estimated during the morning peak hour and 9 trucks are estimated during the afternoon peak hour.</p> <p>Eighteen projects, including Ocean KAMP, were analyzed as part of the cumulative impacts within the Environmental Impact Report. As documented in the Local Transportation Study (Section 3.7), if Eddie Jones is the only project, the intersection of SR-76/Benet Rd would require no improvements.</p> <p>Existing traffic plus traffic from 18 cumulative projects are what cause an exceedance of the level of service goal for SR-76/Benet Rd., and that is only in the 2030 cumulative condition once these other developments in the city are added. As a result, consistent with City standards, the project and other contributing developments will make fair share payments towards future improvements to the SR-76/Benet intersection, based on each project's proportion of the trips expected to be generated.</p>
Hours of Operation	<p>The hours of operation are determined by the tenant. An Operations Management Plan will be prepared for each tenant that will include business operation details, hours of operation, security protocol, and property maintenance. It will also include a "good neighbor policy" detailing delivery vehicle schedules, truck haul routes, noise mitigation, best management practices, and protocol to address any concerns from neighboring property owners or tenants. Rumors that the facility will operate 24 hours a day, 7 days a week are not substantiated.</p>
Environmental Impacts	<p>As noted above, the site is disturbed from decades of industrial use. The redevelopment of this site benefits the immediate area by identifying and removing significant levels of hazardous materials left there by the former owner of this site, and in so doing reduces risks to the neighborhood.</p> <p>The cleanup is being accomplished within the exacting framework of the California Land Reuse and Revitalization Act (CLRRRA) program in close cooperation with the Department of Toxic Substances Control (DTSC). Even after soil remediation, the site will be deed restricted for industrial use.</p> <p>The project maintains the 100-foot buffer as required from the edge of the San Luis Rey River riparian habitat. The buffer area is adjacent to the San Luis Rey River Trail embankment and is designated to be replanted with native coastal species. A biology report was completed as part of the CEQA process to identify possible impacts on surrounding habitat and sensitive species. No significant impacts were identified.</p> <p>The Storm Water Quality Management Plan includes Best Management Practices (BMPs), like landscaping areas and underground detention vaults, to treat and clean storm water. With these measures, storm water from the site will be treated, resulting in cleaner runoff than today.</p>

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

<p>Tenant</p>	<p>We understand the interest in future tenants and aim to provide additional assurances where we can to alleviate concerns.</p> <p>The building is designed to include manufacturing, warehouse and office use. In Oceanside, there is a demand for this kind of facility to meet the needs of this growing industry. While it is too soon to sign a tenant now, RPG is focused on a life science tenant.</p> <p>RPG estimated the project will take roughly \$150 Million to entitle and build. A high-quality tenant is needed to make the project financially viable. In other words, the type of tenant that would be able to afford the rent.</p> <p>Although not required, RPG commissioned an economic study for the project. The study focused on industrial life sciences as well and indicated that the project would create 500 new jobs.</p>
<p>Entitlements</p>	<p>Although the project is consistent with the site’s zoning, RPG is seeking a Conditional Use Permit (CUP). This is customary in Oceanside for all facilities larger than 50,000 sq. ft., regardless of the size of the project site.</p> <p>The request for a CUP creates the perception that the proposed use is not allowed. However, city development standards would allow roughly 1 million SF on a property of this size (31 acres). Maximizing the site to its fullest potential would not have been appropriate. Given the lack of industrial land available in Oceanside, the proposed project aims to strike a balance to meet the city need while also being sensitive to the site’s context.</p>
<p>Views</p>	<p>The site’s proximity to the river required that it be protected from the 100-year flood. To accomplish this, the project proposes a flood wall to be constructed around the perimeter of the site. The alternative to the flood wall was to raise the pad elevation out of the flood plain. This would have resulted in the building being situated higher up. We did not think this would be compatible with the surrounding community, so we opted to construct the flood wall to reduce view impacts for the residential uses. In response to community feedback, view simulations were prepared from images provided by a neighboring homeowner.</p>

Figure C: Oceanside’s Required 1,500-foot Radius Notification Boundary



- 1,500-foot radius – roughly 200 recipients
- 100-foot radius – 0

Figure D: Expanded Notification Boundary



- Expanded Boundary
- Project Boundary

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI



Figure E: Excerpt from Airport Expansion Master Plan

Implementation of Outreach Plan – Community Education and Engagement

On-Site Signage

All on-site signage is posted to the site in accordance with city policy.

Mail Notification

Pursuant to Policy 300-14, a project notification was sent by the City to property owners within a 1,500-foot radius and occupants within a 100-foot radius of the project site on February 2, 2022 (exhibit 2).

Subsequent mailings were sent on December 29, 2022 (exhibit 3) and June 26, 2023 (exhibit 4) describing the proposed project, project email and website, and upcoming outreach meetings.

Virtual Notification

Email: info@eddiejonesproject.com

A community email address was established for the project in December 2022; neighbors and community stakeholders have been encouraged to submit their questions and comments. As of January 17, 2025, seventeen residents have emailed us for more information about the project.

Members of the community were also given the opportunity to join the project email list to receive updates on project milestones and be notified about engagement opportunities. There are currently 416 residents registered for the email list. The following email updates have been sent (see exhibit 1 for links to view email updates):

- 1/12/23: Outreach meeting #1 reminder
- 1/19/23: Meeting #1 follow up
- 2/16/23: Project Update re: website launch
- 6/29/23: Outreach meeting #2 invite
- 7/5/23: Notice that a virtual option had been added
- 7/7/23: Notice of new meeting location (Meeting was moved at the neighbors' request)
- 7/21/23: Meeting #2 follow up
- 9/18/23: Project update
- 10/26/23: Project update
- 10/27/23: DEIR availability
- 10/31/24: Outreach meeting #3 invite
- 11/19/24: Meeting #3 follow up

Project Website: www.eddiejonesproject.com

A project informational website was launched January 19, 2023, following the second public meeting. Since inception, there have been 1,900 unique visitors to the website (figure F). The site provides details of the plans and allows members of the community to RSVP for upcoming events and contact the team. The site

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

also includes a Frequently Asked Questions (FAQs) page, which was updated as questions were submitted by the community. Summaries and exhibits from all public meetings are posted to the website.

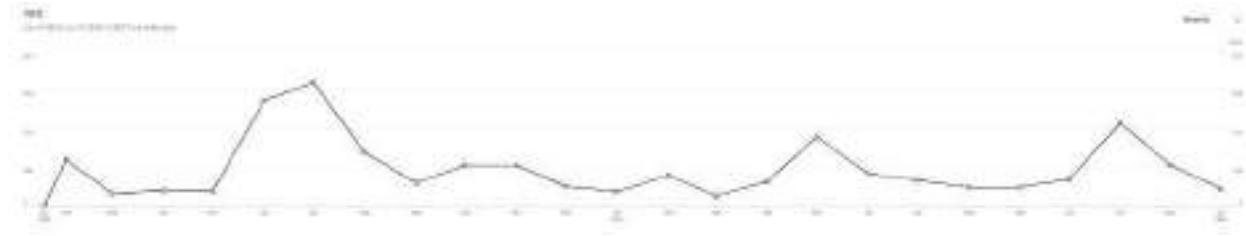


Figure F: Website Analytics (Jan. 2023 – Jan. 2025)

Public Meetings

RPG has hosted four public meetings on the project.

Public Meeting #1 – August 3, 2022

The first public meeting was a joint informational meeting and CEQA Scoping Meeting. 22 registrants signed in (exhibit 5) although we estimate there were 25-30 people present. The presentation materials are included herein as exhibit 6. Comments resulting from the meeting were incorporated into the CEQA process.

Public Meeting #2 – January 17, 2023

A second neighborhood meeting was hosted on January 17, 2023, via Zoom. Residents were invited to the meeting via postcard (exhibit 3).

There were 107 unique registrants (exhibit 7). However, some members of the community reported hosting watch parties with their neighbors since it was a virtual meeting.

Arlene Tendick, community outreach manager for the project, presented an overview of the site constraints and context, and project components and features (exhibit 8).

Members of the project team, including the architect and traffic consultant, were present to discuss their areas of expertise in greater detail. The presentation was structured to specifically address the five focus areas presented to us by members of the public (exhibit 9).

Residents were asked to submit all comments through the virtual portal to better organize the discussion (exhibit 10). For example, there were many questions about traffic and circulation so we combined many of those so the traffic engineer could speak comprehensively to address questions and concerns and avoid duplication. The question-and-answer period lasted roughly 90 minutes, and all questions/comments were addressed. Comments that were not addressed were those that had either already been answered or ones that were not productive (i.e. calling the project team names).

The graph below summarizes the key themes (figure G). The questions and meeting themes provided the basis for the publication of a Frequently Asked Questions page on the project website:

www.eddiejonesproject.com/faqs.

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

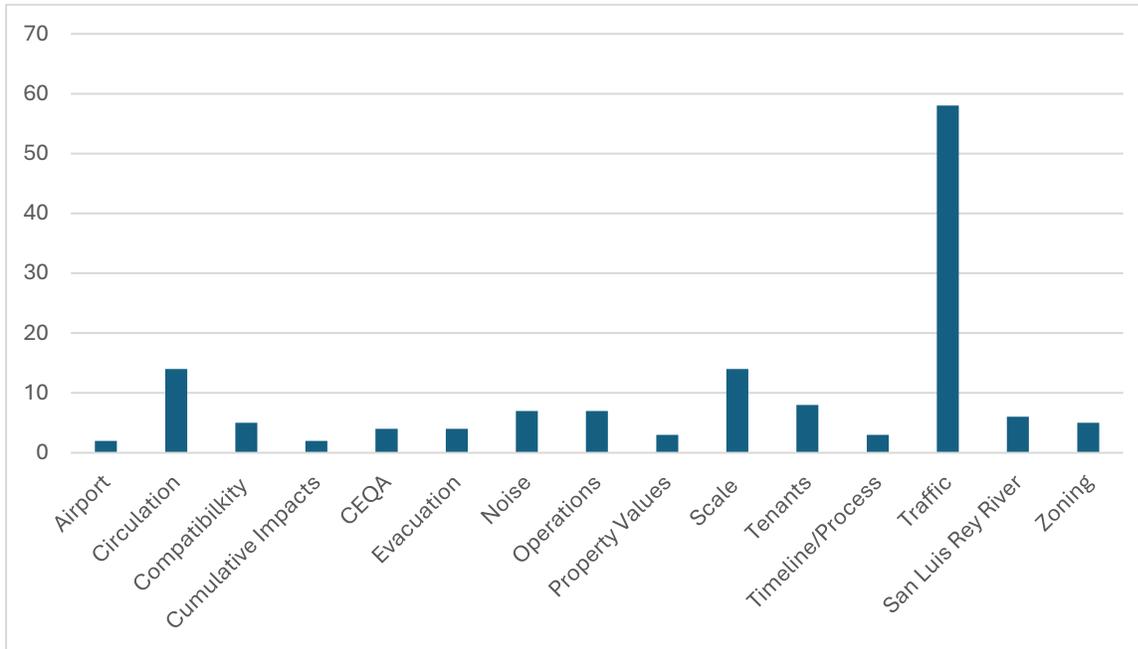


Figure G: Meeting #2 Comment Summary

Public Meeting #3 – July 12, 2023

The third community meeting was hosted at the Prince of Peace Abbey. A zoom link was also provided for those that could not make it in person, although Wi-fi was limited at the Abbey. The meeting included a brief project overview and update, and focused on three primary topics: tenants, trucks, and traffic/circulation. The presentation slides (exhibit 11) and meeting recording were posted to the project website: www.eddiejonesproject.com/712-summary.



Attendees were provided index cards to submit their comments throughout the meeting (exhibit 12). The evening concluded with nearly two hours of questions from attendees. Questions were primarily about traffic, emergency evacuation, air quality and noise, and community compatibility.

68 people attended in person, and an additional 42 participated online (exhibit 13).

Public Meeting #4 – November 14, 2024

An email invite was sent to more than 400 residents on October 31, 2024 to notify interested parties of RPG's decision to add a four building option (figure D). The option is in response to community feedback and DEIR comments. The meeting was hosted via Zoom. We requested questions in advance to help guide the

Project Numbers: D22-00001, CUP22-00001, V22-00001

Project Name: EDDIE JONES WAREHOUSE, MANUFACTURING, AND DISTRIBUTION FACILITY

Project Planner: ROBERT DMOHOWSKI

discussion up front. Questions were also accepted through the Q&A portal and responded to at the end of the meeting (exhibit 14). Presentation slides are included herein as exhibit 15 and also posted to the project website.

Meeting 4 had 59 attendees (exhibit 16).



Figure H: Project Comparison. Project Proposal (left) v. Four Building Option (right)

One on One and Small Group Meetings

Understanding that questions will arise outside of formal public meetings, members of our team have also been available for one-on-one or small group meetings with neighboring property owners and members of the community.

Additionally, the project team has and will continue to reach out to local stakeholder groups and nonprofits for project update presentations and briefings, including Oceanside Chamber of Commerce, North County Economic Development Council, Preserve Calavera, Buena Vista Audubon Society, Sierra Club, Oceanside Speaks Out (project opposition also known as The Blue Crew) and Wanis HOA. Interested parties are also able to request presentations.

Eddie Jones Community Outreach Report Exhibit Index

Supporting Figures

- Figure A – Project Location
- Figure B – Existing Site Zoning
- Figure C – 1,500-foot notification boundary
- Figure D – Expanded notification boundary
- Figure E – Airport Expansion Plan
- Figure F – Website analytics by month (Jan. 2023 – Jan. 2025)
- Figure G – 1/17/23 Meeting comment summary
- Figure H – Project Comparison

Outreach Exhibits and Backup Materials

- Exhibit 1 – Outreach Timeline
- Exhibit 2 – Notice of Application (2/2/2022)
- Exhibit 3 – Project Update Postcard #1 (12/29/2022)
- Exhibit 4 – Project Update Postcard #2 (6/26/23)
- Exhibit 5 – 8/3/22 Registrants
- Exhibit 6 – 8/3/22 Slides
- Exhibit 7 – 1/17/23 Registrants
- Exhibit 8 – 1/17/23 Slides
- Exhibit 9 – Project concerns sheet provided by opposition
- Exhibit 10 – 1/17/23 Questions
- Exhibit 11 – 7/12/23 Slides
- Exhibit 12 – 7/12/23 Questions
 - a. Index cards
 - b. Virtual
- Exhibit 13 – 7/12/23 Attendees
 - a. In person
 - b. Online
- Exhibit 14 – 11/14/24 Questions
- Exhibit 15 – 11/14/24 Slides
- Exhibit 16 – 11/14/24 Attendees

Exhibit 1 – Outreach Timeline

- 8/3/22 – CEQA Scoping Meeting
- 11/9/22 – Oceanside Speaks Out (Dee Keck, Gene O’Neal, Kim Stone, Debby Wanamaker)
- 12/29/22 – [Project Update Postcard](#)
- 1/17/23 – Virtual Information Meeting
- 1/19/23 Website Launch
- 1/26/23 – Oceanside Speaks Out (Dee Keck, et. al.)
- 3/3/23 – Resident one on one meeting
- 6/26/23 – [Project Update Postcard 2](#)
- 6/29/23 – [Meeting Notification Email](#)
- 7/5/23 – Notice that a virtual option had been added
- 7/7/23 – Notice of new meeting location (Meeting was moved at the neighbors’ request)
- 7/12/23 – Project Update Meeting
- 7/21/23 – [Meeting Materials Email](#)
- 8/4/23 – Resident one on one meeting
- 9/18/23 – [Project Update Email](#)
- 10/26/23 – Project update email
- 10/26/23 – City [Notice of Availability](#)
- 10/27/23 – [Email Update](#) re: Public Circulation of [DEIR](#)
- 12/11/23 – Close of Public Comment period
- 1/23/24 – Preserve Calavera, Buena Vista Audubon
- 2/2/24 – Oceanside Speaks Out (Dee Keck, Darrin Selnick, et. al)
- 10/31/24 – [Email Invite](#) to Virtual Update
- 11/14/24 – Project Update Meeting
- 11/19/24 – Update meeting follow up
- 1/6/25 – Oceanside Speaks Out (Dee Keck, Dave Keck, Darrin Selnick, Gene O’Neal)
- 1/29/25 – Wanis Homeowners Association

- Exhibit 2 – Notice of Application (2/2/2022)



CITY OF OCEANSIDE

DEVELOPMENT SERVICES DEPARTMENT / PLANNING DIVISION

RECEIVED

MAR 02 2022

TLP6

NOTICE OF APPLICATION

Per City Council Policy 300-14, you should know that an application has been filed with the City of Oceanside for a discretionary entitlement project. Please see the project details below.

PROJECT NUMBER: D22-00001 & CUP22-00001

PROJECT NAME: EDDY JONES WAREHOUSE & DISTRIBUTION FACILITY

DATE OF APPLICATION: February 2, 2022

PROJECT DESCRIPTION: NEW WAREHOUSE AND DISTRIBUTION FACILITY WITH 526,280 SF OF WAREHOUSE SPACE AND 40,000 SF OF OFFICE SPACE ON THE FORMER TE CONNECTIVITY SITE

LOCATION: 250 EDDY JONES WAY

PARCEL NUMBERS: 145-021-29, 30 & 32

CONTACT NAME: DAN NIEBAUM, THE LIGHTFOOT PLANNING GROUP
(760) 692-1924

PROJECT PLANNER: ROBERT DMOHOWSKI, SENIOR PLANNER
(760) 435-3563, rdmohowski@oceansideca.org

The decision to approve or deny this application will be made by the Planning Commission. You will receive a notice of the Public Hearing at that time within 15 days of the Hearing Date. Additionally, you will be notified of any Environmental Notices that are posted for public review. Planning Commission decisions are appealable to the City Council, with an appeal made within 10 days after the date of the Planning Commission decision.

The project application and materials related to this project are available for public review on the City's website through e-Trakit at the following link:

<http://www.ci.oceanside.ca.us/gov/dev/planning/search.asp>

Use the project number D22-00001 for your search.

If you have any questions regarding this application, please contact the City of Oceanside Project Planner listed above.

- Exhibit 3 – Project Update Postcard #1 (12/29/2022)

EDDIE JONES

12.29.22 Project Update

Dear Neighbor,

Happy Holidays! We're writing to share a brief update with you on the proposed plans for the 31-acre industrial site at the corner of Eddie Jones Way and Benet Road.

The former industrial building on the site was demolished last month so we could begin to clean up the soil contamination that resulted from its past use. This remediation work is currently in process and not part of the proposed warehouse project.

The comments and feedback from the Scoping Meeting in August are being incorporated into the technical studies for the Environmental Impact Report (EIR). The public will have the opportunity to review the Draft EIR in the Spring and provide additional comments.

In the meantime, we'd like to invite you to an informational meeting on January 17th at 6:30 p.m. After speaking with members of the community, we understand there is a desire to learn more about the anticipated traffic impacts, safety and circulation, air quality and environmental mitigation, and the project's alignment with the City's vision for the area. Members of the project team will present on these topics, as well as answer additional questions about the proposed plans and the City's review process.

We also understand the request for increased transparency, so we've volunteered to extend the notification boundary beyond what's required by the City. In order to accommodate the larger group, this meeting will be held virtually. **We hope you can join us!**

We're still early in the process and are looking to introduce ourselves to those of you who were unable to join us back in August, share our progress and proposed changes, and hear your feedback.

We know that the land planning process generates questions and concerns, so feel free to email us at outreach@eddiejonesproject.com with comments or to request a meeting with a member of our team.

We look forward to meeting you.



Adam Robinson (property owner)
President
RPG

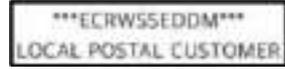


Eddie Jones
Informational Meeting
Join us January 17, 2023
from 6:30 - 8:30 p.m.

RSVP at
outreach@eddiejonesproject.com

A Zoom link will be emailed to you
for your convenience.

Project Milestones



We will be offering opportunities to provide feedback, ask questions, and learn more about the Project throughout the City's review and CEQA process. We encourage you to be part of the conversation. Our team is available for one-on-one and small group meetings or you can reach us by email at outreach@eddiejonesproject.com. We look forward to hearing from you!

start.

Jan. 2022 - Present

Planning and Design

The application review is an iterative process that can take several months and involve multiple application submittals. Each submittal incorporates project refinements in response to City comments and community feedback.

Aug. 2022

Info Mtg 1 + CEQA Scoping Mtg

The first info meeting was held on August 3, 2022. The meeting gave a brief overview of the project and provided the opportunity for public input on the scope of the Environmental Impact Report.

Nov. 2022

Community Outreach Begins

In November, we added a community liaison to the project team. The community liaison will be coordinating additional engagement opportunities and ensuring community feedback is shared with the technical team.

Jan. 2023

Info Mtg 2

RSVP at outreach@eddiejonesproject.com

Q1 - 2023

Continued Community Outreach

Unable to join us on the 17th? Our team is available for one on one and small group meetings.

Spring 2023

Public Review of Draft EIR

Respond to Comments

Finalize EIR

In accordance with CEQA, the draft EIR will be available for public review and comment. All comments will be responded to in the final EIR.

Fall 2023

Inform and Update

Join our email list to be notified of project milestones and upcoming meetings.

Oceanside Planning Commission

finish.

 Opportunity for public input

- Exhibit 4 – Project Update Postcard #2 (6/26/23)

EDDIE JONES WAREHOUSE

6.26.23 Project Update

Dear Neighbor,

We hope you're having a nice summer! We're writing to share a brief update with you on the proposed plans for the 31-acre industrial site at the corner of Eddie Jones Way and Benet Road.

Public circulation of the Draft EIR is delayed till late summer/early fall. The revised schedule is primarily due to some late project modifications that followed the January 17 informational meeting. The community will be noticed in accordance with CEQA once the document is available for public review.

In an effort to maintain transparency, we're hosting a second public meeting on July 12 at 6:00 at the Mission Library Community Room. This meeting will provide an update on our progress and present the project modifications we've made since our last meeting. Members of the project team and I will also be available to answer questions.

We hope you'll continue to be part of the process!

Thank you to those of you who have reached out to us directly. We know that the land planning process generates questions and concerns, so we encourage you to email us at outreach@eddiejonesproject.com with comments or to request a meeting.

We look forward to seeing you on the 12th,



Adam Robinson (property owner)
President
RPG



**Eddie Jones
Project Update Meeting**
Join us July 12, 2023
from 6:00 - 8:00 p.m.

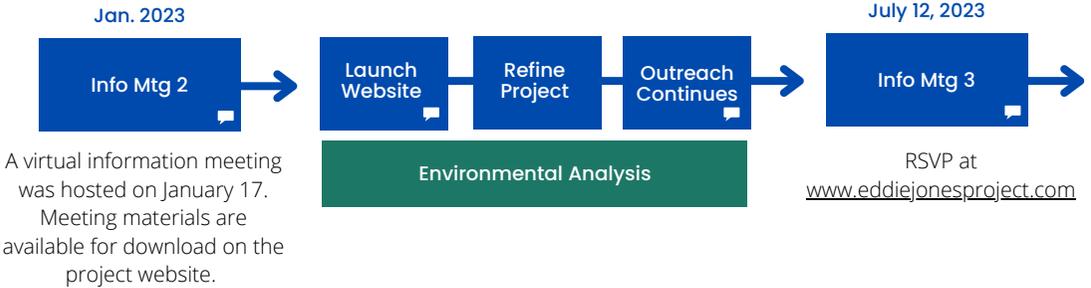
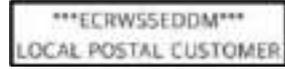
Details and RSVP at
www.eddiejonesproject.com



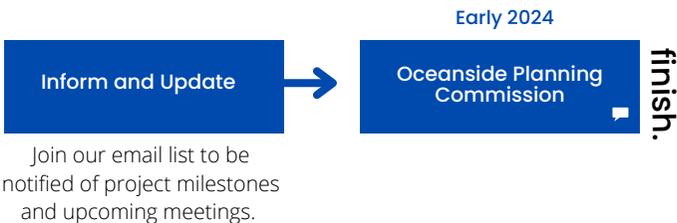
Learn more at www.eddiejonesproject.com

Project Milestones

We encourage you to be part of the conversation. Our team is available for one-on-one and small group meetings or you can reach us by email at outreach@eddiejonesproject.com.



In accordance with CEQA, the draft EIR will be available for public review and comment. All comments will be responded to in the final EIR. The City of Oceanside is the lead agency for the EIR. The document is prepared by a third-party CEQA consultant with City review/oversite.



 **Opportunity for public input**

- Exhibit 5 – 8/3/22 Registrants

SCOPING MEETING – EDDIE JONES INDUSTRIAL PROJECT

6:00 p.m. on August 3, 2022

Civic Center Library Community Room, located at 300 North Coast Highway in the City of Oceanside

Name (please print)	Mailing Address (optional)	E-mail (optional)
GENE R. O'NEAL JR	478 TISHMAL CT O'SIDE 92058	HP11325@YAHOO.COM
CAROL MCCONNELL	3172 Toopal	
JAYNE MOYNIHAN	3	
SUSAN J. MORA	3271 Toopal DR	
Bill Tingleff	648 Wala Dr	
Jolie Tingleff	648 Wala Drive	
PAUL TERWILLIGER		
Bobbi Schidtt	3203 Toopal DR	BobbiSchidtt@yahoo.com
Rick Schidtt	3203 Toopal DR	" "
Doug Johnson	1341 DEERBROOK SAN MARCOS	
Bill Cornett	570 Toopal CT	beornett@cox.net
Long Nguyen	8595 Wala Drive	
MARK NTAKES	475 TISHMAL CT 92058	MBA/NTAKES@COX.NET
Bill Quiscaberry	1123 COMBRIK COUNTRY	
JOHN BUCALO	471 TISHMAL CT	BRACKETT@SPECIALTIES@GMAIL
MARCELA VALDEZ	795 WALA DR	mvaldes@cox.edu
ROSAS	592 MAYLA CT	Ajrosas4@yahoo.com
HANSEN'S	3281 TOOPAL DR	RANDY@HANSEN19@GMAIL.COM
DANIEL LIUVONSKAS	746 WALA DR	dudil1@hotmail.com
James Hill	774 Wala Dr	jehill6@yahoo.com
Manique Renard	3288 Canyon View DR	
Paul Zentari	3288 Canyon View DR	

- Exhibit 6 – 8/3/22 Slides

Eddie Jones Warehouse, Manufacturing & Distribution Facility Project

Notice of Preparation (NOP) Scoping Meeting &
Community Presentation

August 3, 2022

Meeting Purpose and Agenda

PURPOSE

To obtain feedback on the environmental concerns and proposed project to be addressed within the Environmental Impact Report (EIR)

AGENDA

- Proposed Project Overview
- Airport Land Use Plan
- Project Design
- Pedestrian Improvements
- Project Benefits
- Environmental Review Process
- Environmental Issues to be Covered
- Public Comments



Proposed Project Overview

31.79-acre site located at 250 Eddie Jones Way. Located within the Airport Neighborhood Planning Area, bound by Oceanside Municipal Airport to the south, Benet Road to the west, the San Luis Rey River and recreational trail to the north, and vacant light industrial land to the east.

Consists of demolition of the existing vacant 172,300-square-foot industrial building and proposes development of a new 566,905-square-foot warehouse and distribution facility

The project would maintain a 100-foot buffer from the edge of the San Luis Rey River riparian habitat along the project boundary's northern edge.

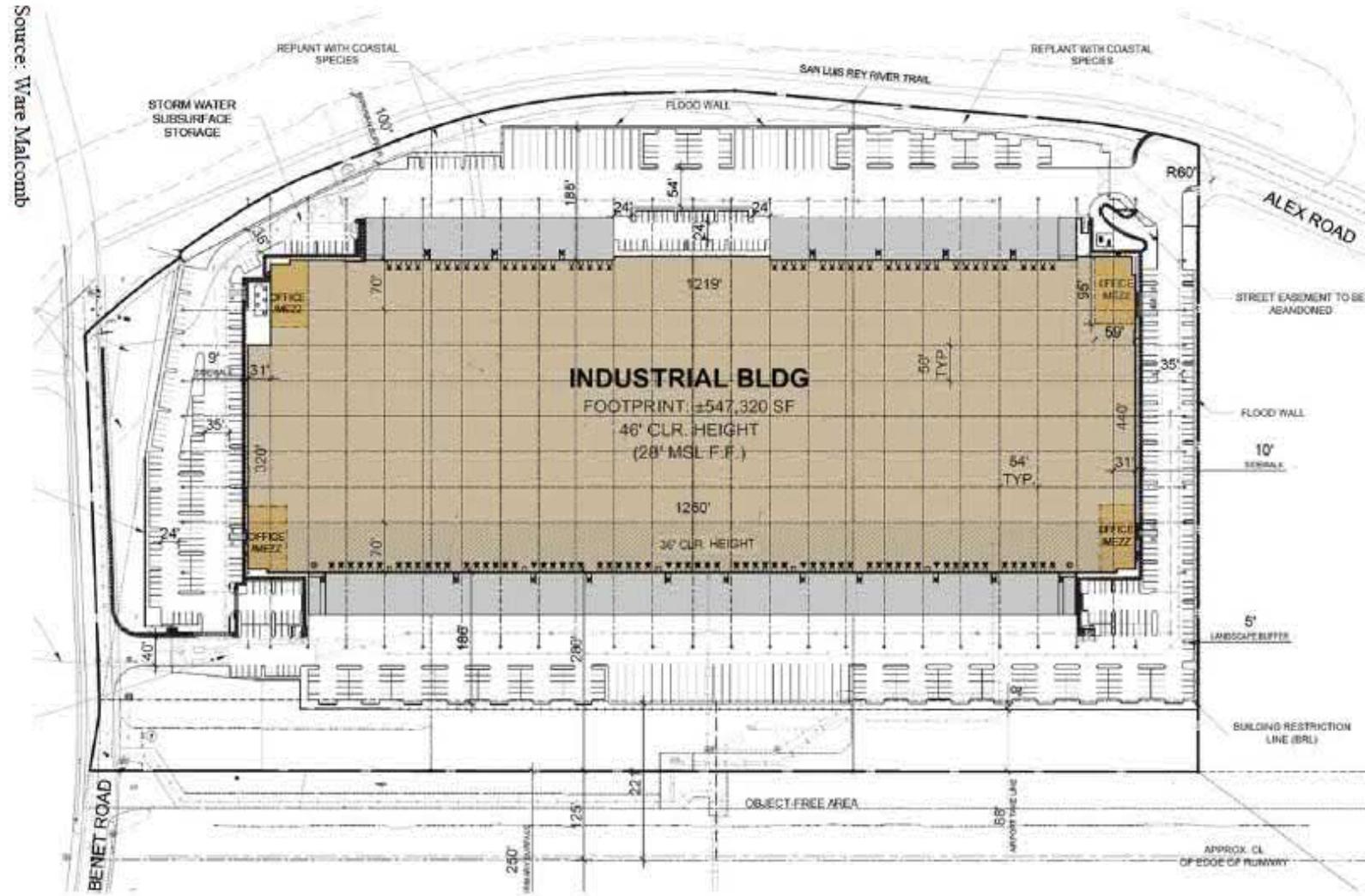
The proposed warehouse and distribution facility is classified as a "Wholesaling, Distribution, and Storage Facility" use by the Oceanside Zoning Ordinance. A CUP and Variance are required

Arial View of Existing Site Looking East



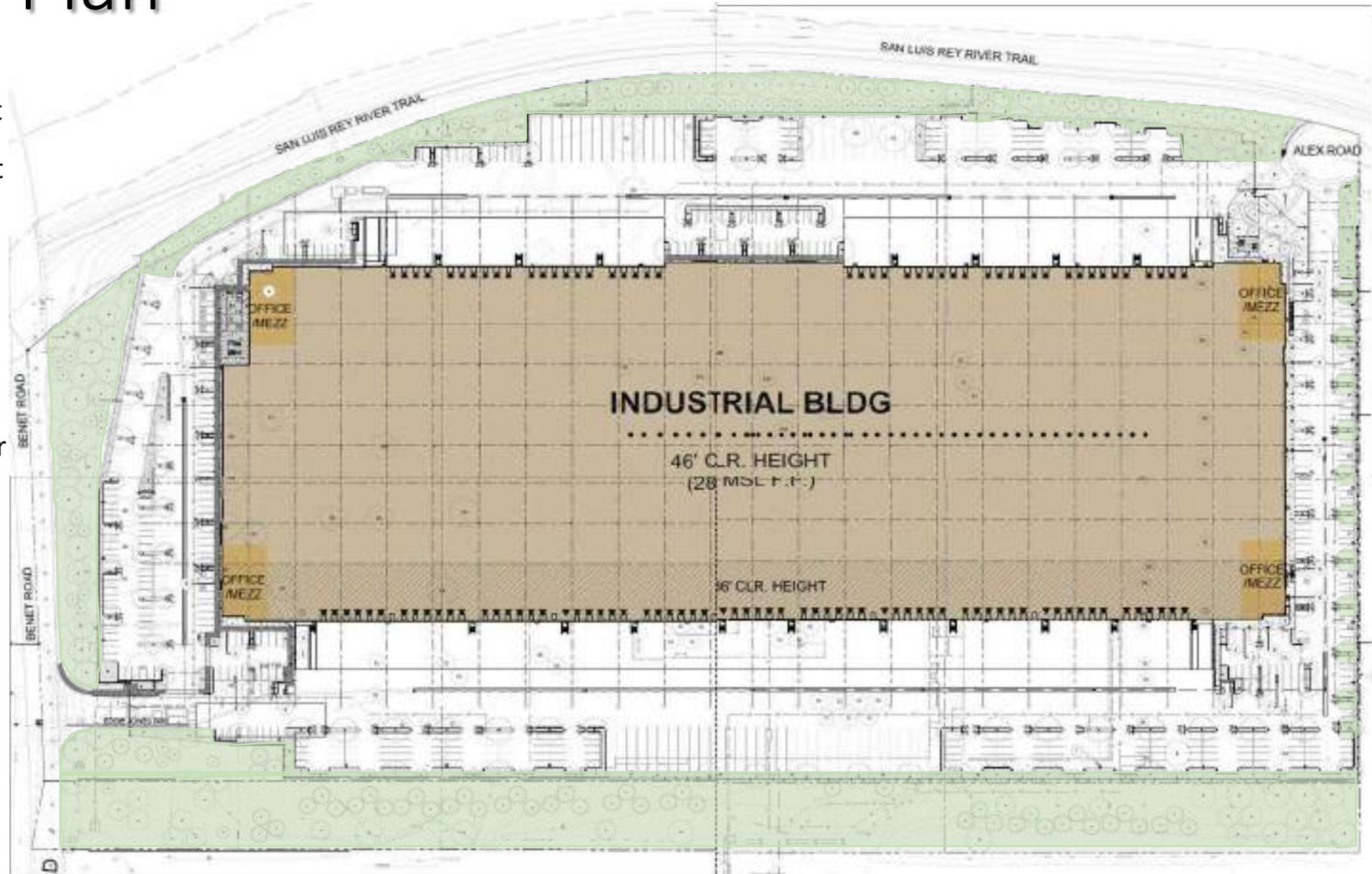
Proposed Project Overview

- 566,905-sf warehouse and distribution facility consisting of 369,415-sf of warehouse area, 158,320-sf of manufacturing space, and 39,170-sf of office area
- 590 parking spaces for employee/visitor parking, 60 truck trailer parking stalls and vehicle circulation area
- 114 truck terminals
- Access would be maintained and improved from Alex Road and Benet Road. Alex Road access would be limited to passenger vehicles while heavy truck traffic would be limited to the Benet Road access point
- Landscaping Designed throughout site – Significant buffer designed along northern boundary adjacent to SLR River Trail



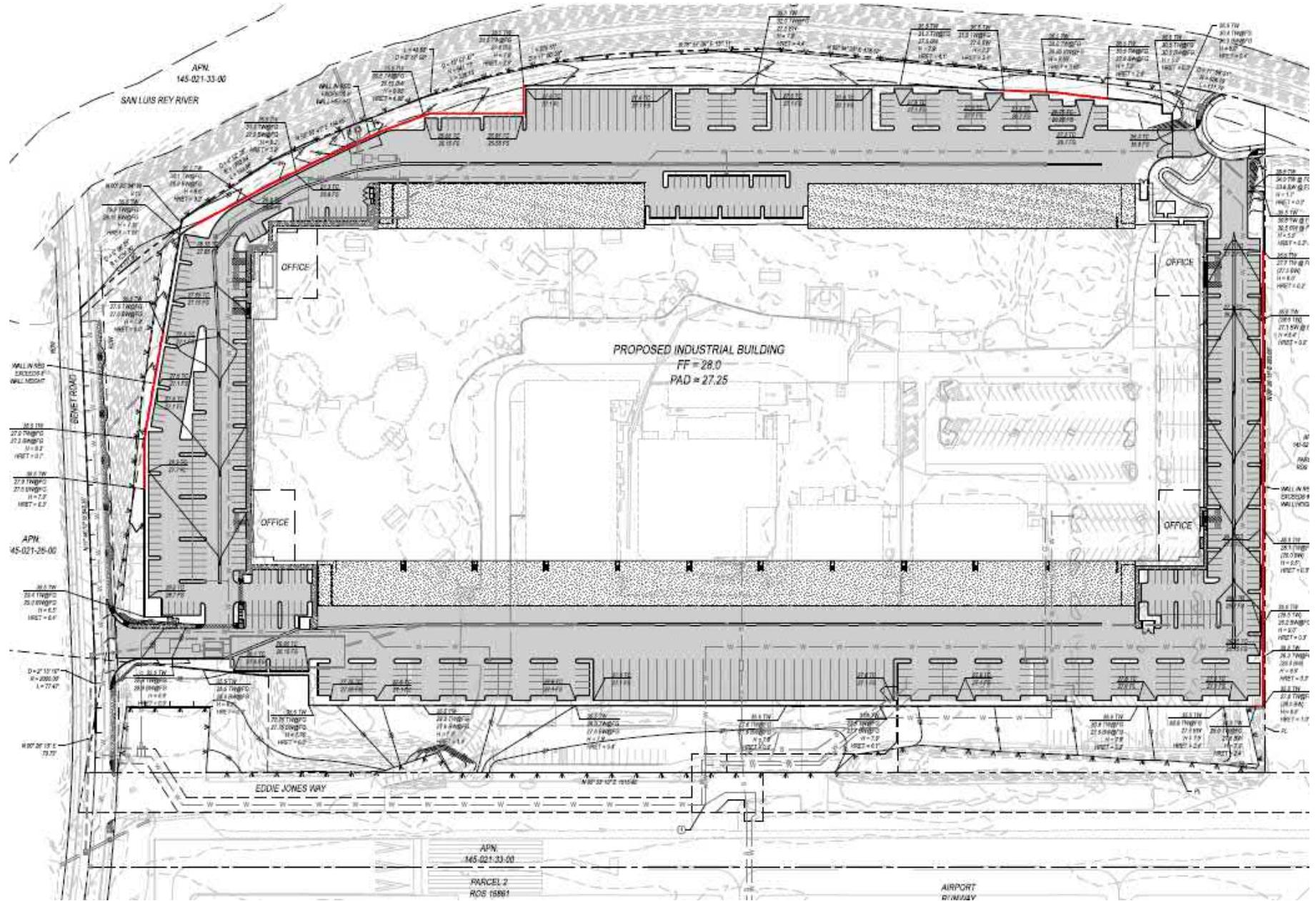
Landscape Plan

- Landscaping Designed throughout site – Significant buffer designed along northern boundary adjacent to SLR River Trail
- 21% of site landscaped - Only 12% min. required
- Landscaping along the entire northern site boundary designed to buffer site and screen parking, circulation and loading areas.
- Landscape along Benet Rd. and Alex Rd. will provide upgraded streetscapes
- Perimeter trees feature California Sycamore, Coast Live Oak, and Bay Laurels



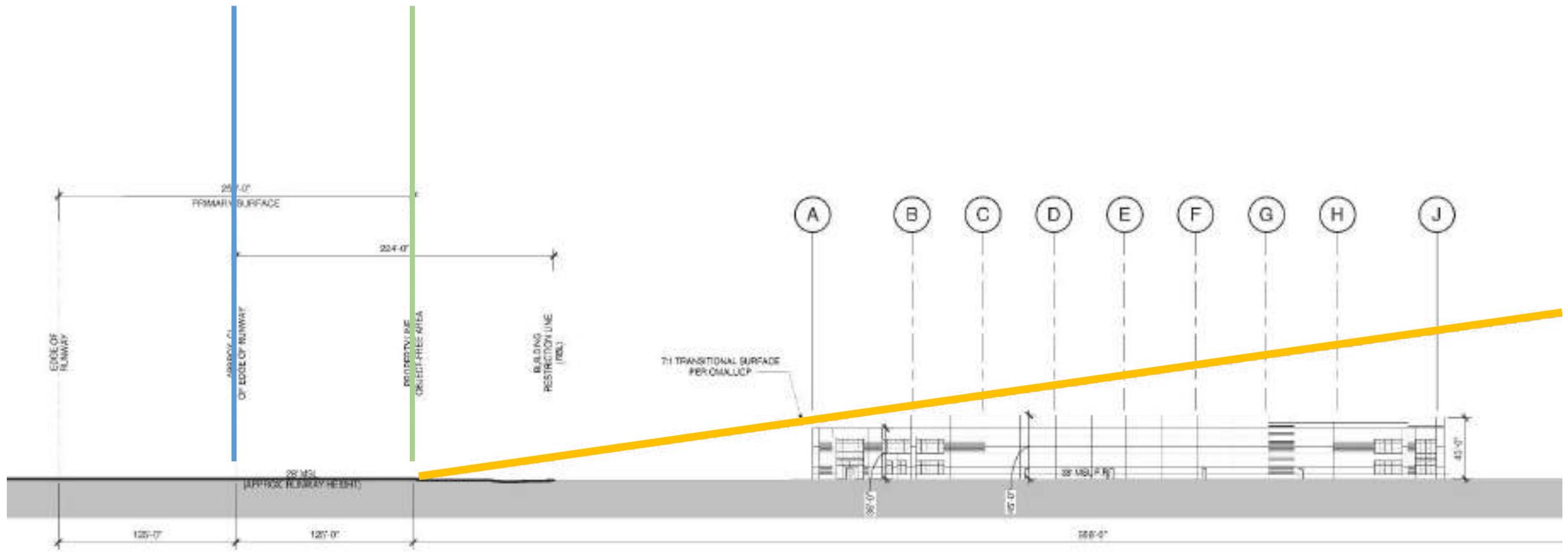
Civil Plan

- Storm Water Design
- 23% Permeable Surface
- Perimeter Floodwall
- Stormwater Vaults



Airport Land Use Plan –

Runway Protection - Height / Transition Zones



Project Design



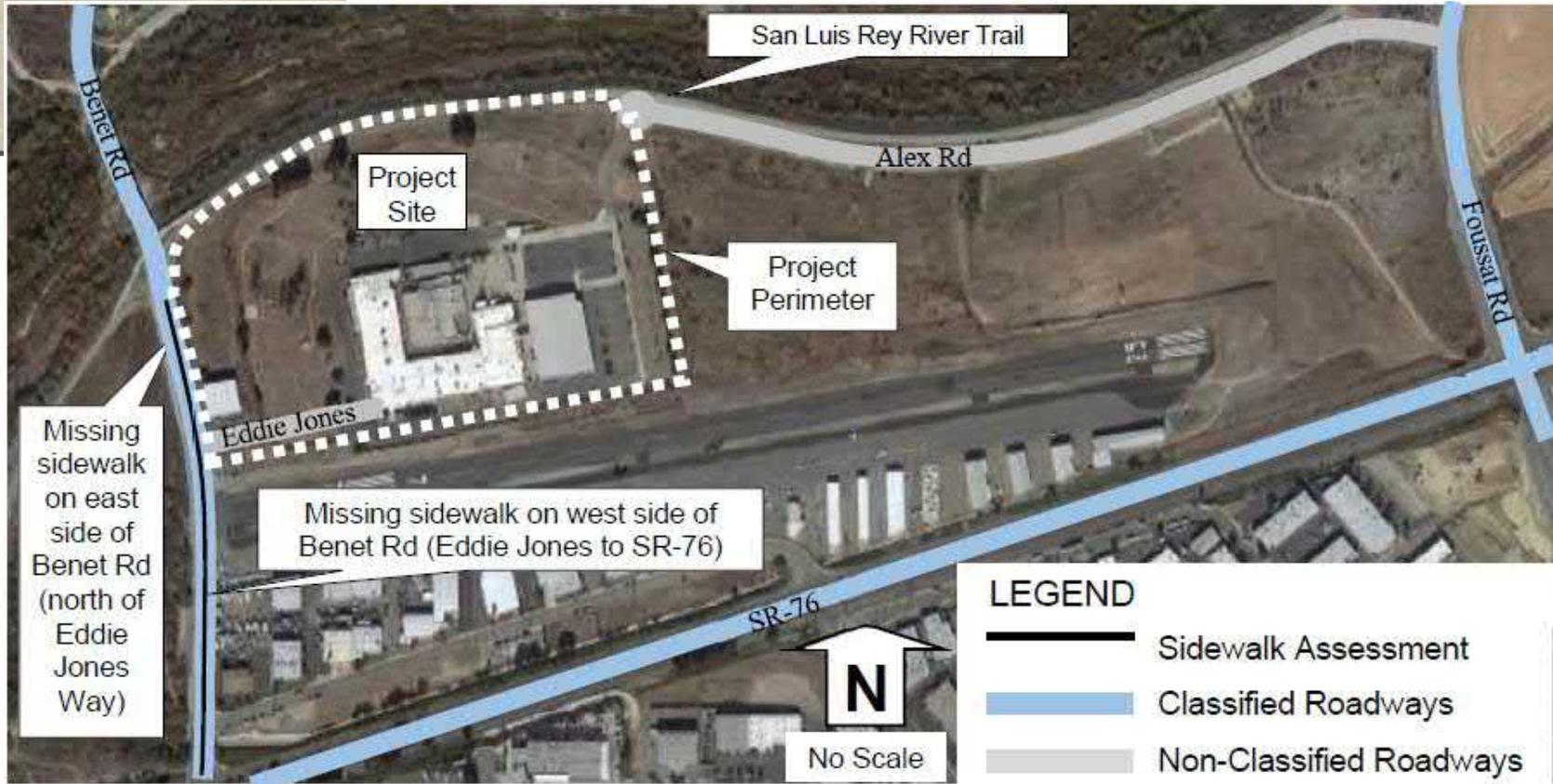
- Contemporary light-industrial architecture
- Complementary materials, finishes and colors across all building elevations
- Vertical & horizontal accent banding and color details integrated with canopy features across wall areas
- Clerestory windows along upper level



Pedestrian Improvements



Complete Sidewalk along Benet Road



Project Benefits

Consistent with the existing General Plan designation and zoning for the site

Redevelops a vacated and vandalized existing industrial development with a new industrial development

Provides additional jobs and related economic activity within the City

Maintains the 100-foot buffer from the edge of the San Luis Rey River riparian habitat, and the project has been designed to specifically avoid biological and planning buffers

The project would be compatible with the Oceanside Municipal Airport Land Use Compatibility Plan

Improvements on-site would be incorporated including landscaping, stormwater features, site access, and replanting of biological buffer areas with native coastal species

Environmental Review Process

Purpose and Objectives of the California Environmental Quality Act (CEQA)

- Inform the public and decision makers about the project and potential environmental impacts
- Provide an opportunity for the public and local/state agencies to comment on the environmental issues
- Identify feasible ways to avoid or reduce environmental impacts
- Consider alternatives that reduce or avoid impacts
- Disclose significant and unavoidable impacts

The Environmental Impact Report Review Process



Environmental Review Process

EIR Process Schedule

- **Notice of Preparation Published:** *July 20, 2022*
- **Close of Scoping Period:** *August 18, 2022 at 5:00 pm*
- **Notice of Preparation Scoping Meeting and Community Meeting:** *August 3, 2022*
- **Estimated Future Schedule:**
 - **Public Review of Draft EIR:** *Fall 2022*
 - **Final EIR:** *Winter 2022/2023*
 - **Project Hearings:** *Spring/Summer 2023*

Environmental Issues Covered in the EIR

- Aesthetic/Visual
- Agriculture Land
- Air Quality
- Archeological/Historical
- Biological Resources
- Drainage/Absorption
- Flood Plain/Flooding
- Geology/Seismic
- Minerals
- Noise
- Population/Housing
- Public Services/Facilities
- Recreation/Parks
- Schools/Universities
- Sewer Capacity
- Soil Erosion/Compaction/Grading
- Solid Waste
- Toxic/Hazardous
- Traffic/Circulation
- Water Quality/Supply/Groundwater
- Growth Inducement
- Land Use
- Cumulative Effects

Public Comment Period on the NOP

Comments must be received by 5:00pm on Thursday August 18, 2022

How to Comment on the environmental scope of the EIR:



At the meeting today
(comment letter form)



Email comments to:
Rob Dmohowski
rdmohowski@oceansideca.org



Mail comments to:
Rob Dmohowski
300 N. Coast Hwy.
Oceanside, CA 92057

- Exhibit 7 – 1/17/23 Registrants

Attendee Report: Eddie Jones Community Meeting**# Registered Participants: 115**

Webinar ID: 891 7443 6085

Start Time: 1/17/2023 6:02:00 PM

Duration (minutes): 215

Panelist Details

User Name	Email	Time in Session (minutes)
Arlene Tendick (outreach)	arlene@ant-sc.com	215
Molly Walters (outreach)	molly@ant-sc.com	208
Brian Koshley (architect)	bkoshley@waremalcomb.com	196
Maria Fernanda - Cabrera (architect)	mcabrera@waremalcomb.com	195
Jessica Logerberg (architect)	jlogerberg@waremalcomb.com	195
Justin Rasas (traffic)	justin@loengineering.com	192
Dan Niebaum (planning)	dan@lightfootpg.com	196
Adam Robinson (owner, applicant)	adam@rafpg.com	193

Attendee Details

User Name (Original Name)	Email	Time in Session (minutes)
Al Fanchin	afanchin@yahoo.com	107
Alan Waite	batcar@sbcglobal.net	157
Alison Topaloglu	atopaloglu@cox.net	100
Alyce Budde	ajbudde@cox.net	126
Anna Floyd	akasperowicz@yahoo.com	157
Anne Antique	anie1267@gmail.com	44
Brad	susanellenberg@outlook.com	72
Brad Mayo	Bradmayohoa@hotmail.com	156
BRANDY BRAGG	aloha8scooby@yahoo.com	155
Brittney Serrato	bnserrato5@gmail.com	79
Carol Broeland	menaeeee@aol.com	279
Carol McConnell	cmccConnell4444@gmail.com	151
Carol Steffen	cs22620@gmail.com	34
Carol Stone	stone2ofus@sbcglobal.net	110
cecilia moreno	morenocecilia6196@gmail.com	199
Cecilia Frierson	CeciliaFrierson.hoa@gmail.com	75
Celerina Cornett	ccornett@ucsd.edu	186
Chris Kim	Chrskim@gmail.com	172
Cody Powers	Jmaccabeus1@gmail.com	152
Cynthia Reemtsma	Cynthia.gladstone@gmail.com	33
D Keck	4des@cox.net	183
Dan and Amy Noack	amy.noack@me.com	161
Darin Selnick	darin.selnick@gmail.com	149
Darren Parsons	dtpmb@yahoo.com	109
David Baurac	dbaurac@gmail.com	142
David Gaul	Davidgaul@cox.net	129
David Kertenian	dkertenian@yahoo.com	84
DAVID TAUB	david.m.taub@gmail.com	34

Debby Herbert	kayadeb36@outlook.com	192
Demetrius Steele	d_steele_8@yahoo.com	130
Dilan Good	dilangood@gmail.com	71
Donald Pham	dphamk25@yahoo.com	85
douglas smith	smithdp5121@gmail.com	197
Drew	Dstern@mackregroup.com	28
Drew Andrioff# Core Management	andrew@core-mgmt.com	170
Drew Stern	Dstern@mackregroup.com	94
Dua Pham	dp999_98@yahoo.com	140
E J	Ejorce@oceansideca.org	185
Emily Ramsey	emilyeramsey@gmail.com	41
ernesto McCallick	ekraccbiz@outlook.com	164
Ernesto Torres	viii.neptune@gmail.com	82
FOX 5 San Diego	kdagle@fox5sandiego.com	184
Fran Cusano	Fran.cusano@gmail.com	124
Gary Wright	marlmony999@cox.net	79
Gene and Carrie O'Neal	hp11325@yahoo.com	158
Gretchen Gary	gretchengary@gmail.com	159
J M	Iris.sealass@outlook.com	12
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Jay Steinbrecher	jsteinbrecher@nexstar.tv	45
Jeremy Ramsey	jrscocer3431@gmail.com	44
Jochen Kressler	clalber_lang@yahoo.com	52
joey ludwiczak	joelud@gmail.com	50
john chu	johnchu4@deloitte.com	62
John Guthrie	jguthrie@cox.net	48
Joseph Edwin Jenkins	ejenkins@sprintmail.com	196
JP Roberts	jpr@mail.org	69
Judy Glasser	glassersallee@cox.net	115
Karen Kelly	tlcgrl@yahoo.com	185
Kasia Gregorczyk	kgregorczyk@nexstar.tv	22
Kaye Thompson	kayet51@gmail.com	97
Kenneth Cassman	kgclconsulting@gmail.com	47
Kevin & Janet Nolan	hislid07@gmail.com	181
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Leilani Bost	leilanib48@gmail.com	197
Linda Hagen	lhagen624@gmail.com	105
Lisa Urbach	lurbach3@gmail.com	164
Maria Palomares	marlaguadalupe97@live.com	59
Mark Macfarlane	pls.markmacfarlane@gmail.com	138
Mark Noakes	mbnoakes@cox.net	120
Marty Basham	PRTYMRTY@HOTMAIL.COM	147
MELINDA Soto	Hudsonx4@sbcglobal.net	21
Melissa Flores	madubin1214@yahoo.com	85
Micaela Canton	Cantonmicaela@gmail.com	105
Michael Caltabiano	local1234@hotmail.com	188
midlandrunner	mikekosec@gmail.com	45

Mike Tenhover	Nanopole@yahoo.com	186
Mikhael Madello	mikhaelmadello@gmail.com	31
Nadine Scott	nadia550@sbcglobal.net	78
NEIL HANCOCK	tahoe772@yahoo.com	183
Patrice Smith	patandclancy@gmail.com	54
Patrice Smith	patandclancy@gmail.com	4
PATRICIA BROWN	1patriciabrown@gmail.com	132
Patricia Frazier	patanth5@gmail.com	159
Patti Kirchwehm	pkiich00@yahoo.com	154
Raymond Ho	raymondhho@gmail.com	174
Richard Eason	reasonr1@yahoo.com	142
Richard Mortimer	dickmortimer@cox.net	84
Rick Adams	dadadams@hotmail.com	138
Rick Empson	rickempson@hotmail.com	102
Rick Nafis	RNCarlsbad@gmail.com	108
Rob Regan	Rob@regandist.com	115
Robert & Judith Kale	judykale@gmail.com	188
Robert Dmohowski	rdmohowski@oceansideca.org	189
RONALD STEFFEN	22287Mulholland@gmail.com	126
Russell Frierson	russfrierson@grpfinancial.com	118
Sandra Smith	Sandraksmith8120@gmail.com	181
Scott Denham	sdenham@mackregroup.com	10
Sergio Madera	smadera@oceansideca.org	177
Sheryl Bannister	rosychild517@gmail.com	77
Stephanie Dimakides	Stephanie.Dimakides@gmail.com	142
steven alvarez	steven@sa2consultants.com	182
Susan Mora	smora2525@gmail.com	130
Thomas Woessner	tomwoessner@gmail.com	73
Tom Ventola	tomventola@caventola.com	119
Virginia Spencer	ginspencer@hotmail.com	49
Wanda Smith	Wandahelpsu@gmail.com	4
William Molineaux	mxoillyjo@gmail.com	62

- Exhibit 8 – 1/17/23 Slides



EDDIE JONES

WAREHOUSE, MANUFACTURING &
DISTRIBUTION CENTER

COMMUNITY INFO MEETING
JANUARY 17, 2023



Meeting Agenda

Jan. 17, 6:30 – 8:30 p.m.

01

Project Milestones

Provide an overview of the City and CEQA review processes, and share opportunities to provide input

02

Project Vision

Provide rationale for project and the vision for the site

03

Site Constraints and Opportunities

Review land use designations and community contact

04

Project Overview

Discussion of the project components and design features

05

Issues of Concern

Project consultant team to discuss the topics we've heard the most about

06

Q&A

▼ Project Vision

- **Who is RPG?**

Local, north county developer, known for our state-of-the-art, creative Industrial, mixed-use and office facilities

- **Why this project?**

There's a market need for this type of facility in Oceanside, specifically, and North County in general

At the end of 2022, 99% of industrial facilities in Oceanside were occupied.

- **What's going on now?**

Demolition of former industrial building and soil remediation



▼ Site Opportunities + Constraints

- **Location**

30.79 acres located on Eddie Jones Way at Benet Road within the City's industrial district



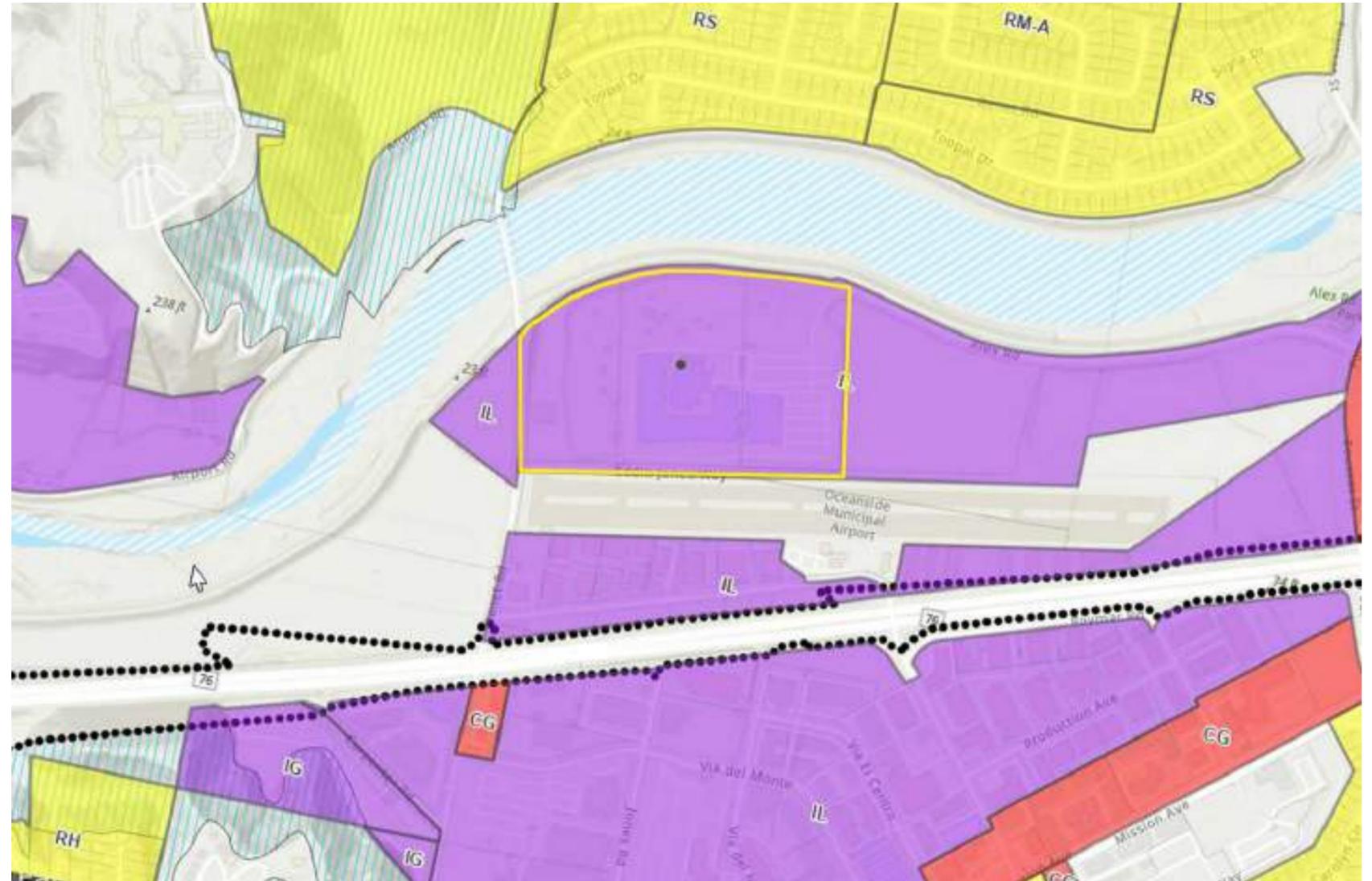
▼ Site Opportunities + Constraints

- **Location**

30.79 acres located on Eddie Jones Way at Benet Road within the City's industrial core

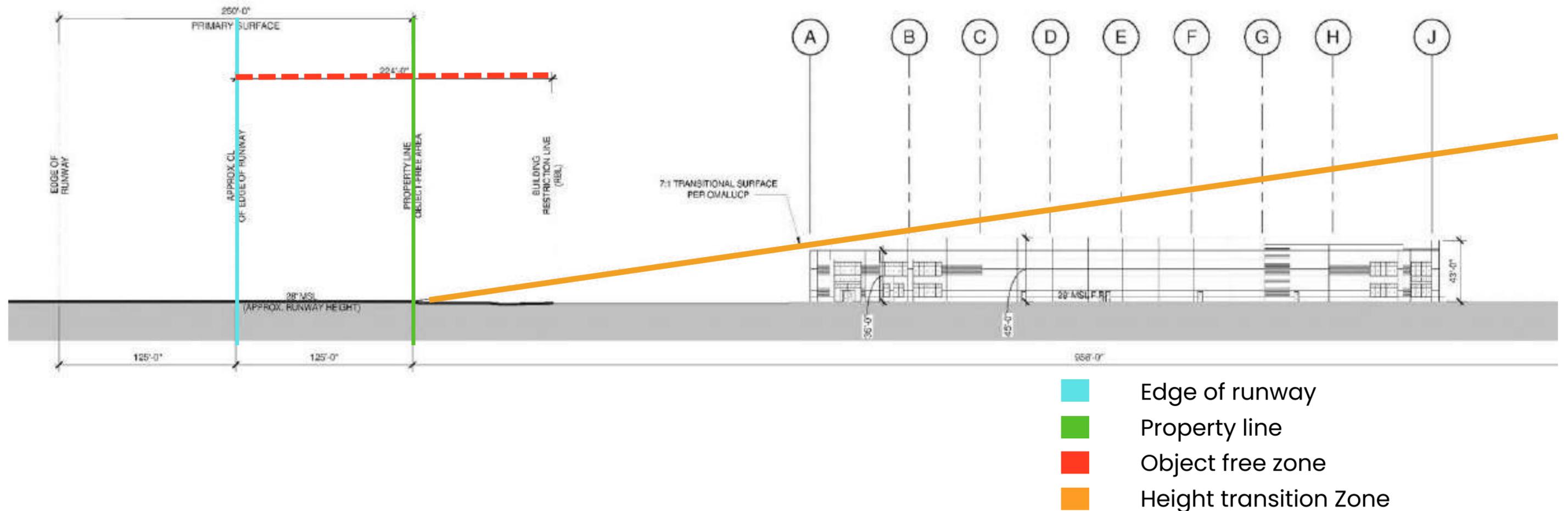
- **Land Use Designations**

- Currently Zoned IL - Light Industrial
- Project aligns with historical use as industrial
- We are not requesting a zone change or General Plan Amendment



Site Opportunities + Constraints

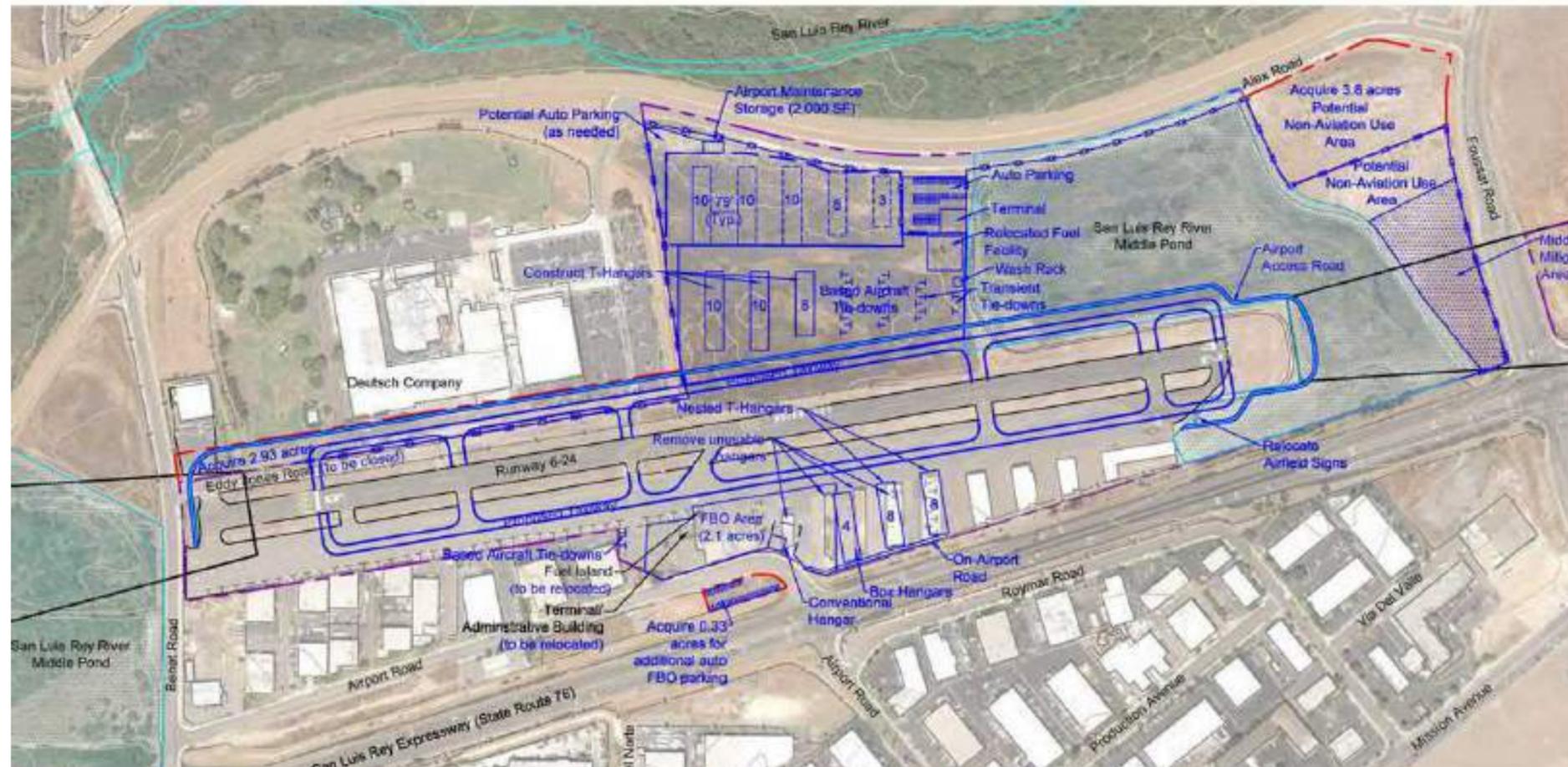
- **Site Constraint 1 – Adjacency to the Airport**
 - Airport Land Use Plan – Runway Protection – Height / Transition Zones



Site Opportunities + Constraints

- **Site Constraint 1 – Adjacency to the Airport**

- Airport Land Use Plan – Runway Protection – Height / Transition Zones
- Airport Master Plan Expansion



Site Opportunities + Constraints

- **Site Constraint 1 – Adjacency to the Airport**
 - Airport Land Use Plan – Runway Protection – Height / Transition Zones
 - Airport Master Plan Expansion
- **Site Constraint 2 – Proximity to San Luis Rey River**
 - Flood Zone – flood wall avoids need to raise the site
 - 100 foot buffer

- Flood zone
- 100-foot buffer



▼ Project Overview

The proposed facility incorporates industry best practices to provide a modern yet functional industrial space that could support multiple tenants.

Building Footprint: 547K SF

- 65% Warehouse (+ 114 truck bays)
- 28% Manufacturing
- 7% Office

21% of site landscaped

- Exceeds requirement of 12%
- Designed to buffer site and screen parking and loading areas

Circulation improvements

- Completes the missing sidewalk on the east side of Benet Rd.

Operations Management Plan

- Will vary based on ultimate users
- Will include business operation details, hours of operation, security protocol, and property maintenance
- Good neighbor policy detailing delivery vehicle schedules, truck haul routes, noise mitigation, best management practices, and protocol to address any concerns from neighboring property owners or tenants



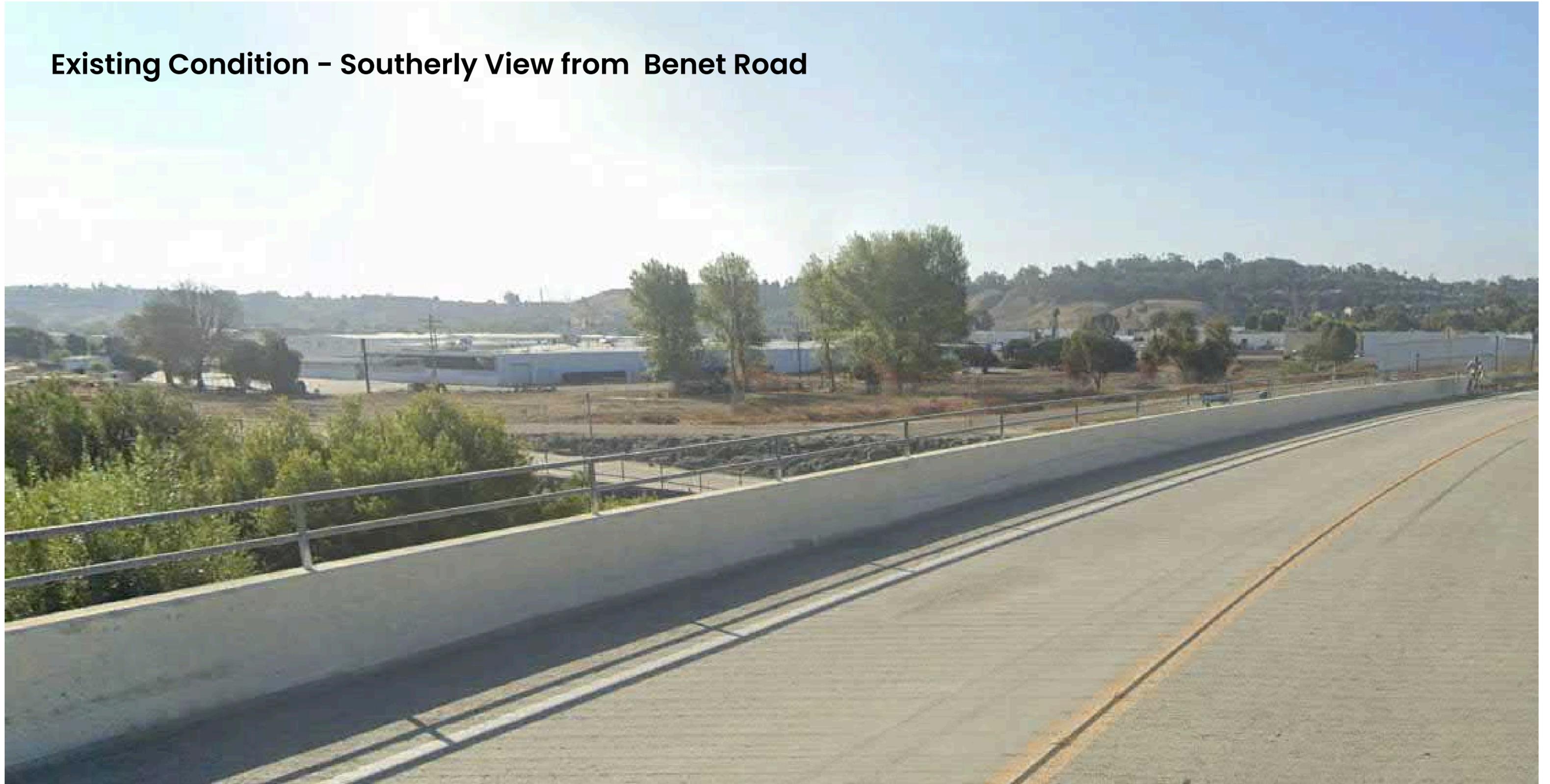
Existing Condition - View from Tishmal Court



Simulation 1 - View from Tishmal Court



Existing Condition - Southerly View from Benet Road



Simulation 2 – Southerly View from Benet Road



▼ Aesthetics

- Contemporary light-industrial architecture
- Complementary materials, finishes and colors across all building elevations
- Vertical & horizontal accent banding and color details integrated with canopy features across wall areas
- Clerestory windows along upper level

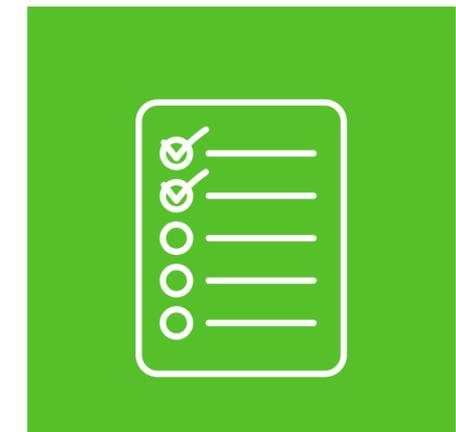


▼ What We've Heard

The primary goals of the community engagement process are to identify opportunities to refine the plans based on feedback, and to share accurate and timely information so residents are informed.

The five topics that have been shared with us the most are:

- Transparency
- Traffic and Circulation
- Safety and Emergency Services
- Environmental Considerations
- Alignment with City Vision



▼ Transparency

- **Building Trust**

We are committed to providing accurate and consistent information.

- **Expanding Reach**

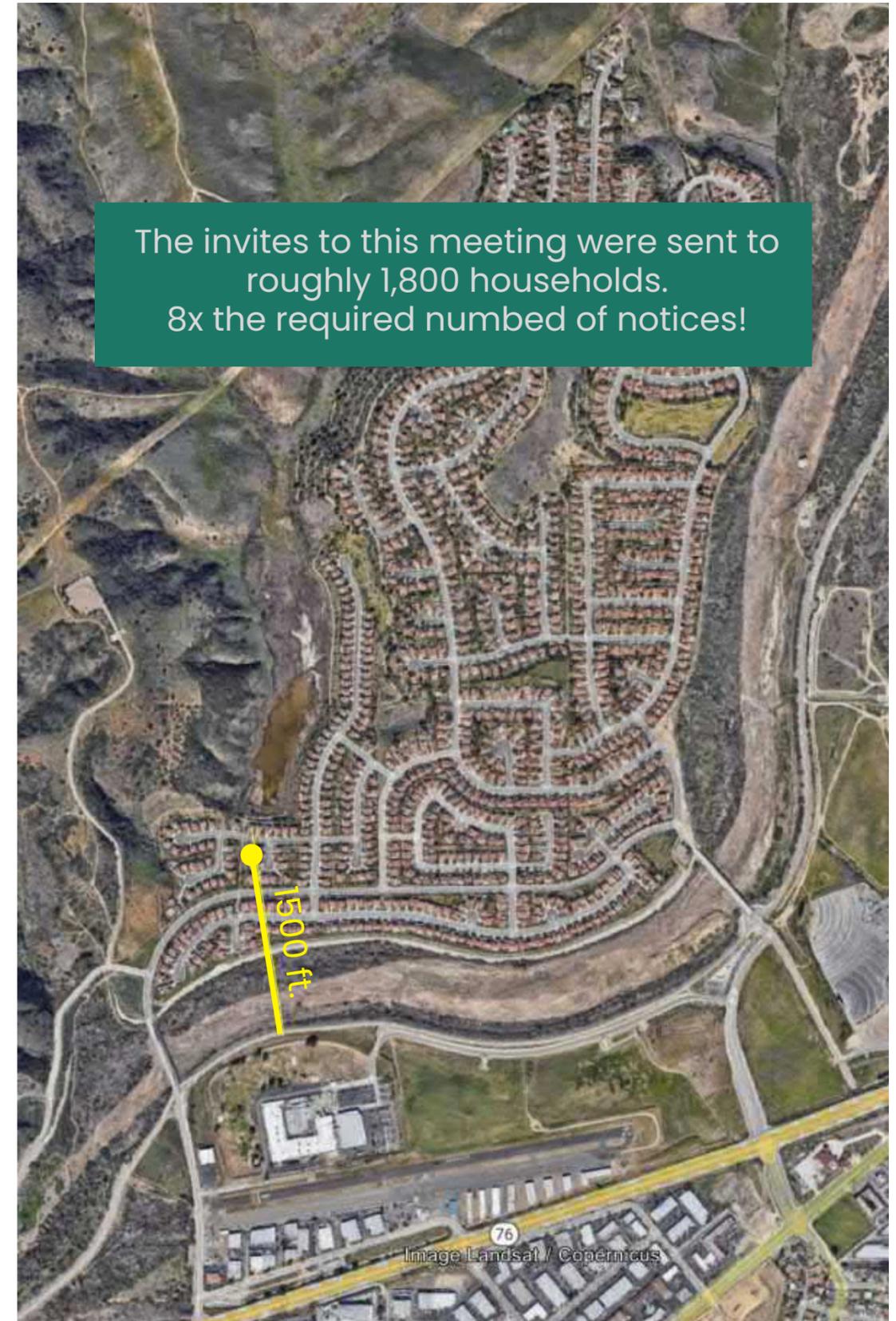
We agreed to exceed the City's required expanded notification boundary of 1,500 feet from the project site to include the entirety of the Airport Community.

- **Sharing the Process**

In addition to required public meetings, members of the community can schedule one on one or small group meetings, submit questions via email any time.

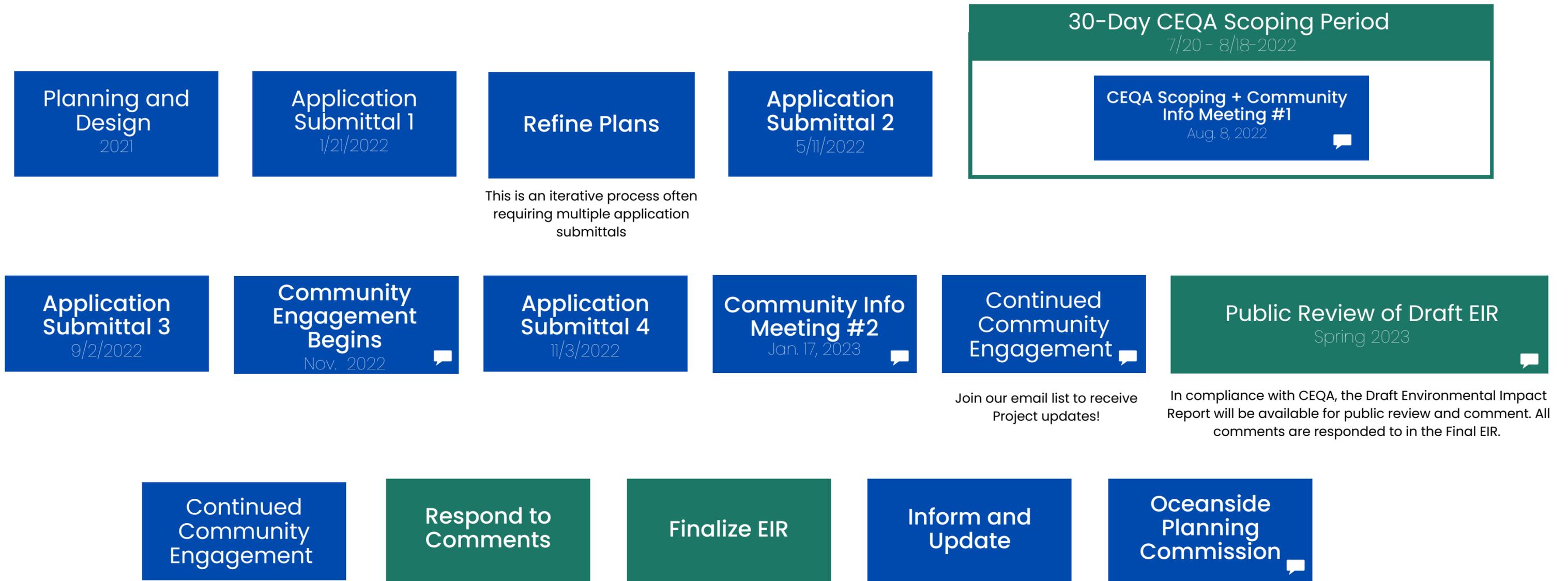


outreach@eddiejonesproject.com



▼ Project Milestones

We will be offering opportunities to provide feedback, ask questions, and learn more about the Project throughout the process. Our team is available for one-on-one and small group meetings or you can reach us by email at outreach@eddiejonesproject.com. We look forward to hearing from you!





▼ Traffic + Circulation

City requires two traffic reports:

- Vehicle Miles Traveled (VMT) report for the CEQA - the VMT analysis will be available for public review with the circulation of the Draft EIR this spring
- Local Transportation Study to determine if there are measurable transportation impacts based on the City of Oceanside thresholds
 - Pedestrian, Bicycle, Transit, and Traffic
 - Analysis of six traffic scenarios
 - Incorporates 12 cumulative projects, including Ocean Kamp
 - Project contributes to a public fund to improve the SR-76/Benet Rd. in the future

Objective metrics

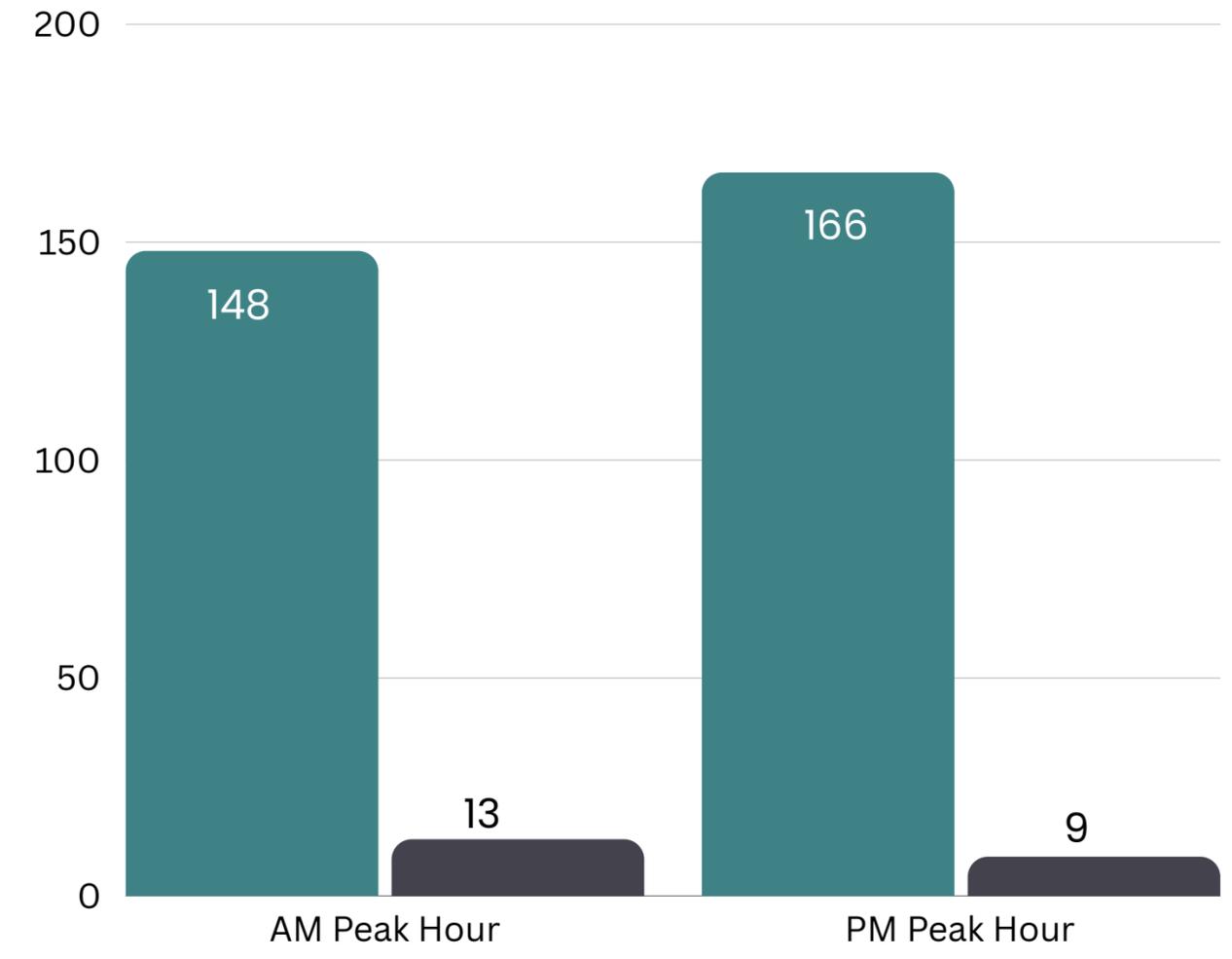
- Data for the traffic study was collected in October of 2021
- Analysis is based on objective metrics and standard formulas to calculate average daily trips
- The project is designed to ensure large trucks, including fire trucks, are able to navigate the site



▼ Trip Generation Comparisons

- **Employee Cars v. Delivery Trucks**

■ Employee Vehicles
■ Delivery Trucks

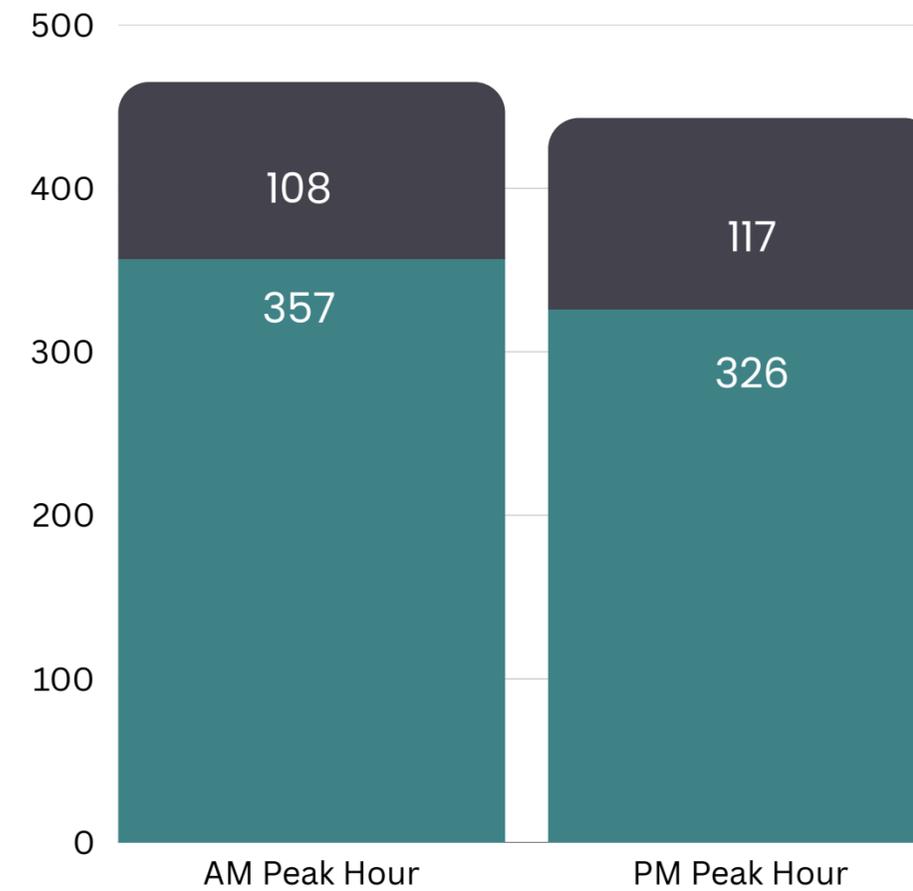




▼ Trip Generation Comparisons

- **Background v. Project Traffic - Benet Rd.**

 Background Traffic
 Project Traffic

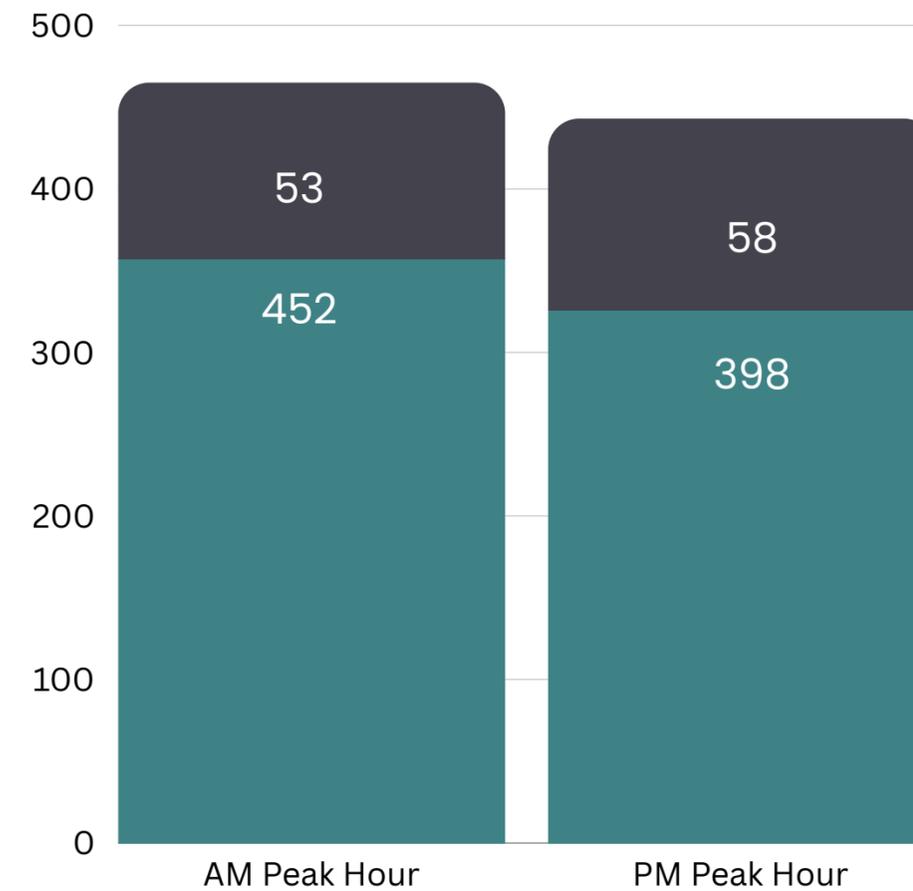




▼ Trip Generation Comparisons

- **Background v. Project Traffic - Foussat Rd.**

 Background Traffic
 Project Traffic

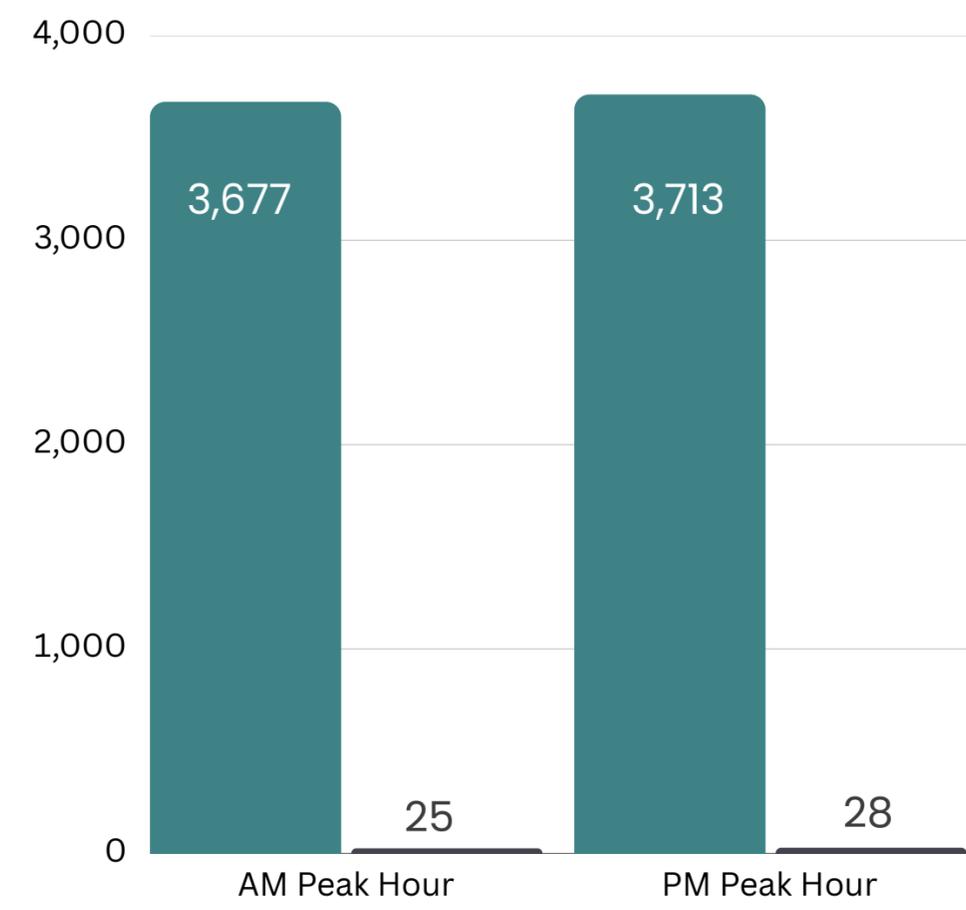




▼ Trip Generation Comparisons

- **Background v. Project Traffic - SR-76 at Benet Rd.**

■ Background Traffic
■ Project Traffic



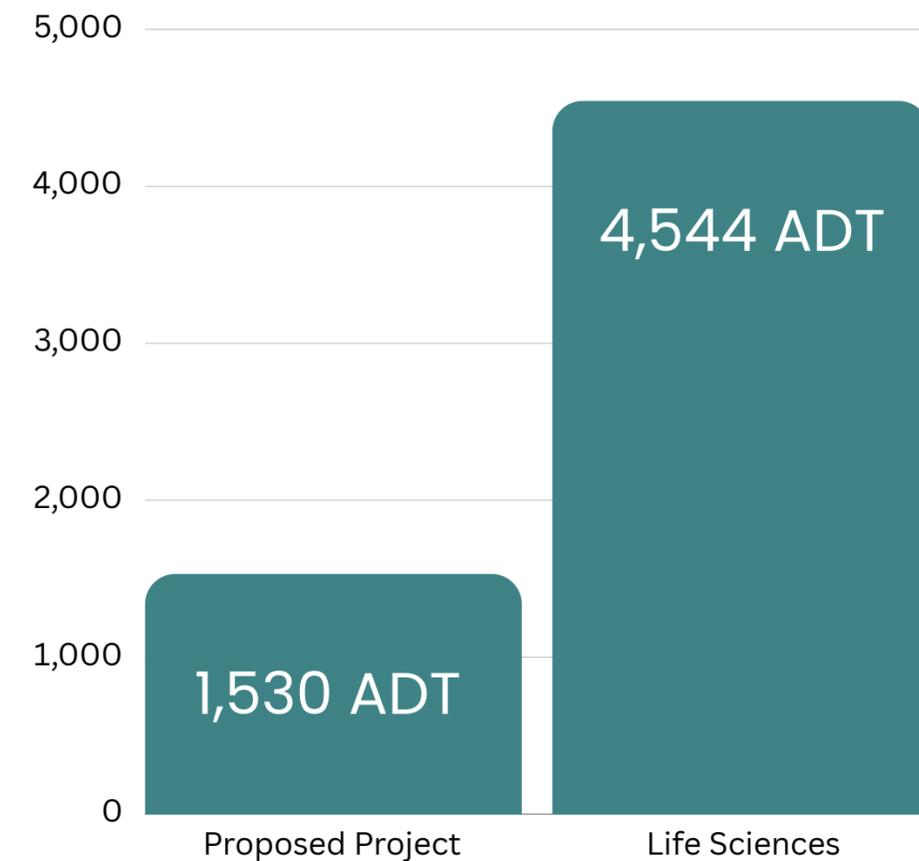


▼ Trip Generation Comparisons

A switch to life science use was requested by members of the community.

A life sciences facility of the same square footage would generate **THREE TIMES** the Average Daily Trips as the proposed project!

Project v. Life Sciences





▼ Safety and Emergency Services

- Fire is one of the many City disciplines that reviews the project application
- Project is designed to accommodate fire turnaround
- Evacuation protocol would ultimately depend on the nature of the emergency
- Building features are included to safe guard the building in the event of an emergency

▼ Environmental Analysis

Oceanside CAP Compliance

- Useful checklist when exempt from CEQA – EIR is most comprehensive review
- Air Quality and GHG reports have been prepared for the project to ensure that the emission targets identified in the CAP are achieved

▼ Environmental Analysis

With the project, all water will be treated on site, resulting in a cleaner condition than today

Protection of the San Luis Rey River

- Storm Water Management is heavily regulated in CA
- A hydraulics and hydrology report has been prepared for the project in addition to a Drainage report and a Storm Water Quality Management Plan (SWQMP).
- The proposed site design includes an underground detention vault system which incorporates modular wetlands for treatment of the storm water.

Biology and Habitat

- Biology report completed as part of the CEQA process
- The project maintains the 50-foot biological buffer and 50-foot planning buffer as required from the edge of the San Luis Rey River riparian habitat.
- Buffer to be replanted with native coastal species.

Alignment with City Priorities

This new warehouse and distribution facility is consistent with General Plan policy that industrial lands "should be devoted to industrial uses for the preservation of the City's economic future, employment opportunities, and general welfare." (GP policy 2.1.A)



Airport Influence Area

The project is consistent with the General Plan's objectives related to the current and future Airport operations



Industrial Lands Deficit

City's Economic Development Element (2019) predicts Oceanside has "less than half of the available industrial land needed to satisfy demand through the planning period" (2018 - 2035)

In light of a declining supply of employment land, the EDE includes a number of goals, policies, and action items that work together to achieve the following:

- Development of highly-constrained commercial/industrial properties;
- Retention and attraction of high-density commercial/industrial uses;
- Preservation of existing commercial/industrial space for commercial/industrial uses;
- Efficient (re)development of commercial/industrial properties;
- Greater compatibility and synergy between businesses located in proximity to one another;
- Greater flexibility in commercial and industrial land use standards; and
- Greater flexibility in home occupation standards.

These goals, policies, and action items reflect a willingness to identify and implement creative approaches to supporting existing businesses, attracting new businesses, and augmenting both the quantity and quality of jobs in Oceanside.

Oceanside Economic Development Element, Section 2-15

▼ Summary

The Project aims to leverage the site's Industrial zoning with a new modern and efficient facility that meets today's needs.



General Plan Consistency

- Consistent with the existing General Plan designation and zoning for the site
- Aligns with City priorities for economic development and preservation of industrial land



Comprehensive Analysis

- Thorough analysis in compliance with CEQA - The DEIR is anticipated to be available for public review in the spring
- Design features improve stormwater treatment, site access, and include replanting of biological buffer areas with native coastal species



Context-Sensitive Design

- Maintains the 100-foot buffer from the edge of the San Luis Rey River riparian habitat
- Complies with Airport Overlay Zone
- Includes landscape buffers to shield from public roadways and residential uses
- Includes Operations Management Plan with Good Neighbor Policy



Increased Transparency

- Exceeded City's Expanded Notification Boundary
- Continued Community Engagement

▼ Thank you

Contact us to submit additional questions or to schedule a meeting with the project team



outreach@eddiejonesproject.com

- Exhibit 9 – Project concerns sheet provided by opposition

Opposition to the

Eddy Jones Warehouse, Manufacturing and Distribution Facility

(sometimes referred to as EDDIE Jones)

Brief Summary

The **Eddy Jones Warehouse, Manufacturing and Distribution Facility** is being proposed to be built on the 31.79 acres between the Oceanside Municipal Airport on Eddie Jones Way and Benet Rd and the residential development known as Airport. The plans include **114 semi-truck terminals, 60 truck trailer parking stalls and 590 auto parking spaces** with a building of **566,905 square feet**.

In addition, with the **Ocean Kamp** project under construction, our ability to maneuver in and out of our community via either Benet Rd or Foussat Rd will be adversely impacted.

Our Concerns

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as ALL who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There is NO money earmarked for any road / traffic improvements.

TRANSPARENCY - To date the lack of transparency/communications is very apparent when very few of the Airport residents and surrounding areas are even aware of the intended development. We are not opposed to something being built but what type of business, what level and hours of operations and what impact will it have on our community and our quality of life are not being addressed or taken into consideration.

POLLUTION - We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24 hours a day. Noise pollution from these vehicles is concerning. Pollution of the San Luis River is also an issue to be considered.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. The same will be true on Foussat Rd due to increased traffic from the Ocean Kamp and the 590 vehicles expected for the employees and visitors at this new facility. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. We do not oppose development as long as it is well thought out and is compatible with the residential area that it borders. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on our nearby communities.

HOW YOU CAN HELP:

- Contact the City Planner, all the members of the City Council, and the Mayor. Be sure to copy the City Clerk and eddyjoneswarehouse@gmail.com so there are records of your correspondence (all addresses are below)
- Sign our petition and get signatures from your neighbors.
- Join our grassroots group. We all need to be actively involved to get this changed. A few of us cannot do it all. You will get updates as to what is going on and what we are doing via this newsletter. We need YOU! Contact us at eddyjoneswarehouse@gmail.com
- Follow us on Facebook - EDDIE JONES PROJECT / OCEANSIDE
- **Oceanside City Principal Planner**
Rob Dmohowski
(760) 435-3563
rdmohowski@oceansideca.org
- **Oceanside Mayor**
Ester Sanchez
(760) 435-3057
esanchez@oceansideca.org
- **Deputy Mayor - District 3**
Ryan Keim
(760) 435-3048
rkeim@oceansideca.org
- **Councilmember - District 1**
- **Eric Joyce**
(760) 435-3032
ejoyce@oceansideca.org
- **Councilmember - District 2**
Richard (Rick) Robinson
(760) 435-3033
rrobinson@oceansideca.org
- **Councilmember - District 4**
Peter Weiss
(760) 435-3066
pweiss@oceansideca.org
- **Oceanside City Clerk**
Zeb Navarro, Ed.D.
(760) 435-3010
znavarro@oceansideca.org



Additional Information

Oceanside Planning Division:
Eddy Jones Project File Number – D22-00001
Contains publicly available project documents
(TrackIT) <https://oceanside.maps.arcgis.com/apps/webappviewer/index.html?id=77db3575bf5b49f290fc050cb46cfae2>

- Exhibit 10 – 1/17/23 Questions

#	Asker Name	Asker Email	Question	Answer	Question Time
1	Dua Pham	dp999_98@yahoo.com	There are many people want to attend the meeting today, but you didn't send the invitation. I gave them the phone number but the ID didn't work	The link was sent out. It's also on an auto-reply to the email address	1/17/2023 18:22
2	Dua Pham	dp999_98@yahoo.com	Hello		1/17/2023 18:22
3	Dua Pham	dp999_98@yahoo.com	But the ID= 015718 not working	you don't need an ID to register. They can join by putting in their name and email	1/17/2023 18:24
4	Demetrius Steele	d_steele_8@yahoo.com	Are all attendees muted by default?	yes. we'll take questions though throughout Please provide them with the link:	1/17/2023 18:25
5	Dua Pham	dp999_98@yahoo.com	There is a couple next to my house who want to join by enter this number 669 900 6833 then then enter IT = 015718 but it didn't work.	https://us02web.zoom.us/j/89174436085 #success	1/17/2023 18:26
6	Dua Pham	dp999_98@yahoo.com	ID = 015718	The code is for panalists only. Not attendees.	1/17/2023 18:27
7	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	When do you anticipate to present to planning commission and city council?	They do not need an ID.	1/17/2023 18:27
8	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	Thank you.	Estimated public hearings late 2023	1/17/2023 18:28
9	Dua Pham	dp999_98@yahoo.com	What phone number to dial in and ID = ????	The best way to join is by the link provided: https://us02web.zoom.us/j/89174436085 #success	1/17/2023 18:29
10	Debby Herbert	kayadeb36@outlook.com	The traffic study was done in August of 2020 which was only a few months after Covid lockdowns. Many thousands more are using Hwy 76 and Benet and Foussat Rds now. Are they doing another traffic study? This should be done to accurately capture current traffic volumes.	live answered	1/17/2023 18:29
11	David Baurac	dbaurac@gmail.com	How many trucks will the warehouse add to current traffic in the neighborhood? How big will the trucks be? What times of the day will the trucks be on neighborhood roads?	live answered	1/17/2023 18:31

12	Kevin & Janet Nolan	hiskid07@gmail.com	We could only see you, but none of the others you introduced.	If you change your view to Gallery, you'll be able to see everyone. If you have it in Speaker view, it'll stay zoomed on the person who is presenting at the time.	1/17/2023 18:33
13	BRANDY BRAGG	alona8scooby@yahoo.com	I heard that a conditional use permit was issued allowing more tractor trailers for over 50K sq foot? and the land was zoned prior to the homes being built in the airport neighborhood.	live answered	1/17/2023 18:38
14	Dua Pham	dp999_98@yahoo.com	Do you give us a chance to speak out or raise our question by unmute later?	Please submit any questions through this portal. Thank you! Yes. 12 additional projects were analyzed for cumulative impacts.	1/17/2023 18:43
15	Anna Floyd	akasperowicz@yahoo.com	Have you taken into consideration the other large development project opposite Foussat in regards to constraints on right of way off Bennett including 76 access	The Eddie Jones project accounts for roughly 1% of the cumulative projects.	1/17/2023 18:45
16	Chris Kim	Chriskim@gmail.com	The link to this meeting is not working for many people. It is asking them for a password.	live answered	1/17/2023 18:49
16	Chris Kim	Chriskim@gmail.com	The link to this meeting is not working for many people. It is asking them for a password.	The password is for panalists only. They have to join as an attendee. Have them email us so we can help them outreach@eddiejonesproject.com	1/17/2023 18:49
17	Rick Nafis	RNCarlsbad@gmail.com	With the planned approach of building a flood wall to protect the building, it seems that would deflect flood water to the airport and/or Benet. What impacts are being anticipated, planned for?	live answered	1/17/2023 18:49
18	Rick Nafis	RNCarlsbad@gmail.com	Would the portion of the building for manufacturing use be planned for the most southern side to help avoid noise to the neighborhoods? If not, what measures will be taken to buffer the noise?	live answered	1/17/2023 18:52

19	Anonymous Attendee		they need to click on as "Attendee" and then will not need password	Thank you, we are finding they are missing that. Appreciate the help! There are no tenants chosen yet, it is too soon in the process.	1/17/2023 18:52
20	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	I understand its not a spec build, but is there a perspective tenant?	Because there is no "move in date" expected, it is too soon to say.	1/17/2023 18:52
21	Dilan Good	dilangood@gmail.com	please address the noise levels we should expect. I'm imagining a fedex warehouse.	live answered	1/17/2023 18:53
22	Mark Noakes	mbnoakes@cox.net	Who are you looking for as tenants? Can you give uds specific names?	The tenants for the buildings have not yet been determined so it is too soon to know.	1/17/2023 18:54
23	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	prospective*		1/17/2023 18:54
24	Donald Pham	dphamk25@yahoo.com	On an average , how frequent trucks will travel in and out Benet rd to get to Hwy 76?. What materials they transport?. Thank you.	1 truck every 4-5 minutes in the am, 1 truck every 6-7 mins in the pm	1/17/2023 18:54
25	Cynthia Reemtsma	Cynthia.gladstone@gmail.com	114 Trucks of what size fit here? Eniornmental air pollution air from these large ? gas operated vehicles are you widening the road? and what are the limits of hours of operation?	The site is designed to fit a single trailer truck. We are not widening roads. Hours of opeations are informed by the specific tenants.	1/17/2023 18:56
26	Anonymous Attendee		The traffic study was collected Oct 2021??? People are still easing out of quarentine....	live answered	1/17/2023 18:58
27	Debby Herbert	kayadeb36@outlook.com	The plan requires the project to be compatible with surrounding neighborhoods. How is this compatible with 1200 homes just beyond the river, including 200 homes within 1500 square feet, considering the trucking terminal aspect and all the extra cars for the offices and workers?	live answered	1/17/2023 18:59

28	Anna Floyd	akasperowicz@yahoo.com	Development plan indicated only light vehicles will pass on Alex rd/Foussat in and out and all trucks must go in and out Bennett.. how will you restrict trucks from passing thru community. Sec 2 of findings of variance requires to approve will not be detrimental to property or health safety if general public.	live answered	1/17/2023 19:00
29	Anonymous Attendee		Does your EIR take into consideration the EIR of the other big development project? If so, was that report current and updated (been 3 years now) your EIR must take into consideration many people were still working from home then		1/17/2023 19:00
30	Gretchen Gary	gretchengary@gmail.com	If so few truck trips, why so many truck bays in the plan?	live answered	1/17/2023 19:01
31	Darren Parsons	dtp_mb@yahoo.com	Will the traffic study be updated given many more people are back working in offices vs Oct 2021?	live answered	1/17/2023 19:01
32	Anna Floyd	akasperowicz@yahoo.com	EIR must also take into consideration the impact on value of residential homes 1,000 .. if correlation to a mass decline compared to other developments nearby damages could be \$200m or more in damages if approved	The EIR does not review impacts to property values. The analysis only looks at environmental topics.	1/17/2023 19:01
33	Kim Stone	kimstone899@gmail.com	How many feet per loading docks will be allowed for queing to prevent queing on streets?	live answered	1/17/2023 19:02
34	Marty Basham	PRTYMRTY@HOTMAIL.COM	you are estimating 25 delivery trucks yet there is 114 spaces for them, Why is this?	live answered	1/17/2023 19:02
35	Anna Floyd	akasperowicz@yahoo.com	That does. It answer my question		1/17/2023 19:02
36	Anna Floyd	akasperowicz@yahoo.com	Does not answer my question		1/17/2023 19:02
37	Marty Basham	PRTYMRTY@HOTMAIL.COM	trip generations does in include oceankamp projecctions as weell/	OceanKamp is one of the 12 projects analyzed as a cumulative impact.	1/17/2023 19:03
38	Anna Floyd	akasperowicz@yahoo.com	Developer could be facing class action lawsuit for damages in property value	Thank you for your comment. It will be shared with Adam.	1/17/2023 19:04
39	Kim Stone	kimstone899@gmail.com	Additional trips made are by cars or trucks?	The trips he is referring to are a combination. The vast majority of trips is from employee vehicles.	1/17/2023 19:05
40	Anonymous Attendee		Are you going to address truck noise?	live answered	1/17/2023 19:05

41	Anonymous Attendee		Please explain the slide with 1530 ADT- is that for the project?	Some community members have requested a change in the use to life sciences (away from manufacturing and warehouse). This slide compares the two uses. A life sciences facility would generate much more average daily trips because there are more people within a life sciences building.	1/17/2023 19:05
42	douglas smith	smithdp5121@gmail.com	Are you going to restrict trucks of more than 5 tons to south of Benet Road at Airport Road west Benet ?	Industrial zoning aligns with roadway capacities.	1/17/2023 19:05
43	douglas smith	smithdp5121@gmail.com	Chat is denied!	Please submit any questions through this portal. Thank you!	1/17/2023 19:06
44	Anna Floyd	akasperowicz@yahoo.com	Got kicked		1/17/2023 19:06
45	Mark Noakes	mbnoakes@cox.net	Will you commit to widen Benet Rd to accomodate all the trucks that will be using it?	Widening of Benet is not included in the plan.	1/17/2023 19:06
46	Debby Herbert	kayadeb36@outlook.com	Justin; regarding the 1% increase in traffic you quoted. Is that 1% more compared to the residential traffic going down Benet Rd., or is that compared to ALL the traffic at Hwy 76 and Benet Rd?	live answered	1/17/2023 19:06
47	Mikhael Madello	mikhaelmadello@gmail.com	What makes it a good idea to build another large project in a small neighborhood when another even larger project (wave pool/700 new homes + hotels, parking, etc.)? Our neighborhood is defined by Pendleton to the north, and Benet and Fossatt are the ONLY means of egress. We live in the top culdesa and had a fire this summer. With the warehouse and the wave pool, there is going to be a massive increase in traffic on Benet and Fousat (and this doesn't include the 700 new houses/hotels that will be added with Ocean Kamp.) One project is enough. We already have 1000 houses in our neighborhood; add trucks and 700 new homes and it will absolutely devastate the community with increased traffic, air and noise pollution.	live answered	1/17/2023 19:06

48	Mark Macfarlane	pls.markmacfarlane@gmail.com	Nobody cares about your building. We care about the tripling of the size of use		1/17/2023 19:07
49	Kaye Thompson	kayet51@gmail.com	Is the Camp Pendleton rush hour considered regarding congestion for Benet/76? There is already tremendous congestion on 76 during these hours.	yes - data accounts for background elements, or normal operations	1/17/2023 19:07
50	Gretchen Gary	gretchengary@gmail.com	The Eddie Jones project has a planned 114 truck bays which is nearly 8X more than the rejected Amazon project in Ocean Ranch which only had plans for 15 truck terminals. Why are there so many? And why does the building have to be 550K sq ft? What is driving such massive scale?	live answered	1/17/2023 19:08
51	David Gaul	Davidgaul@cox.net	Why are trucks using Benet vice Foussat? Intersection at Foussat has many more lanes then Benet.	live answered	1/17/2023 19:08
52	Mark Macfarlane	pls.markmacfarlane@gmail.com	How are you handling the massive increase in the ammount of impervious surface to comply with CEQA	live answered	1/17/2023 19:08
53	Mark Macfarlane	pls.markmacfarlane@gmail.com	Endangered species in the river basin?	live answered	1/17/2023 19:09
54	Mark Macfarlane	pls.markmacfarlane@gmail.com	What are the hours of operation?	This question was also answered live. The hours of operation will be determined by the eventual tenants.	1/17/2023 19:10
55	Mark Macfarlane	pls.markmacfarlane@gmail.com	What are the increases in noise?	live answered	1/17/2023 19:10
56	Anonymous Attendee		Are there considerations for putting a traffic light in at Benet Rd or Foussat Rd? From the traffic analysis, employee vehicles may be the main contributors to increased traffic and not the delivery trucks.	live answered	1/17/2023 19:10
57	Kim Stone	kimstone899@gmail.com	What about protecting the aquifer?	live answered	1/17/2023 19:11
58	Mark Macfarlane	pls.markmacfarlane@gmail.com	It has nearly 3.5 times as much building		1/17/2023 19:11
59	Anna Floyd	akasperowicz@yahoo.com	This is very one way .. not interactive avoiding real questions and concerns skirting what they know are real issues to get approved	All questions are being answered. We will also post to the website.	1/17/2023 19:11
60	Marty Basham	PRTYMRTY@HOTMAIL.COM	I would like a copy of your presentation.	Sure. I can email it to you tomorrow.	1/17/2023 19:12

61	BRANDY BRAGG	aloha8scooby@yahoo.com	But Oct 2021 would still have alot of people working from home. People are still starting to go back to the office and in another year or so, there will be alot less people working from home than there was in Oct 2021...	live answered	1/17/2023 19:13
62	Mark Macfarlane	pls.markmacfarlane@gmail.com	How is this “light” industrial use?	live answered	1/17/2023 19:13
63	Anonymous Attendee		Sorry I was a bit late- is this a 24-hour facility??	Hours of operation will be determined by the end user. Tenants won't be selected until the project is approved.	1/17/2023 19:13
64	DAVID TAUB	david.m.taub@gmail.com	what's the plan for traffic on Benet, Airport Road, and Foussat along with increased traffic at the 76/Benet and that corridor? If you are unsure of how the faciality is being used, why does the plan include 114 semi trucks bays? This is truly problematic for residents with regards to noise, polution and traffic.	live answered	1/17/2023 19:13
65	Anonymous Attendee			live answered	1/17/2023 19:13
66	Dua Pham	dp999_98@yahoo.com	Arlene, Adam, Dan, and Brian,		1/17/2023 19:13
67	Mark Macfarlane	pls.markmacfarlane@gmail.com	No, actually it is not.		1/17/2023 19:13
68	Sheryl Bannister	roseychild517@gmail.com	can you expand on "greater flexibility in home occupation standards"?	i'm not sure i understand. can you please resubmit or follow up via email: outreach@eddiejonesproject.com	1/17/2023 19:14
69	Dua Pham	dp999_98@yahoo.com	Would you give us a chance to ask question by unmute the audio?	We will have a Q&A portion at the end of the presentation if questions were not covered. Please submit all questions through this portal. Thank you!	1/17/2023 19:14
70	Mark Macfarlane	pls.markmacfarlane@gmail.com	There is plenty of industrial use land not in resedential areas	live answered	1/17/2023 19:14
71	DAVID TAUB	david.m.taub@gmail.com	do you have a planned tenant type for this project?	There are no tenants chosen yet, it is too soon in the process. Because there is no “move in date” expected, it is too soon to say.	1/17/2023 19:14

72	Debby Herbert	kayadeb36@outlook.com	How will the diesel pollution from the trucks that will be landing in the river be mitigated?	live answered	1/17/2023 19:14
73	Mark Macfarlane	pls.markmacfarlane@gmail.com	You clearly have not found a “ballance” and are not interested in the views or interests of the community. How will you restricts trucks from going down Benet street. We as home owners are concerned with these vehicles going by our homes at 45 miles a hour all day? How do you see this	live answered	1/17/2023 19:15
74	Anna Floyd	akasperowicz@yahoo.com	commercial property as effecting home values. Since homes are single family. Families will no longer want to walk on benet and enjoy the community. New families will not want to move here as well due to your project	live answered	1/17/2023 19:16
75	Mark Macfarlane	pls.markmacfarlane@gmail.com	Of course you think that It sounds like there is a misunderstanding about the surrounding residential community's safety concern: As much as it's important for tenants in the building, the REAL concern and question is for access & ingress of fire & safety protection for residents due north of the site, ie, there's only 2 roads in & out, and with an average of 10 large ("18 wheelers) on Benet during peak hours, how can you claim there would NOT be a dely of emergency vehicles due to one or more of these large delivery vehicles!	live answered	1/17/2023 19:16
76	Anonymous Attendee				
77	Sheryl Bannister	roseychild517@gmail.com	Will the expansion of the airport be due to the facility having air deliveries to warehouse? I understand the building is being built to ordinances and the building is truly a beautiful building. Our whole issue is the traffic and with that traffic comes noise and pollution. What can be done to mitigate that?	No, the master plan for the airport pre-dates this project.	1/17/2023 19:16
78	Gene and Carrie O'Neal	hp11325@yahoo.com		live answered	1/17/2023 19:16
79	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	Has a fulfillment center or parcel hub been considered for this project site?	No it has not	1/17/2023 19:17
80	Brad Mayo	Bradmayohoa@hotmail.com	When was the traffic study of Benet and Foussat done?	The data was collected Oct. 2021. It's combined with objective metrics as Justin is describing now.	1/17/2023 19:17

81	Sheryl Bannister	roseychild517@gmail.com	will these trucks be Amazon trucks?	The tenants for the buildings have not yet been determined so it is too soon to know.	1/17/2023 19:18
82	Gene and Carrie O'Neal	hp11325@yahoo.com	Is this facility be operating 24 hours a day? This will be loading and unloading, correct?	Hours of operation will be determined by the end user. Tenants won't be selected until the project is approved.	1/17/2023 19:18
83	Richard Eason	reasonr1@yahoo.com	Is therre a projected % occupancy for the facility?	Ideally, RPG would be able to lease 100% of the site to future tenants. Please email us if we misunderstood the nature of your question.	1/17/2023 19:18
84	Mark Macfarlane	pls_markmacfarlane@gmail.com	Employee vehicles are not semi trailers	live answered	1/17/2023 19:18
85	Dua Pham	dp999_98@yahoo.com	I have 2 question:		1/17/2023 19:18
86	Darren Parsons	dtp_mb@yahoo.com	The question about how many trucks was not answered.	Thanks for the reminder :) Total is 164 for the day	1/17/2023 19:19
87	Anna Floyd	akasperowicz@yahoo.com	Why can we not talk to ask question	live answered	1/17/2023 19:19
88	Mark Macfarlane	pls_markmacfarlane@gmail.com	634 over what time period?	live answered	1/17/2023 19:19
89	Debby Herbert	kayadeb36@outlook.com	But cumulative where? At the entire intersection?	live answered	1/17/2023 19:19
90	Gretchen Gary	gretchengary@gmail.com	Question for Adam - would you build this project by your home and tell your neighbors the same that this would be good for your neighborhood, especially those within 1,500 feet of it?	live answered	1/17/2023 19:19
91	DAVID TAUB	david.m.taub@gmail.com	how do you fit a building that is roughly 3 times larger than the last building on the same property?	live answered	1/17/2023 19:19
92	Alyce Budde	ajbudde@cox.net	Noise level (backup beeping during the night) lighting ??? Are flood lights going to light up the riverbed during the night	The project will abide by Oceanside's lighting requirements.	1/17/2023 19:19
93	Anna Floyd	akasperowicz@yahoo.com	Will u be allowing participants to speak?	live answered	1/17/2023 19:20
94	Judy Glasser	glassersallee@cox.net	When is the expected completion date to begin tennet occupation	live answered	1/17/2023 19:20
95	Gene and Carrie O'Neal	hp11325@yahoo.com	With semi trucks that "sit" on the property, per your statement, will there be a mechanism to enforce allowing the truck to sit and idle. i.e. cold nights, rain, etc??	live answered	1/17/2023 19:20

96	Chris Kim	Chriskim@gmail.com	How are you able to accurately predict traffic from the facility when you don't know who will be using the business space? If I understand correctly, you're building to spec because you don't know who will be using the space, yet, somehow you have a specific number of trips the unknown tenant will make.	live answered	1/17/2023 19:20
97	NEIL HANCOCK	tahoe772@yahoo.com	Is there been any thought to creating a dedicated lane of traffic just for the site - expanding the width of Benet to lessen the impact on passenger cars.	live answered	1/17/2023 19:21
98	Mark Macfarlane	pls.markmacfarlane@gmail.com	I'd bet those buildings weren't in a residential area		1/17/2023 19:21
99	Richard Eason	reasonr1@yahoo.com	Is there aany potential additional air traffic due to used by the facility (when finished) for personnel or materials?	no	1/17/2023 19:21
100	Marty Basham	PRTYMRTY@HOTMAIL.COM	The size and the bay doors does noy eliminated a amazon/walmart warehouse distribution center, correct?	live answered	1/17/2023 19:21
101	Dua Pham	dp999_98@yahoo.com	1.- Safety issue. Trucks will create botle neck traffic between HW 76 and Benet. Myself and my children ride bike daily. We just want to bring up safe issue and concern for our children to pass daily in Benet and HW76.	live answered	1/17/2023 19:22
102	NEIL HANCOCK	tahoe772@yahoo.com	Continuing - asking for additioal lane of traffic on each side of Benet	live answered	1/17/2023 19:22
103	Rob Regan	Rob@regandist.com	Will trucks be able to maneuver their 53 foot trailer into this site. And the roads will be tore up after this is done. Will our roads be upgraded bc of these trucks comings and goings even after project or during pending project....environmental impact on diesel etc from these trucks	live answered	1/17/2023 19:22
104	Richard Eason	reasonr1@yahoo.com	What happens if Bennet closes due to an accident, fire, flooding, etc,, will Alex be used for the truck mnovement?	live answered	1/17/2023 19:23
105	cecelia moreno	morenocecelia6196@gmail.com	who will be responsible for maintaining the landscaping and parking lot once the building is completed? will the tenants then be responsible for maintaining the property? also concerned about exterior lighting.	90% of the time, RPG maintains their buldings. Sometimes a larger user will ask for more control.	1/17/2023 19:24

106	Dua Pham	dp99_98@yahoo.com	2.- If you (Dan, Brian) said estimate about 1% daily trucks moving. Can we request the employees can turn from HW76 to Benet, BUT THOSE TRUCKS should turn in Airport road instead in Benet to prevent incidents will occur in the future.	The transportation study determined we did not trigger additional public improvements. Sending trucks up to Airport would result in more pollution.	1/17/2023 19:24
107	cecelia moreno	morenocecelia6196@gmail.com	will there be ingress and egress on fousstat as well for this property without travelling through residential streets? What year was that area zoned?	live answered	1/17/2023 19:25
108	Anonymous Attendee		I am sure it was was zoned Light industrial prior to the homes being built in the airport neighborhood, so the needs of that area has changed over the years.	live answered	1/17/2023 19:25
109	Anna Floyd	akasperowicz@yahoo.com	Your traffic study is outdated data. Why have you not done recent study of community	This question has been addressed. The traffic counts are combined with objective industry standards. Analysis isn't solely based on one metric.	1/17/2023 19:25
110	Michael Caltabiano	local1234@hotmail.com	so if you dont have tenants how do you know the truck traffic? If you don't know who the tennent (s) will be, how can you estimate traffic? Would not a Fedex or Amazon distibution center have significantlty more traffic than a manufacturing facility?	live answered	1/17/2023 19:26
111	Russell Frierson	russ.frierson@grpfinancial.com	Does the responsible party (CalTrans/City of Oceanside?) have a proposed improvement plan for the benet/SR76 intersection?	live answered	1/17/2023 19:26
112	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	Our community will experience a choke hold on Benet in the case of an emergency evacuation. You did not address how this will be mitigated.	live answered	1/17/2023 19:27
113	Celerina Cornett	ccornett@ucsd.edu	I would really like to understand the truck count. I only hear 22 during each peak morning & evening. What does a 24 hour truck count look like? Will there be more trucks during non-peak morning & evening traffic times?	This question has been answered. I will reach out to you tomorrow to set up a time to dig into this issue further.	1/17/2023 19:27
114	Darren Parsons	dtp_mb@yahoo.com	Will potential hazardous material (manufacturing) be transported by trucks on these streets?	live answered	1/17/2023 19:28
115	Richard Eason	reasonr1@yahoo.com			

116	cecelia moreno	morenocecelia6196@gmail.com	what other mitigation is being done besides the sidewalk on eastside and trees? will there be another left turn lane on 76 at benet going north?	A secondary left turn lane is not included in this project, however the project contributes to a fund that will improve that intersection in the future.	1/17/2023 19:28
117	Dua Pham	dp999_98@yahoo.com	Sorry typo error. I mean accident will happen because the trucks couldn't see our kids below when they ride bike across Benet RE: Flood wall. That is great that "the property" will not flood,		1/17/2023 19:28
118	Gene and Carrie O'Neal	hp11325@yahoo.com	however, with redirection of water, is there any study that examines if the onslaught of water will flood homes on the north side of the San Luis Rey River?	live answered	1/17/2023 19:29
119	Melissa Flores	madubin1214@yahoo.com	The firing range is an indoor range and does not make any noise.		1/17/2023 19:30
120	David Gaul	Davidgaul@cox.net	You mentioned applicant was contributing to improvements of the 76/Benet intersection. What type of improvements are being considered.	live answered	1/17/2023 19:30
121	Anonymous Attendee		Will the developer ever offer a forum where the community can ask questions live?	This is just the first big group meeting. We definitely want to hear from people. This format was the most efficient way to make sure everyone had the same opportunity to be heard. Please feel free to reach out and we can talk about next steps and additional opportunities.	1/17/2023 19:31
122	Gene and Carrie O'Neal	hp11325@yahoo.com	Have you received any waiver from Cal OSHA to allow fork lift and heavy equipment alarms to be silenced over night, say 5:00 p.m. to 8:00 p.m?	live answered	1/17/2023 19:31
123	Anna Floyd	akasperowicz@yahoo.com	Why not collect data in January 2023 of traffic in area	live answered	1/17/2023 19:32
124	Debby Herbert	kayadeb36@outlook.com	My question above regarding the 1% I do not feel was answered. That is 1% of 61,000 cumulative total, but you did not provide where the 61,000 count was taken from. Is that from the entire intersection at Hwy 76 and Benet Rd?	live answered	1/17/2023 19:32

125 Marty Basham	PRTYMRTY@HOTMAIL.COM	I live well outside of the 1500 foot radius and I can hear trucks backing up and their beeping clearly. What is the plan to mitigate exterior truck/forklift noise?	live answered	1/17/2023 19:33
126 Dua Pham	dp999_98@yahoo.com	I would like Mr. Justin or Dan reply my question		1/17/2023 19:34
127 NEIL HANCOCK	tahoe772@yahoo.com	You spoke of a "Contribution" to improve the intersection of 76 & Benet. Does that mean we will be left waiting for caltrans to improve the intersection - or not? Can thought be given for a right turn lane or bonus lane to get started similar to the lane at Fousatt and 76 Westbound?	live answered	1/17/2023 19:34
128 Gretchen Gary	gretchengary@gmail.com	We suspect you are cherry picking questions and screening those which everyone can see. Realize if you don't answer all of the questions (especially the hard ones), we will not conclude that this was a transparent share out. We know what questions have been asked.	live answered	1/17/2023 19:35
129 Brad Mayo	Bradmayohoa@hotmail.com	Benet is not a "residential road" so nothing will prevent the trucks from using it when the easier route is immediately backed up.	live answered	1/17/2023 19:36
130 Micaela Canton	Cantonmicaela@gmail.com	4 min for each truck on this small road and only two lane highway does not make this project unrealistic and damaging for this residential area? Also due to fact that we will have a resort, lagoon and 700 housing?!?	Benet has the capacity to accommodate the project based on objective metrics.	1/17/2023 19:36
131 PATRICIA BROWN	1patriciabrown@gmail.com	You stated that you were going to work on making changes to the Foussat and 78 interchange. How? It curenly takes as many as 3 traffic light changes. You also said the increase in traffic will only be 1%. With the mega project for the wave pool and all that goes with it, I have to question your math. it may only increase traffic on Bennet defintely not true for Foussat side. I know because I have lived here or 20 yrs and know how much traffic there is. You can always manipulate statistics.	live answered	1/17/2023 19:37
132 Fran Cusano	Fran.cusano@gmail.com	Will there be any improvements to Benet Road. Possibly widening it	Widenign of Benet is not included in the plan.	1/17/2023 19:37

133 Ernesto Torres	viii.neptune@gmail.com	We have concerns about trucks driving through our neighborhood. We've already seen them training new drivers. Are there going to be restrictions on those trucks transiting north of the bridge? We should see signs restricting weight over a certain amount.	live answered	1/17/2023 19:39
134 Michael Caltabiano	local1234@hotmail.com	So no light at the new intersection and or extra lanes for the trucks to get to the 76?	live answered	1/17/2023 19:40
135 Cody Powers	Jmaccabeus1@gmail.com	Would the development be agreeable to a site survey by private environmental organization like the National Audubon Society, World Wildlife Fund, or Earthjustice? I believe there are 3-4 endangered species that live within the surrounding area.	I'll email you to set a meeting.	1/17/2023 19:41
136 douglas smith	smithdp5121@gmail.com	does the tractor trailer combination include a 53 foot trailer?	yes	1/17/2023 19:41
137 douglas smith	smithdp5121@gmail.com	does the tractor trailer combination include a 53 foot trailer?	Yes	1/17/2023 19:41
138 Cody Powers	Jmaccabeus1@gmail.com	Outside of the traffic study, are you personally aware that traffic already backs up an entire exit on SR-78 during peak PM hours?	live answered	1/17/2023 19:43
139 Dua Pham	dp999_98@yahoo.com	Final: My proposal that if Justin or Dan mentioned that there is 1% truck moving a day. My proposal instead those trucks turn left from HW76 to Benet. Those trucks should go straight one more block and turn left from HW76 to Airport Road. instead of Benet. Please Please.....	live answered	1/17/2023 19:44
140 Dua Pham	dp999_98@yahoo.com	Hey don't ignore me.	You will not be ignored - we are going through the questions as quickly and efficiently as possible. If your question is not answered in time, we will follow up with you by email.	1/17/2023 19:45
141 douglas smith	smithdp5121@gmail.com	THIS PRESENTATION PACKAGE GOING TO BE MADE AVAILABLE?	yes	1/17/2023 19:46

142	Dua Pham	dp999_98@yahoo.com	Hey Molly, Can you bring up my question. Don't ignore me	You will not be ignored - we are going through the questions as quickly and efficiently as possible. If your question is not answered in time, we will follow up with you by email.	1/17/2023 19:47
143	douglas smith	smithdp5121@gmail.com	is there going to be a recording of this meeting	live answered	1/17/2023 19:47
144	Anonymous Attendee		Life science use IS NOT LOUD like diesel trucks. If this is indeed simply a warehouse why so many bays?	live answered	1/17/2023 19:47
145	douglas smith	smithdp5121@gmail.com	Will the reording be made available?	The meeting recording will not be available, but we can provide the presentation slides to those interested.	1/17/2023 19:47
146	Dua Pham	dp999_98@yahoo.com	Arlene, Can you bring up my question. Don't ignore me	You will not be ignored - we are going through the questions as quickly and efficiently as possible. If your question is not answered in time, we will follow up with you by email.	1/17/2023 19:48
147	Micaela Canton	Cantonmicaela@gmail.com	Life science will create better jobs and quiet area instead of trucks and increase in smog	live answered	1/17/2023 19:48
148	Dua Pham	dp999_98@yahoo.com	Arlene, Can you bring up my question????? Don't ignore me	You will not be ignored - we are going through the questions as quickly and efficiently as possible. If your question is not answered in time, we will follow up with you by email.	1/17/2023 19:48
149	Gretchen Gary	gretchengary@gmail.com	The homes pre-date this project - silly statement about historic use of this land. It was zoned in the 50s when there was nothing out in this area	live answered	1/17/2023 19:49

150	Dua Pham	dp999_98@yahoo.com	No, I want to reply me today. Not follow up by email. I don't have time.	We will do our best to answer your question. We want to be respectful of everyone's time. Thank you.	1/17/2023 19:50
151	Dua Pham	dp999_98@yahoo.com	Molly, No, I want to reply today. Not follow up by email. I don't have time	We will do our best to answer your question. We want to be respectful of everyone's time. Thank you.	1/17/2023 19:51
152	Anonymous Attendee		This session has been very informative, thank you. I think many of us in our wonderful neighborhood are envisioning an Amazon like distribution center with a ton of vehicles coming in and out, clogging an already busy 76 hwy and the two only routes coming in and out of the neighborhood for about 1,800 homes. With Ocean Kamp also coming, you can see how concerned we are about regular daily traffic as well as access during emergency situations (we've had a couple in the past three years). That said, it's great to see more business coming in to Oceanside, which economically can help with tax base, jobs, etc. Are you able to break down the traffic comparison by hour and incoming vs. outgoing? Lumping everything together isn't providing insightful information since Life Science traffic going into the facility during AM rush hour doesn't have a negative impact on the neighborhood. By lumping it together it's impossible to parse the data and compare apples to apples. Wil be looking closely at your economic study which I presume you are preparing.	Thank you so much for your comment!	1/17/2023 19:54
153	Anne Antiqe	anife1267@gmail.com		live answered	1/17/2023 19:55
154	Anonymous Attendee			This is only required when a developer is changing the use.	1/17/2023 19:55

155	Cecilia Frierson	CeciliaFrierson.hoa@gmail.com	<p>How can you make so many traffic estimates when you have zero idea, so you say, on who the tenant is?</p> <p>And you can't keep these trucks from passing through our neighborhood, can you? In fact, it seems, other than a prettier building to hide with more vegetation than before, there's nothing you can promise on the impact to our peaceful lifestyle of our community. You might also consider that we really don't care about preserving industrial land; that's what the developers care about.</p>	live answered	1/17/2023 19:55
156	john chu	johnchu4@deloitte.com	<p>Would a light industrial site use mean something like an amazon distribution facility?</p> <p>This is a huge fire hazard for all the homes behind the airport. It is very impactful if my family cant get out. This is a huge problem!!!</p>	live answered	1/17/2023 19:56
157	Anonymous Attendee		<p>The entire river valley is an aquifer</p>	live answered	1/17/2023 19:56
158	Anonymous Attendee				1/17/2023 19:58
159	douglas smith	smithdp5121@gmail.com	<p>Where is the treated storm water being released from your property?</p>	The water is released into the City's stormwater system after being treated on site.	1/17/2023 19:59
160	Micaela Canton	Cantonmicaela@gmail.com	<p>Is it the height of building dictate by regulations close to airport ?</p> <p>Also Benet Rd will be almost used by the warehouse trucks other than residents and will cause a major safety issue</p>	live answered	1/17/2023 20:01
161	"Drew Andrioff, "Core Management	andrew@core-mgmt.com	<p>I just wanted to follow up to see if there is any proposed improvements to the benet / SR76 intersection as mentioned earlier from the responsible parties (CalTrans/City of Oceanside) ?</p>	This question has been addressed publicly.	1/17/2023 20:01
162	Dua Pham	dp999_98@yahoo.com	<p>This is not 2 ways meeting for resident in Oceanside on Benet area. The Eddie Jones Warehouse staff and San Diego planning are dominant the meeting. I have feeling you just want to send these messages to us.</p>		1/17/2023 20:07
163	Dua Pham	dp999_98@yahoo.com	No professional		1/17/2023 20:08
164	David Gaul	Davidgaul@cox.net	Why are you using Benet and not the larger Fousat?	live answered	1/17/2023 20:08

165	Anonymous Attendee		Can the property owner check in on their private street and work to keep the homeless vehicles out of there. We consistently see their vehicles parked there long term and people sleeping / living in them.	Yes - we have contracted with the police department to patrol the area. We've also brought in security to keep people off the property.	1/17/2023 20:09
166	Michael Caltabiano	local1234@hotmail.com	bull pucky a semi truck is like 4 cars in size and there is only one turning lane?????????	live answered	1/17/2023 20:09
167	Susan Mora	smora2525@gmail.com	Benet is a two lane road. Semi trucks make wide right turns. Will trucks be able to execute their turns into the property without crossing into the oncoming lane?	yes. The necessary turning radii have been accounted for.	1/17/2023 20:10
168	Dua Pham	dp999_98@yahoo.com	waste my time		1/17/2023 20:10
169	Anonymous Attendee		Will all employees at the facility be required to enter only from Fousatt Rd?	no.	1/17/2023 20:11
170	Dua Pham	dp999_98@yahoo.com	DICTATOR		1/17/2023 20:12
171	Micaela Canton	Cantonmicaela@gmail.com	Do you think that it makes the best area to have a warehouse close to a resort and a residential area just because the city planner did not change the area yet?!?And in regard the city planners, they are way back in data and quality control that is why Encinitas and other have a stricter regulations and requirements	live answered	1/17/2023 20:13
172	Celerina Cornett	ccornett@ucsd.edu	How will the increased traffic impact Oceanside's master bike plan on Benet?	Benet already has Class 2 bike lanes.	1/17/2023 20:16
173	Gene and Carrie O'Neal	hp11325@yahoo.com	Arlene, Can we get a recording of this Zoom meeting??	This meeting has not been recorded, but all materials will be publicly available.	1/17/2023 20:16
174	Dua Pham	dp999_98@yahoo.com	I recorded all activity and timing		1/17/2023 20:16

175	NEIL HANCOCK	tahoe772@yahoo.com	You talk about "vehicle trips" There is a difference between a passenger car and a 85 foot truck. Further explain fair share contribution of 76 intersection. Large trucks move very slowly anticipating turns and movement. Benet and 76 is a nightmare now for westbound turns. What have you thought about trucks exiting the property onto Benet - Justin just said trucks will be coming onto property every 4-5 minutes which is much more than 8-9 trips per your traffic study = sorry multiple questions!	live answered	1/17/2023 20:19
176	Anonymous Attendee		More sidewalks for the homeless ... no thanks Specifically, at the entrances and exits to the facility ... either for cars or trucks ... will there be merging lanes and a traffic light?		1/17/2023 20:23
177	Patricia Frazier	patanth5@gmail.com	At the other site ... where you built ... did property values decrease?	live answered	1/17/2023 20:24
178	Chris Kim	Chriskim@gmail.com	There is a homeless car parked in front of your sign right now. The same car goes back every night.		1/17/2023 20:24
179	NEIL HANCOCK	tahoe772@yahoo.com	FYI 18 wheeler trucks pull a 53 foot trailer plus a tractor for close to 85 feet long. They can carry up to 40 tons or 80,000 pounds.	live answered	1/17/2023 20:26
180	Cecilia Frierson	CeciliaFrierson.hoa@gmail.com	Hope your comments about weight limits on streets are correct. May we have the name of the development company?	RPG	1/17/2023 20:26
181	Lisa Urbach	lurbach3@gmail.com	Does the truck (the large 18 wheeler truck) count as one vehicle? They were advising the # of vehicles planned for each day with this project.	live answered	1/17/2023 20:27
182	Dua Pham	dp999_98@yahoo.com	It indicated that your guy ignor me. Now it is 8:30PM		1/17/2023 20:33
183	Micaela Canton	Cantonmicaela@gmail.com	It is normal to change the zoning, so as you saying it was there 50 years ago does not change the fact that it could be change and can be litigate in the future.	While some property owners may choose to seek a General Plan amendment, RPG is choosing to utilize the existing land use designations. The Oceanside General Plan update is in process and this site is still identified for industrial use.	1/17/2023 20:37

184	Michael Caltabiano	local1234@hotmail.com	how about adding lanes to the road in and out that would be the deal maker	live answered	1/17/2023 20:37
185	NEIL HANCOCK	tahoe772@yahoo.com	Will you release/post the CEQA and traffic study now before the EIR release	live answered	1/17/2023 20:47
186	Anonymous Attendee		Do we have any 18 wheelers coming in and out of the site?	historically, there were and the project will have them	1/17/2023 20:53
187	Marty Basham	PRTYMRTY@HOTMAIL.COM	Thank you for hosting the call.	Thank you for joining us.	1/17/2023 20:56
188	Micaela Canton	Cantonmicaela@gmail.com	Data was during covid? Also is it included 24 hrs work daily?	live answered	1/17/2023 20:58
189	Debby Herbert	kayadeb36@outlook.com	But we need to know what percentage traffic increase will be from the left turn onto Benet Rd from Hwy 76.	I will follow up with you when i get the # from Justin	1/17/2023 20:58
190	Robert Dmohowski	rdmohowski@oceansideca.org	Hi Arlene, I just wanted to acknowledge that city staff has listened in all night.	Thank you for joining us.	1/17/2023 20:58
191	Michael Caltabiano	local1234@hotmail.com	yes but 90% of traffic is turning right on to west bound 76 not going straight or left	live answered	1/17/2023 21:01
192	Anonymous Attendee		i think they are saying Benet to Foussat		1/17/2023 21:01
193	Michael Caltabiano	local1234@hotmail.com	and now we will have to compete with all the trucks doing the same		1/17/2023 21:02
194	NEIL HANCOCK	tahoe772@yahoo.com	Thank You this was helpful	Thank you for joining us.	1/17/2023 21:03
195	Michael Caltabiano	local1234@hotmail.com	If there is no stops what is your completion date looking like?	Public hearing is anticipated for the end of 2023, with construction starting in 2024.	1/17/2023 21:06
196	Gene and Carrie O'Neal	hp11325@yahoo.com	Thank you for this opportunity. Good night.	Thank you for joining us.	1/17/2023 21:09
197	Debby Herbert	kayadeb36@outlook.com	I would also like a copy of this presentation.	We will be sure to send you a copy, Debby. Thank you!	1/17/2023 21:11
198	Gretchen Gary	gretchengary@gmail.com	You've spent hours deflecting questions with the same "the land is zoned as industrial" position. Basically I'm hearing "too bad you guys bought here" and "we're doing what we want regardless". Clearly this was a waste of time for all - I thought the developer was willing to hear our concerns and consider modifications to the plan. Was that never the case?	live answered	1/17/2023 21:16

199 Michael Caltabiano	local1234@hotmail.com	I understand that the city and caltrans make the improvement to the roads but traffic is already bad and you guys keep saying that your with in the rulls and requirments but that still does not fix the traffic issue today or when your project is done		1/17/2023 21:20
200 Debby Herbert	kayadeb36@outlook.com	Thank you.	Thank you for joining us.	1/17/2023 21:26
201 cecelia moreno	morenocecelia6196@gmail.com	I think what would really help the neighborhood would be that developers get together and lobby Cal Trans to imprve hwy 76		1/17/2023 21:30
202 Debby Herbert	kayadeb36@outlook.com	I think it's the size of the project that is a misfit with the residential communities and is why it is not compatible. You can't exclude the interests of 1200 homes, thousands of residents.	live answered	1/17/2023 21:30
203 cecelia moreno	morenocecelia6196@gmail.com	I travel it daily and there is roo for restripping and adding lanes...the shoulders are hugeand the lights are poorly coordinated.	We will look into this. Thank you Cecelia.	1/17/2023 21:31
204 Debby Herbert	kayadeb36@outlook.com	It's too large, it could be made smaller.	live answered	1/17/2023 21:33
205 Michael Caltabiano	local1234@hotmail.com	thank for your time guys, I think if you all lived here and were inpacted as we are you might see it diff.....	live answered	1/17/2023 21:33
206 cecelia moreno	morenocecelia6196@gmail.com	Thank you for a very informative meeting. all of your time and patiece is greatly appreciated. Personally I think this project is a good and fair use of the land.	Thank you so much for your comment!	1/17/2023 21:33

- Exhibit 11 – 7/12/23 Slides



EDDIE JONES

WAREHOUSE, MANUFACTURING &
DISTRIBUTION CENTER

PROJECT UPDATE MEETING
JULY 12, 2023



Meeting Agenda

July 12, 6:00 – 8:00 p.m.

01

Project Overview

02

Issues of Concern

- Transparency, Traffic, Safety, Environmental Impacts, Alignment with City vision

03

Tenant(s)

04

Traffic

- Trucks
- Analysis and methodology

05

Next Steps

- CEQA
- Next Community Meeting

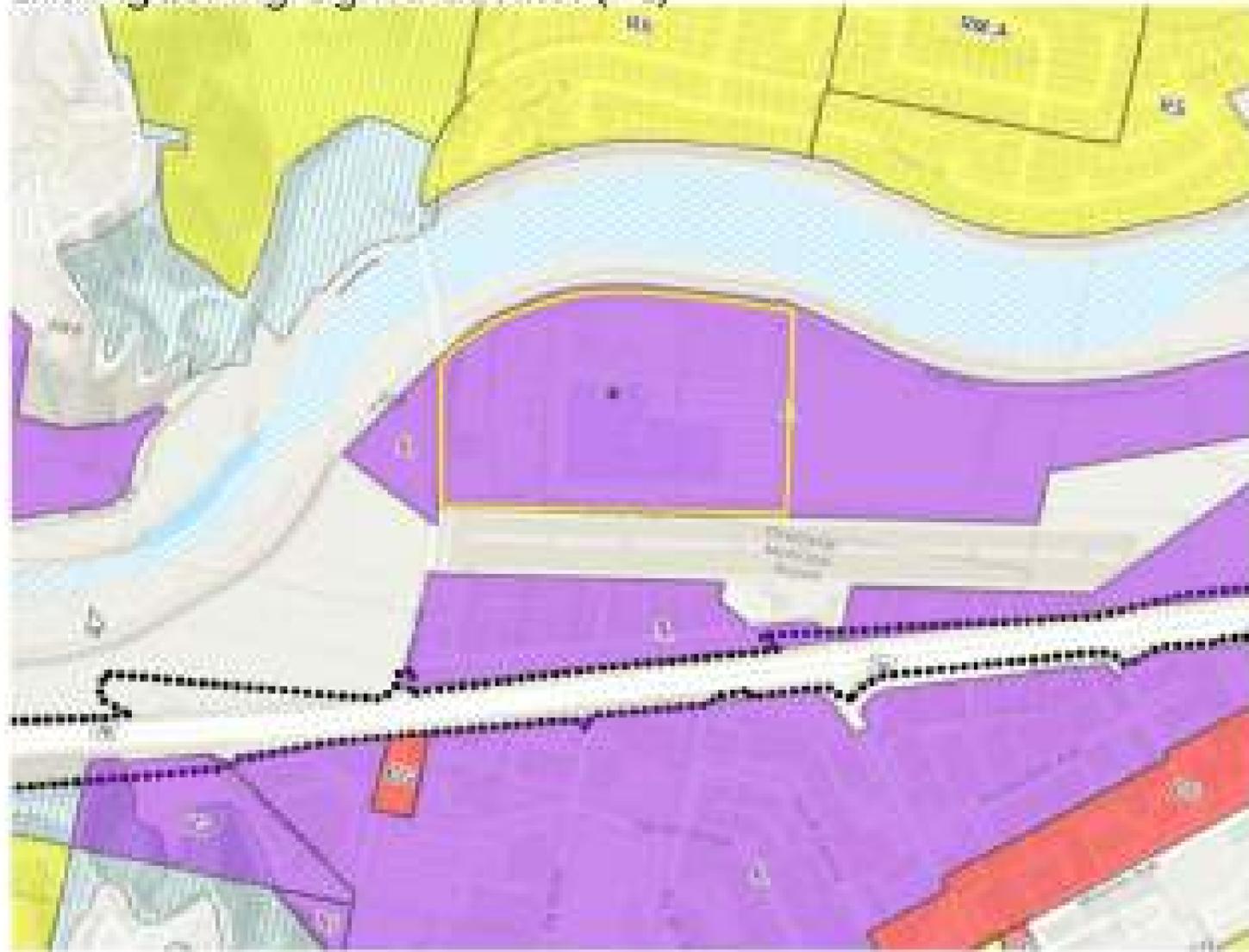
06

Q&A

Location

30.79 acres of industrially zoned land located at 250 Eddie Jones Way

Existing Zoning: Light Industrial (I-L)



Community Context



▼ Project Overview

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient facility in a context-sensitive way.

General Plan Consistency

- Consistent with the existing General Plan designation and zoning for the site



▼ Project Overview

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient facility in a context-sensitive way.

General Plan Consistency

Building Footprint: 547K SF

- 65% Warehouse
- 28% Manufacturing
- 7% Office



▼ Project Overview

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient facility in a context-sensitive way.

General Plan Consistency

Building Footprint: 547K SF

Context-Sensitive Design

- Maintains the 100-foot buffer from the edge of the San Luis Rey River riparian habitat
- Floodwall (instead of increased pad elevation) and stormwater management
- Complies with Airport Overlay Zone
- Includes landscape buffers to shield from public roadways and residential uses



▼ Tenants

RPG is a local, north county developer, known for attracting the best tenants with our our state-of-the-art, creative Industrial, mixed-use and office facilities.



▼ Tenants

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▼ Tenants

RPG is committed to pursuing the same quality tenant and high paying jobs as we have in other local communities.

We have done the following to ease concerns over the lack of a tenant:

- Commissioned an economic study focusing on life sciences
- Engaged a broker
 - Very unusual at this stage
 - Companies will not sign a lease for something that does not yet exist
- Refined Tenant Wish List
 - Identify local companies that would need this type of facility



▼ Trucks

We are reducing the number of truck bays by 50%!

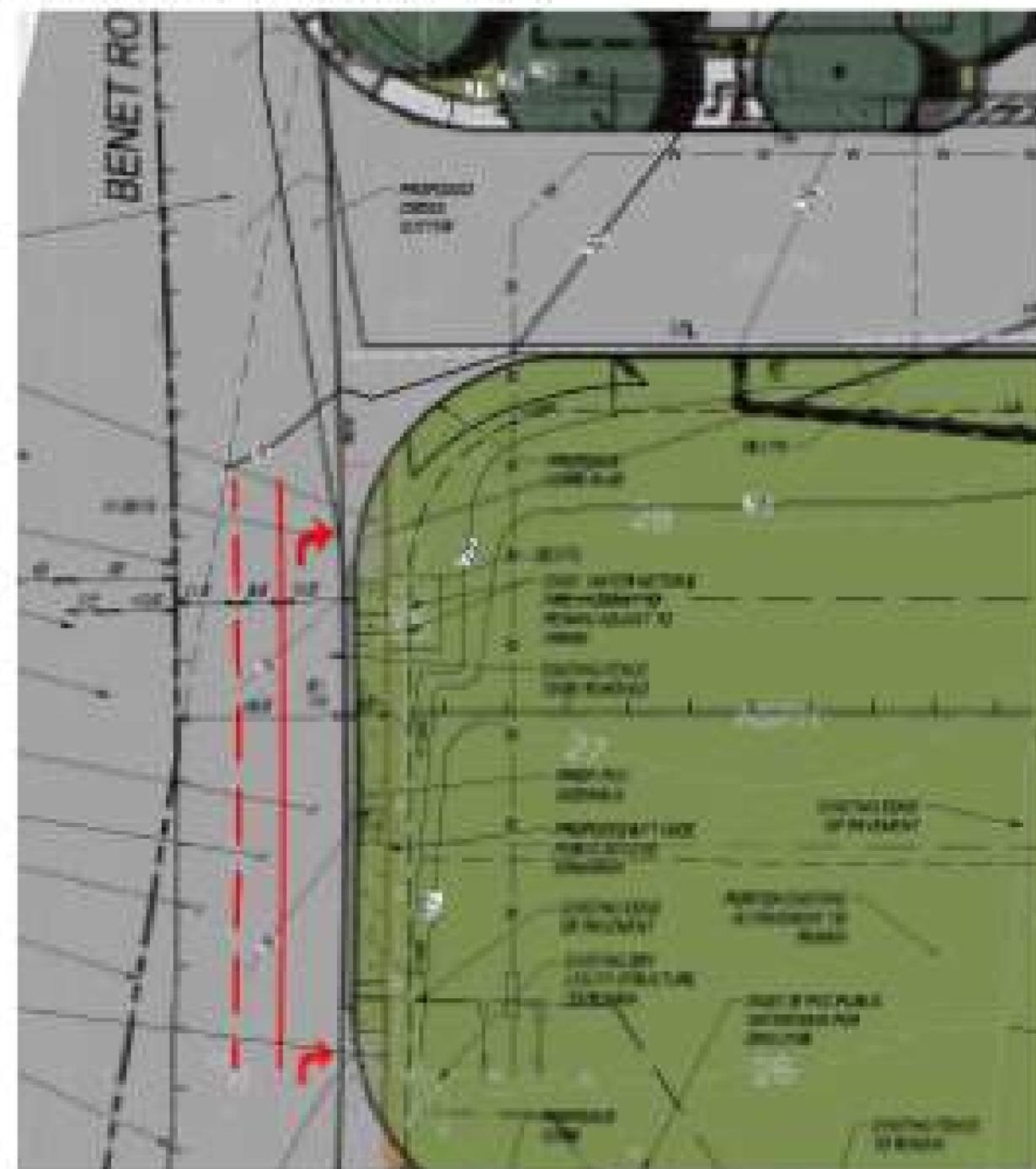
- The original proposal included 114 truck bays. The new proposal will include 57.
- Flexibility with Transparency
 - Respond to community feedback
 - Still meet marking need for distribution capabilities
 - Including the bays doesn't mean they'll be used
 - Prospective tenants will be reviewed by City and public meeting at Planning Commission

▼ Traffic + Circulation

Project Modifications since Jan. 17 Meeting:

- Reduction in truck bays
- Dedicated right turn lane into project
- More conservative approach to analysis in traffic study
 - Additional cumulative Projects
 - Removal of credits

Application update in process



▼ Traffic + Circulation

Project Modifications since Jan. 17 Meeting:

- Reduction in truck bays
- Dedicated right turn lane into project
- More conservative approach to analysis in traffic study
 - Additional cumulative projects
 - Removal of credits

Analysis Methodology

- Analysis is based on objective metrics and standard formulas to calculate average daily trips

The traffic study will be available for review and comment as part of the public circulation of the Draft EIR this fall.

▼ Traffic + Circulation

Project Modifications since Jan. 17 Meeting:

- Reduction in truck bays
- Dedicated right turn lane into project
- More conservative approach to analysis in traffic study
 - Additional cumulative projects
 - Removal of credits

Analysis Methodology

- Analysis is based on objective metrics and standard formulas to calculate average daily trips

Evacuation

- Fire is one of the many City disciplines that reviews the project application
- Building is designed to be ignition resistant so unlikely it would have the same evacuation protocol as residential uses
- Evacuation protocol would ultimately depend on the nature of the emergency

The traffic study will be available for review and comment as part of the public circulation of the Draft EIR this fall.

Availability of public facilities, including Fire, is also evaluated in the EIR.

▼ Next Steps

Public Review
of Draft EIR

September 2023

Community Meeting #4 to
take place during public
review period

Continued
Community
Engagement

On-Going

Available for one on one
and small group meetings

Respond to
Comments

Q3 2023

All CEQA-related comments
will be responded to in the
final EIR

Oceanside
Planning
Commission

Early 2024

Public Hearing





Thank you

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient facility in a context-sensitive way.



outreach@eddiejonesproject.com



- Exhibit 12 – 7/12/23 Questions
 - c. Index cards
 - d. Virtual

Arlene Tendick

From: MREGAN
Sent: Wednesday, July 12, 2023 11:00 AM
To: Arlene Tendick
Subject: Eddie June's Community Meeting Tonite

[Report This Email](#) [FAQ](#) [GoDaddy Advanced Email Security](#), Powered by INKY

One of my questions and concerns are how will you be handling the overflow of more traffic into the designated place to be built especially when a fire should erupt and we only have 2 access bridges to our homes...TY

Sent from my iPad
Mary Regan

Arlene Tendick

From: MREGAN
Sent: Wednesday, July 12, 2023 11:17 AM
To: Arlene Tendick
Subject: Eddie Jones Community Meeting Tonight

External (rregan311@msn.com)

[Report This Email](#) [FAQ](#) [GoDaddy Advanced Email Security](#), Powered by INKY

Another question if I may....

Will traffic coming into the facility from Hwy 76 come thru Foussatt only? TY

Sent from my iPad
Mary Regan

We have 2 roads
in and out. One end
will have an Ocean
Park is then you with
the trucks.
Kathy

Your rep said only ~~13~~
13 trucks? Why are there
114 days?
Who is your renter?

JEORDIE

Assessment +
evaluation - ongoing
after tenant +
development in place

So for fire evacuation, this project
won't have an impact? Totally
disregarded. It's the traffic that this
project adds that could impact the
ability of residents to leave in a
timely matter. Last summer an
elderly was crushed & the fire would
fast. You are saying the additional
traffic on Benet & Fausset would
delay evacuation. Totally untrue.

You stated both exit/entry roads,
Benet & Fausset, will be used.

Did the traffic report consider the
Wave Park + hotel + 700 homes +
Airstream park in addition to
current traffic levels? Fire
evacuation standards is a major
safety concern. Thank you.

Right turn for Truwater entering
the site. What about traffic come
out to go out of the site?

Only 2 ways in or out, evacuate this whole community. Even with a dedicated right lane, still massive traffic on Bendland

No. Redundant? How would we really know how many will →

ELIZABETH WITH
CLEAR AIR 4 KIDS +
MOUTH COUNTY CLIMATE
CHANGE ALLIANCE.

WILL THE MANY MANY
TRUCKS BE ELECTRIC?
~~WILL~~ WILL THEY STORE?
FUEL ANYWHERE ON SITE.
LEAD?
LEAD?

Can you explain
% ownership of
property with
Carlyle Group?

IF THERE ARE ONLY
13 TRUCK TRIPS IN A.M.
AND 9 IN P.M.
ACCORDING TO ARLENÉ—
WHY ARE YOU PROPOSING
57 DOCKS?

HOW TALL IS THE FLOOD WALL
—WILL TO LOOK LIKE A PRISON

HOW IS SOUTH BOUND TREATED
EODIE ~~ROADS~~ TO 70' ~~UP~~
APPROX 800 FT & SEMIS
CAN BE OVER 70'.

Why haven't you designed
a project that meets
existing city planning standards
and does not require CUPs.?

How will you ensure the bike lane on Benet will be safer with "big rigs" on it?

you have shown ^{many} other neighborhoods - How many of these neighborhoods have only two entrances & exits

(Leah Conroy)

- ① How tall is the flood
- ② Wall & how close will it be to the public bike path?
- ③ Are you concerned w/ the impact (traffiz) upon Hwy 76?

What assurances do we have
warehouse workers will shelter in
place during a fire. Personal bravery,
Oceanside code enforced by OPD?

Thank you !!

George

MARENGO

Expected Truck Traffic at
various times of day/night

How will this project
Benefit Oceanside?

IF THIS Project is approved,
You can rent/lease to
ANY Tennant? You can
also resell the property
to ANYONE FOR ANY
USE?

If this project with a
warehouse does not
get approved, what other
project has been considered
for the area?

Would you be in favor
of this project if it
was in your neighborhood?

Thank you.

John Guthrie

~~THE~~

HOW ACCURATE CAN THE
TRAFFIC STUDY WITH FINAL
TENANTS ARE UNKNOWN

Can you explain how
Carlise and Thermo Fisher
Scientific fit into your
development of this site
Dave Keel

Are you going to widen Benet
road?

Donna Jenkins

HAVE SEVERAL QUESTIONS

On June 23, CBS news
ran a story about the
Eddie Jones Project. In the
news story, the reporter
said another lane would
be added @ Benet \$ 76
for the thousands of

semi trucks. Last meeting you said that was not happening.

Has a turn lane been added in the project plan?

Timmy Knott III

- 1) 1916 Flood Review? SD CWA All Docs.
Upstream tree construction
- 2) Dumpster to have on Site? no
- 3) Buffering Light Pollution? Prob & Aircraft
Habitat
- 4) Lithium Safety on Site? New Vehicles
- 5) Community Participation? (over)

6) Environmental Justice - Helping
Low Income, Seniors, Homeless

7) Salt Water Intrusion -
Drought / Flooding issues

8) Public Safety Training /
Supplies
education
Advanced CERT?
Equipment on Site?

Ken McGillick 3425 SPANORIK
CT.

1. you're telling us that in
case of a fire 13-57
Diesel trucks full of fuel will
shelter in place at your site
That doesn't seem very safe!

~~2. you keep asking for zoning~~

2. you keep asking for zoning
But you're asking for 3.5 times

Questions Submitted Via Zoom - 7/12/23

Asker Name	Question
G. Gary	Please tell me you are proposing a different project tonight rather than just pushing the same project everyone in the community has already said they do not accept.
M. Basham	I would like a copy of the visual you provided this evening.
M. Basham	This project is obscenely large. Do you have any other examples of projects this large built within 1500 feet of residential homes just 2 miles from the beach? Projects of this size and magnitude are typically built in heavy industrial areas like international airports. Why are you pushing so hard to build something like this in coastal Oceanside? Why do you have such a lack of building innovation?
G. Gary	No one is asking for that extra Lane. That was something you guys came up with.
H. Diaz	This is really interesting and I appreciate the conversation about working to resolve the existing soil contamination. Additionally, I understand that this project will be north of \$100 million. Seeing as mentioned, there is not a similar site in Northern San Diego County currently, what is known about associated air contamination during site preparation, during build, and during on going operations? Can we expect similar air quality levels to the referenced Otay Mesa site?
D. Pham	Why Adam show us these building that Eddie Jones built in Calrsbad. I need to see how engineers staff deal with SAFETY ISSUE and POLUTION ENVIROMENT THAT EDDIE JONES's trucks will in and out on the Bernet Rd. These trucks will be impacted to our community residents
Annonymous	Are there any studies on air quality levels or best practices for air montiroing? I tend to think of not only the manufacturing processes, but also the pulverized tire thrown into the air from the heavy vehicles utilizing the many truck bays
D. Pham	Benet Road
G. Gary	Traffic is not my primary concern. I'm more concerned about back up alarms of 57+ big rig trucks and all supporting fork lifts sounding off round the clock. I can heard the back up alarms from the trucks at the rock quarry when they start working early. Sound carries across this valley. You never address this concern along with the impact to our property values if this project is approved. No one wants to buy or rent homes by a massive distribution center.
H. Diaz	Further, has it been considered that this site sits at the mouth of a river valley, which currently brings an ocean breeze deep into the valley. Has there been any studies or consideration of potential pollution impacts? Will particulate matter, benzoapyrene, or other chemicals commonly associated with heavy truck use be carried into other neighborhoods proximate to the river valley?

G. Gary	Does the developer really know what OSO is asking for? It's not an extra lane on Benet Rd. We want a modified project proposal. The firm position to the city council and planning commission is we want a different project on this land.
D. Nygaard	I know of two projects in oceanxside that did project related evacuation time studies- which seems very appropriate to do here. Would you considerf doing that study to ensure the project will not adversely impact evacuation of that area.
D. Pham	I would like to send my EMPHASIZE two main point are: 1.- SAFETY ISSUE when those trucks turn right on Benet Rd it will cause incident for residents, who ride bike on the bike road next to the building of Eddie Jones project. It is not safe at all. 2.- AIR POLUTION of all trucks exhault, they will cause air polution for your health to breath. Those toxit air will harm our lungs. Please consider
B. Scholte	Thank you for holding this meeting and demonstrating good faith in responding to many of the communities concerns. Do you have an image of what the flood wall is?
J. DeAnda	Hi. Thank you for saying that you will work with the city to add a right turn lane into the project. What about the truck and car traffic for coming and going from the 76 hwy.
L. Davis	How about traffic on 76, not just Benet traffic?
C. Diego	I dont think the dedicated turning lane on Benet is wide enough to fit more than 1 truck to stop the flow of Traffic on Benet.
V. P	What about the truck traffic on Highway 76 BEFORE getting to Benet?
C. Diego	Are you able to tell us what the property is contaminated with? There has recently been well insilation to extract grounbd water samples and we'd like to know the results of those.
J. DeAnda	Are your prospective tenants going to operate 24/7
J. DeAnda	If there is a fire and you have 57 trucks trying to evacuate. How will we also evacuate. Having a fire just just less than a year ago on Wala this is a major concern.
G. Gary	The homes in the surrounding residential community now pre-date your project so you have to consider the impacts. Why do you continue to disregard the impact this will have on residential homes surrounding this land?
C. Diego	Are you going to tell us what the property is contaminated with? There has recently been well insilation to extract grounbd water samples and we'd like to know the results of those.
D. Pham	My suggestion is: Those trucks should take Foussat Rd instead Benet Rd.
H. Diaz	What kind of plume modeling was done prior to disrupting the contaminated soil to avoid an airborne toxic event?
D. Pham	I would like to bring up about AIR POLUTION according to Violation of California Health & Safety code 17920-3
J. Bottorff	How are you addressing the noise pollution from all of the diesel trucks coming and going and idling? Diesel air pollution is incredibly toxic

J. Bottorff	What complaints have you received from the Carlsbad facility?
L. Davis	You keep referencing the previous industrial building/site however it was a fraction of the size of your project. Perhaps proposing a similar sized project would make your point more meaningful but it's a very different comparison. Thoughts on not just reducing the number of truck bays but reducing the size of the building? That may be more meaningful if you're truly trying to work with the community.
K. Whittemore	1) Has Prince of Peace Abbey weighed in on the Project? 2) It would seem the Mission Cove, Community Housing Works, La Mision and other residents do not seem to have been informed like the residents of north of San Luis Rey. I suggest you reach out to these hundreds of residents ASAP
L. Davis	Is CalTrans going to be looped into this because turning onto Benet is already a massive problem?
L. Davis	What years were used in the traffic data collection being referenced?
C. Moreno	what is OSO
N. Scott	Late to join. Apologies. Are you doing an economic impact study? We'd like to know how the city and its residents could possibly benefit from a project that is way beyond what would normally be allowed there.
D. Selnick	Based on the reduced number of truck bays to 57, what percentage of the project do you expect to be warehouse space
G. Gary	A 58k sq ft building in Escondido is a tiny fraction compared to what is proposed at Eddie Jones at 550k sq ft - hardly a good comparison. The building proposed you are proposing in our backyard is massive, bigger than any standing building I know of in coastal North County. How do you justify the size?
C. Diego	The gentleman just said the ground water/soil is being monitored for contamination, I believe it is important for us to know the results of what the ground is contaminated with. And how do you plan to "remove" the contaminated soil???
G. Gary	Truck trips was a conservative metric. If the lower end estimate is 3 hours to unload and assuming that most trucks will not reload on site (only a big player like Walmart or amazon would unload and load trucks in the same location), there could technically be 4X the estimate that was just discussed.
G. Gary	What could be worse than a 550K sq foot distribution center in your backyard? I can't think of anything.

- Exhibit 13 – 7/12/23 Attendees
 - a. In person
 - b. Virtual

Exhibit 13a - 7/12/23 Attendees (in person)

First	Last	Email
Scott	Becker	flyual737@aol.com
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Judy	Clarkquist	jclarkquist@gmail.com
Edmund	Conroy	ecseagreen@gmail.com
Leah	Conroy	lconroy68@gmail.com
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Marcy	Daniels	MLDaniels@cox.net
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Andrew	Donaldson	screte@cox.net
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Mario	Flores	marflo35@gmail.com
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Danielle	Geggie	geggied@gmail.com
John	Guthrie	jpguthrie@cox.net
Linda	Hagen	lhagen624@gmail.com
Donna	Jenkins	jenkins_cbo@yahoo.com
Edwin	Jenkins	ejenkins@sprintmail.com
Dee and Dave	Keck	4des@cox.net
James	Knott	jhk3@cox.net
Michael	Kosec	mikekosec@gmail.com
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Craig	Lang	cloffshoresemis@gmail.com
Madison	Lang	madisonlng@yahoo.com
Mikhael	Madello	mikhaelmadello@gmail.com
Isabel	Marcus	1cinderina@gmail.com
George	Marengo	george.marengo@gmail.com
Ann	Marshall	willann909@yahoo.com
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Ernesto	McCallick	kenmccallick76@gmail.com
Merlin	Mcgovern jr	mmcgvornjr@aol.com
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Susan	Mora	smora2525@gmail.com
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Toni	Padron	tonipad92@gmail.com
Dua	Pham	dp999_98@yahoo.com
Joanne	Pilgrim	joanne.pilgrim2425@gmail.com
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Marilyn	Smith	smith.marilynkay@gmail.com
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Kim	Stone	kimstone899@gmail.com
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Rick	Taylor	rkt.sce@gmail.com
Paul	Terwilliger	irishsealass@outlook.com
Arlene	Thompson	nycitygirl2014@gmail.com
Martin & Lisa	Urbach	lurbach3@gmail.com
Alan	Waite	battcar@sbcglobal.net
Robin	Waite	Robinwaite077@gmail.com
Debra	Wanamaker	ocswana@gmail.com
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Michelle	Wilson	mdolaybar@hotmail.com
FATHER CHARLES	WRIGHT O.S.B	2abCharles@msn.com

Exhibit 13b - 7/12/23 Attendees (Virtual)

Webinar ID: 826 7866 4663 Actual Start Time: 7/12/2023 Duration (minutes): 147 Unique Viewers: 42

Host Details	Email	Time in Session (minutes)
Arlene Tendick	arlene@ant-sc.com	148

Attendee Details	Email	Time in Session (minutes)
Marty Basham	Prtymrty@hotmail.com	135
John Bottorff	j@cleaneearth4kids.org	68
Fran Cusano	Fran.cusano@gmail.com	52
Gail Dachenhausen	gfdach55@gmail.com	48
Laura Davis	Lajadavis@gmail.com	133
Julie DeAnda	juliedeanda@mac.com	139
Haley Diaz	Haleydiaz3@gmail.com	193
Cassandra Diego	casruiz1011@gmail.com	141
Richard Eason	reasonr1@yahoo.com	123
Anna Floyd	akasperowicz@yahoo.com	88
Pat Frazier	patanth5@gmail.com	146
Gretchen Gary	gretchengary@gmail.com	144
David Gaul	Davidgaul@cox.net	13
CYNTHIA GLADSTONE	cynthia.gladstone@gmail.com	16
Joe Hill	wethehills@gmail.com	130
Suzanne Hume	S@CleanEarth4Kids.org	7
Robert Kale	judykale@gmail.com	139
David Kertenian	dkertenian@icloud.com	36
Jochen Kressler	clabber_lang@yahoo.com	95
Keya Kressler	Taylorobe@gmail.com	96
Tina Luo	Luct9@hotmail.com	15
Lee Masters	leemasters100@yahoo.com	134
William Molineaux	mxbillyjo@gmail.com	114
cecelia moreno	Morenocecelia6196@gmail.c	216
wendy morris	beachwendy@gmail.com	70
Diane Nygaard	dnygaard3@gmail.com	97
Vee P	vptraveler@juno.com	136
Dua Pham	dp999_98@yahoo.com	69
Cody Powers	Jmaccabeus1@gmail.com	31
Brian Scholte	bscholte@gmail.com	119
N Scott	Nadia550@sbcglobal.net	68
Anne Selnick	Anne.Selnick@gmail.com	147
Darin Selnick	darin.selnick@gmail.com	144
Dave Sergeant	dave@sergeant.com	145
Sandra Smith	Sandraksmith8120@gmail.co	72
Jeff Stanford	stanfordjp@verizon.net	143
Carol Steffen	cs22620@gmail.com	144
Darren Swain	darrenswainoside@gmail.com	68
Patricia Tirona	Ptirona@gmail.com	22
Alison Topaloglu	atopaloglu@cox.net	38
Kim Whitemore	kimwhit@gmail.com	133
gary wright	madmony999@cox.net	101

- Exhibit 14 – 11/14/24 Questions
 - a. Questions submitted in advance
 - b. Questions submitted during meeting

Exhibit 14a. - 11/24/24 Questions submitted in advance of meeting

Submitted	Commenter	Question	Response
10/31/24	Richard Taylor	What are these "alternatives "? I believe the first two were either one or more industrial projects with over 100 truck docks and more. What are the latest two options?	Project alternatives can be reviewed in the project Draft EIR on the city's website: https://www.ci.oceanside.ca.us/government/development-services/planning/environmental-documents-ceqa .
10/31/24	Randy Hanson	We recent saw a yellow sign on the property talking about the future project. Just last week that sign was taken down. Is that helpful for those who oppose the project?	The sign has nothing to do with support nor opposition. It is a city policy to post an application notice to any project site that is under review. We were told yesterday that one of them had been stolen so we are in the process of replacing it.
11/1/24	Michael Bowron	What will the City Of Oceanside do to modify the Hwy 76 and 2 side roads that are perimeter roads leading to and from this project, is so, who will pay for the modifications?	The project contributes to a fund that will improve that intersection in the future. The project doesn't generate enough trips on its own to trigger immediate action.
11/1/24	Michael Bowron	Thieve streets and highway are not designed for this volume of vehicles.	The purpose of the LTS is to objectively document the capacities of the study roadways and to determine if project traffic will exceed the City threshold of additional traffic.
11/4/24	Marilyn Smith	Project ownership has agreed to add a fourth project alternative. What is it? Would like to know so I can ask questions ahead of meeting.	The fourth option is reduces square footage to 491k and 57 truck bays. It responds to community concerns by providing a less intensive project that still falls within analysis of larger project.
11/4/24	Miranda Palmer	Why consider expanding outside of the current zoning regulations?	We're not. The project as proposed is consistent with the existing General Plan designation and historic industrial zoning for the site. The General Plan is currently being updated and the Industrial Zone is being maintained. Additionally, the project aligns with City priorities. The City's Economic Development Element estimates that the City will have a deficit in industrial land during this planning period and that Industrial land should be preserved.
11/4/24	Miranda Palmer	What benefit is there to the community to bring in more semi traffic on a road that is consistently backed up and going at a snails pace at around 20 mph?	The project cleans up an under utilized site, installs necessary infrastructure to treat stormwater runoff, brings good jobs within the city's industrial core, provides a modern facility to help meet the needs, and is consistent with the city's general plan.

11/4/24	Miranda Palmer	I still believe the previous traffic studies were done when the traffic was still impacted by covid shutdowns- how current is the traffic study?	<p>The traffic study was conducted in October 2021. The complete traffic study will be available for review in the Draft EIR this fall. It is important to note, there were no stay at home orders in place at the time and school was in session.</p> <p>Analysis isn't solely based on this one metric, so there is no need to re-collect the data. The traffic counts are combined with objective industry standards which are collected over time.</p>
11/5/24	Scott Becker	Please reveal fourth project option with great specificity! Hopefully it will be acceptable if adjacent to the developer's residence!	The fourth option reduces square footage to 491k and 57 truck bays. It responds to community concerns by providing a less intensive project that still falls within analysis of larger project.
11/8/24	Jason Anderson	The traffic impact report identified the intersection of SR-76 and Benet Rd as the only intersection that would be impacted beyond acceptable traffic levels. The Eddie Jones project is the only project in the area that will impact traffic taking the left turn from the eastbound side onto Benet Rd, and traffic turning from Benet onto SR-76. What measures will the project take to ensure traffic for those turns specifically do not exceed acceptable levels?	<p>Traffic from 18 cumulative projects causes the impact at SR-76/Benet Rd (LOS E). The project applicant will pay a fair share toward the cumulative impact at SR-76/Benet because the Eddie Jones project is only a portion of the traffic from 18 cumulative projects.</p> <p>As documented in the LTS in Section 3.7, if Eddie Jones is the only project, then there would NOT be an impact to the intersection of SR-76/Benet Rd.</p>
11/11/24	Susan Mora	<p>Please explain the 4 different versions of this project.</p> <p>Im unable to ask any further questions about the new proposal as no information was provided prior to the question submission deadline. It's disingenuous to say you're allowing resident input when no information is provided before questions are cut off. In future please provide written information about the proposal in advance so that appropriate questions can be raised.</p>	<p>The invite went out to a lot of people, some of whom have never attended our meetings. We requested questions, not to be disingenuous, but to identify where the gaps are and make sure we didn't spend too much time on questions that have already been answered in past meetings or those that are duplicative. We fully plan to review the new option in great detail during the meeting.</p> <p>Project alternatives can be reviewed in the project EIR on the city's website: https://www.ci.oceanside.ca.us/government/development-services/planning/environmental-documents-ceqa.</p>
11/12/24	David Gaul	Wouldn't it be better to have majority of traffic use Fousatt rather than Benet road for ingress and egress due to its larger size, turn lanes, and number of lanes.	Drivers typically select the shortest route to reach a destination. The majority (60%) of project traffic travels to/from the west and therefore, there is a natural draw to use Benet Road over Foussat Road. The LTS analyzed the project traffic based on the heavier draw to I-5 with 67% using Benet Rd.

11/13/24	Sandra Smith	How can we submit questions when we don't know what the proposal is? If you're supposed to be transparent with the residents, you need to provide all of the information!	The invite went out to a lot of people, some of whom have never attended our meetings. We requested questions, not to be disingenuous, but to identify where the gaps are and make sure we didn't spend too much time on questions that have already been answered in past meetings or those that are duplicative. We fully plan to review the new option in great detail during the meeting.
11/14/24	Windy Bravo	The 30.79 acre Eddie Jones Project has already destroyed all test water quality wells to promote a slick 'shell-project design idea' in search of a tenant, just seven feet above the San Luis Rey River water table in the flood plain - so what plans has RPG for moving the highly unpopular Eddie Jones Project out of vulnerable San Luis Rey Riverbed, away from 4000 homes and a 132 acre monastery to a more suitable site elsewhere in Oceanside or in another town with appropriate supporting infrastructure and no multi-species riparian habitat to impact?	This line of questioning is based on a flawed premise. No impacts to water wells. No river or MHPA impacts.

Exhibit 14b - 11/14/24 Questions Submitted During Meeting

#	Question	Asker Name	Answer
1	I'd like to know what traffic mitigation will be addressed on Hwy 76. It's already terrible; it will be much worse with this project.	Gern B	live answered
2	When you say "4th options" how does that relate to what you are asking the city to approve? Do you give them 4 options and ask them to approve one or multiple from which you can decide which of the approved options you want to go with.	Brian Scholte	live answered The Final EIR, which will address the proposed project plus four alternatives, including the 4 building option. The complete document will be considered by the city decision makers at a public hearing. Through that process, the city will decide whether to certify the Final EIR. Our official ask is for our proposed project, but the city ultimate decides what to implement from the certified document.
3	Is anything being done to allow an additional exit for emergencies?	Andrea Targhetta	live answered
4	I count 56, 12+12+14+18, truck bays and I do not see parking for all of your proposed employees.	Douglas Smith	live answered
5	I understand that you say that you are not utilizing the footprint as permitted; however, you are still over the allowed truck bays that are currently permitted. Industrial use is fine, but still too many truck bays	Anonymous Attendee	live answered

6	<p>I understand that you say that you are not utilizing the footprint as permitted; however, you are still over the allowed truck bays that are currently permitted.</p> <p>Industrial use is fine, but still too many truck bays.</p> <p>Why should you be allowed more truck bays than are currently permitted???</p>	brandy b	<p>live answered</p> <p>As Dan discussed, the number of truck bays isn't part of the zoning or development standards. We're requesting a Conditional Use Permit because the city requires a CUP for any facility over 50,000 SF with more than 6 bays. It's not that a bigger facility or more bays aren't allowed, it's just added oversight the city has codified FOR BUILDINGS ABOVE A CERTAIN THRESHOLD.</p>
7	<p>There is no mention of hours of operations.</p>	Dee Keck	<p>The hours of operation have still not been determined. No change on that front.</p>
8	<p>What is the proposed number of trucks?</p>	Dee Keck	<p>The traffic study for the proposed project (large, singular building) projects 13 trucks at the morning peak hour, and 9 trucks in the pm peak hour. The 4 building option is a less intensive option that still falls under the original proposal.</p>
9	<p>I have read the postings on traffic but do not understand the material. Simply asked, will changes be made to Benet Rd to accommodate the increased traffic? Can truck traffic be routed through Fousatt (more lanes to accommodate truck traffic)?</p>	Gern B	<p>I'd be happy to set up a time for us to meet, either virtually or in person, to discuss. In the meantime, I've added additional questions and clarity to the project FAQ page.</p>
10	<p>How many employees will be employed and will they be working 24 hours</p>	Joe Hill	<p>live answer</p>
11	<p>Please let us know if the traffic aspect will be addressed at some point. Concern is that there will be dozens and dozens of trucks coming and going on Benet.</p>	Jochen Kressler	<p>The traffic study for the proposed project (large, singular building) projects 13 trucks at the morning peak hour, and 9 trucks in the pm peak hour. While there may be trucks throughout the day, those calculations represent the number of truck trips during the city's peak traffic times.</p>
12	<p>Please answer about traffic, I have read what you posted and don't understand it.</p>	Gern B	<p>I will contact you offline :)</p>

13	when will the major cleanup going on be completed?	Joe Hill	live answered
14	When do you think the project will go to the planning commission	Neil Hancock	TBD. Hopefully soon! The CEQA team is actively working on responding to all the public comments.
15	Are the trucks driving around the airport community? Still no extra access to the freeway?	Kathy Stark	The project does not include new access to the freeway. The trucks would have no reason to go north on Benet, across the bridge into the community. They'd exit, and turn right into the facility. In response to community feedback, we added a dedicated right turn lane into the site to facilitate truck trips into the project.
16	Can you be more specific	Neil Hancock	live answered
17	How are residents going to get in and out of the community with all the trucks?	Antonio Michel	live answered
18	when will questions and answers be sent to us?	Joe Hill	We are answering questions in real time. Post event follow up will happen this week.

- Exhibit 15 – 11/14/24 Slides



EDDIE JONES

WAREHOUSE, MANUFACTURING &
DISTRIBUTION CENTER

PROJECT UPDATE MEETING
NOVEMBER 14, 2024



Meeting Agenda

Nov. 14, 6:00 – 7:00 p.m.

01

Project Refresher

- Community Context
- Proposed Project
- Milestones/Process
- Feedback

02

4th Project Option

03

Project Features

Questions that were submitted
will be answered on the project
FAQs page

www.eddiejonesproject.com/faqs

▼ Project Overview

“Why consider expanding outside of the current zoning regulations?” – Miranda P.

Proposed Project

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient new facility in the city’s industrial core that is consistent with the existing General Plan designation and zoning for the site.

Building Footprint: 547K SF

- 65% Warehouse + 114 Truck bays
- 28% Manufacturing
- 7% Office



▼ Project Milestones

Project
Application

Public Mtg #1

Community
Outreach
Continues

Public Mtg #2

Refine
Application

Aug. 2022

CEQA Scoping Mtg

Postcard to community

Launch of website

One on One and Small Group Mtgs

Jan. 17, 2023

Public Mtg #3

Public Review
of Draft EIR

July 12, 2023

Oct. - Dec. 2023

Summary of Public Feedback

- Inconsistency/Incompatibility with residential uses
- Impacts to San Luis Rey River
- Emergency Evacuation
- Noise/Light
- # of truck bays
- Traffic
- Size of building

▼ Project Milestones

Project
Application

Public Mtg #1

Community
Outreach
Continues

Public Mtg #2

Refine
Application

Aug. 2022

CEQA Scoping Mtg

Postcard to community
Launch of website

One on One and Small Group Mtgs

Jan. 17, 2023

Public Mtg #3

Public Review
of Draft EIR

Respond to
Comments

July 12, 2023

Oct. - Dec. 2023

2024

All CEQA-related comments
will be responded to in the
final EIR

▼ Project Milestones

Project Application

Public Mtg #1

Community Outreach Continues

Public Mtg #2

Refine Application

Aug. 2022

CEQA Scoping Mtg

Postcard to community

Launch of website

One on One and Small Group Mtgs

Jan. 17, 2023

Add right turn lane
Reduce truck bays

Public Mtg #3

Public Review of Draft EIR

Respond to Comments

Public Mtg #4

Oceanside Planning Commission

July 12, 2023

Oct. - Dec. 2023

2024

All CEQA-related comments will be responded to in the final EIR

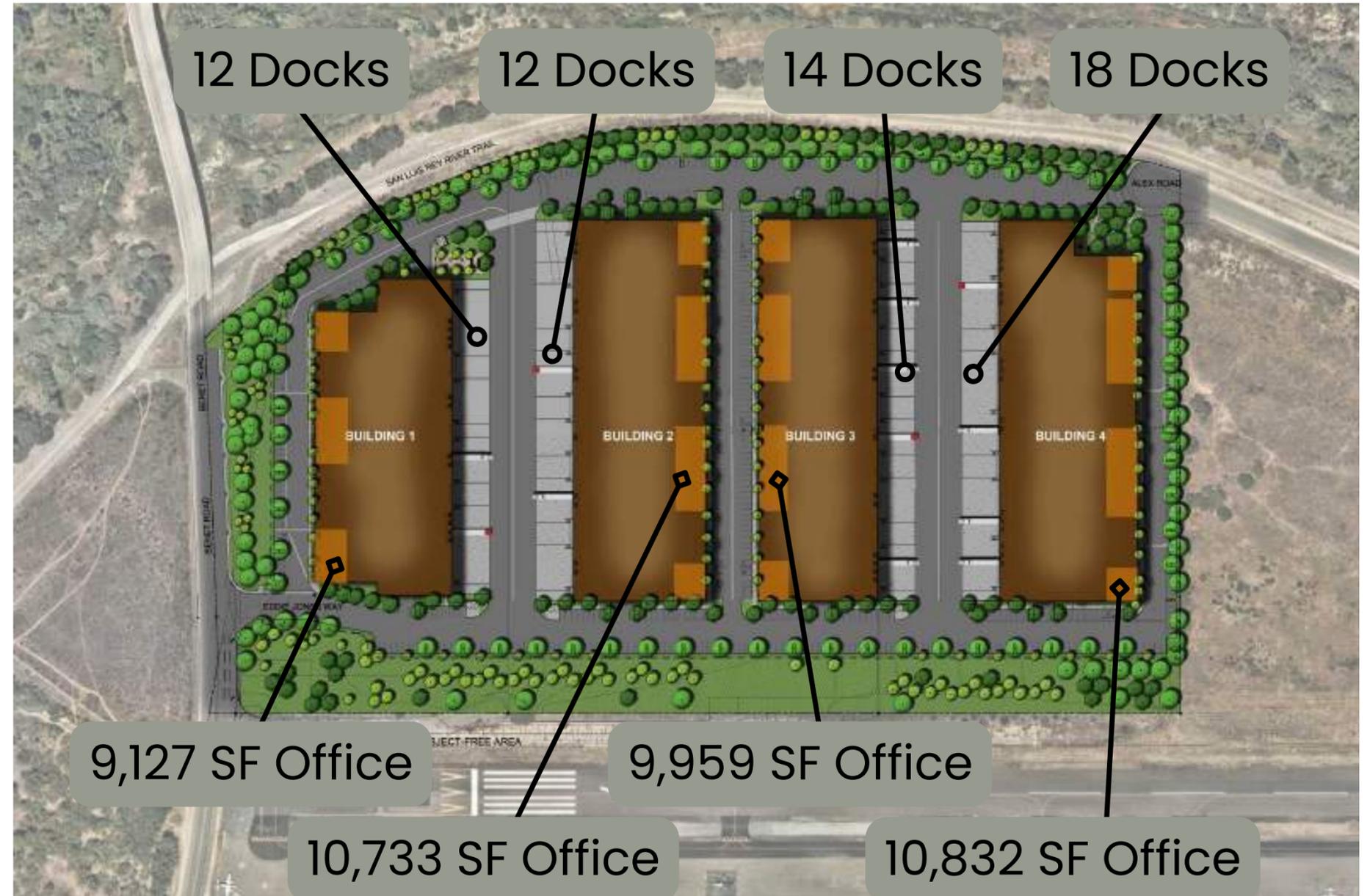
Nov. 14, 2024

▼ 4th Project Option

Please reveal fourth project option with great specificity! Hopefully it will be acceptable if adjacent to the developer's residence! - Scott B.

Building Footprint: 491 SF

- 68% Warehouse + 57 Truck bays
- 25% Manufacturing
- 7% Office



Office Space

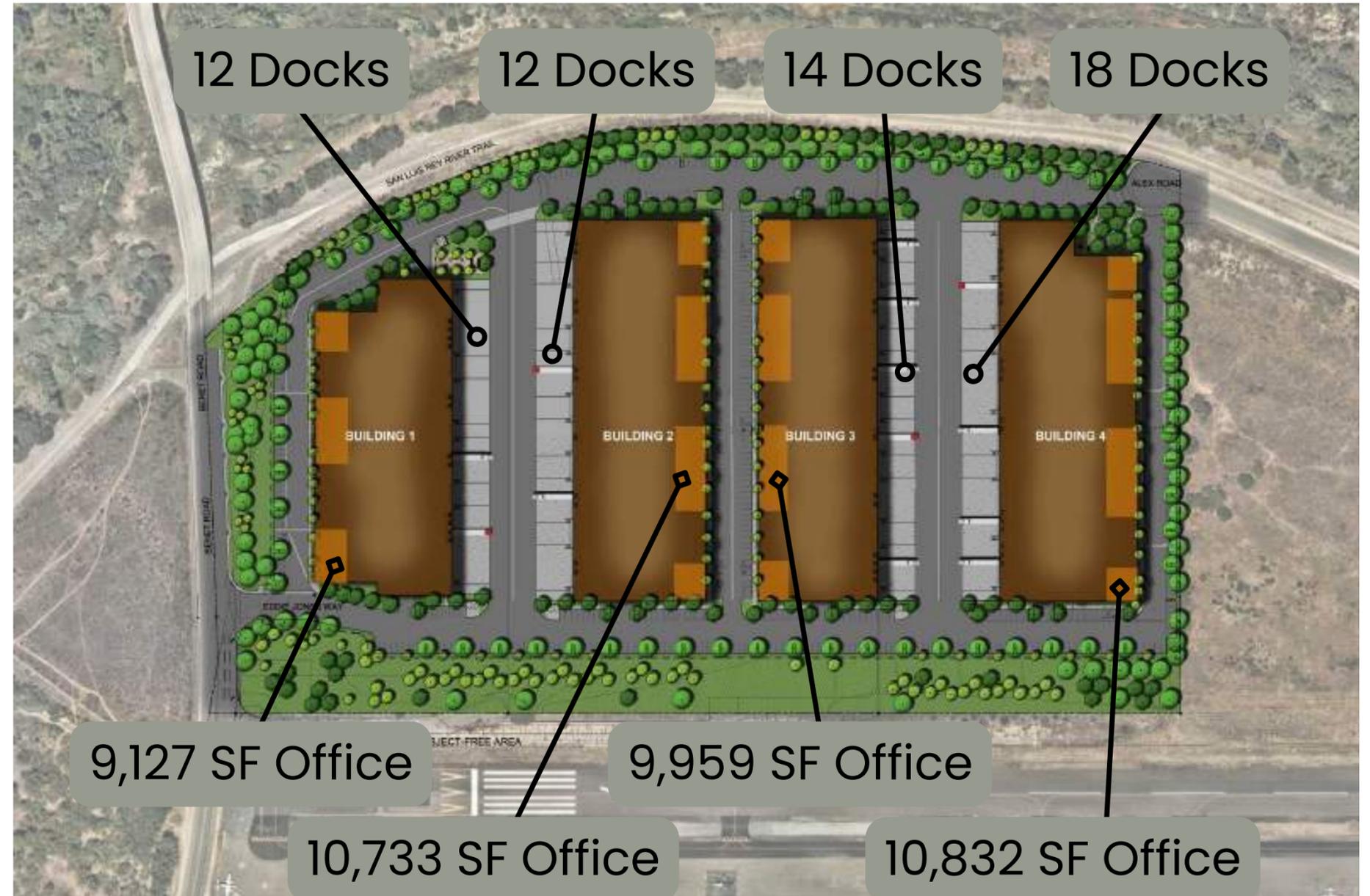
▼ 4th Project Option

Building Footprint: 491 SF

- 68% Warehouse + 57 Truck bays
- 25% Manufacturing
- 7% Office

4th Project Option Features

- Responds to community feedback
- Reduces truck bays
- Reduces square footage
- Less intensive project that still falls within analysis of larger project
- All truck bays face inward toward other project buildings
- Increases onsite landscaping
- Maintains the 100-foot buffer from the edge of the San Luis Rey River riparian habitat
- Maintains floodwall (instead of increased pad elevation) and stormwater management
- Complies with Airport Overlay Zone



▼ 4th Project Option

By the Numbers

	Proposed	4th Option
Footprint	547,320 SF	491,582 SF
Gross SF	566,905 SF	497,882 SF
Truck bays	114	57

Development Standards

	City Code	Proposed	4th Option
Zoning	IL	IL	IL
Max FAR	1.00	.4	.36
Max Coverage	75%	40%	40%
Max Height	80 FT	36 - 45 FT	24 -45 FT

▼ Thank you

The Project preserves industrial land and aligns with City priorities for economic development by providing an efficient facility in a context-sensitive way.



outreach@eddiejonesproject.com



- Exhibit 16 – 11/14/24 Attendees

Eddie Jones Project Update (11/14/24)

Attendee Report

Meeting Duration (minutes): 33

Panelist Details	Email	Time in Session (minutes)
Arlene Tendick	arlene@ant-sc.com	33
Dan Niebaum	dan@lightfootpg.com	30
Adam Robinson	adam@rafpg.com	26

Attendee Details	Email	Time in Session (minutes)
Alison Topaloglu	atopaloglu@cox.net	24
Alyce Budde	ajbudde@cox.net	28
Amy Lockwood	akmoose@cox.net	28
Andrea Targhetta	atarghetta@comcast.net	32
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brandy b	aloha8scooby@yahoo.com	34
Brian Scholte	Bscholte@gmail.com	29
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Rick Robinson	rwrobinson@oceanside	35
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Stephanie Dimakid	stephanie.dimakides@g	13
Tina Luo	Luot9@hotmail.com	28
Virginia Spencer	ginspencer@hotmail.co	27



11622 El Camino Real, Suite 100, San Diego, CA 92130
Phone 619-890-1253

July 11, 2024

RAF Pacifica Group
Mr. Adam Robinson
687 S. Coast Hwy, Suite 111
Encinitas, CA 92024

SUBJECT: Local Transportation Analysis (LTS) and Vehicle Miles Travelled (VMT) findings for the Multi-Building and Truck Bay Reduction Alternative (260 Eddie Jones D22-00001).

Dear Mr. Robinson:

LOS Engineering, Inc. has prepared a comparison of the trip generation between the Proposed Project (“Project”) and the Multi-Building and Truck Bay Reduction Alternative (“Alternative”) to determine if there is a change in the LTS and VMT findings.

The Project is a single building with 568,000 square feet (sf) that will include 170,400 sf of Manufacturing and 397,600 sf of Warehousing. Mezzanine and office areas are embedded in each use. The trip generation for the Project is shown in **Table 1**.

Table 1: Project Trip Generation

ITE 11th Edition				AM Peak Hour			PM Peak Hour		
Land Use	Rates & Size		Daily	IN	OUT	Total	IN	OUT	Total
<i>Proposed Project with Land Use 140: Manufacturing</i>									
Car Trip Rates:	4.75	/KSF		76%	24%	0.68	31%	69%	0.74
Size:	170.400	KSF	Cars: 809	88	28	116	39	87	126
Truck Trip Rates:	0.45	/KSF		56%	44%	0.03	41%	59%	0.03
Size:	170.400	KSF	Trucks: 77	<u>3</u>	<u>2</u>	5	<u>2</u>	<u>3</u>	5
<i>TOTAL Manufacturing Trips:</i>			886	91	30	121	41	90	131
<i>Proposed Project Land Use 154: High-Cube Transload and Short-Term Storage Warehouse</i>									
Car Trip Rates:	1.40	/KSF		77%	23%	0.08	28%	72%	0.10
Size:	397.600	KSF	Cars: 557	24	7	32	11	29	40
Truck Trip Rates:	0.22	/KSF		49%	51%	0.02	47%	53%	0.01
Size:	397.600	KSF	Trucks: 87	<u>4</u>	<u>4</u>	8	<u>2</u>	<u>2</u>	4
<i>TOTAL Warehouse and Distribution Trips:</i>			644	28	11	40	13	31	44
<i>Total car trips:</i>			1,366	112	35	148	50	116	166
<i>Total truck trips with PCE of 2 for daily trips:</i>			328	7	6	13	4	5	9
Total Building SF: 568.000			Project Total: 1,694	119	41	161	54	121	175

Source: Institute of Transportation Engineers (ITE) 11th Edition *Trip Generation*. KSF: 1,000 Square Feet.
PCE: Passenger Car Equivalent multiplier of 2 applied to truck ADTs based on level terrain.

The Alternative includes four buildings with a total of 497,822 sf that will include 133,824 sf of Manufacturing and 363,988 sf of Warehousing. Mezzanine and office areas are embedded in each use. The trip generation for the Alternative is shown in **Table 2**.

Table 2: Alternative Trip Generation

ITE 11th Edition				AM Peak Hour			PM Peak Hour			
Land Use	Rates & Size		Daily	IN	OUT	Total	IN	OUT	Total	
<i>Land Use 140: Manufacturing</i>										
Car Trip Rates:	4.75	/KSF		76%	24%	0.68	31%	69%	0.74	
Size:	133.824	KSF	Cars: 636	69	22	91	31	68	99	
Truck Trip Rates:	0.45	/KSF		56%	44%	0.03	41%	59%	0.03	
Size:	133.824	KSF	Trucks: 60	<u>2</u>	<u>2</u>	4	<u>2</u>	<u>2</u>	4	
<i>TOTAL Manufacturing Trips:</i>			696	71	24	95	32	71	103	
<i>Land Use 154: High-Cube Transload and Short-Term Storage Warehouse</i>										
Car Trip Rates:	1.40	/KSF		77%	23%	0.08	28%	72%	0.10	
Size:	363.998	KSF	Cars: 510	22	7	29	10	26	36	
Truck Trip Rates:	0.22	/KSF		49%	51%	0.02	47%	53%	0.01	
Size:	363.998	KSF	Trucks: 80	<u>4</u>	<u>4</u>	7	<u>2</u>	<u>2</u>	4	
<i>TOTAL Warehouse and Distribution Trips:</i>			590	26	10	36	12	28	40	
<i>Total Car Trips:</i>			1,145	92	29	120	41	95	135	
<i>Total Truck Trips (with PCE for ADT only):</i>			281	6	5	11	3	4	8	
<i>Total Building SF:</i>			497.822	Project Total: 1,286	97	34	131	44	99	143

Source: Institute of Transportation Engineers (ITE) 11th Edition *Trip Generation*. KSF: 1,000 Square Feet.

PCE: Passenger Car Equivalent multiplier of 2 applied to truck ADTs based on level terrain.

The Alternative has 70,178 less sf than the Project resulting in less traffic generation as shown in **Table 3**.

Table 3: Trip Reduction Between Project and Alternative

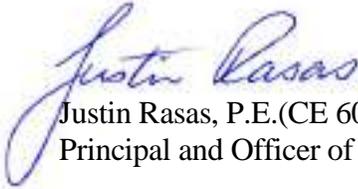
	Daily	AM Peak Hour			PM Peak Hour		
		IN	OUT	Total	IN	OUT	Total
<i>Trip change between Project and Alternative:</i>	-409	-22	-7	-29	-10	-22	-32
<i>Percent change between Project and Alternative:</i>	-24%	-18%	-18%	-18%	-18%	-18%	-18%

The Project trip generation used in the LTS is higher than the Alternative trip generation; therefore, the LTS analyzed more traffic and provides a conservative analysis over the Alternative. No changes are recommended to the LTS.

The Project VMT analysis is based on the San Diego Regional Association of Governments (SANDAG) SB 743 Concept Map to determine if the VMT/Employee exceeds the VMT impact threshold. The Concept Map is based on the location of the project and not the project trip generation. Therefore, the Alternative with 70,178 less sf than the Project would not change the VMT findings. No changes are recommended to the VMT report.

If you have any questions, please call me at (619) 890-1253.

Sincerely,
LOS Engineering, Inc.



Justin Rasas, P.E.(CE 60690), PTOE.
Principal and Officer of LOS Engineering, Inc.

Appendix I

Local Transportation Study

Eddie Jones Industrial Redevelopment Project

D22-00001

250 Eddie Jones Way

City of Oceanside

August 25, 2023

Draft Local Transportation Study

Prepared for:

RAF Pacifica Group
111 C Street, Suite 200
Encinitas, CA 92024

Prepared by Justin Rasas (RCE 60690) with:



LOS Engineering, Inc.

11622 El Camino Real Suite 100 San Diego, CA 92130
Phone 619-890-1253

Job #2112

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Executive Summary

Eddie Jones Industrial Redevelopment

This Local Transportation Study (LTS) determines if there are measurable transportation impacts based on the City of Oceanside local impact thresholds. A separate Vehicle Miles Traveled (VMT) analysis is provided under separate cover to satisfy the California Environmental Quality Act (CEQA) requirements. This report provides a non-CEQA analysis as required by the City of Oceanside.

Pedestrian, Bicycle, Transit, and Traffic study elements were analyzed based on the City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, August 2020 (“Traffic Guidelines”).

The project is located at 250 Eddie Jones Way in Oceanside, California. The project is a single building with up to 568,000 sf that will include 30% Manufacturing (170,400 sf) and 70% Warehousing (397,600 sf). The project site had a 172,305 SF building used for manufacturing that was vacated in the summer of 2021. The 172,305 SF building was removed in 2022. This analysis does not include a trip credit for the previous land use.

The traffic analysis included the analysis of AM peak hour, PM peak hour, and daily traffic volumes. The project trip generation is based on the ITE 11th Edition as shown in **Table E-1**.

TABLE E-1: PROJECT TRAFFIC GENERATION

ITE 11th Edition				AM Peak Hour			PM Peak Hour		
Land Use	Rates & Size		Daily	IN	OUT	Total	IN	OUT	Total
<i>Proposed Project with Land Use 140: Manufacturing</i>									
Car Trip Rates:	4.75	/KSF		76%	24%	0.68	31%	69%	0.74
Size:	170.400	KSF	Cars: 809	88	28	116	39	87	126
Truck Trip Rates:	0.45	/KSF		56%	44%	0.03	41%	59%	0.03
Size:	170.400	KSF	Trucks: 77	<u>3</u>	<u>2</u>	5	<u>2</u>	<u>3</u>	5
<i>TOTAL Manufacturing Trips:</i>			886	91	30	121	41	90	131
<i>Proposed Project Land Use 154: High-Cube Transload and Short-Term Storage Warehouse</i>									
Car Trip Rates:	1.40	/KSF		77%	23%	0.08	28%	72%	0.10
Size:	397.600	KSF	Cars: 557	24	7	32	11	29	40
Truck Trip Rates:	0.22	/KSF		49%	51%	0.02	47%	53%	0.01
Size:	397.600	KSF	Trucks: 87	<u>4</u>	<u>4</u>	8	<u>2</u>	<u>2</u>	4
<i>TOTAL Warehouse and Distribution Trips:</i>			644	28	11	40	13	31	44
<i>Total car trips:</i>			1,366	112	35	148	50	116	166
<i>Total truck trips with PCE of 2 for daily trips:</i>			328	7	6	13	4	5	9
Total Building SF:	568.000		Project Total: 1,694	119	41	161	54	121	175

Source: Institute of Transportation Engineers (ITE) 11th Edition *Trip Generation*. KSF: 1,000 Square Feet.

PCE: Passenger Car Equivalent multiplier of 2 applied to truck ADTs based on level terrain.

Six traffic scenarios were analyzed, which included Existing, Existing plus Project, Near Term (18 cumulative projects), Near Term plus Project, Horizon Year 2030, and Horizon Year 2030 plus Project. The City's Traffic Guidelines define how a project's non-CEQA traffic effect/transportation impact on the roadway system is considered to justify the need for roadway improvements that should be considered on a case-by-case basis. The project is calculated to have one (1) transportation impact at the intersection of SR-76/Benet Rd. The proposed mitigation at the intersection of SR-76/Benet Rd is a fair share payment of 8.5% towards the provisions of converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction resulting in three through lanes in each direction. The fair share payment shall be paid to the City's Thoroughfare and Signal Account. The funds will be used at the City's discretion for projects that will improve traffic safety and mobility in the City of Oceanside. The fair share contribution shall be paid in full prior to issuance of any permit for any phase of any component of the Project. Payment of the fair share shall satisfy the Project's offsite improvement obligations. A conceptual drawing of the proposed improvement and fair share calculations are included in Appendix P.

The multi-modal analysis covered pedestrian, bicycle, and transit elements. There are missing sidewalk segments adjacent to the project site; therefore, the project owner/permittee proposes to complete the following sidewalk improvements:

- 1) Construct a sidewalk from the project access on Alex Road north to the San Luis Rey River Trail (a distance of approximately 50 feet).
- 2) Construct a sidewalk along the project frontage on Benet Road from Eddie Jones Way north to the San Luis Rey River access path (a distance of approximately 600 feet).

There are no deficiencies on bike lanes and paths in the study area; therefore, no bike lane/path improvements are necessary. There are no bus stops within a ½ mile walking distance of the project site; therefore, no transit stop improvements are necessary.

1.0 Introduction

This LTS determines if there are measurable transportation impacts based on the City of Oceanside local impact thresholds. A separate Vehicle Miles Traveled (VMT) analysis is provided under separate cover to satisfy the CEQA requirements. This LTS provides a non-CEQA analysis based on the City of Oceanside Traffic Guidelines, which states on pages 5 and 6.

“The purpose of a Local Transportation Study is to ensure the goals, objectives, and policies adopted by the City are supported and implemented while monitoring the capacity for the roadway networks.”

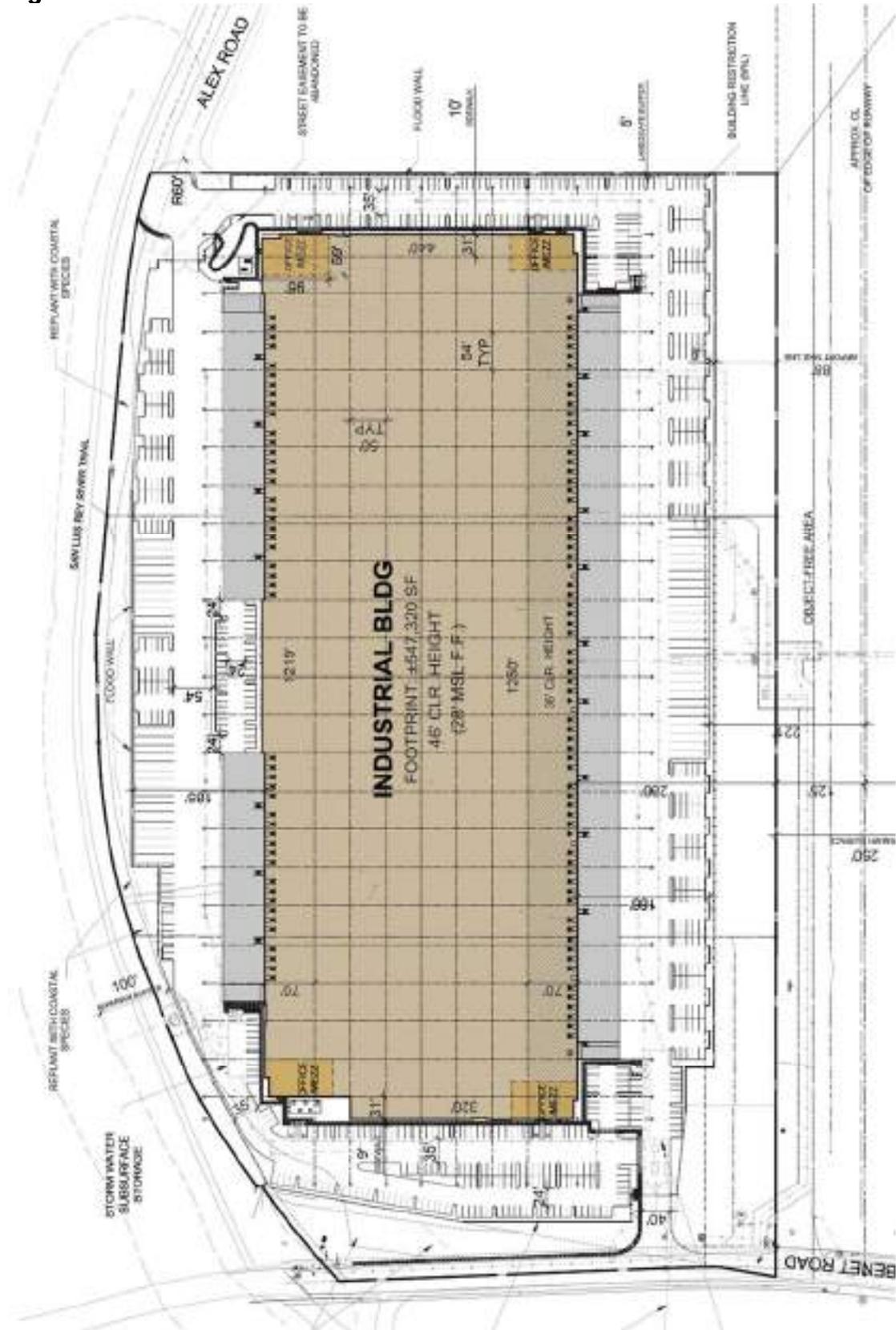
“A Local Transportation Study (LTS) will be required if a project exceeds 1,000 ADT and is consistent with the adopted General Plan.”

The project is located at 250 Eddie Jones Way in Oceanside, California. The project is a single building with up to 568,000 sf that will include 30% Manufacturing (170,400 sf) and 70% Warehousing (397,600 sf). The regional location of the project is shown in **Figure 1** with a site plan shown in **Figure 2**.

This report describes the existing roadway network in the vicinity of the project and includes a review of existing and proposed activities for weekday AM peak hour, PM peak hour, and daily traffic conditions when the project is completed. This study includes the following chapters:

1.0	Introduction
2.0	Alternative Transportation Analysis
3.0	Traffic Analysis
4.0	Conclusion

Figure 2: Site Plan



Source: Ware Malcomb

2.0 Alternative Transportation Analysis

The following alternative transportation modes were analyzed based on criteria outlined in the City of Oceanside Traffic Guidelines, which states on page 22:

1) Pedestrian:

- a. The LTS/LTA shall include pedestrian infrastructure available including any opportunities or deficiencies such as path obstructions or missing sidewalk for ½ mile walking distance from project pedestrian access points.
- b. All pedestrian facilities directly connected to project access points or adjacent to the project development, extending in each direction to the nearest intersection with a classified roadway or connection with a Class I path.
- c. Facilities connecting to transit stops within two blocks of the project.
- d. Only facilities on the side of the project or along the walking route to transit stop.

2) Bicycle:

- a. The LTS/LTA shall include a discussion of bicycle infrastructure available including any opportunities or deficiencies such as bike lanes, bike buffers, or bike boxes. This section must also include discussion of what is planned based on City and regional documentation. The extents are as follows:
 - i. All roadways adjacent to the project, extending in each direction to the nearest intersection with a classified roadway or with a Class I path.
 - ii. Both directions of travel should be evaluated.

3) Transit:

- a. The LTS/LTA shall identify any transit stops or routes existing and planned near the project site.
- b. This section shall also include a discussion and evaluation of transit stop amenities within ½ mile of each pedestrian access point.

2.1 Pedestrian

The pedestrian analysis consists of documenting pedestrian infrastructure available including any opportunities or deficiencies such as path obstructions or missing sidewalk from the project access points extending to the nearest intersection with a classified roadway or to a connection with a Class I path.

Alex Road from Foussat Rd to the westerly cul-de-sac terminus does not have sidewalks on either side of the roadway. Existing and future project access will be from this cul-de-sac.

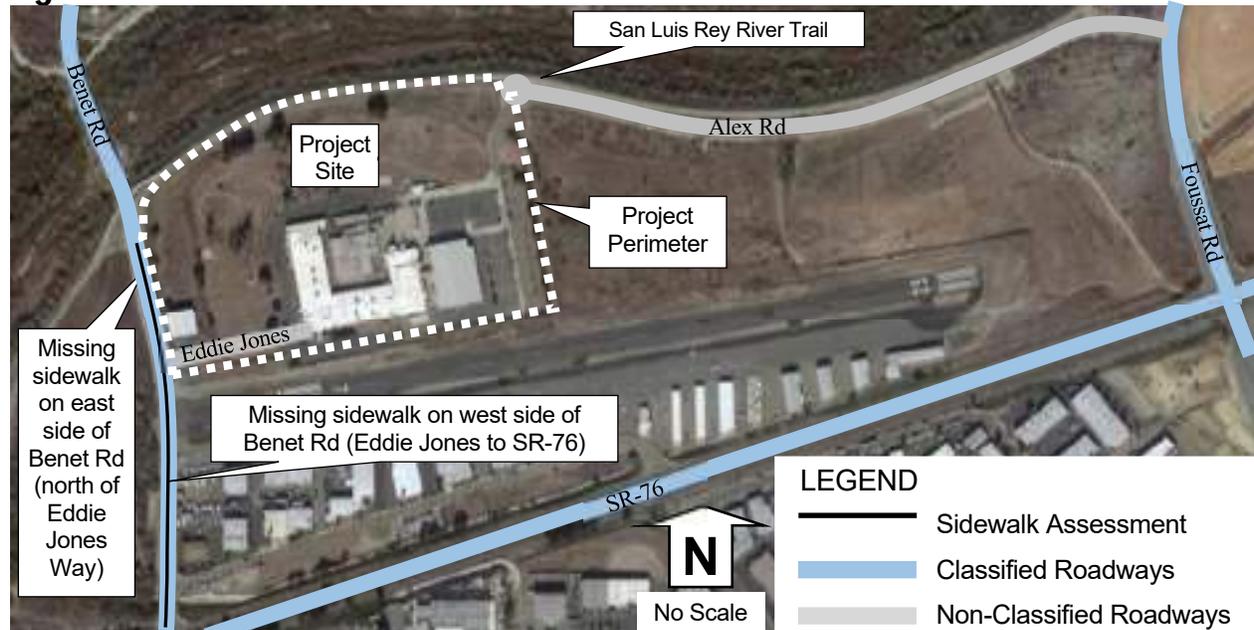
Benet Road from the San Luis Rey River Trail to Eddie Jones Way has a contiguous sidewalk on the west side of the roadway and no sidewalk on the east side of the roadway. From Eddie Jones Way to SR-76, Benet Road has no sidewalk on the west side of the roadway but has a contiguous sidewalk on the east side of the roadway. There were no major sidewalk obstructions observed along the sections that have sidewalks.

The project owner/permittee proposed to complete the following sidewalk improvements:

- 1) Construct a sidewalk from the project access on Alex Road north to the San Luis Rey River Trail (a distance of approximately 50 feet).
- 2) Construct a sidewalk along the project frontage on Benet Road from Eddie Jones Way north to the San Luis Rey River access path (a distance of approximately 600 feet).

The pedestrian elements are shown in **Figure 3**.

Figure 3: Pedestrian Elements



2.2 Bicycle

The bicycle analysis consists of documenting bicycle infrastructure available including any opportunities or deficiencies such as bike lanes, bike buffers, or bike boxes from the project access points extending in each direction to the nearest intersection with a classified roadway or connection with a Class I path. Additionally, this analysis must include a discussion of what is planned based on City and regional documentation. The study area has the following existing and planned bicycle infrastructure based on City documentation.

Benet Road along the project frontage has an existing Class 2 bike lane shown in the *City of Oceanside Bicycle Master Plan 2017 Update*.

San Luis Rey River Trail near the project site has an existing Class 1 bike path as shown in the *City of Oceanside Bicycle Master Plan 2017 Update*.

No deficiencies were observed on the above bike lanes and paths; therefore, no improvements are necessary. Excerpts from the *City of Oceanside Bicycle Master Plan 2017 Update* are included in **Appendix A**. The bicycle elements study area is shown in **Figure 4**.

Figure 4: Bicycle Elements



Source: Google Maps

2.3 Transit

The transit analysis includes identifying the closest transit routes and stops to the project. If the stops are within ½ mile (2,640 ft) walking distance of the project access, the condition of the closest stop amenities are described. There are no bus stops within a ½ mile walking distance of the project site. The closest bus service provided North County Transit District (NCTD) is Bus Route 303 with bus stops at the intersection of Foussat Rd/Mission Ave located approximately 4,300 ft walking distance from the project site via Alex Rd. Since the existing bus stops are greater than ½ mile from the project site, the conditions of bus stop amenities are not required to be documented.

No transit improvements are necessary as part of this project. A summary of the service times for Bus Route 303 is shown in **Table 1** for weekdays and **Table 2** for weekend days. The noted bus schedules are included in **Appendix B**.

TABLE 1: WEEKDAY BUS SERVICE OPERATIONS AND FREQUENCY

Bus Route	Weekday (Mon-Fri) Service Operations (Off-Peak Service Frequency Range)	7-9 AM Peak Hour Service Frequency	4-6 PM Peak Hour Service Frequency
Route 303	≈ 4:30 AM to ≈ 11:00 PM (≈ 15-30 minutes)	15 minutes	15 minutes

Notes: Above service times are summaries, thus please refer to Appendix D for exact service details.

TABLE 2: WEEKEND BUS SERVICE OPERATIONS AND FREQUENCY

Bus Route	Saturday Service Operations (Service Frequency Range)	Sunday Service Operations (Service Frequency Range)
Route 303	≈ 6:00 AM to ≈ 11:00 PM (≈ 20-30 min.)	≈ 6:00 AM to ≈ 11:00 PM (≈ 20-30 min.)

Notes: Above service times are summaries, thus please refer to Appendix D for exact service details.

The nearby transit routes and bus stops are shown in **Figure 5**.

Figure 5: Transit Elements



Source: Google Maps

3.0 Traffic Analysis

The Local Transportation Study includes the analysis of specific study scenarios, methodology for the analysis of roadway operations, and determination of potential off-site improvements. Details for each of these parameters are include herein.

3.1 Study Area and Scenario Criteria

This vehicular analysis was based on criteria outlined in the City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, August 2020, which states on page 6:

A Local Transportation Assessment (LTA) will be required to analyze the following scenarios based on the thresholds identified for the project's ADT.

- A project that generates between 200-500 ADT will be required to analyze existing conditions and existing conditions plus project.
- A project that generates between 500-1,000 ADT will be required to analyze existing conditions, existing conditions plus project, existing conditions plus near-term cumulative projects, and existing conditions plus near-term cumulative projects plus project

The City of Oceanside Traffic Guidelines also define the study area on page 22 as follows:

All signalized intersections and signalized project driveways shall be analyzed if:
The project will add 50 or more peak hour (final cumulative) trips in either direction

All unsignalized intersections and unsignalized project driveways shall be analyzed if:
The project will add 50 or more peak hour (final cumulative) trips in either direction

All freeway ramp intersections and signalized project driveways shall be analyzed if:
The project will add 20 or more peak hour (final cumulative) trips in either direction

Additionally, the study area can be identified by City staff. Please note that the study area in this LTS exceeds the above criteria because it was initial determined based on a different mix of industrial uses that resulted in a higher trip generation that had a larger study area. To be conservative, the original study area is maintained for consistency.

For this project, the following intersections were analyzed as part of this study:

- 1) SR-76/I-5 SB Ramps (Signalized)
- 2) SR-76/I-5 NB Ramps (Signalized)
- 3) SR-76/Loretta St (Signalized)
- 4) SR-76/Canyon Dr (Signalized)
- 5) SR-76/Benet Rd (Signalized)
- 6) SR-76/Foussat Rd (Signalized)
- 7) Benet Rd/Airport Rd (Un-Signalized)
- 8) Benet Rd/Eddie Jones Way (Un-Signalized)
- 9) Foussat Rd/Alex Rd (Un-Signalized)

The following street/expressway segments were analyzed as part of this study:

- 1) Benet Rd from Eddie Jones Way to SR-76
- 2) Foussat Rd from Alex Rd to SR-76
- 3) SR-76 from I-5 to Loretta St
- 4) SR-76 from Loretta St to Canyon Dr
- 5) SR-76 from Canyon Dr to Benet Rd
- 6) SR-76 from Benet Rd to Foussat Rd
- 7) SR-76 Immediately East of Foussat Rd

The number of scenarios to be analyzed is typically based on the size of the project, the number of cumulative projects and whether the project conforms to current zoning. For this project, the following scenarios were included:

- 1) Existing Conditions
- 2) Existing plus Project Conditions
- 3) Near Term (Existing + Cumulative) Conditions
- 4) Near Term (Existing + Cumulative) plus Project Conditions
- 5) Horizon Year 2030 Conditions
- 6) Horizon Year 2030 plus Project Conditions

3.2 Traffic Analysis Criteria

The traffic analyses prepared for this study were based on the *Highway Capacity Manual* (HCM) operations analysis using Level of Service (LOS) evaluation criteria. The operating conditions of the study intersections, street segments, and freeway segments were measured using the HCM LOS designations, which ranges from A through F. LOS A represents the best operating condition and LOS F denotes the worst operating condition. The LOS criteria for each roadway component are described below.

3.2.1 Intersections

The study intersections were analyzed based on the **operational analysis** outlined in the 6th Ed HCM using existing signal timing data. This process defines LOS in terms of **average control delay** per vehicle measured in seconds. LOS at the intersections were calculated using the computer software program Synchro 10 (Trafficware Corporation). The 6th Ed HCM LOS for the range of delay by seconds for un-signalized and signalized intersections is described in **Table 3**.

TABLE 3: INTERSECTION LEVEL OF SERVICE DEFINITIONS (6TH EDITION HCM)

Level of Service	Un-Signalized (TWSC and AWSC)	Signalized
	Control Delay (sec/veh where v/c ≤ 1)	Control Delay (sec/veh where v/c ≤ 1)
A	0-10	≤ 10
B	> 10-15	> 10-20
C	> 15-25	> 20-35
D	> 25-35	> 35-55
E	> 35-50	> 55-80
F	> 50	> 80

TWSC: Two Way Stop Control. AWSC: All Way Stop Control. Source: 6th Edition HCM (exhibit 20-2 for two way stop control, exhibit 21-8 for all way stop control, and exhibit 19-8 for signalized intersections).

3.2.2 Street Segments

The street segments were analyzed based on the functional classification of the roadway using the City of Oceanside *Average Daily Vehicle Trips* capacity lookup table. The roadway segment capacity and LOS standards used to analyze street segments are summarized in **Table 4**.

TABLE 4: STREET SEGMENT DAILY CAPACITY AND LOS (CITY OF OCEANSIDE)

Circulation Element Road Classification	Lanes	LOS A	LOS B	LOS C	LOS D	LOS E
Expressway	6	<30,000	<42,000	<60,000	<70,000	<80,000
Expressway	4	<25,000	<35,000	<50,000	<55,000	<60,000
Prime Arterial	6	<25,000	<35,000	<50,000	<55,000	<60,000
6-Lane Major Arterial	6	<20,000	<28,000	<40,000	<45,000	<50,000
5-Lane Major Arterial	5	<17,500	<24,500	<35,000	<40,000	<45,000
4-Lane Major Arterial	4	<15,000	<21,000	<30,000	<35,000	<40,000
Secondary Collector with TWLTL	4	<10,000	<14,000	<20,000	<25,000	<30,000
Secondary Collector no TWLTL	4	<9,000	<13,000	<18,000	<22,000	<25,000
Collector – Commercial Fronting	2	<5,000	<7,000	<10,000	<13,000	<15,000
Collector – Residential Fronting	2	<4,000	<5,500	<7,500	<9,000	<10,000
Local Street	2	na	na	<2,200*	na	na

Source: City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, August 2020. * City of Oceanside General Plan *Circulation Element*, September 2012 applied.

3.2.3 Transportation Impact Thresholds and Need for Roadway Improvements

A project Owner/Permittee may be required to provide an off-site improvement if the project traffic exceeds the City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, August 2020 defined thresholds as shown in **Table 5** (excerpts included in **Appendix C**).

TABLE 5: DETERMINATION OF THE NEED FOR ROADWAY IMPROVEMENTS

Level of Service with Project	Allowable Increase Due to Project Effect	
	Roadway Segments	Intersections
	V/C	Delay (sec.)
E & F	0.02	2

Source: City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment* (August 2020), page 25.

A project effect is considered a non-CEQA transportation impact based on the City’s Traffic Guidelines and State law. The Traffic Guidelines define how a project’s non-CEQA traffic effect/transportation impact on the roadway system is considered to justify the need for roadway improvements. If a project causes the values identified in Table 5 above to be exceeded, the City will consider roadway improvements as follows on a case by case basis:

- 1) Improvements should be consistent with the General Plan.
- 2) Improvements for transit, bike and pedestrian facilities should be given priority in Transit Priority Areas or Smart Growth Opportunity Areas as identified by SANDAG.
- 3) Projects in Transit Priority Areas or Smart Growth Opportunity Areas as identified by SANDAG, that are consistent with the General Plan at the time of project application, should not be denied due to the inability to provide roadway improvements (i.e., existing right of way is constrained, etc.)

3.3 Existing Conditions

This section describes the study area street system, existing daily roadway and peak hour intersection traffic volumes and existing LOS results.

3.3.1 Existing Street System

In the vicinity of the project, the following roadways were analyzed as part of this study, which are described below. The roadway classification was obtained from the City of Oceanside General Plan *Circulation Element*, September 2012 (excerpts included in **Appendix D**).

Benet Road is classified as a 2 lane *Secondary Collector* from Eddie Jones Way to SR-76. This segment of Benet Rd is currently built as a 2 lane undivided roadway with a 45 MPH posted speed limit. This section of Benet Rd is generally constructed with 40 feet of pavement and two 12 foot travel lanes in each direction. There are striped Class II bike lanes on each side of the roadway. There is one no on-street parking signs posted on the east side of the roadway north of Eddie Jones Way. There is a sidewalk on the west side of the roadway from the bridge over the San Luis Rey River down to Eddie Jones Way and no sidewalk from Eddie Jones Way down to SR-76. There is no sidewalk on the east side of the roadway from the bridge over the San Luis Rey River down to Eddie

Jones Way while there is a sidewalk from Eddie Jones Way down to SR-76. These sidewalks are approximately 5 feet in width. There are unsignalized intersections along this segment with stop control on the minor street.

Foussat Road is classified as a 2 lane *Secondary Collector* from Alex Rd to SR-76. This segment of Foussat Rd is currently built as a 2 lane undivided roadway near Alex Rd and widens closer to SR-76 with additional lanes at the intersection. The pavement width varies from approximately 57 feet just south of Alex Road to approximately 125 feet just north of SR-76. There are no sidewalks on either side of this roadway. There are no posted speed limit signs along this segment between Alex Rd and SR-76. There are on parking signs posted on Foussat Rd just north of Alex Rd. There is one unsignalized intersection (Alex Rd) along this segment with stop control on the minor street.

SR-76 is classified as an *Expressway* and is currently built as a divided roadway with a center concrete barrier and a posted 55 Miles per Hour (MPH) speed limit. From I-5 to Loretta St, SR-76 is constructed as a 5 lane divided roadway with 3 eastbound travel lanes and 2 westbound travel lanes within approximately 115 feet of pavement. From Loretta St to Foussat Rd, SR-76 is constructed as a 4 lane divided roadway within approximately 100 feet of pavement. All of the intersecting streets along this segment have signalized traffic control. There are no sidewalks along this section of SR-76.

3.3.2 Existing Traffic Volumes and LOS Analyses

Existing 7-9 AM and 4-6 PM peak hour traffic volumes were collected for the following intersections with the date of collection noted below:

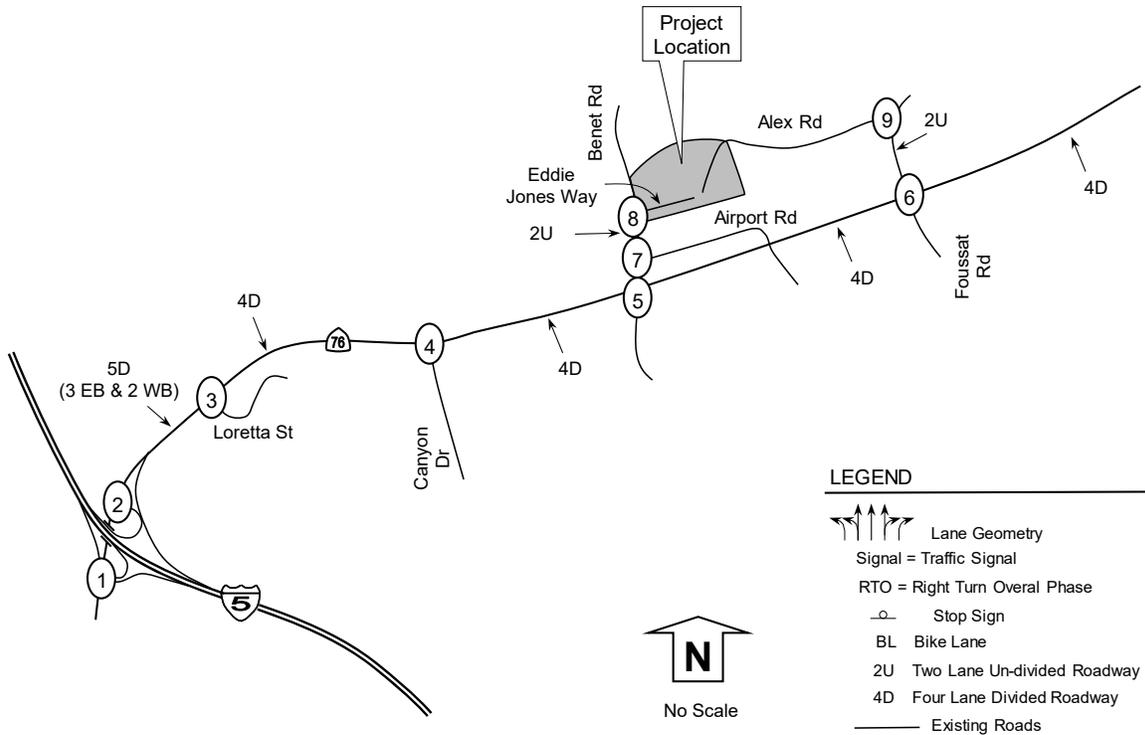
- 1) SR-76/I-5 SB Ramps (Wed. 10/6/21)
- 2) SR-76/I-5 NB Ramps (Wed. 10/6/21)
- 3) SR-76/Loretta St (Wed. 10/6/21)
- 4) SR-76/Canyon Dr (Wed. 10/6/21)
- 5) SR-76/Benet Rd (Wed. 10/6/21)
- 6) SR-76/Foussat Rd (Wed. 10/6/21)
- 7) Benet Rd/Airport Rd (Wed. 10/6/21)
- 8) Benet Rd/Eddie Jones Way (Wed. 10/6/21)
- 9) Foussat Rd/Alex Rd (Wed. 10/6/21)

Existing daily traffic volumes were collected or obtained from Caltrans with the date of collection noted below:

- 1) Benet Rd from Eddie Jones Way to SR-76 (Wed. 10/6/21)
- 2) Foussat Rd from Alex Rd to SR-76 (Wed. 10/6/21)
- 3) SR-76 from I-5 to Loretta St (highest Caltrans volume between 2015-2021)
- 4) SR-76 from Loretta St to Canyon Dr (highest Caltrans volume between 2015-2021)
- 5) SR-76 from Canyon Dr to Benet Rd (highest Caltrans volume between 2015-2021)
- 6) SR-76 from Benet Rd to Foussat Rd (highest Caltrans volume between 2015-2021)
- 7) SR-76 east of Foussat Rd (highest Caltrans volume between 2015-2021)

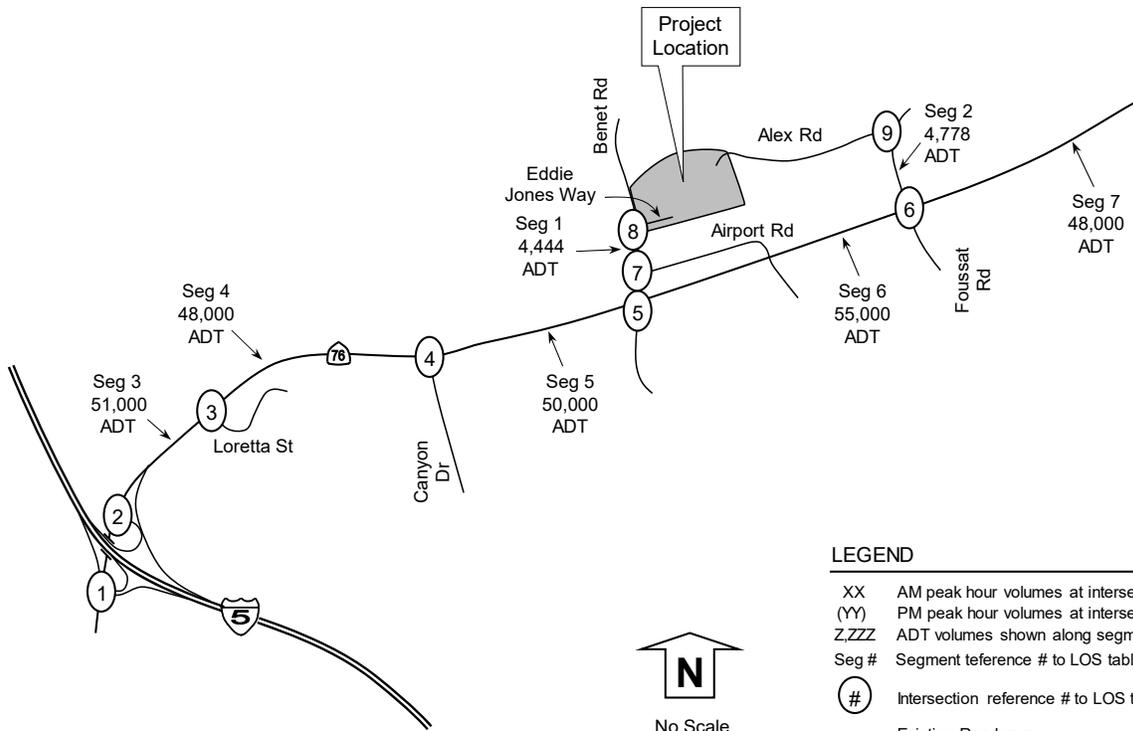
The last 7 years of Caltrans data from 2015 through 2021 was reviewed. Historical Caltrans data did not always increase year over year (for these segments), rather it has both increases and decreases over time. To represent a conservative volume for Caltrans segments, the higher volumes between 2015 and 2021 were applied. The applied highest Caltrans data between 2015 to 2021, existing traffic volumes, and signal timing data are included in **Appendix E**. The existing roadway conditions are shown in **Figure 6** and the existing AM peak hour, PM peak hour, and daily volumes are shown on **Figure 7**.

Figure 6: Existing Roadway Conditions



<p>SR-76</p> <p>Free Right Turn outside of intersection</p> <p>Signal 1</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>Free Right Turn outside of intersection</p> <p>Signal 2</p> <p>I-5 NB Ramp</p>	<p>SR-76</p> <p>RTO</p> <p>Signal 3</p> <p>RTO</p> <p>Loretta St</p>
<p>SR-76</p> <p>RTO</p> <p>Signal 4</p> <p>RTO</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>RTO</p> <p>Signal 5</p> <p>Benet Rd</p>	<p>SR-76</p> <p>Signal 6</p> <p>Foussat Rd</p>
<p>Benet Rd</p> <p>Signal 7</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>Signal 8</p> <p>Eddie Jones Way</p>	<p>Alex Rd</p> <p>Signal 9</p> <p>Foussat Rd</p>

Figure 7: Existing Volumes



<p>SR-76</p> <p>445 (677) ↓</p> <p>1060 (675) ↓</p> <p>1</p> <p>280 (460) ↑</p> <p>215 (241) ↑</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>1381 (1053) ↓</p> <p>881 (395) ↓</p> <p>2</p> <p>540 (1054) ↑</p> <p>89 (101) ↑</p> <p>I-5 NB Ramp</p>	<p>SR-76</p> <p>1110 (1876) →</p> <p>23 (73) ↓</p> <p>3</p> <p>43 (25) ↓</p> <p>26 (12) ↓</p> <p>2240 (1409) ←</p> <p>12 (20) ↓</p> <p>Loretta St</p>
<p>SR-76</p> <p>1138 (1826) →</p> <p>14 (48) ↓</p> <p>20 (7) ↓</p> <p>4</p> <p>75 (91) ↓</p> <p>74 (136) ↓</p> <p>2249 (1416) ←</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>98 (180) ↑</p> <p>1023 (1711) →</p> <p>129 (88) ↓</p> <p>50 (129) ↓</p> <p>248 (117) ↓</p> <p>12 (9) ↓</p> <p>8 (13) ↓</p> <p>5</p> <p>7 (12) ↓</p> <p>2022 (1307) ←</p> <p>30 (35) ↓</p> <p>46 (91) ↓</p> <p>Benet Rd</p>	<p>SR-76</p> <p>43 (26) ↓</p> <p>85 (70) ↓</p> <p>148 (56) ↓</p> <p>6</p> <p>33 (27) ↓</p> <p>971 (1723) →</p> <p>53 (127) ↓</p> <p>116 (78) ↓</p> <p>54 (124) ↓</p> <p>84 (136) ↓</p> <p>71 (102) ↓</p> <p>1875 (1236) ←</p> <p>143 (98) ↓</p> <p>Fou-ssat Rd</p>
<p>Benet Rd</p> <p>269 (114) ↓</p> <p>5 (5) ↓</p> <p>7</p> <p>77 (193) ↑</p> <p>21 (14) ↑</p> <p>16 (11) ↓</p> <p>26 (17) ↓</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>253 (120) ↓</p> <p>1 (0) ↓</p> <p>8</p> <p>104 (206) ↑</p> <p>1 (0) ↑</p> <p>1 (1) ↑</p> <p>1 (1) ↑</p> <p>Eddie Jones Way</p>	<p>Alex Rd</p> <p>3 (2) ↓</p> <p>268 (140) ↓</p> <p>9</p> <p>2 (2) ↓</p> <p>5 (8) ↓</p> <p>10 (20) ↓</p> <p>164 (226) ↓</p> <p>FousSAT Rd</p>

The LOS calculated for the intersections and segments are shown in **Tables 6 and 7**, respectively. Existing intersection LOS worksheets are included in **Appendix F**.

TABLE 6: EXISTING INTERSECTION LEVEL OF SERVICE

Intersection and (Analysis) ¹	Movement	Study Period	Existing	
			Delay ²	LOS ³
1) SR-76 at I-5 SB Ramp (S)	All	AM PM	11.2 10.4	B B
2) SR-76 at I-5 NB Ramp (S)	All	AM PM	9.4 8.0	A A
3) SR-76 at Loretta St (S)	All	AM PM	23.9 22.8	C C
4) SR-76 at Canyon Dr (S)	All	AM PM	9.7 15.2	A B
5) SR-76 at Benet Rd (S)	All	AM PM	45.8 37.8	D D
6) SR-76 at Foussat Rd (S)	All	AM PM	33.3 34.8	C C
7) Benet Rd at Airport Rd (U)	WB WB	AM PM	9.7 10.0	A B
8) Benet Rd at Eddie Jones Way (U)	WB WB	AM PM	9.9 10.6	A B
9) Foussat Rd at Alex Rd (U)	EB EB	AM PM	10.5 9.6	B A

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service.

TABLE 7: EXISTING SEGMENT LEVEL OF SERVICE

Segment	Functional Classification	LOS E Capacity	Existing		
			Daily Volume	V/C	LOS
Benet Road					
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,444	0.444	B
Foussat Rd					
2) Alex Rd to SR-76	2 Lane Collector	10,000	4,778	0.478	B
State Route 76					
3) I-5 to Loretta St	5 Lane Expressway	70,000	51,000	0.729	C
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	48,000	0.800	C
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	50,000	0.833	D
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	55,000	0.917	E
7) East of Foussat Rd	4 Lane Expressway	60,000	48,000	0.800	C

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

Under Existing conditions, the study elements are calculated to operate at LOS D or better except for segment #6: SR-76 from Benet Rd to Foussat Rd (LOS E).

3.4 Project Traffic Generation

The project is located at 250 Eddie Jones Way in Oceanside, California. The project is a single building with up to 568,000 sf that will include 30% Manufacturing (170,400 sf) and 70% Warehousing (397,600 sf). The project site had a 172,305 SF building used for manufacturing that was vacated in the summer of 2021. The 172,305 SF building was removed in 2022. This analysis does not include a trip credit for the previous land use.

The project traffic generation is calculated using the Institute of Transportation Engineers (ITE) *11th Edition Trip Generation*, Sept 2021 (**Appendix G**). The project is calculated to generate 1,694 daily trips, 161 AM peak hour trips (119 inbound and 41 outbound), and 175 PM peak hour trips (54 inbound and 121 outbound) as shown in **Table 8**.

TABLE 8: PROJECT TRAFFIC GENERATION

ITE 11th Edition				AM Peak Hour			PM Peak Hour		
Land Use	Rates & Size		Daily	IN	OUT	Total	IN	OUT	Total
<i>Proposed Project with Land Use 140: Manufacturing</i>									
Car Trip Rates:	4.75	/KSF		76%	24%	0.68	31%	69%	0.74
Size:	170.400	KSF	Cars: 809	88	28	116	39	87	126
Truck Trip Rates:	0.45	/KSF		56%	44%	0.03	41%	59%	0.03
Size:	170.400	KSF	Trucks: 77	<u>3</u>	<u>2</u>	5	<u>2</u>	<u>3</u>	5
<i>TOTAL Manufacturing Trips:</i>			886	91	30	121	41	90	131
<i>Proposed Project Land Use 154: High-Cube Transload and Short-Term Storage Warehouse</i>									
Car Trip Rates:	1.40	/KSF		77%	23%	0.08	28%	72%	0.10
Size:	397.600	KSF	Cars: 557	24	7	32	11	29	40
Truck Trip Rates:	0.22	/KSF		49%	51%	0.02	47%	53%	0.01
Size:	397.600	KSF	Trucks: 87	<u>4</u>	<u>4</u>	8	<u>2</u>	<u>2</u>	4
<i>TOTAL Warehouse and Distribution Trips:</i>			644	28	11	40	13	31	44
<i>Total car trips:</i>			1,366	112	35	148	50	116	166
<i>Total truck trips with PCE of 2 for daily trips:</i>			328	7	6	13	4	5	9
Total Building SF:	568.000		Project Total: 1,694	119	41	161	54	121	175

Source: Institute of Transportation Engineers (ITE) 11th Edition *Trip Generation*. KSF: 1,000 Square Feet.

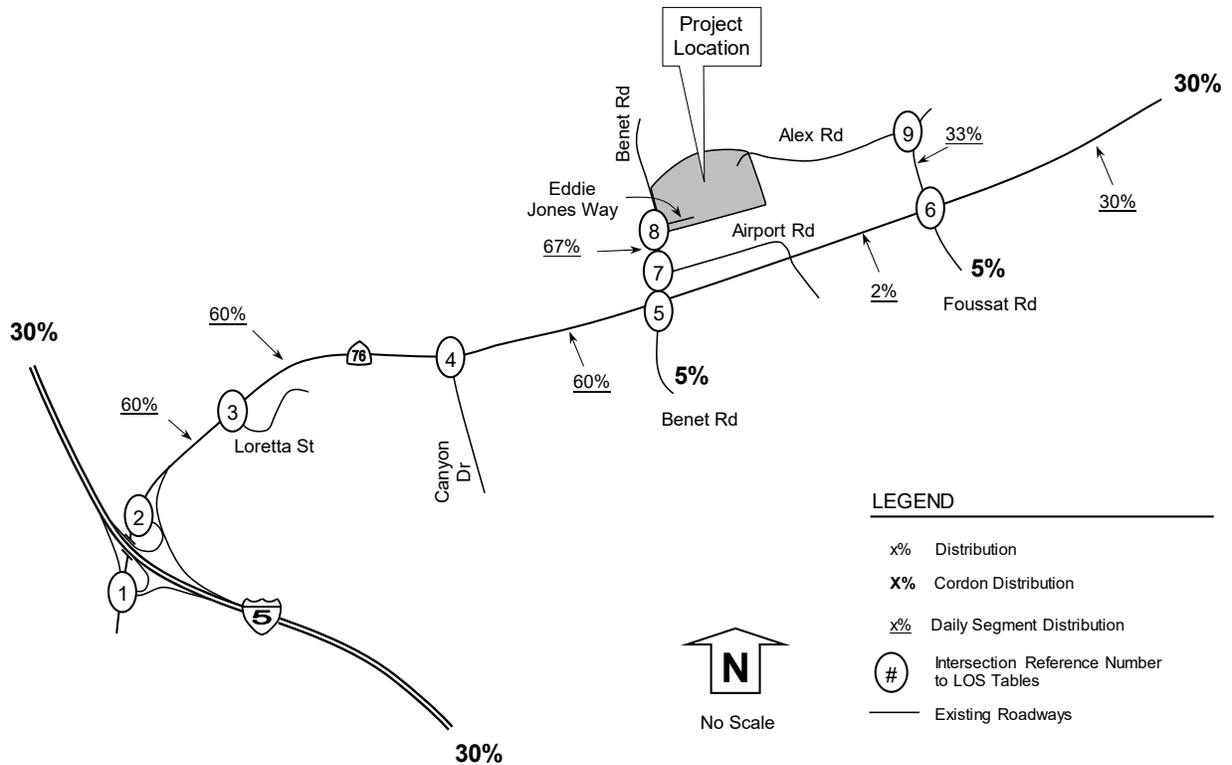
PCE: Passenger Car Equivalent multiplier of 2 applied to truck ADTs based on level terrain.

A Passenger Car Equivalent (PCE) factor is used to convert a mixed stream of cars and trucks to a single uniform PCE stream for analysis. However, a PCE is applied only to the segment ADT analysis and not the intersection peak hour analysis because the intersection analysis already incorporates a heavy vehicle factor. According to the HCM, adding a PCE to a simulation analysis that already accounts for heavy vehicles (such as the intersection analysis) would result in duplicative adjustments. A PCE factor of 2 (based on the HCM 6th Edition Exhibit 12-25 for level terrain) is applied to the 164 daily trucks resulting in 328 truck ADT as shown above.

3.5 Project Distribution and Assignment

Project trips were distributed to the adjacent roadway network using traffic engineering judgement and factors such as proximity to regional freeways and local productions and attractions. The project distribution is shown in **Figure 8** while the project assignment is shown in **Figure 9**.

Figure 8: Project Distribution



<p>SR-76</p> <p>30% OUT</p> <p>I-5 SB Ramp</p> <p>1</p>	<p>SR-76</p> <p>30% OUT</p> <p>30% OUT</p> <p>I-5 NB Ramp</p> <p>2</p> <p>30% IN</p>	<p>SR-76</p> <p>60% IN</p> <p>60% OUT</p> <p>Loretta St</p> <p>3</p>
<p>SR-76</p> <p>60% IN</p> <p>60% OUT</p> <p>Canyon Dr</p> <p>4</p>	<p>SR-76</p> <p>60% OUT</p> <p>5% OUT</p> <p>2% OUT</p> <p>Benet Rd</p> <p>2% IN</p> <p>5% IN</p> <p>5</p>	<p>SR-76</p> <p>5% OUT</p> <p>28% OUT</p> <p>Foussat Rd</p> <p>28% IN</p> <p>2% IN</p> <p>2% OUT</p> <p>6</p> <p>5% IN</p>
<p>Benet Rd</p> <p>67% OUT</p> <p>Airport Rd</p> <p>67% IN</p> <p>7</p>	<p>Benet Rd</p> <p>Eddie Jones Way</p> <p>67% OUT</p> <p>67% IN</p> <p>8</p>	<p>Alex Rd</p> <p>33% OUT</p> <p>33% IN</p> <p>9</p> <p>Foussat Rd</p>

3.6 Construction Traffic Generation

Construction traffic includes, without limitation, haul trips, deliveries, and workers based on the different construction phases. Hours of construction will adhere to the City's permitted hours for construction operations. The number of construction workers and trucks are from the California Emissions Estimator Model (CalEEMod) and were provided by Dudek.

Construction workers will use the project driveway on Benet Rd and on Alex Rd while construction trucks will only the project driveway on Benet Rd. Construction parking will occur on-site.

The overall project construction is forecasted to require up to 12 months. Two specific construction phases have the highest concentration of construction workers and trucks as described below.

The grading phase has up to 10 daily workers and up to 30 daily vendor and haul trucks. A Passenger Car Equivalent (PCE) is used to convert a mixed stream of cars and construction trucks to a single uniform PCE stream for analysis. However, a construction truck PCE is applied only to the segment ADT analysis and not the intersection peak hour analysis because the intersection analysis already incorporates a heavy vehicle factor. According to the HCM, adding a PCE to a simulation analysis that already accounts for heavy vehicles (such as the intersection analysis) would result in duplicative adjustments. A PCE factor of 2 (based on the HCM 6th Edition Exhibit 12-25 for level terrain) is applied to the 30 vendor and haul trucks resulting in 120 construction truck ADT ((30 inbound + 30 outbound) x PCE of 2). The combined worker 20 ADT (10 x 2) and 120 truck ADT results in a total of 140 construction ADT during the grading phase over a period of approximately 2 months.

The building phase has up to 280 daily workers and up to 109 daily vendors/deliveries. A PCE factor of 2 (based on level terrain) is applied to the 109 vendor/delivery construction trucks resulting in 436 truck ADT ((109 inbound + 109 outbound) x PCE of 2). The combined worker 560 ADT (280 x 2) and 436 truck ADT results in a total of 996 construction ADT during the building phase over a period of approximately 5-6 months.

Construction work does not start until 7 AM; however, workers arrive before 7 AM for pre-work task meetings and/or coordination. Since workers arrive before 7 AM, there will not be construction worker traffic added to the 7-9 AM peak hour analysis; however, there will be deliveries and to account for an occasional late worker (estimated at 5 during the grading phase and 10 during the building phase), there are construction related trips during the AM peak hour. The construction workers will typically leave around 3:30 PM; however, a few workers (estimated at 5 during the grading phase and 10 during the building phase) may stay late due to unforeseeable circumstances, which is not anticipated to occur on a regular basis. To account for the unforeseeable circumstances, the estimated number of workers staying later is also added to the PM peak hour. Work is anticipated to occur Monday through Friday. There may be some limited weekend construction work, which would adhere to City of Oceanside ordinances; however, this is not anticipated to occur on any regular basis.

As shown in **Table 9**, the highest number of construction workers, deliveries, and haul trips occur during the building phase and are forecasted at 996 ADT, 38 AM peak hour trips, and 38 PM peak hour trips.

TABLE 9: PROJECT CONSTRUCTION TRAFFIC GENERATION (HIGHEST PHASE)

Construction Phase Name	Approximate Duration by Phase	Daily On-Site Construction Workers	Daily Haul or Delivery Trucks	Truck ADT with PCE Conversion	Total ADT	AM Pk Hr 1hr btw 7-9		PM Pk Hr 1hr btw 4-6	
						IN	OUT	IN	OUT
Grading	around 2 months	up to 10	30	120	140	9	4	4	9
Building	up to 6 months	up to 280	109	436	996	24	14	14	24
Maximums:					996	24	14	14	24

Daily and peak hour data based on client provided data based on similar facilities. PCE: Passenger Car Equivalent of 2.0 (based on level terrain) applied to daily truck trips. ADT: Average Daily Traffic.

The ADT, AM, and PM peak hour construction trips are less than what was analyzed for the project; therefore, no further construction traffic analysis is necessary.

3.7 Existing plus Project Conditions

This scenario analyzes the addition of project traffic onto the existing background traffic for AM peak hour, PM peak hour and daily traffic conditions. The peak hour intersection volumes and daily traffic volumes for this scenario of existing plus project are shown in **Figure 10**. The intersection LOS calculated with the addition of project traffic is shown in **Table 10** with segment LOS shown in **Table 11**. Intersection LOS worksheets are included in **Appendix H**.

TABLE 10: EXISTING PLUS PROJECT INTERSECTION LEVEL OF SERVICE

Intersection and (Analysis) ¹	Movement	Study Period	Existing		Existing + Project			
			Delay ²	LOS ³	Delay ²	LOS ³	Delta ⁴	Impact ⁵
1) SR-76 at I-5 SB Ramp (S)	All	AM	11.2	B	11.3	B	0.1	No
2) SR-76 at I-5 NB Ramp (S)	All	AM	9.4	A	9.5	A	0.1	No
	All	PM	8.0	A	8.7	A	0.7	No
3) SR-76 at Loretta St (S)	All	AM	23.9	C	24.9	C	1.0	No
	All	PM	22.8	C	23.7	C	0.9	No
4) SR-76 at Canyon Dr (S)	All	AM	9.7	A	9.8	A	0.1	No
	All	PM	15.2	B	15.3	B	0.1	No
5) SR-76 at Benet Rd (S)	All	AM	45.8	D	53.2	D	7.4	No
	All	PM	37.8	D	48.7	D	10.9	No
6) SR-76 at Foussat Rd (S)	All	AM	33.3	C	34.2	C	0.9	No
	All	PM	34.8	C	35.8	D	1.0	No
7) Benet Rd at Airport Rd (U)	WB	AM	9.7	A	10.3	B	0.6	No
	WB	PM	10.0	B	10.5	B	0.5	No
8) Benet Rd at Eddie Jones Way (U)	WB	AM	9.9	A	11.1	B	1.2	No
	WB	PM	10.6	B	11.5	B	0.9	No
9) Foussat Rd at Alex Rd (U)	EB	AM	10.5	B	10.5	B	0.0	No
	EB	PM	9.6	A	9.7	A	0.1	No

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service. 4) Delta is the increase in delay from project. 5) Impact if project traffic exceeds threshold.

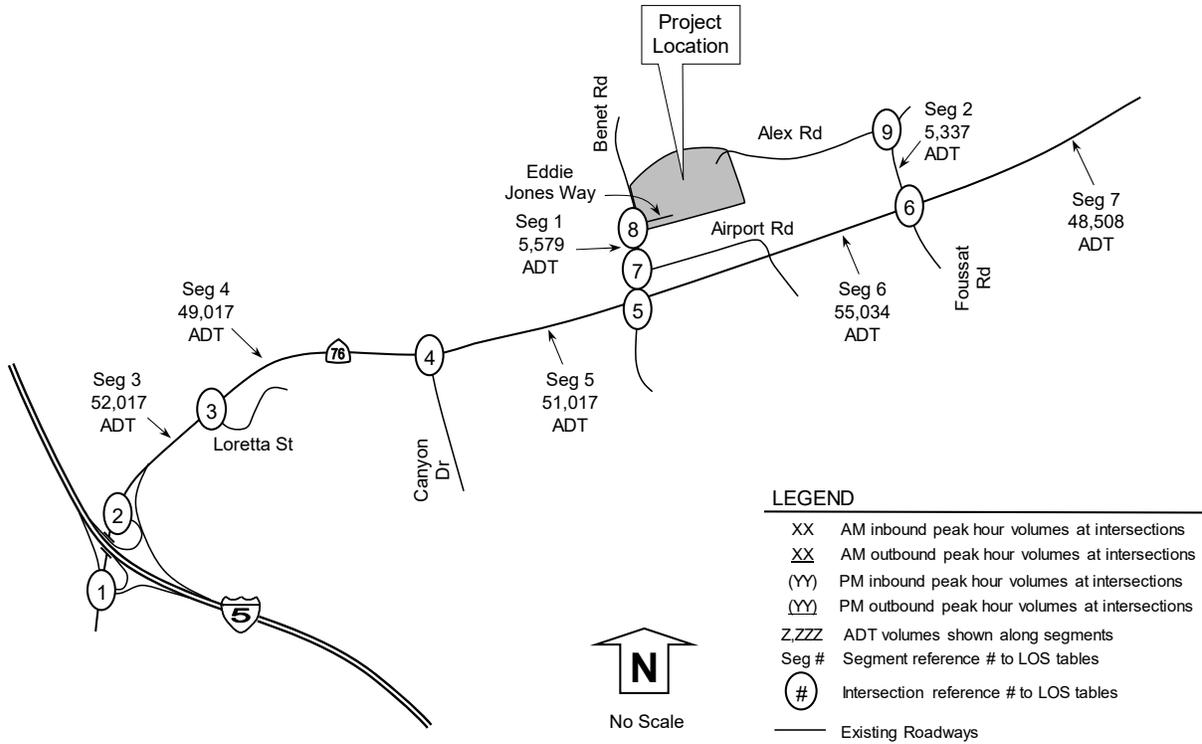
TABLE 11: EXISTING PLUS PROJECT SEGMENT LEVEL OF SERVICE

Segment	Functional Classification	LOS E Capacity	Existing			Project Daily Volume	Existing + Project				
			Daily Volume	V/C	LOS		Daily Volume	V/C	LOS	Change in V/C	Impact ?
Benet Road											
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,444	0.444	B	1,135	5,579	0.558	C	0.114	No
Foussat Rd											
2) Alex Rd to SR-76	2 Lane Collector	10,000	4,778	0.478	B	559	5,337	0.534	B	0.056	No
State Route 76											
3) I-5 to Loretta St	5 Lane Expressway	70,000	51,000	0.729	C	1,017	52,017	0.743	C	0.015	No
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	48,000	0.800	C	1,017	49,017	0.817	C	0.017	No
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	50,000	0.833	D	1,017	51,017	0.850	D	0.017	No
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	55,000	0.917	E	34	55,034	0.917	E	0.001	No
7) East of Foussat Rd	4 Lane Expressway	60,000	48,000	0.800	C	508	48,508	0.808	C	0.008	No

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

Under Existing plus Project conditions, the study elements are calculated to operate at LOS D or better except for segment #6: SR-76 from Benet Rd to Foussat Rd (LOS E). The project will not result in traffic impacts as defined in the Traffic Guidelines; therefore, no off-site improvements are necessary.

Figure 10: Existing plus Project Volumes



<p>SR-76</p> <p>445 (677) ↓</p> <p>1072 (711) ↓</p> <p>(1)</p> <p>280 (460) ↑</p> <p>215 (241) ↑</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>1393 (1089) ↓</p> <p>893 (431) ↓</p> <p>(2)</p> <p>576 (1070) ↑</p> <p>89 (101) ↑</p> <p>I-5 NB Ramp</p> <p>81 (235)</p>	<p>SR-76</p> <p>1182 (1909) →</p> <p>23 (73) ↓</p> <p>(3)</p> <p>43 (25) ↓</p> <p>26 (12) ↓</p> <p>2264 (1481) ←</p> <p>12 (20) ↓</p> <p>Loretta St</p>
<p>SR-76</p> <p>1210 (1859) →</p> <p>14 (48) ↓</p> <p>(4)</p> <p>20 (7) ↓</p> <p>75 (91) ↓</p> <p>2273 (1488) ←</p> <p>74 (136) ↓</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>170 (213) ↓</p> <p>1023 (1711) →</p> <p>129 (88) ↓</p> <p>(5)</p> <p>50 (129) ↓</p> <p>10 (23) ↓</p> <p>46 (91) ↓</p> <p>272 (189) ↓</p> <p>14 (15) ↓</p> <p>9 (15) ↓</p> <p>Benet Rd</p> <p>9 (13)</p> <p>2022 (1307) ←</p> <p>30 (35) ↓</p>	<p>SR-76</p> <p>43 (26) ↓</p> <p>87 (76) ↓</p> <p>160 (91) ↓</p> <p>(6)</p> <p>33 (27) ↓</p> <p>972 (1725) →</p> <p>53 (127) ↓</p> <p>116 (78) ↓</p> <p>60 (127) ↓</p> <p>84 (136) ↓</p> <p>104 (117) ←</p> <p>1877 (1237) ←</p> <p>143 (98) ↓</p> <p>Foussat Rd</p>
<p>Benet Rd</p> <p>297 (195) ↓</p> <p>5 (5) ↓</p> <p>(7)</p> <p>157 (229) ↑</p> <p>21 (14) ↑</p> <p>Airport Rd</p> <p>16 (11)</p> <p>26 (17)</p>	<p>Benet Rd</p> <p>253 (120) ↓</p> <p>0 (0) ↓</p> <p>(8)</p> <p>104 (206) ↑</p> <p>80 (36) ↑</p> <p>0 (0)</p> <p>Eddie Jones Way</p> <p>28 (81)</p>	<p>Alex Rd</p> <p>3 (2) ↓</p> <p>268 (140) ↓</p> <p>(9)</p> <p>2 (2) ↓</p> <p>18 (48) ↓</p> <p>49 (38) ↓</p> <p>164 (226) ↓</p> <p>Foussat Rd</p>

3.8 Cumulative Projects

Cumulative projects are other proposed projects within the study area that once completed will add traffic to the study roadways. The following cumulative projects are anticipated to add traffic to the study area.

- 1) Airport Hotel: a hotel project with 86 rooms to be located on the southwest corner of SR-76 and Airport Rd. The project is calculated to generate 688 daily trips with 34 AM peak hour trips, and 48 PM peak hour trips. The Airport Hotel trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.
- 2) Airport Road Industrial: Industrial buildings totaling 20,007 sf located at 555 Airport Rd. The project is calculated to generate 160 daily trips with 18 AM peak hour trips, and 19 PM peak hour trips. The project assignment is included in **Appendix I**.
- 3) Alta Oceanside: a mixed-use project with 309 multi-family units and 5,800 sf of high-turnover restaurant located on the west side of North Coast Highway adjacent to Costa Pacifica Way. According to the traffic study prepared by Dudek (2020), this cumulative project is calculated to generate 2,504 daily trips with 199 AM peak hour trips, and 202 PM peak hour trips. Excerpts from the Alta traffic study are included in **Appendix I**.
- 4) Concordia Collection at Cypress Point: a residential subdivision with 54 homes to be located at the terminus of Pala Road and Los Arbolitos Blvd. According to the traffic study prepared by LLG (2020), this cumulative project is calculated to generate 540 daily trips with 42 AM peak hour trips, and 53 PM peak hour trips. The Concordia Collection trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.
- 5) El Corazon Specific Plan: a mixed-use project with commercial, retail, hotel, residential, and recreation facilities on 465 acres generally bounded by Mesa Drive to the north, Rancho del Oro Drive to the east, Oceanside Boulevard to the south, and El Camino Real to the west in the City of Oceanside. According to the traffic study prepared by LLG, this cumulative project is calculated to generate 15,498 daily trips with 715 AM trips and 1,534 PM trips. The El Corazon trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.
- 6) Liberty RV and Boat Storage: a 2.13 acre storage facility located at 507 Jones Rd. The project is calculated to generate 64 daily trips with 4 AM peak hour trips, and 6 PM peak hour trips. The project assignment is included in **Appendix I**.
- 7) Melrose Heights: a mixed-use project with 313 homes, 10,000 sf restaurant space, and 10,000 sf office space. According to the traffic study prepared by LLG (2017), this cumulative project is calculated to generate 4,059 daily trips with 121 AM peak hour trips, and 150 PM peak hour trips. The Oceanside + Melrose trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.

- 8) Mission Flats: a mixed-use project with approximately 137 apartment units, up to 4,200 square feet of retail/commercial space, and other supporting amenities on a 4.73-acre site located on 3907 Mission Avenue in the City of Oceanside. According to the traffic study prepared by Dudek (2020), this cumulative project is calculated to generate 990 daily trips with 71 AM peak hour trips, and 89 PM peak hour trips. The trip assignment was obtained from the Mission Flats traffic study with excerpts included in **Appendix I**.
- 9) Modera Melrose: a mixed-use project with up to 324 apartments and 2,338 sf of local serving retail located on the southeast corner of Oceanside Blvd at N. Melrose Drive. According to the traffic study prepared by LLG (2022), this cumulative project is calculated to generate 2,038 daily trips with 159 AM peak hour trips, and 183 PM peak hour trips. The trip assignment was obtained from the Modera Melrose traffic study with excerpts included in **Appendix I**.
- 10) Modera Neptune: a mixed-use project with 62 hotel guestrooms and 360 residential units located west of the N. Coast Highway / SR-76 intersection. According to the traffic study prepared by LLG (2022), this cumulative project is calculated to generate 1,966 net daily trips with 157 AM peak hour trips, and 177 PM peak hour trips. The trip assignment was obtained from the Modera Neptune traffic study with excerpts included in **Appendix I**.
- 11) Nitto Denko Building Addition: an addition of 16,500 sf of Research and Development use located at 501 Via del Monte. The project is calculated to generate 132 daily trips with 21 AM peak hour trips, and 19 PM peak hour trips. The project assignment is included in **Appendix I**.
- 12) North River Farms: a mixed-use project with up to 689 homes, 25,000 sf commercial space, 5,000 sf restaurant space, 30 acres farm use, and 100 room hotel located on N. River Road east of Stallion Dr (approx. ½ mile east of Vandergraft Blvd). According to the traffic study prepared by LLG (2018), this cumulative project is calculated to generate driveway trips in the amount of 7,921 daily trips with 562 AM peak hour trips, and 777 PM peak hour trips. Please note that this cumulative project is included based on the May 2021 Superior Court ruling upholding the project approval and to keep the analysis more conservative. Excerpts from the North River Farms traffic study are included in **Appendix I**.
- 13) Ocean Kamp: a mixed use project with 700 residential units, a 300 room hotel, approximately 126,000 sf of retail/commercial uses, and a wave lagoon. According to the traffic study prepared by LLG (2021), this cumulative project is calculated to generate 19,040 daily trips with 1,057 AM peak hour trips, and 1,834 PM peak hour trips. Excerpts from the Ocean Kamp traffic study are included in **Appendix I**.
- 14) Oceanpointe: a residential subdivision with up to 200 multi-family homes generally located south of SR-76 mid-way between Stage Coach Road and San Ramon Drive in the City of Oceanside. According to the traffic study prepared by LOS Engineering, Inc. (2005), this cumulative project is calculated to generate 1,600 daily trips with 26 AM inbound, 102 AM outbound, 112 PM inbound, and 48 PM outbound trips. The Oceanpointe trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.

- 15) Onpoint Oceanside: a commercial center with a gas station including 3,000 sf food mart and car wash, 7,980 sf retail space, 2,500 sf fast food restaurant space, and 2,320 sf high turnover restaurant space generally located on the southwest corner of SR-76 at Foussat Rd. According to the traffic study prepared by Kimley Horn (2019), this cumulative project is calculated to generate 5,068 daily trips with 213 AM peak hour trips, and 273 PM peak hour trips. The Onpoint Oceanside trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.
- 16) Rio Rockwell: a residential project with 104 homes located near Old Grove Rd at Frazee Rd. According to the traffic study prepared by LLG (2020), this cumulative project is calculated to generate 932 daily trips with 75 AM peak hour trips, and 93 PM peak hour trips. The trip assignment was obtained from the Rio Rockwell traffic study with excerpts included in **Appendix I**.
- 17) Tierra Norte Residential Development Plan: a residential project with up to 400 homes located at 4617 and 4665 N. River Rd. According to the traffic study prepared by LOS Engineering, Inc. (2021), this cumulative project is calculated to generate 3,200 daily trips, 256 AM peak hour trips, and 320 PM peak hour trips. The Tierra Norte trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**. Please note that Tierra Norte was previously referred to as North River Road Residential Subdivision (Kawano-Nagata) and is referenced as such in the Ocean Kamp traffic study from which this cumulative trip assignment was obtained.
- 18) Villas at Mission San Luis Rey: a 222-unit retirement community to be located on the north side of Mission Ave between Douglas Dr and Rancho Del Oro. According to a traffic study prepared by Darnell & Associates, this cumulative project is calculated to generate 694 daily trips with 32 AM peak hour trips, and 49 PM peak hour trips. The Villas at Mission San Luis Rey trip assignment was obtained from the Ocean Kamp traffic study cumulative project section with excerpts included in **Appendix I**.

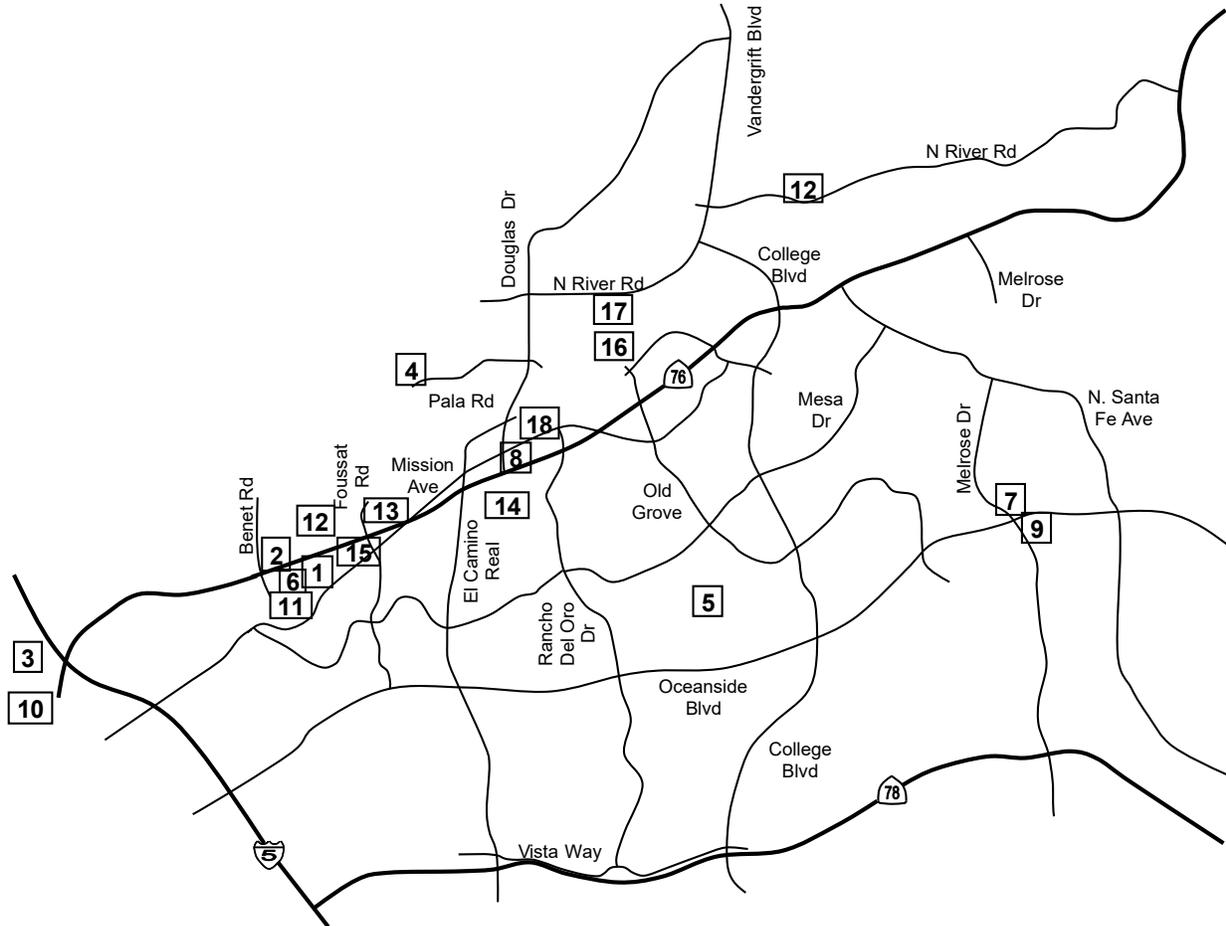
The traffic generated from the proposed cumulative projects is summarized in **Table 12**:

TABLE 12: CUMULATIVE PROJECT TRAFFIC GENERATION

Cumulative Project	ADT	AM Peak Hour Trips	PM Peak Hour Trips
1) Airport Hotel (86 rooms), from Ocean Kamp traffic study	688	34	48
2) Airport Rd Industrial (20,007 sf)	160	18	19
3) Alta Oceanside (mixed use), from Alta traffic study	2,504	199	202
4) Concordia Collection (54 homes), from Ocean Kamp traffic study	540	42	53
5) El Corazon (mixed use), from Ocean Kamp traffic study	15,498	715	1,534
6) Liberty RV and Boat Storage (2.13 acres)	64	4	6
7) Melrose Heights (mixed use), from LLG traffic study	4,059	121	150
8) Mission Flats (mixed use), from Dudek traffic study	990	71	89
9) Modera Melrose (mixed use), from LLG traffic study	2,038	159	183
10) Modera Neptune (mixed use), from LLG traffic study	1,966	157	177
11) Nitto Denko Addition 16,500 sf Research and Development	132	21	19
12) North River Farms (mixed use), from North River Farms traffic study	7,921	562	777
13) Ocean Kamp (mixed use), from Ocean Kamp traffic study	19,040	1,057	1,834
14) Oceanpointe Development (200 multi-family homes), from Ocean Kamp traffic study	1,600	128	160
15) Onpoint Oceanside (commercial center), from Ocean Kamp traffic study	5,068	213	273
16) Rio Rockwell (104 homes), from LLG traffic study	932	75	93
17) Tierra Norte (400 homes), from Ocean Kamp traffic study	3,200	256	320
18) Villas at Mission San Luis Rey (222 retirement homes), from Ocean Kamp traffic study	694	32	49
TOTALS	67,094	3,864	5,986

The cumulative project locations are shown in **Figure 11**. The Ocean Kamp cumulative project is proposing roadway and access improvements along Foussat Rd and SR-76. Ocean Kamp’s improvements along Foussat Rd were included in the near-term roadway conditions as these improvements are within the City of Oceanside jurisdiction and required for their access. Ocean Kamp’s SR-76 proposed improvements on SR-76 were not included as this is within Caltrans’ jurisdiction and cannot be assured to be implemented. The near-term roadway conditions are shown in **Figure 12**. The cumulative project traffic volumes are shown in **Figure 13**.

Figure 11: Cumulative Project Locations



LEGEND

Cumulative Projects:

- | | |
|--------------------------------|------------------------------------|
| 1) Airport Hotel | 11) Nitto Denko |
| 2) Airport Rd Industrial | 12) N. River Farms |
| 3) Alta Oceanside | 13) Ocean Kamp |
| 4) Concordia Collection | 14) Oceanpointe |
| 5) El Corazon | 15) Onpointe Oceanside |
| 6) Liberty RV and Boat Storage | 16) Rio Rockwell |
| 7) Melrose Heights | 17) Tierra Norte |
| 8) Mission Flats | 18) Villas at Mission San Luis Rey |
| 9) Modera Melrose | |
| 10) Modera Neptune | |



No Scale

Figure 12: Near-Term Roadway Conditions with Cumulative Project Improvements

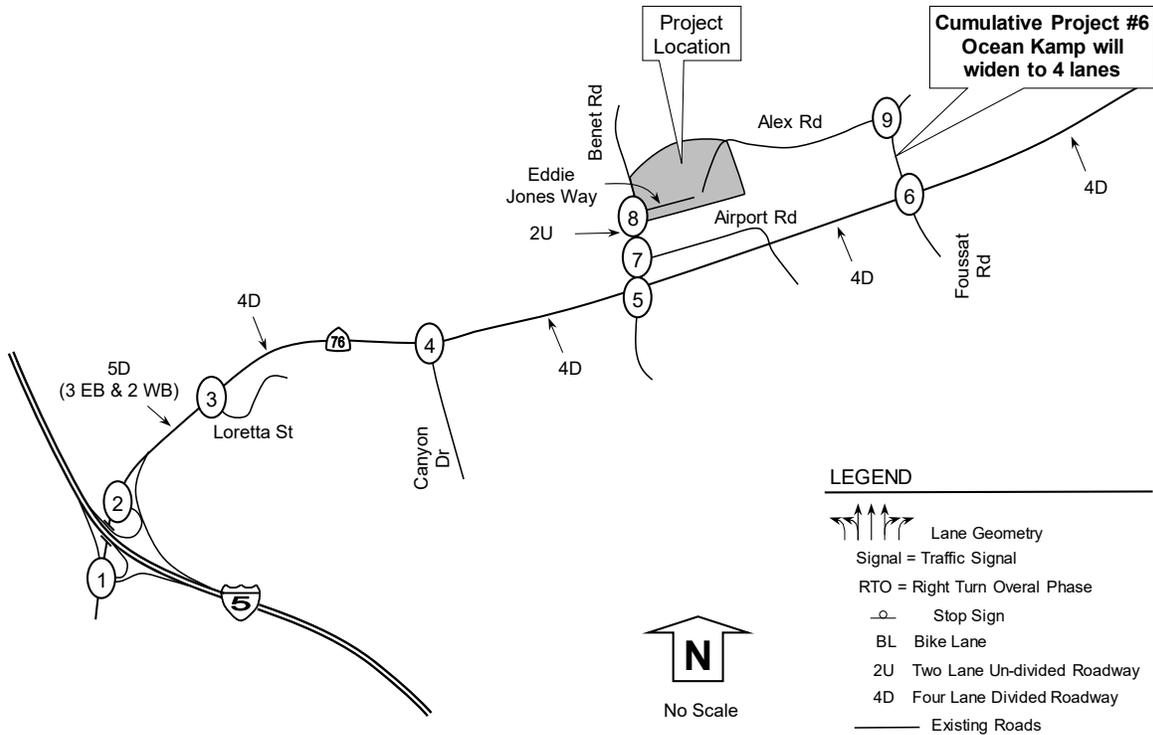
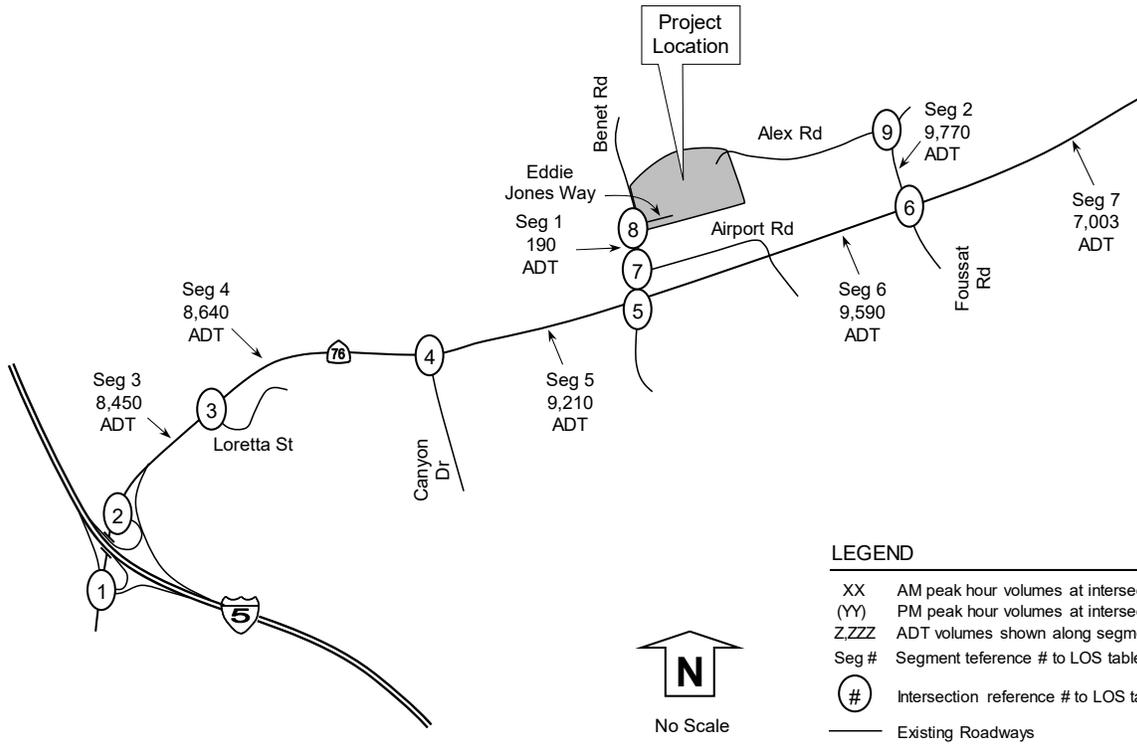


Figure 13: Cumulative Project Volumes



<p>SR-76</p> <p>140 (169) ↓</p> <p>104 (98) ↘</p> <p>I-5 SB Ramp</p> <p>1</p> <p>83 (72) ↑</p> <p>106 (47) ↗</p>	<p>SR-76</p> <p>215 (203) ↓</p> <p>82 (86) ↘</p> <p>I-5 NB Ramp</p> <p>2</p> <p>121 (195) ↑</p> <p>27 (22) ↗</p>	<p>SR-76</p> <p>196 (328) →</p> <p>0 () ↓</p> <p>297 (289) ←</p> <p>14 (12) ↘</p> <p>5 (9) ↙</p> <p>Loretta St</p> <p>3</p>
<p>SR-76</p> <p>201 (336) →</p> <p>0 () ↓</p> <p>0 () ↘</p> <p>0 () ↙</p> <p>4</p> <p>14 (27) ↗</p> <p>26 (25) ↘</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>1 (8) ↗</p> <p>0 () ↓</p> <p>5 (9) ↘</p> <p>Benet Rd</p> <p>6 (6) ↘</p> <p>318 (298) ←</p> <p>17 (8) ↙</p> <p>2 (8) ↙</p> <p>0 () ↓</p> <p>2 (8) ↘</p> <p>5</p> <p>5</p>	<p>SR-76</p> <p>175 (226) ↗</p> <p>77 (101) ↓</p> <p>54 (70) ↘</p> <p>Foussat Rd</p> <p>41 (95) ↘</p> <p>134 (92) ←</p> <p>35 (41) ↙</p> <p>6</p> <p>14 (8) ↙</p> <p>60 (134) ↓</p> <p>32 (51) ↘</p>
<p>Benet Rd</p> <p>5 (9) ↓</p> <p>0 () ↘</p> <p>0 () ↙</p> <p>Airport Rd</p> <p>0 ()</p> <p>1 (8) ↘</p> <p>7</p> <p>6 (6) ↑</p> <p>8 (2) ↗</p>	<p>Benet Rd</p> <p>5 (9) ↓</p> <p>0 () ↘</p> <p>0 () ↙</p> <p>Eddie Jones Way</p> <p>0 ()</p> <p>0 ()</p> <p>6 (6) ↑</p> <p>0 () ↗</p> <p>0 () ↘</p> <p>8</p>	<p>Alex Rd</p> <p>0 () ↗</p> <p>0 () ↓</p> <p>0 () ↘</p> <p>FousSAT Rd</p> <p>0 () ↘</p> <p>141 (323) ↑</p> <p>9</p>

3.9 Near Term (Existing + Cumulative) Conditions

This scenario analyzes the addition of cumulative project traffic onto the existing traffic for AM peak hour, PM peak hour, and daily traffic conditions. The peak hour intersection volumes and daily traffic volumes for this scenario are shown in **Figure 14**. The intersection LOS calculated with the addition of cumulative traffic is shown in **Table 13** with segment LOS shown in **Table 14**. Intersection LOS worksheets are included in **Appendix J**. Please note the cumulative project Ocean Kamp is proposing a roundabout at intersection #9 (Foussat Rd/Alex Rd) and widening Foussat Rd, which are incorporated in the LOS calculations below.

TABLE 13: NEAR TERM (EXISTING + CUMULATIVE) INTERSECTION LEVEL OF SERVICE

Intersection and (Analysis) ¹	Movement	Peak Hour	Near Term (Existing + Cumulative)	
			Delay ²	LOS ³
1) SR-76 at I-5 SB Ramp (S)	All	AM	14.2	B
2) SR-76 at I-5 NB Ramp (S)	All	PM	11.6	B
3) SR-76 at Loretta St (S)	All	AM	10.0	B
4) SR-76 at Canyon Dr (S)	All	PM	10.7	B
5) SR-76 at Benet Rd (S)	All	AM	49.6	D
6) SR-76 at Foussat Rd (S)	All	PM	45.3	D
7) Benet Rd at Airport Rd (U)	WB	AM	13.1	B
8) Benet Rd at Eddie Jones Way (U)	WB	PM	20.1	C
9) Foussat Rd at Alex Rd (Roundabout)	All	AM	91.6	F
	All	PM	57.8	E
	All	AM	57.3	E
	All	PM	58.5	E
	WB	AM	9.7	A
	WB	PM	10.2	B
	WB	AM	9.9	A
	WB	PM	10.7	B
	All	AM	6.8	A
	All	PM	10.7	B

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service. DNE: Does Not Exist. NA: Not Applicable.

TABLE 14: NEAR TERM (EXISTING + CUMULATIVE) SEGMENT LEVEL OF SERVICE

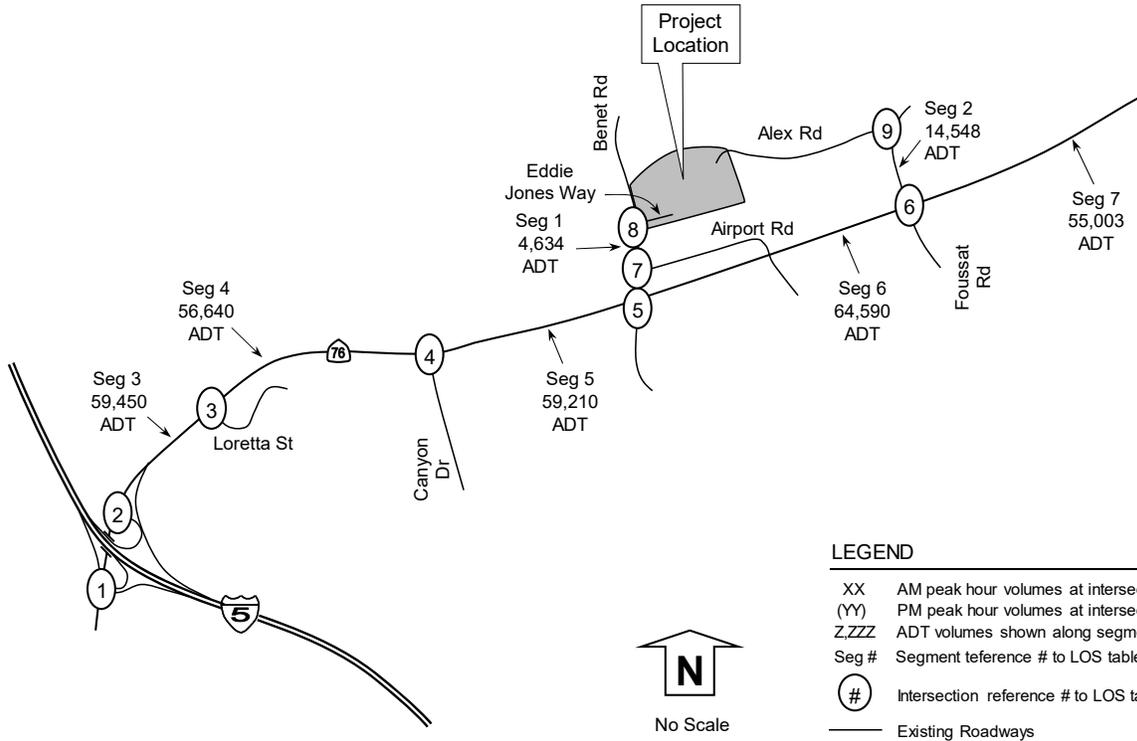
Segment	Current Classification	LOS E Capacity	Near-Term (E+C)		
			Daily Volume	V/C	LOS
Benet Road					
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,634	0.463	B
Foussat Rd					
2) Alex Rd to SR-76	4 Lane Secondary Collector	25,000	14,548	0.582	C
State Route 76					
3) I-5 to Loretta St	5 Lane Expressway	70,000	59,450	0.849	D
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	56,640	0.944	E
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	59,210	0.987	E
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	64,590	1.077	F
7) East of Foussat Rd	4 Lane Expressway	60,000	55,003	0.917	E

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

Under Near Term (Existing + Cumulative) conditions, the following study elements are calculated to operate at LOS E/F:

- 1) Intersection #5: SR-76/Benet Rd
- 2) Intersection #6: SR-76/Foussat Rd
- 3) Segment #4: SR-76 from Loretta St to Canyon Dr
- 4) Segment #5: SR-76 from Canyon Dr to Benet Rd
- 5) Segment #6: SR-76 from Benet Rd to Foussat Rd
- 6) Segment #7: SR-76 east of Foussat Rd

Figure 14: Near Term (Existing + Cumulative) Volumes



SR-76 1164 (773) ↓ 1 (846) ↑ 363 (532)	1164 (773) ↘ I-5 SB Ramp 321 (288)	SR-76 1596 (1256) ↓ 2 (1249) ↑ 661 (1249)	963 (481) ↘ I-5 NB Ramp 116 (123)	SR-76 1306 (2204) → 23 (73) ↓ 43 (25)	3 (21) ← 2537 (1698) 26 (32)
SR-76 1339 (2162) → 14 (48) ↓ 20 (7)	2553 (1711) ← 100 (161) ↘ Canyon Dr 89 (118)	SR-76 249 (125) ↑ 12 (9) ↓ 52 (137)	13 (22) ↘ Benet Rd 13 (18) 47 (43)	SR-76 165 (332) ↑ 1018 (1819) → 84 (164) ↓ 130 (86)	6 (126) ← 202 (126) Fou-ssat Rd 112 (197) 2009 (1328) 178 (139)
Benet Rd 274 (123) ↓ 7 (199) ↑ 83 (199)	5 (5) ↘ Airport Rd 16 (11) 27 (25)	Benet Rd 258 (129) ↓ 8 (212) ↑ 110 (212)	1 (1) ↘ Eddie Jones Way 1 (1)	Alex Rd 3 (2) ↓ 2 (2) ↑ 5 (8) ↓ 10 (20)	9 (381) ← Foussat Rd 454 (549) 305 (549)

3.10 Near Term (Existing + Cumulative) plus Project Conditions

This scenario analyzes the addition of Project traffic onto Near Term (Existing + Cumulative) conditions for AM peak hour, PM peak hour, and daily traffic conditions. The peak hour intersection volumes and daily traffic volumes for this scenario are shown in **Figure 15**. The intersection LOS is shown in **Table 15** and the segment LOS is shown in **Table 16**. LOS worksheets are included in **Appendix K**.

TABLE 15: NEAR TERM (EXISTING + CUMULATIVE) PLUS PROJECT INTERSECTION LEVEL OF SERVICE

Intersection and (Analysis) ¹	Movement	Peak Hour	Existing + Cumulative		Existing + Cumulative + Project			
			Delay ²	LOS ³	Delay ²	LOS ³	Delta ⁴	Impact ⁵
1) SR-76 at I-5 SB Ramp (S)	All	AM	14.2	B	14.3	B	0.1	No
2) SR-76 at I-5 NB Ramp (S)	All	PM	11.6	B	11.8	B	0.2	No
3) SR-76 at Loretta St (S)	All	AM	10.0	B	10.2	B	0.2	No
4) SR-76 at Canyon Dr (S)	All	PM	10.7	B	12.8	B	2.1	No
5) SR-76 at Benet Rd (S)	All	AM	49.6	D	52.2	D	2.6	No
6) SR-76 at Foussat Rd (S)	All	PM	45.3	D	48.6	D	3.3	No
7) Benet Rd at Airport Rd (U)	All	AM	13.1	B	13.4	B	0.3	No
8) Benet Rd at Eddie Jones Way (U)	All	PM	20.1	C	20.7	C	0.6	No
9) Foussat Rd at Alex Rd (Roundabout)	All	AM	91.6	F	105.8	F	14.2	Yes
		PM	57.8	E	85.5	F	27.7	Yes
		AM	57.3	E	57.6	E	0.3	No
		PM	58.5	E	58.8	E	0.3	No
	WB	AM	9.7	A	10.4	B	0.7	No
	WB	PM	10.2	B	10.7	B	0.5	No
	WB	AM	9.9	A	11.7	B	1.8	No
	WB	PM	10.7	B	11.8	B	1.1	No
	All	AM	6.8	A	7.2	A	0.4	No
	All	PM	10.7	B	11.0	B	0.3	No

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service. 4) Delta is the increase in delay from project. 5) Impact if project traffic exceeds threshold.

TABLE 16: NEAR TERM (EXISTING + CUMULATIVE) PLUS PROJECT SEGMENT LEVEL OF SERVICE

Segment	Functional Classification	LOS E Capacity	Near-Term (E+C)			Project Daily Volume	Near-Term with Project (E+C+P)				
			Daily Volume	V/C	LOS		Daily Volume	Daily Volume	V/C	LOS	Change in V/C
Benet Road											
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,634	0.463	B	1,135	5,769	0.577	C	0.114	No
Foussat Rd											
2) Alex Rd to SR-76	4 Ln Secondary Coll.	25,000	14,548	0.582	C	559	15,107	0.604	C	0.022	No
State Route 76											
3) I-5 to Loretta St	5 Lane Expressway	70,000	59,450	0.849	D	1,017	60,467	0.864	D	0.015	No
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	56,640	0.944	E	1,017	57,657	0.961	E	0.017	No
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	59,210	0.987	E	1,017	60,227	1.004	F	0.017	No
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	64,590	1.077	F	34	64,624	1.077	F	0.001	No
7) East of Foussat Rd	4 Lane Expressway	60,000	55,003	0.917	E	508	55,511	0.925	E	0.008	No

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

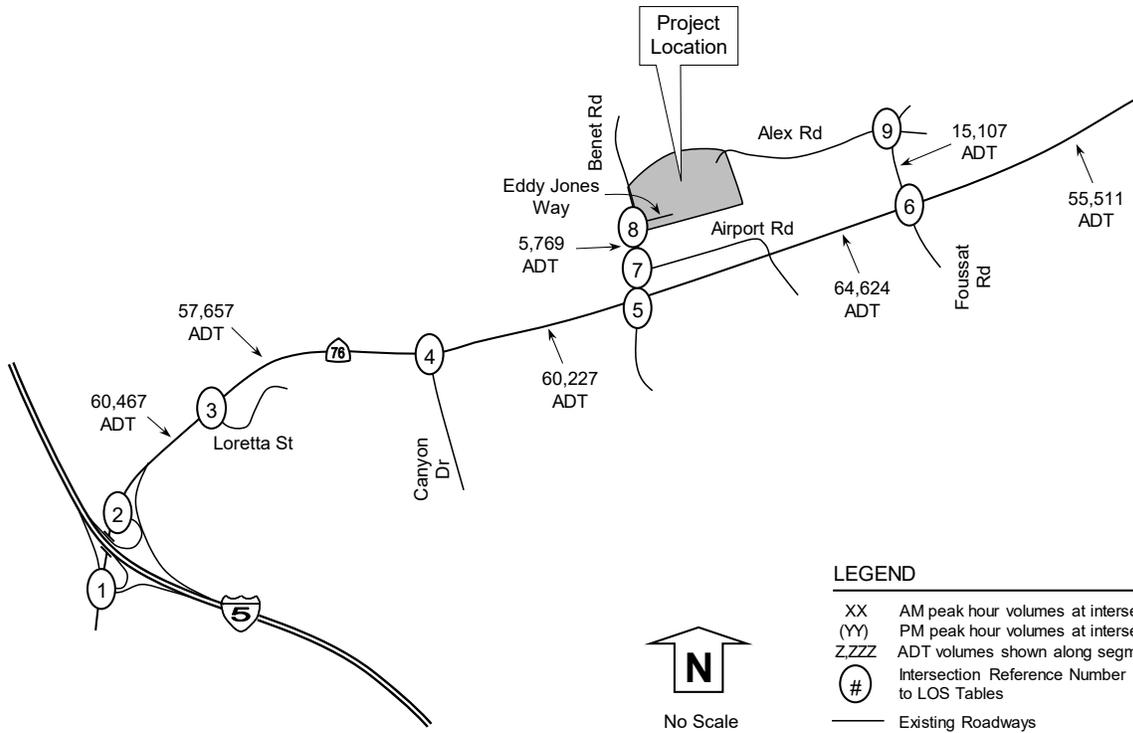
Under Near Term (Existing + Cumulative) plus Project conditions, the following study locations are calculated to operate at LOS E/F AND the project has a transportation impact:

- 1) Intersection #5: SR-76/Benet Rd

The following study locations are calculated to operate at LOS E/F without impacts because the project traffic does not exceed the transportation impact thresholds defined in the Traffic Guidelines; therefore, no off-site improvements are necessary:

- 2) Intersection #6: SR-76/Foussat Rd
- 3) Segment #4: SR-76 from Loretta St to Canyon Dr
- 4) Segment #5: SR-76 from Canyon Dr to Benet Rd
- 5) Segment #6: SR-76 from Benet Rd to Foussat Rd
- 6) Segment #7: SR-76 east of Foussat Rd

Figure 15: Near Term (Existing + Cumulative) plus Project Volumes



<p>SR-76</p> <p>585 (846) ↓</p> <p>1176 (809) ↘</p> <p>①</p> <p>363 (532) ↑</p> <p>321 (288) ↗</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>1608 (1292) ↓</p> <p>975 (517) ↘</p> <p>②</p> <p>273 (197) ↗</p> <p>14 (15) ↓</p> <p>14 (24) ↘</p> <p>105 (290) ↙</p> <p>697 (1265) ↑</p> <p>116 (123) ↗</p> <p>Benet Rd</p>	<p>SR-76</p> <p>1378 (2237) →</p> <p>23 (73) ↘</p> <p>③</p> <p>43 (25) ↙</p> <p>31 (21) ↗</p> <p>2561 (1770) ←</p> <p>26 (32) ↘</p> <p>Loretta St</p>
<p>SR-76</p> <p>1411 (2195) →</p> <p>14 (48) ↘</p> <p>④</p> <p>20 (7) ↙</p> <p>89 (118) ↗</p> <p>100 (161) ↘</p> <p>2577 (1783) ←</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>178 (215) ↗</p> <p>1220 (2072) →</p> <p>138 (90) ↘</p> <p>⑤</p> <p>52 (137) ↙</p> <p>10 (23) ↓</p> <p>48 (99) ↘</p> <p>2340 (1605) ←</p> <p>47 (43) ↘</p> <p>Benet Rd</p>	<p>SR-76</p> <p>218 (252) ↗</p> <p>164 (177) ↓</p> <p>214 (161) ↘</p> <p>⑥</p> <p>130 (86) ↙</p> <p>120 (261) ↓</p> <p>116 (187) ↘</p> <p>145 (212) ↗</p> <p>2011 (1329) ←</p> <p>178 (139) ↘</p> <p>Foussat Rd</p>
<p>Benet Rd</p> <p>302 (204) ↓</p> <p>5 (5) ↘</p> <p>⑦</p> <p>163 (235) ↑</p> <p>29 (16) ↗</p> <p>16 (11) ↘</p> <p>27 (25) ↙</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>258 (129) ↓</p> <p>0 (0) ↘</p> <p>⑧</p> <p>110 (212) ↑</p> <p>80 (36) ↗</p> <p>28 (81) ↘</p> <p>0 (0) ↙</p> <p>Eddy Jones Way</p>	<p>Alex Rd</p> <p>2 (2) ↗</p> <p>4 (11) →</p> <p>18 (48) ↘</p> <p>⑨</p> <p>49 (38) ↙</p> <p>305 (549) ↑</p> <p>91 (211) ↗</p> <p>43 (55) ↗</p> <p>6 (8) ↘</p> <p>121 (156) ↙</p> <p>32 (74) ↘</p> <p>454 (381) ↓</p> <p>Foussat Rd</p> <p>Ocean Kamp Dwy</p>

3.11 Horizon Year 2030 Conditions

This scenario represents horizon year 2030 conditions based on SANDAG forecast segment volumes. Two sets of volumes were reviewed, which included the City of Oceanside Circulation Element Year 2030 Master Transportation Plan volumes and SANDAG Series 13 Year 2035 traffic model volumes. SANDAG does not provide a Series 13 Year 2030 volume; therefore, the closest Year 2035 volumes were applied.

Horizon year segment volumes were obtained from both the Circulation Element and the SANDAG Series 13 model. The applied horizon year volumes for this analysis are shown in **Table 17**. Horizon Year segment volumes are included in **Appendix L**.

TABLE 17: HORIZON YEAR ADT VOLUME SOURCES, ADJUSTMENTS, AND APPLICATION

Segment	Year & ADT	2030 City Circ. Series 11	2035 SANDAG Series 13	Horizon Year Applied ADT	Applied Horizon Year Change Over Existing
Benet Road Year 2021					
1) Eddy Jones to SR-76	4,444	No Vol.	4,600	4,700 *	5.8%
Foussat Rd Year 2021					
2) Alex Rd to SR-76	4,778	No Vol.	8,500	14,600**	205.6%
State Route 76 Year 2019					
3) I-5 to Loretta St	50,000	60,700	SR-76	60,700	21.4%
4) Loretta St to Canyon Dr	45,000	60,700	volumes	60,700	34.9%
5) Canyon Dr to Benet Rd	46,500	65,900	lower than	65,900	41.7%
6) Benet Rd to Foussat Rd	46,500	65,900	existing, thus	65,900	41.7%
7) East of Foussat Rd	46,500	59,800	not used	59,800	28.6%
SR-76 Average:					33.7%

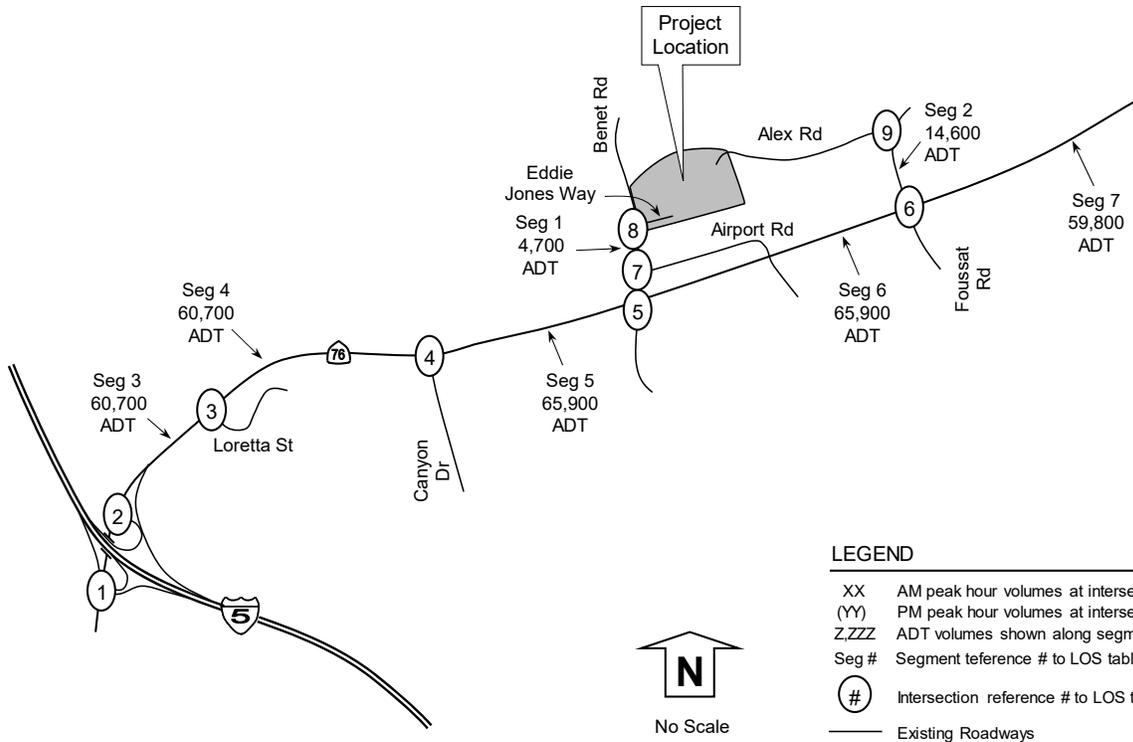
City Circ. = City Circulation Element. *Series 13 ADT of 4,600 increased to 4,700 to be higher than Existing + Cumulative ADT of 4,634. **Series 13 ADT of 8,500 increased to 14,600 to be higher than Existing + Cumulative ADT of 14,548.

Horizon year 2030 intersection volumes were forecasted based on the growth factors noted in Table 19 by intersection location. For example, intersections along SR-76 (Int #1 thru #6) had existing volumes increased by an average growth factor of 33.7% to represent 2030 volumes. Intersections #7 and #8 had existing volumes increase by 5.8%. Intersection #9 had existing volumes increase by 205.6%. The applied growth factors for future intersection volumes are shown below.

- 1) SR-76/I-5 SB Ramps (33.7% growth)
- 2) SR-76/I-5 NB Ramps (33.7% growth)
- 3) SR-76/Loretta St (33.7% growth)
- 4) SR-76/Canyon Dr (33.7% growth)
- 5) SR-76/Benet Rd (33.7% growth)
- 6) SR-76/Foussat Rd (33.7% growth)
- 7) Benet Rd/Airport Rd (5.8% growth)
- 8) Benet Rd/Eddie Jones Way (5.8% growth)
- 9) Foussat Rd/Alex Rd (205.6% growth)

The horizon year 2030 peak hour intersection volumes and daily traffic volumes are shown in **Figure 16**.

Figure 16: Horizon Year 2030 Volumes



<p>SR-76</p> <p>590 (910) ↓</p> <p>1420 (900) ↓</p> <p>(1)</p> <p>370 (620) ↑</p> <p>290 (320) ↑</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>1850 (1410) ↓</p> <p>1180 (530) ↓</p> <p>(2)</p> <p>720 (1410) ↑</p> <p>120 (140) ↑</p> <p>I-5 NB Ramp</p>	<p>SR-76</p> <p>1480 (2510) →</p> <p>30 (100) ↓</p> <p>(3)</p> <p>60 (30) ↓</p> <p>2990 (1880) ←</p> <p>20 (30) ↓</p> <p>Loretta St</p>
<p>SR-76</p> <p>1520 (2440) →</p> <p>20 (60) ↓</p> <p>(4)</p> <p>30 (10) ↓</p> <p>100 (120) ↓</p> <p>3010 (1890) ←</p> <p>100 (180) ↓</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>330 (160) ↓</p> <p>20 (10) ↓</p> <p>10 (20) ↓</p> <p>(5)</p> <p>1370 (2290) →</p> <p>170 (120) ↓</p> <p>70 (170) ↓</p> <p>10 (20) ↓</p> <p>2700 (1750) ←</p> <p>40 (50) ↓</p> <p>Benet Rd</p>	<p>SR-76</p> <p>60 (30) ↓</p> <p>110 (90) ↓</p> <p>200 (70) ↓</p> <p>(6)</p> <p>40 (40) ↓</p> <p>1300 (2300) →</p> <p>70 (170) ↓</p> <p>160 (100) ↓</p> <p>70 (170) ↓</p> <p>110 (180) ↓</p> <p>Fou-ssat Rd</p>
<p>Benet Rd</p> <p>280 (120) ↓</p> <p>10 (10) ↓</p> <p>(7)</p> <p>80 (200) ↑</p> <p>20 (10) ↓</p> <p>20 (10) ↓</p> <p>30 (20) ↓</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>270 (130) ↓</p> <p>0 (0) ↓</p> <p>(8)</p> <p>110 (220) ↑</p> <p>0 (0) ↓</p> <p>0 (0) ↓</p> <p>Eddie Jones Way</p>	<p>Alex Rd</p> <p>10 (5) ↓</p> <p>(5) (290) ↓</p> <p>(9)</p> <p>5 (5) ↓</p> <p>10 (20) ↓</p> <p>20 (40) ↓</p> <p>340 (460) ↑</p> <p>Foussat Rd</p>

The Horizon Year 2030 scenario reflects the current roadway network with Ocean Kamp improvements planned along Foussat Rd that are required for their access as shown previously in Figure 12. The intersection LOS calculated is shown in **Table 18** with segment LOS shown in **Table 19**. Intersection LOS worksheets are included in **Appendix M**.

TABLE 18: HORIZON YEAR 2030 INTERSECTION LOS

Intersection and (Analysis) ¹	Movement	Study Period	Horizon Year 2030	
			Delay ²	LOS ³
1) SR-76 at I-5 SB Ramp (S)	All	AM	16.3	B
	All	PM	13.1	B
2) SR-76 at I-5 NB Ramp (S)	All	AM	12.3	B
	All	PM	14.5	B
3) SR-76 at Loretta St (S)	All	AM	106.2	F
	All	PM	80.1	E
4) SR-76 at Canyon Dr (S)	All	AM	45.3	D
	All	PM	40.1	D
5) SR-76 at Benet Rd (S)	All	AM	183.7	F
	All	PM	121.9	F
6) SR-76 at Foussat Rd (S)	All	AM	105.1	F
	All	PM	103.1	F
7) Benet Rd at Airport Rd (U)	WB	AM	9.8	A
	WB	PM	10.2	B
8) Benet Rd at Eddy Jones Way (U)	WB	AM	0.0	A
	WB	PM	0.0	A
9) Foussat Rd at Alex Rd (Roundabout)	All	AM	8.2	A
	All	PM	9.6	A

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service.

TABLE 19: HORIZON YEAR 2030 SEGMENT VOLUMES AND LOS

Segment	Applied Classification	LOS E Capacity	Horizon Year (2030)		
			Daily Volume	V/C	LOS
Benet Road					
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,700	0.470	B
Foussat Rd					
2) Alex Rd to SR-76	4 Ln Secondary Coll.	10,000	14,600	1.460	C
State Route 76					
3) I-5 to Loretta St	5 Lane Expressway	70,000	60,700	0.867	D
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	60,700	1.012	F
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	65,900	1.098	F
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	65,900	1.098	F
7) East of Foussat Rd	4 Lane Expressway	60,000	59,800	0.997	E

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

Under Horizon Year 2030 conditions, the following study locations are calculated to operate at LOS E/F:

- 1) Intersection #3: SR-76/Loretta St
- 2) Intersection #5: SR-76/Benet Rd
- 3) Intersection #6: SR-76/Foussat Rd
- 4) Segment #4: SR-76 from Loretta St to Canyon Dr
- 5) Segment #5: SR-76 from Canyon Dr to Benet Rd
- 6) Segment #6: SR-76 from Benet Rd to Foussat Rd
- 7) Segment #7: SR-76 east of Foussat Rd

3.12 Horizon Year 2030 plus Project Conditions

This scenario documents the addition of project traffic onto Horizon Year 2030 conditions. The horizon year + project volumes are shown in **Figure 17**. The intersection LOS is shown in **Table 20** with segment LOS shown in **Table 21**. Intersection LOS worksheets are included in **Appendix N**.

TABLE 20: HORIZON YEAR 2030 PLUS PROJECT INTERSECTION LEVEL OF SERVICE

Intersection and (Analysis) ¹	Movement	Study Period	Horizon Year 2030		Horizon Year 2030 + Project			
			Delay ²	LOS ³	Delay ²	LOS ³	Delta ⁴	Impact? ⁵
1) SR-76 at I-5 SB Ramp (S)	All	AM	16.3	B	16.5	B	0.2	No
2) SR-76 at I-5 NB Ramp (S)	All	PM	12.3	B	12.6	B	0.3	No
3) SR-76 at Loretta St (S)	All	AM	106.2	F	107.9	F	1.7	No
4) SR-76 at Canyon Dr (S)	All	PM	80.1	E	81.5	F	1.4	No
5) SR-76 at Benet Rd (S)	All	AM	45.3	D	47.5	D	2.2	No
6) SR-76 at Foussat Rd (S)	All	PM	40.1	D	43.2	D	3.1	No
7) Benet Rd at Airport Rd (U)	All	AM	183.7	F	202.1	F	18.4	Yes
8) Benet Rd at Eddy Jones Way (U)	All	PM	121.9	F	166.0	F	44.1	Yes
9) Foussat Rd at Alex Rd (Roundabout)	All	AM	105.1	F	105.3	F	0.2	No
	All	PM	103.1	F	104.3	F	1.2	No
	WB	AM	9.8	A	10.4	B	0.6	No
	WB	PM	10.2	B	10.7	B	0.5	No
	WB	AM	0.0	A	11.4	B	11.4	No
	WB	PM	0.0	A	11.7	B	11.7	No
	All	AM	8.2	A	8.7	A	0.5	No
	All	PM	9.6	A	9.8	B	0.2	No

Notes: 1) Intersection Analysis - (S) Signalized, (U) Unsignalized. 2) Delay - HCM Average Control Delay in seconds. 3) LOS: Level of Service. 4) Delta is the increase in delay from project. 5) Impact if project traffic exceeds threshold.

TABLE 21: HORIZON YEAR 2030 PLUS PROJECT SEGMENT LEVEL OF SERVICE

Segment	Applied Classification	Horizon Year (2030)				Horizon Year (2030) with Project				Change In V/C	Impact ?
		LOS E Capacity	Daily Volume	V/C	LOS	Project Daily Volume	Daily Volume	V/C	LOS		
Benet Road											
1) Eddy Jones to SR-76	2 Lane Collector	10,000	4,700	0.470	B	1,135	5,835	0.584	C	0.114	No
Foussat Rd											
2) Alex Rd to SR-76	4 Ln Secondary Coll.	25,000	14,600	0.584	C	559	15,159	0.606	C	0.022	No
State Route 76											
3) I-5 to Loretta St	5 Lane Expressway	70,000	60,700	0.867	D	1,017	61,717	0.882	D	0.015	No
4) Loretta St to Canyon Dr	4 Lane Expressway	60,000	60,700	1.012	F	1,017	61,717	1.029	F	0.017	No
5) Canyon Dr to Benet Rd	4 Lane Expressway	60,000	65,900	1.098	F	1,017	66,917	1.115	F	0.017	No
6) Benet Rd to Foussat Rd	4 Lane Expressway	60,000	65,900	1.098	F	34	65,934	1.099	F	0.001	No
7) East of Foussat Rd	4 Lane Expressway	60,000	59,800	0.997	E	508	60,308	1.005	F	0.008	No

Notes: Daily volume is a 24 hour volume. LOS: Level of Service. V/C: Volume to Capacity Ratio.

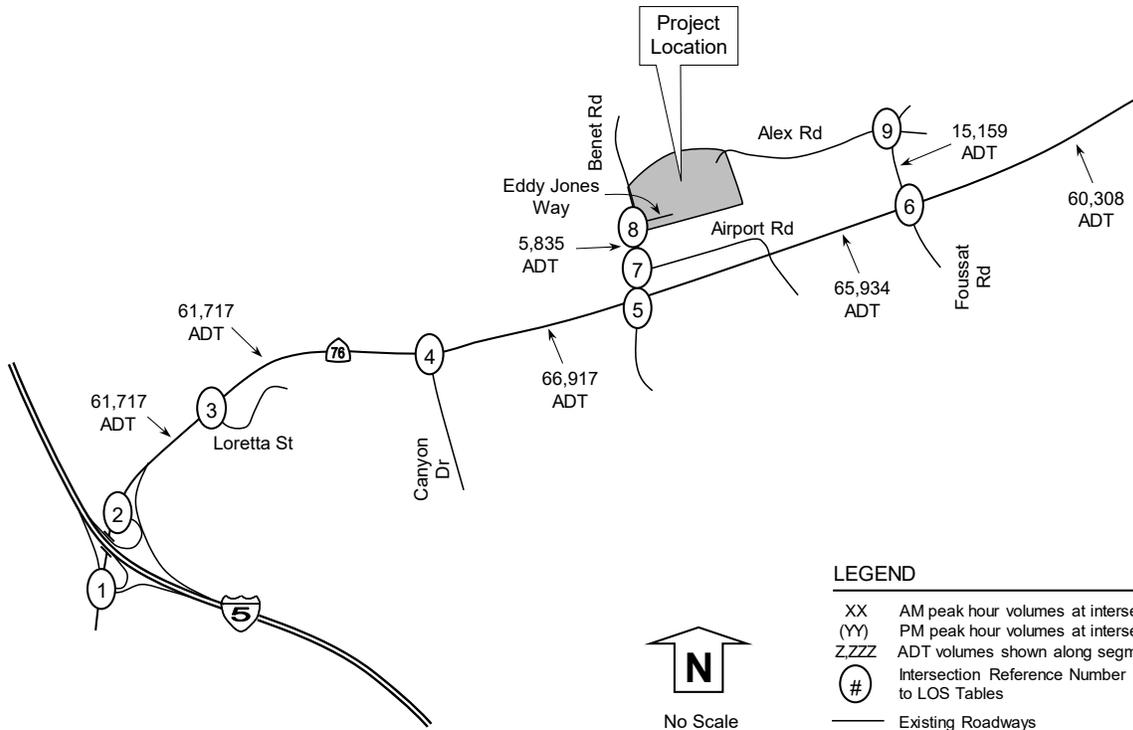
Under Horizon Year 2030 plus Project conditions, the following study location is calculated to operate at LOS E/F AND the project has a transportation impact:

- 1) Intersection #5: SR-76/Benet Rd

The following locations are calculated to operate at LOS E/F without impacts because the project traffic does not exceed the transportation impact thresholds defined in the Traffic Guidelines; therefore, no off-site improvements are necessary:

- 2) Intersection #3: SR-76/Loretta St
- 3) Intersection #6: SR-76/Foussat R
- 4) Segment #4: SR-76 from Loretta St to Canyon Dr
- 5) Segment #5: SR-76 from Canyon Dr to Benet Rd
- 6) Segment #6: SR-76 from Benet Rd to Foussat Rd
- 7) Segment #7: SR-76 east of Foussat Rd

Figure 17: Horizon Year 2030 plus Project Volumes



<p>SR-76</p> <p>590 (910) ↓</p> <p>1432 (936) ↘</p> <p>①</p> <p>370 (620) ↑</p> <p>290 (320) ↗</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>1862 (1446) ↓</p> <p>1192 (566) ↘</p> <p>I-5 NB Ramp</p> <p>110 (310)</p> <p>756 (1426) ↑</p> <p>120 (140)</p>	<p>SR-76</p> <p>1552 (2543) →</p> <p>30 (100) ↘</p> <p>60 (30) ↘</p> <p>30 (20) ↘</p> <p>3014 (1952) ←</p> <p>20 (30) ↘</p> <p>Loretta St</p>
<p>SR-76</p> <p>1592 (2473) →</p> <p>20 (60) ↘</p> <p>30 (10) ↘</p> <p>④</p> <p>100 (120) ↘</p> <p>100 (180) ↘</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>202 (273) ↘</p> <p>1370 (2290) →</p> <p>170 (120) ↘</p> <p>70 (170) ↘</p> <p>354 (232) ↘</p> <p>22 (16) ↓</p> <p>11 (22) ↓</p> <p>12 (21) ↓</p> <p>2700 (1750) ←</p> <p>40 (50) ↓</p> <p>Benet Rd</p>	<p>SR-76</p> <p>60 (30) ↘</p> <p>112 (96) ↓</p> <p>212 (105) ↓</p> <p>40 (40) ↘</p> <p>1301 (2302) →</p> <p>70 (170) ↘</p> <p>160 (100) ↘</p> <p>76 (173) ↓</p> <p>110 (180) ↓</p> <p>⑥</p> <p>123 (155) ↘</p> <p>2512 (1651) ←</p> <p>190 (130) ↘</p> <p>Foussat Rd</p>
<p>Benet Rd</p> <p>308 (201) ↓</p> <p>10 (10) ↘</p> <p>20 (10) ↘</p> <p>30 (20) ↘</p> <p>⑦</p> <p>160 (236) ↑</p> <p>20 (10) ↗</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>270 (130) ↓</p> <p>0 (0) ↘</p> <p>0 (0) ↘</p> <p>28 (81) ↘</p> <p>110 (220) ↑</p> <p>80 (36) ↗</p> <p>Eddy Jones Way</p>	<p>Alex Rd</p> <p>5 (5) ↘</p> <p>5 (15) ↘</p> <p>23 (60) ↘</p> <p>10 (5) ↘</p> <p>550 (290) ↓</p> <p>35 (75) ↓</p> <p>45 (55) ↘</p> <p>10 (10) ↘</p> <p>125 (160) ↘</p> <p>⑨</p> <p>59 (58) ↘</p> <p>340 (460) ↑</p> <p>95 (215) ↗</p> <p>Foussat Rd</p> <p>Ocean Kamp Dwy</p>

3.13 Summary of Transportation Impacts and Necessary Improvements

Based on the City of Oceanside traffic significance criteria, the project is calculated to have a transportation impact under Near Term, and Horizon Year conditions as shown in **Table 22**.

TABLE 22: PROJECT TRANSPORTATION IMPACT BY SCENARIO

Intersection	Existing + Project	Near Term + Project	Horizon Year 2030 + Project
T-1 (Int #5): SR-76/Benet Rd	No Impact	Transportation Impact	Transportation Impact

Notes: Transportation impact if project traffic is calculated to exceed the allowable thresholds under LOS E or F conditions.

3.13.1 T-1: Intersection #5 SR-76/Benet Rd

The project impact is calculated to occur under near term (existing + cumulative) and horizon year conditions. The poor LOS at SR-76/Benet Rd is from cumulative project traffic being added to this intersection. Therefore, the proposed improvement is a fair share payment toward a future improvement based on the project’s roughly proportional share of project traffic added to background traffic. Future improvements may include an adaptive signal system or converting the existing eastbound and westbound right turn lanes to a combination through-right lane resulting in three through lanes in each direction. With three through lanes in the eastbound and westbound directions, the intersection is calculated to operate at better than pre-project conditions as shown in **Table 23** for near term and horizon year conditions (LOS calculations included in **Appendix O**).

TABLE 23: INTERSECTION #5 SR-76/BENET RD OPERATIONS WITH IMPROVEMENT

Intersection and Scenario	Move-ment	Study Period	Delay ¹	LOS ²	Delay ¹	LOS ²	Delta ³	Impact? ⁴
<i>Near Term + Project</i>			<i>Without Improvement</i>		<i>With Improvement</i>			
5) SR-76 at Benet Rd (S)	All	AM	105.8	F	25.7	C	-80.1	No
	All	PM	85.5	F	43.0	D	-42.5	No
<i>Horizon Year + Project</i>			<i>Without Improvement</i>		<i>With Improvement</i>			
5) SR-76 at Benet Rd (S)	All	AM	202.1	F	69.8	E	-132.3	No
	All	PM	166.0	F	63.9	E	-102.1	No

Notes: (S) Traffic Signal. 1) Delay - HCM Average Control Delay in seconds. 2) LOS: Level of Service. 3) Delta is the increase in delay from project. 4) Impact if project traffic exceeds threshold.

The proposed mitigation at the intersection of SR-76/Benet Rd is a fair share payment of 8.5% towards the provisions of converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction resulting in three through lanes in each direction. The fair share payment shall be paid to the City’s Thoroughfare and Signal Account. The funds will be used at the City’s discretion for projects that will improve traffic safety and mobility in the City of Oceanside. The fair share contribution shall be paid in full prior to issuance of any permit for any phase of any component of the Project. Payment of the fair share shall satisfy the Project’s offsite improvement obligations. A conceptual drawing of the proposed improvement and fair share calculations are included in **Appendix P**.

4.0 Conclusion

This LTS determined if there were measurable transportation impacts based on the City of Oceanside local impact thresholds. Pedestrian, Bicycle, Transit, and Traffic study elements were analyzed based on the City of Oceanside Traffic Guidelines.

The traffic analysis included the analysis of AM peak hour, PM peak hour, and daily traffic volumes. The project is calculated to generate 1,694 daily trips, 161 AM peak hour trips (119 inbound and 41 outbound), and 175 PM peak hour trips (54 inbound and 121 outbound). Six traffic scenarios were analyzed, which included Existing, Existing plus Project, Near Term (18 cumulative projects), Near Term plus Project, Horizon Year 2030, and Horizon Year 2030 plus Project. The City's Traffic Guidelines define how a project's non-CEQA traffic effect/transportation impact on the roadway system is considered to justify the need for roadway improvements that should be considered on a case-by-case basis. The project is calculated to have one (1) transportation impact at the intersection of SR-76/Benet Rd. The proposed mitigation at the intersection of SR-76/Benet Rd is a fair share payment of 8.5% towards the provisions of converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction resulting in three through lanes in each direction. The fair share payment shall be paid to the City's Thoroughfare and Signal Account. The funds will be used at the City's discretion for projects that will improve traffic safety and mobility in the City of Oceanside. The fair share contribution shall be paid in full prior to issuance of any permit for any phase of any component of the Project. Payment of the fair share shall satisfy the Project's offsite improvement obligations. A conceptual drawing of the proposed improvement and fair share calculations are included in Appendix P.

The multi-modal analysis covered pedestrian, bicycle, and transit elements. There are missing sidewalk segments adjacent to the project site; therefore, the project owner/permittee proposes to complete the following sidewalk improvements:

- 1) Construct a sidewalk from the project access on Alex Road north to the San Luis Rey River Trail (a distance of approximately 50 feet).
- 2) Construct a sidewalk along the project frontage on Benet Road from Eddie Jones Way north to the San Luis Rey River access path (a distance of approximately 600 feet).

There are no deficiencies on bike lanes and paths in the study area; therefore, no bike lane/path improvements are necessary. There are no bus stops within a ½ mile walking distance of the project site; therefore, no transit stop improvements are necessary.

###

Appendix A

Excerpts from City of Oceanside Bicycle Master Plan 2017 Update

City of Oceanside Bicycle Master Plan

2017 Update 2008

*League of
American
Bicyclists*

*Bicycle
Friendly
Community*



2017 Bicycle Master Plan Update Prepared by
STC Traffic, Inc.



2008 Bicycle Master Plan Prepared by
KTU+A Planning + Landscape Architecture

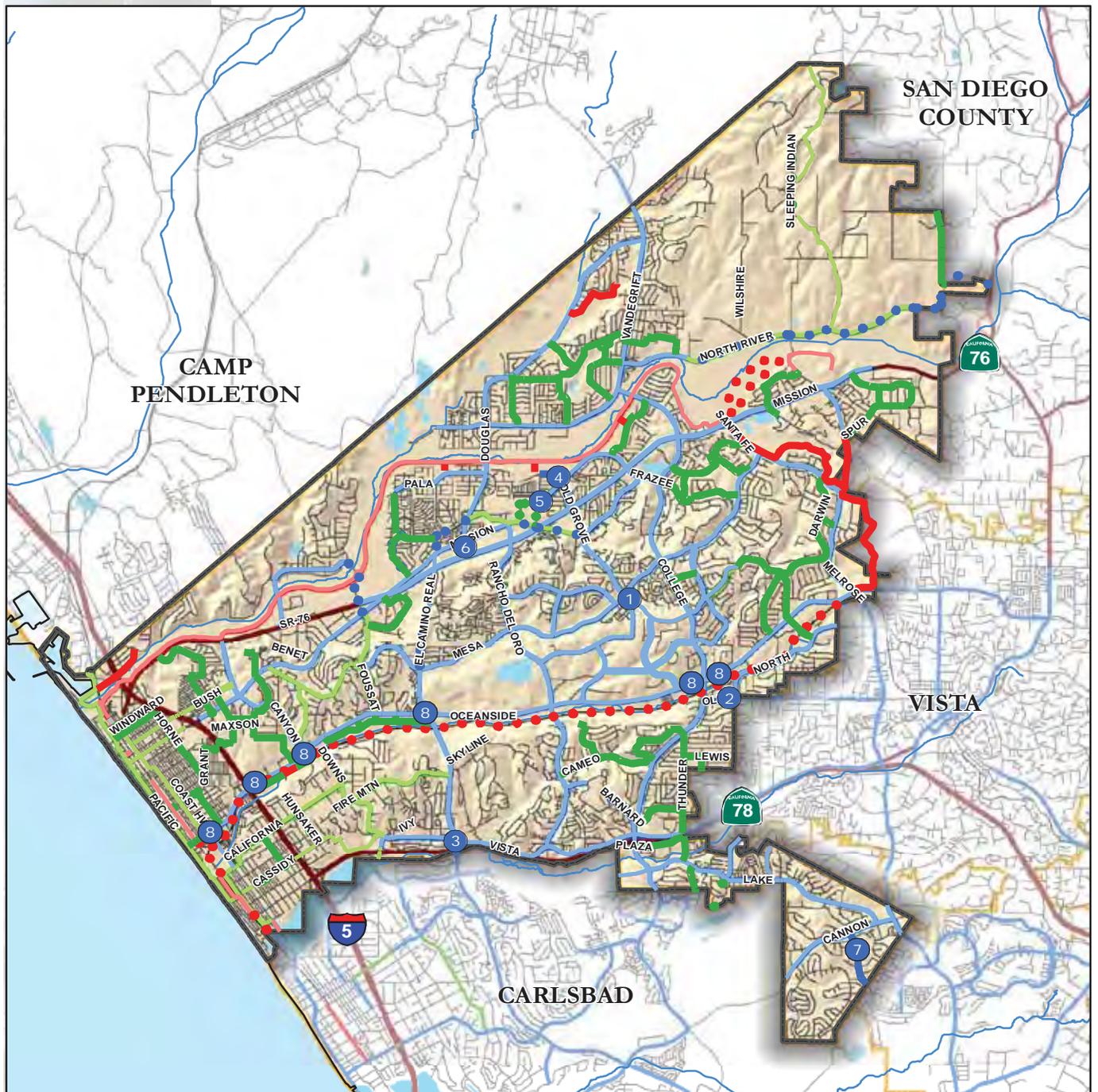


In association with
IBI Group Transportation Planning



for the
City of Oceanside, California
Eddie Jones Industrial Project LTS Appendix





Existing Bicycle Facilities Planned Bicycle Facilities

-  Class 1: Bike Path
 -  Class 2: Bike Lane
 -  Class 3: Bike Route
-  Class 1: Bike Path
 -  Class 2: Bike Lane
 -  Class 3: Bike Route

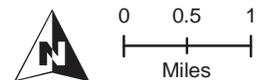
Recommended Facilities

-  Class 1: Bike Path
-  Class 2: Bike Lane
-  Class 3: Bike Route

Figure 4.2 A: Class 2 Bike Lane Recommended Projects

(2017 Update)

Data Source: City of Oceanside



Appendix B

Bus Schedules

EFFECTIVE
July 12, 2020
VÁLIDA
12 de julio, 2020

**NORTH COUNTY
TRANSIT DISTRICT**



Rider's GUIDE

Your complete guide to public transit in North County

**North County Transit District
Guía de Pasajeros**

Su guía completa del transporte público de North County



303

Oceanside to Vista via Town Center North

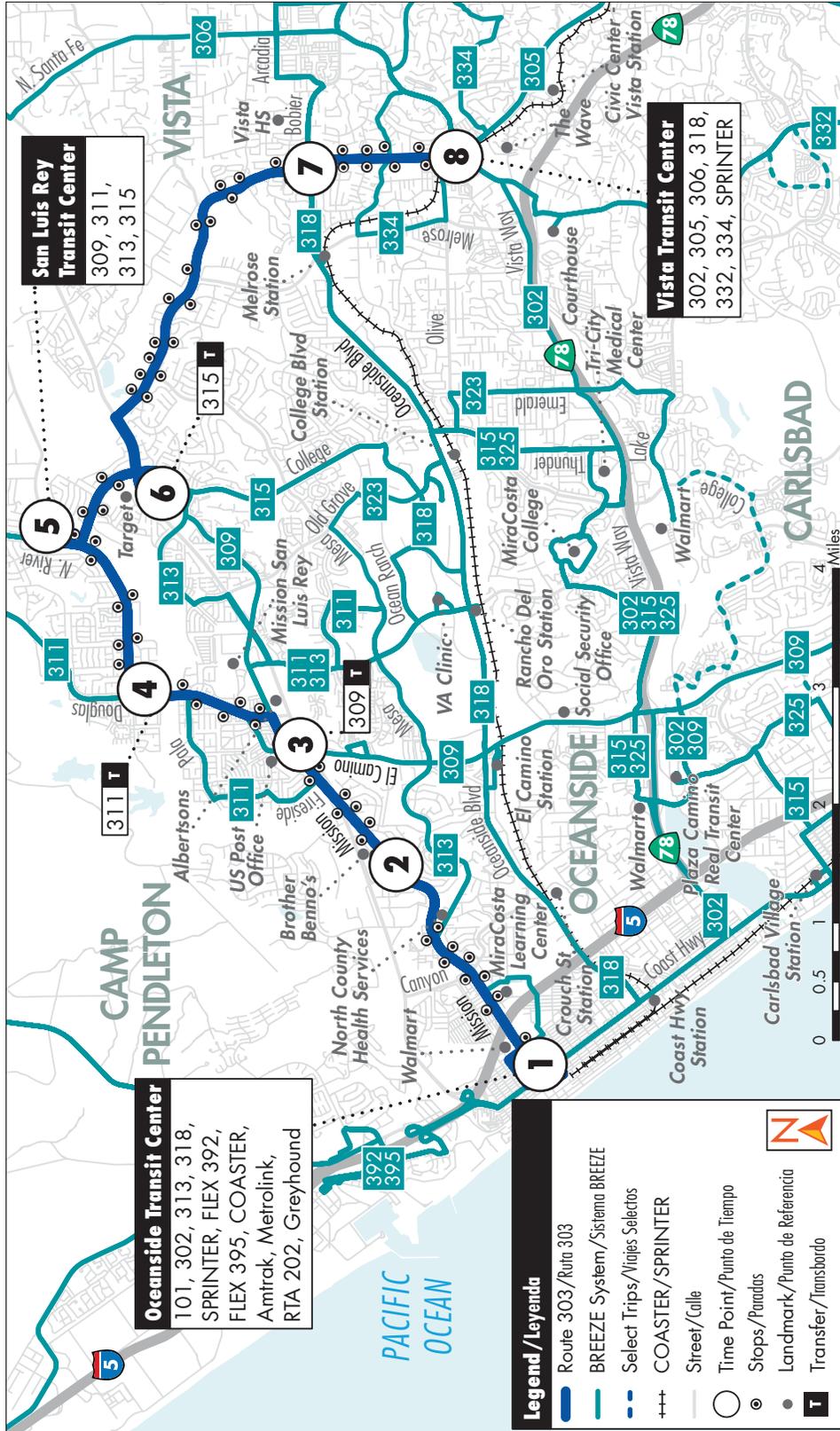
Oceanside a Vista via Town Center North

M-F • SA • SU
L-V • SÁ • DO

Destinations/Destinos

- North County Health Services
- MiraCosta College Learning Center
- Town Center North Shopping Center
- Vista High School

- Antique Gas & Steam Engine Museum
- Jefferson Middle School
- Oceanside High School
- Vista Community Clinic



See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Monday - Friday Eastbound to Vista <i>Lunes a Viernes • Dirección hacia el este a Vista</i>							
Oceanside Transit Center	Mission Ave. & Airport Rd.	Mission Ave. & El Camino Real	Douglas Dr. & N. River Rd.	San Luis Rey Transit Center	Town Center North	N. Santa Fe & Bobier Dr.	Vista Transit Center
1	2	3	4	5	6	7	8
4:05	4:12	4:16	4:21	4:28	4:34	4:43	4:51 ^a
4:35	4:42	4:46	4:51	4:58	5:04	5:13	5:21
5:01	5:09	5:13	5:18	5:26	5:33	5:44	5:52
5:15	5:23	5:27	5:32	5:40	5:47	5:58	6:06
5:31	5:39	5:43	5:48	5:56	6:03	6:14	6:22
5:43	5:52	5:56	6:01	6:09	6:16	6:27	6:36
5:56	6:05	6:09	6:14	6:22	6:31	6:42	6:51
6:09	6:18	6:22	6:28	6:36	6:45	6:57	7:06
6:24	6:33	6:37	6:43	6:51	7:00	7:12	7:21
6:31	6:40	6:46	6:52	7:01	7:10	7:25	7:36
6:46	6:55	7:01	7:07	7:16	7:25	7:40	7:51
7:01	7:12	7:18	7:24	7:33	7:42	7:57	8:08
7:14	7:25	7:31	7:37	7:46	7:55	8:10	8:21
7:34	7:45	7:51	7:57	8:06	8:15	8:28	8:36
7:49	8:00	8:06	8:12	8:21	8:30	8:43	8:51
8:05	8:16	8:22	8:28	8:37	8:46	8:58	9:06
8:20	8:31	8:37	8:43	8:52	9:01	9:13	9:21
8:35	8:46	8:52	8:58	9:07	9:16	9:28	9:36
8:50	9:01	9:07	9:13	9:22	9:31	9:43	9:51
9:05	9:16	9:22	9:28	9:37	9:46	9:58	10:06
9:20	9:31	9:37	9:43	9:52	10:01	10:13	10:21
9:35	9:46	9:52	9:58	10:07	10:16	10:28	10:36
9:50	10:01	10:07	10:13	10:22	10:31	10:43	10:51
10:04	10:15	10:21	10:27	10:36	10:46	10:58	11:06
10:19	10:30	10:36	10:42	10:51	11:01	11:13	11:21
10:34	10:45	10:51	10:57	11:06	11:16	11:28	11:36
10:49	11:00	11:06	11:12	11:21	11:31	11:43	11:51
11:03	11:15	11:21	11:27	11:36	11:46	11:58	12:06p
11:17	11:29	11:35	11:42	11:51	12:01	12:13	12:21
11:31	11:43	11:50	11:57	12:07	12:17	12:29	12:37
11:46	11:58	12:05	12:12	12:22	12:32	12:44	12:52
12:01	12:13	12:20	12:27	12:37	12:47	12:59	1:07
12:16	12:28	12:35	12:42	12:52	1:02	1:14	1:22
12:31	12:43	12:50	12:57	1:07	1:17	1:29	1:37
12:46	12:58	1:05	1:12	1:22	1:32	1:44	1:52
1:01	1:13	1:20	1:27	1:37	1:47	1:59	2:07
1:14	1:26	1:33	1:41	1:51	2:01	2:13	2:22

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Monday - Friday Eastbound to Vista <i>Lunes a Viernes • Dirección hacia el este a Vista</i>							
Oceanside Transit Center	Mission Ave. & Airport Rd.	Mission Ave. & El Camino Real	Douglas Dr. & N. River Rd.	San Luis Rey Transit Center	Town Center North	N. Santa Fe & Bobier Dr.	Vista Transit Center
1	2	3	4	5	6	7	8
1:28	1:40	1:47	1:55	2:05	2:15	2:27	2:36
1:39	1:52	1:59	2:07	2:17	2:27	2:40	2:51
1:51	2:04	2:11	2:19	2:29	2:40	2:55	3:06
*2:02	*2:16	*2:23	*2:32	*2:42	*2:53	*3:08	*3:19
2:05	2:19	2:26	2:35	2:45	2:56	3:11	3:22
2:19	2:33	2:40	2:49	2:59	3:10	3:25	3:36
2:32	2:46	2:53	3:02	3:12	3:24	3:39	3:50
2:34	2:48	2:55	3:04	3:14	3:26	3:41	3:52
2:47	3:01	3:08	3:17	3:27	3:40	3:55	4:06
3:02	3:16	3:23	3:32	3:42	3:55	4:10	4:21
3:17	3:31	3:38	3:47	3:57	4:10	4:25	4:36
3:31	3:46	3:53	4:02	4:12	4:25	4:40	4:51
3:46	4:01	4:08	4:17	4:27	4:40	4:55	5:06
4:04	4:17	4:24	4:33	4:43	4:56	5:10	5:19
4:21	4:34	4:41	4:50	5:00	5:13	5:27	5:36
4:34	4:47	4:54	5:03	5:13	5:26	5:40	5:49
4:51	5:04	5:11	5:20	5:30	5:43	5:57	6:06
5:08	5:21	5:28	5:37	5:47	5:59	6:12	6:21
5:23	5:36	5:43	5:52	6:02	6:14	6:27	6:36
5:41	5:54	6:01	6:10	6:20	6:31	6:42	6:51
6:14	6:27	6:34	6:41	6:51	7:02	7:13	7:21
6:47	6:59	7:06	7:13	7:22	7:32	7:43	7:51
7:20	7:31	7:37	7:44	7:53	8:02	8:13	8:21
7:52	8:03	8:09	8:15	8:24	8:33	8:44	8:51
8:32	8:43	8:49	8:55	9:03	9:10	–	–
9:01	9:11	9:16	9:22	9:29	9:36	9:46	9:52
9:35	9:45	9:50	9:55	10:02	10:09	–	–
10:02	10:12	10:17	10:22	10:29	10:36	10:45	10:51
10:39	10:47	10:50	10:55	11:01	11:07	11:15	11:21
11:10	11:18	11:21	11:26	11:32	11:38	11:46	11:52
11:36	11:44	11:47	11:52	11:58	12:04a	–	–

* Operates Wednesdays only.
Opera solamente los Miércoles.

Trip operates when Oceanside High School is open. Trip operates on school days in regular school year (not during summer school).

Los viajes operan cuando Oceanside High School está abierto. Puede haber un servicio adicional durante este horario para acomodar a una alta demanda de pasajeros.

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Monday - Friday Westbound to Oceanside <i>Lunes a Viernes • Dirección hacia el oeste a Oceanside</i>							
Vista Transit Center	N. Santa Fe & Bobier Dr.	Town Center North	San Luis Rey Transit Center	Douglas Dr. & N. River Rd.	Mission Ave. & El Camino Real	Mission Ave. & Airport Rd.	Oceanside Transit Center
8	7	6	5	4	3	2	1
-	-	4:05	4:13	4:21	4:26	4:33	4:44 ^a
5:06	5:10	5:21	5:29	5:37	5:42	5:49	6:00
5:36	5:40	5:51	5:59	6:07	6:14	6:21	6:32
5:51	5:55	6:06	6:14	6:22	6:29	6:36	6:47
-	-	6:16	6:25	6:33	6:40	6:47	7:00
6:06	6:11	6:23	6:32	6:40	6:47	6:54	7:07
6:21	6:26	6:38	6:47	6:55	7:02	7:09	7:22
-	-	6:38	6:47	6:55	7:02	7:10	7:25
6:36	6:41	6:54	7:03	7:11	7:18	7:26	7:41
6:51	6:56	7:09	7:18	7:26	7:33	7:41	7:56
7:06	7:12	7:29	7:38	7:46	7:53	8:01	8:16
7:18	7:24	7:41	7:50	7:58	8:05	8:13	8:28
7:36	7:42	7:59	8:08	8:16	8:23	8:31	8:46
7:46	7:52	8:09	8:18	8:26	8:33	8:41	8:56
8:06	8:11	8:24	8:32	8:40	8:47	8:55	9:09
8:21	8:26	8:39	8:46	8:54	9:01	9:08	9:22
8:36	8:41	8:54	9:01	9:09	9:15	9:22	9:36
8:51	8:56	9:09	9:16	9:24	9:30	9:37	9:51
9:06	9:11	9:24	9:31	9:39	9:45	9:52	10:06
9:21	9:26	9:39	9:46	9:54	10:00	10:07	10:21
9:36	9:41	9:54	10:01	10:09	10:15	10:22	10:36
9:51	9:56	10:09	10:16	10:24	10:30	10:37	10:51
10:06	10:11	10:24	10:33	10:41	10:47	10:54	11:08
10:23	10:28	10:41	10:50	10:58	11:04	11:11	11:25
10:36	10:41	10:54	11:03	11:11	11:17	11:24	11:38
10:53	10:58	11:11	11:20	11:28	11:34	11:41	11:55
11:06	11:11	11:24	11:33	11:41	11:47	11:54	12:09p
11:22	11:27	11:40	11:49	11:57	12:03	12:10	12:25
11:36	11:41	11:54	12:03	12:11	12:17	12:24	12:39
11:52	11:57	12:10	12:19	12:27	12:33	12:40	12:55
12:06	12:11	12:24	12:33	12:42	12:48	12:55	1:10
12:21	12:26	12:39	12:48	12:57	1:03	1:10	1:25
12:36	12:41	12:54	1:03	1:12	1:18	1:25	1:40
12:51	12:56	1:09	1:18	1:27	1:33	1:40	1:55

Trip operates on school days in regular school year (not during summer school).
 El viaje opera los días escolares durante el año regular de clases (no durante el verano).

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Monday - Friday Westbound to Oceanside <i>Lunes a Viernes • Dirección hacia el oeste a Oceanside</i>							
Vista Transit Center	N. Santa Fe & Bobier Dr.	Town Center North	San Luis Rey Transit Center	Douglas Dr. & N. River Rd.	Mission Ave. & El Camino Real	Mission Ave. & Airport Rd.	Oceanside Transit Center
8	7	6	5	4	3	2	1
1:06	1:11	1:24	1:33	1:42	1:48	1:55	2:10
1:21	1:26	1:39	1:48	1:57	2:03	2:10	2:25
1:36	1:41	1:54	2:03	2:12	2:18	2:25	2:40
1:51	1:56	2:09	2:18	2:27	2:33	2:40	2:55
2:06	2:11	2:25	2:34	2:43	2:50	2:57	3:12
2:17	2:22	2:39	2:49	2:58	3:05	3:12	3:27
2:36	2:43	3:01	3:11	3:20	3:28	3:36	3:51
2:51	2:57	3:15	3:25	3:34	3:41	3:49	4:04
3:06	3:12	3:30	3:40	3:49	3:56	4:04	4:19
3:15	3:21	3:39	3:49	3:58	4:05	4:13	4:28
3:36	3:42	4:00	4:10	4:19	4:26	4:33	4:48
3:46	3:52	4:10	4:20	4:29	4:36	4:43	4:58
4:06	4:12	4:30	4:40	4:49	4:56	5:03	5:18
4:17	4:23	4:41	4:51	5:00	5:07	5:13	5:28
4:36	4:42	5:00	5:10	5:19	5:26	5:32	5:47
4:47	4:53	5:11	5:21	5:30	5:37	5:43	5:58
5:06	5:12	5:30	5:40	5:49	5:55	6:01	6:15
5:19	5:25	5:43	5:53	6:02	6:08	6:14	6:28
5:36	5:42	5:59	6:09	6:18	6:24	6:30	6:43
5:51	5:57	6:14	6:24	6:33	6:39	6:45	6:58
6:06	6:12	6:27	6:35	6:43	6:49	6:55	7:08
6:36	6:42	6:56	7:04	7:12	7:18	7:24	7:37
7:06	7:12	7:26	7:33	7:41	7:46	7:52	8:05
7:36	7:42	7:56	8:03	8:11	8:16	8:22	8:35
8:06	8:12	8:24	8:31	8:39	8:44	8:49	9:00
8:36	8:42	8:54	9:01	9:09	9:14	9:19	9:30
9:06	9:11	9:23	9:30	9:36	9:41	9:46	9:57
10:06	10:10	10:21	10:27	10:33	10:38	10:43	10:53

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Saturday & Sunday Eastbound to Vista <i>Sábado y Domingo • Dirección hacia el este a Vista</i>							
Oceanside Transit Center	Mission Ave. & Airport Rd.	Mission Ave. & El Camino Real	Douglas Dr. & N. River Rd.	San Luis Rey Transit Center	Town Center North	N. Santa Fe & Bobier Dr.	Vista Transit Center
1	2	3	4	5	6	7	8
4:33	4:40	4:44	4:47	4:53	4:59 ^a	–	–
5:38	5:46	5:50	5:55	6:01	6:08	6:18	6:25
6:04	6:13	6:17	6:22	6:28	6:35	6:45	6:52
6:33	6:42	6:46	6:51	6:59	7:06	7:16	7:23
6:57	7:06	7:10	7:16	7:24	7:31	7:42	7:50
7:27	7:37	7:41	7:48	7:56	8:04	8:15	8:23
7:52	8:02	8:06	8:13	8:21	8:29	8:40	8:48
8:12	8:22	8:26	8:33	8:41	8:49	9:00	9:08
8:29	8:40	8:46	8:53	9:01	9:09	9:20	9:28
8:49	9:00	9:06	9:13	9:21	9:29	9:40	9:48
9:08	9:19	9:25	9:32	9:40	9:49	10:00	10:08
9:33	9:44	9:50	9:57	10:05	10:14	10:25	10:33
9:48	9:59	10:05	10:12	10:20	10:29	10:40	10:48
10:01	10:12	10:19	10:27	10:36	10:46	10:57	11:05
10:18	10:29	10:36	10:44	10:53	11:03	11:14	11:22
10:42	10:53	11:01	11:09	11:18	11:28	11:39	11:48
10:58	11:09	11:17	11:25	11:36	11:46	11:57	12:06p
11:13	11:24	11:32	11:40	11:51	12:02	12:13	12:22
11:39	11:50	11:58	12:06	12:17	12:28	12:39	12:48
12:00	12:11	12:19	12:27	12:38	12:49	1:00	1:09
12:13	12:24	12:32	12:40	12:51	1:02	1:13	1:22
12:36	12:48	12:57	1:05	1:16	1:27	1:39	1:48
12:53	1:05	1:14	1:22	1:33	1:44	1:56	2:05
1:10	1:22	1:31	1:39	1:50	2:01	2:13	2:22
1:36	1:48	1:57	2:05	2:16	2:27	2:39	2:48
1:53	2:05	2:14	2:22	2:33	2:44	2:56	3:05
2:11	2:23	2:31	2:39	2:50	3:01	3:13	3:22
2:40	2:52	3:00	3:08	3:19	3:30	3:42	3:51
2:54	3:06	3:14	3:22	3:33	3:44	3:56	4:05
3:11	3:23	3:31	3:39	3:50	4:01	4:13	4:22
3:40	3:52	4:00	4:08	4:19	4:30	4:42	4:51
4:12	4:24	4:32	4:40	4:51	5:01	5:12	5:21
4:43	4:55	5:02	5:10	5:21	5:31	5:42	5:51
5:14	5:26	5:33	5:41	5:52	6:02	6:13	6:21
5:44	5:56	6:03	6:11	6:22	6:32	6:43	6:51
6:15	6:27	6:34	6:41	6:52	7:02	7:13	7:21
6:46	6:58	7:04	7:11	7:22	7:32	7:43	7:51

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Saturday & Sunday Eastbound to Vista <i>Sábado y Domingo • Dirección hacia el este a Vista</i>							
Oceanside Transit Center	Mission Ave. & Airport Rd.	Mission Ave. & El Camino Real	Douglas Dr. & N. River Rd.	San Luis Rey Transit Center	Town Center North	N. Santa Fe & Bobier Dr.	Vista Transit Center
1	2	3	4	5	6	7	8
7:19	7:30	7:35	7:42	7:53	8:02	–	–
7:50	8:01	8:06	8:13	8:24	8:33	8:44	8:51
8:32	8:43	8:48	8:55	9:03	9:11	–	–
8:58	9:08	9:13	9:19	9:26	9:34	9:44	9:51
9:31	9:40	9:45	9:50	9:57	10:04	–	–
10:01	10:10	10:15	10:20	10:27	10:34	10:44	10:51
10:31	10:39	10:43	10:48	10:55	11:01	–	–
11:08	11:15	11:19	11:24	11:31	11:37	–	–
11:31	11:38	11:42	11:47	11:54	12:00 ^a	–	–

See pg. 6 for Holiday schedules/Ver pág. 236 para obtener los horarios de días festivos

Saturday & Sunday Westbound to Oceanside <i>Sábado y Domingo • Dirección hacia el oeste a Oceanside</i>							
Vista Transit Center	N. Santa Fe & Bobier Dr.	Town Center North	San Luis Rey Transit Center	Douglas Dr. & N. River Rd.	Mission Ave. & El Camino Real	Mission Ave. & Airport Rd.	Oceanside Transit Center
8	7	6	5	4	3	2	1
6:06	6:10	6:21	6:29	6:37	6:42	6:49	7:00 _a
6:36	6:40	6:52	7:00	7:08	7:13	7:20	7:31
7:06	7:11	7:23	7:31	7:39	7:44	7:51	8:04
7:36	7:41	7:53	8:01	8:09	8:14	8:21	8:34
8:06	8:11	8:23	8:31	8:39	8:45	8:52	9:05
8:25	8:30	8:42	8:50	8:58	9:04	9:11	9:24
8:45	8:50	9:02	9:10	9:18	9:24	9:31	9:44
9:05	9:10	9:23	9:31	9:39	9:46	9:53	10:07
9:23	9:28	9:41	9:49	9:57	10:04	10:11	10:25
9:45	9:50	10:03	10:11	10:19	10:26	10:33	10:47
10:06	10:11	10:25	10:33	10:42	10:50	10:57	11:11
10:22	10:27	10:41	10:49	10:58	11:06	11:13	11:27
10:38	10:43	10:57	11:05	11:14	11:22	11:29	11:43
11:05	11:10	11:24	11:32	11:41	11:49	11:56	12:10_p
11:19	11:24	11:38	11:46	11:55	12:03	12:10	12:24
11:38	11:43	11:57	12:05	12:14	12:22	12:29	12:43
12:05	12:10	12:25	12:33	12:42	12:50	12:57	1:11
12:19	12:24	12:39	12:47	12:56	1:04	1:11	1:25
12:38	12:43	12:58	1:06	1:15	1:23	1:30	1:44
1:05	1:10	1:25	1:33	1:41	1:49	1:56	2:10
1:20	1:25	1:40	1:48	1:56	2:04	2:11	2:25
1:38	1:43	1:58	2:06	2:14	2:22	2:29	2:43
2:05	2:10	2:25	2:33	2:41	2:49	2:56	3:10
2:21	2:26	2:41	2:49	2:57	3:05	3:12	3:26
2:38	2:43	2:58	3:06	3:14	3:22	3:29	3:43
3:05	3:10	3:25	3:33	3:41	3:49	3:56	4:10
3:21	3:26	3:41	3:49	3:57	4:05	4:12	4:26
3:38	3:43	3:58	4:06	4:14	4:22	4:29	4:43
4:06	4:11	4:26	4:34	4:42	4:50	4:57	5:11
4:36	4:41	4:56	5:04	5:12	5:20	5:27	5:41
5:06	5:12	5:27	5:35	5:43	5:50	5:57	6:11
5:36	5:42	5:57	6:05	6:13	6:20	6:27	6:41
6:06	6:11	6:26	6:34	6:42	6:48	6:55	7:08
6:36	6:41	6:54	7:02	7:10	7:16	7:23	7:36
7:06	7:11	7:24	7:32	7:40	7:46	7:52	8:05
8:06	8:11	8:24	8:32	8:40	8:46	8:52	9:05
9:06	9:11	9:22	9:30	9:37	9:42	9:47	9:59
10:06	10:10	10:21	10:29	10:35	10:40	10:45	10:55

Appendix C

Excerpts from City of Oceanside VMT and LOS Guidelines

City of Oceanside

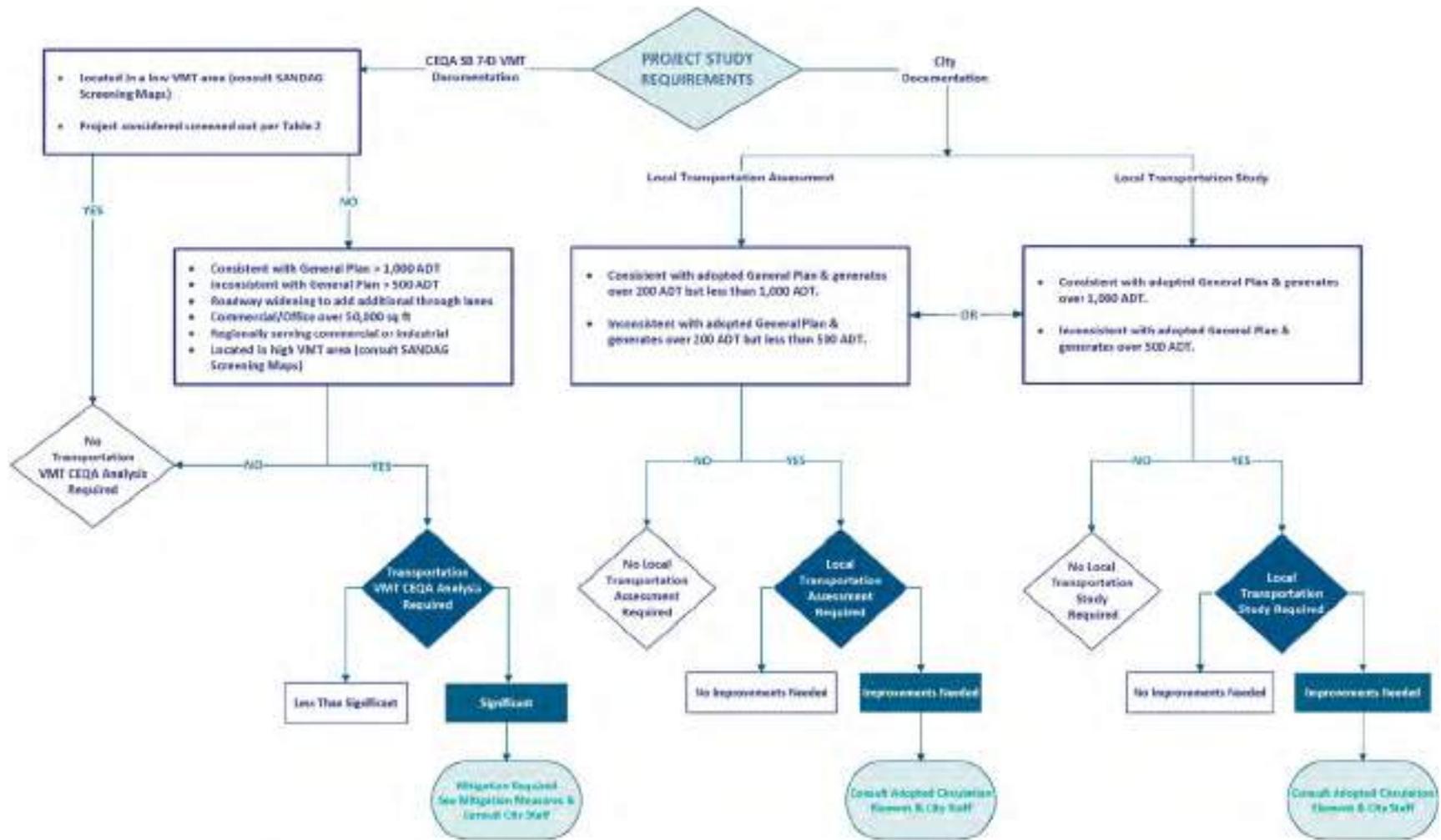
Traffic Impact Analysis Guidelines for

Vehicle Miles Traveled (VMT) and Level of Service Assessment



August 2020
Final Version

Figure 8-1 Project Study Requirements



* Projects are not confined to what is listed above and will need to be coordinated with City Staff to determine study requirements for developments not listed. This flowchart is a generalization, it is up to the City's discretion to determine if additional analyses will be required and if potential mitigation or improvements are acceptable.

9.0 SAN DIEGO REGIONAL GUIDELINES FOR VMT

The City of Oceanside utilizes the Institute of Transportation Engineers (ITE) San Diego Regional Guidelines (May 2019) to establish thresholds and methodology for VMT analysis. For analysis purposes the most recent version of these guidelines shall be utilized. The following sections summarize the VMT thresholds requirements for Oceanside in alignment with ITE. Thorough analysis explanation can be found in the most recent ITE guidance.

Minimum Threshold for VMT Analysis

Based on the recommendations of the Institute of Transportation Engineers (ITE) for the San Diego section, **Table 3** indicates when a VMT analysis for CEQA is required. This is based on keeping consistent with the thresholds previously used and *SANDAG’s Not So Brief Guide Trip Generation (2002)*. These thresholds are based on the understanding that SANDAG trip generation rates differ from ITE trip generation rates which OPR’s recommendations are based on.

Projects Consistent with the Adopted General Plan

The City’s adopted General Plan represents the vision and goals the City has for the community. Projects that support these goals will adhere to the following VMT analysis thresholds identified in Table 3.

Table 3 – Threshold for VMT Analysis for Projects Consistent with the Adopted General Plan

	VMT Analysis Not Needed	VMT Analysis Needed ⁽¹⁾
Average Daily Traffic Volume (ADT)	Less than 1,000 ADT	Greater than 1,000 ADT

(1) If ADT is equal to 1,000 ADT, VMT analysis is required.

Projects Inconsistent with the Adopted General Plan

The City’s adopted General Plan represents the vision and goals the City has for the community. Projects that are not in support of the General Plan have a lower VMT threshold and will require a General Plan Amendment. The following VMT analysis thresholds for projects that are inconsistent are identified in **Table 4**.

Table 4 – Threshold for VMT Analysis for Projects Inconsistent with the Adopted General Plan

	VMT Analysis Not Needed	VMT Analysis Needed ⁽¹⁾
Average Daily Traffic Volume (ADT)	Less than 500 ADT	Greater than 500 ADT

(1) If ADT is equal to 500 ADT, VMT analysis is required.

The thresholds identified in Table 3 and Table 4 stem from the professional expertise and judgement of the ITE San Diego section. These thresholds reflect what is appropriate for the San Diego region to use for VMT and have previously helped determine LOS impacts.

VMT Thresholds

This section identifies what type of VMT analysis is required based on the land use and thresholds identified in the previous section. If a project qualifies for a VMT analysis, the VMT analysis can be compared based on City-wide, Regional, or community basis. The method of comparison shall be agreed upon by the City Traffic Engineer and shall be appropriate based on the use of the site.

The following defines the metrics identified in **Table 5**. It is important the appropriate metrics are applied for each project.

VMT/Capita:

Includes all vehicle-based person trips grouped and summed to the home location of individuals who are drivers or passengers on each trip. It includes home-based and non-home-based trips. The VMT for each home is then summed for all homes in a particular census tract and divided by the population of that census tract to arrive at Resident VMT/Capita.

VMT/Employee:

Includes all vehicle-based person trips grouped and summed to the work location of individuals on the trip. This includes all trips, not just work-related trips. The VMT for each work location is then summed for all work locations in a particular census tract and divided by the number of employees of that census tract to arrive at Employee VMT/Employee.

Small Projects

Small projects, under 2,400 ADT, shall utilize the most recent version of the SANDAG SB 743 Concept Maps. SANDAG has prepared an online mapping system that calculates average VMT/capita and VMT/employee at the census tract level. This tool determines the project's VMT/employee or VMT/capita to be compared to community, city, and/or regional averages. **Appendix C** provides an example of how to use the SANDAG Concept Maps to determine the project's VMT.

Large Projects

Projects consisting of 2,400 ADT or higher will require the use of the most recent SANDAG model to determine VMT. The SANDAG transportation model provides a systematic analytical platform so that different alternatives and inputs can be evaluated in an iterative and controlled environment.

Table 5 identifies the significance thresholds for proposed land uses. Projects that exceed the significance thresholds are considered significant and will require VMT analysis and mitigation.

Table 5 – City of Oceanside Project Threshold

Project Type	Metric	Significance Threshold ⁽¹⁾
Residential	Resident VMT / Capita	15 % below regional average
Commercial	Employee VMT / Employee	15 % below regional average
Industrial	Employee VMT / Employee	15 % below regional average
Retail ⁽²⁾	Net increase in the regional VMT	Net increase in regional VMT
Mixed-Use	Evaluate each land use separately	Based on proposed land use
Redevelopment ⁽³⁾	Based on the proposed land use	Based on the proposed land use

(1) The City may request the applicant to analyze VMT using a more localized threshold if the project requires.

(2) Locally serving retail is presumed to decrease VMT however retail projects over 50,000 square feet are considered regionally serving.

(3) A redevelopment project that reduces VMT is presumed to have less than a significant impact and is screened out. The removal of affordable housing will require VMT analysis.

10.0 MITIGATION MEASURES AND STRATEGIES FOR VMT REDUCTION

A project that exceeds the thresholds identified in the previous tables is considered to have a significant impact and will require mitigation measures and strategies. With appropriate mitigation the project may be able to apply VMT reductions to part or all of the project depending on the land use and strategy chosen. It is critical to implement strategies that are appropriate for the land use, for example, a residential project would not implement a telecommute strategy but may include providing a bike facility and amenities on-site.

SANDAG MOBILITY MANAGEMENT GUIDEBOOK

The purpose of the mitigation measures and strategies is to reduce the VMT generated by the project through a reduction of the distance driven or reducing the number of vehicle trips. It is recommended the SANDAG Mobility Management Guidebook (2019) be consulted to determine mitigation measures for the project site.

The guidebook consists of the following resources:

- Mobility Management Guidebook
- VMT Reduction Calculator Tool
- Calculator Design Document
- Recommendations for Application
- User Training Videos

Figure 10-1 identifies the potential mobility management strategies included in the guidebook that are recommended for a project exceeding the VMT thresholds. It is also recommended the SANDAG iCommute and MTS programs be utilized for projects generating employment. Several opportunities included in these programs are identified in **Table 6. Appendix D** contains the SANDAG Mobility Management Guidebook for reference.

Figure 10-1 Mobility Management Strategies

	Strategy Type	Strategy Name	Included in VMT Calculator?
Project/Site Level	Employer Commute Programs	Comprehensive Employer Commute Program	✓
		Employer Carpool Program	✓
		Employer Transit Pass Subsidy	✓
		Employer Vanpool Program	✓
		Employer Telework Program	✓
		Employer Guaranteed Ride Home Program	
	Land Use Strategies	On-Site Bike Amenities	
		Higher-Density Development	
		Transit-Oriented Development	✓
	Parking Management	Mixed-Use Development	✓
		Parking Pricing	✓
		Parking Cash-Out	✓
		Reduced Parking	
		Unbundled Parking	
		Smart Parking	
Shared Parking			
Shared Mobility Parking			
	Flexible Curb Space		
Community/City Level	Neighborhood Enhancements	Street Connectivity Improvement	✓
		Pedestrian Facility Improvement	✓
		Bikeway Network Expansion	✓
		Bike Facility Improvement	✓
		Bikeshare	✓
		Carshare	✓
		Community-Based Travel Planning	✓
		Transit Service Expansion	✓
	Transit Strategies	Transit Frequency Improvements	✓
		Transit-Supportive Treatments	✓
		Transit Fare Reduction	✓
		Microtransit NEV Shuttle	✓
		Microtransit Commuter Shuttle	
		Adaptive Traffic Signal Systems	
	Transportation System Management	Smart Signals and Intersections	
		Optimized Signal Timing for Bicycles	
		Advanced Bicycle Detection	
		Real-Time Traveler Information	
		Active Traffic Management	
		Traffic Incident Management	
	Roadway Weather Management		

Source: SANDAG Mobility Management Strategy Guidebook, June 2019

11.0 LOCAL TRANSPORTATION STUDY AND LOCAL TRANSPORTATION ASSESSMENT GUIDELINES

The City of Oceanside utilizes the Institute of Transportation Engineers (ITE) San Diego Regional Guidelines (May 2019) to establish thresholds and methodology for a Local Transportation Study (LTS). A Local Transportation Study is different from VMT analysis for CEQA purposes and may be required in addition to the VMT analysis or individually. A Local Transportation Study will analyze the projects influence on the surrounding intersections and roadway network utilizing level of service (LOS) for all project scenarios. The purpose of the LTS is to help quantify the local impact of the development and expected changes in transportation conditions. The LTS should include roadway, bicycle, pedestrian, and transit evaluations. The following sections identify the project requirements for a Local Transportation Study. The Local Transportation Study helps the City ensure the goals, objectives, and policies adopted by the City are supported and implemented while monitoring the capacity for the roadway networks.

Data should be collected during typical operation hours. Data should be recent and no more than 2 years old for an LTS. **The acceptable level of service for the City of Oceanside that is consistent with the adopted Circulation Element is LOS D.**

Minimum Threshold for Local Transportation Study

Based on the recommendations of the Institute of Transportation Engineers (ITE) for the San Diego section, **Table 8** indicates when a Local Transportation Study is required for the City. This is based on keeping consistent with the thresholds previously used and *SANDAG's Not So Brief Guide (2002) Trip Generation*.

Projects Consistent with the Adopted General Plan

The City's adopted General Plan represents the vision and goals the City has for the community. Projects that support these goals will adhere to the following LTS thresholds identified in Table 8.

Table 8 – Threshold for LTS for Projects Consistent with the Adopted General Plan

	LTS Analysis Not Needed	LTS Analysis Needed ⁽¹⁾
Average Daily Traffic Volume (ADT)	Less than 1,000 ADT	Greater than 1,000 ADT

(1) If ADT is equal to 1,000 ADT, an LTS is required.

A Local Transportation Study (LTS) will be required if a project exceeds 1,000 ADT and is consistent with the adopted General Plan.

Projects Inconsistent with the Adopted General Plan

The City's adopted General Plan represents the vision and goals the City has for the community. Projects that are not in support of the General Plan have a lower LTS threshold and will require a General Plan Amendment. The following LTS analysis thresholds for projects that are inconsistent are identified in **Table 9**.

Table 9 – Threshold for LTS for Projects Inconsistent with the Adopted General Plan

	LTS Analysis Not Needed	LTS Analysis Needed ⁽¹⁾
Average Daily Traffic Volume (ADT)	Less than 500 ADT	Greater than 500 ADT

(1) If ADT is equal to 500 ADT, an LTS is required.

A Local Transportation Study (LTS) will be required if a project exceeds 500 ADT and is inconsistent with the adopted General Plan.

The thresholds identified in Table 7 and Table 8 stem from the professional expertise and judgement of the ITE San Diego section. These thresholds keep consistent with regional practice and will help ensure developments will not overburden the transportation network.

If a project would add peak hour trips to any existing on- or off-ramp it is recommended to consult with the City and Caltrans to determine if an LTS would be required.

Study Scenarios

The following scenarios are included in an LTS and may be modified in agreement with the City Traffic Engineer.

- Existing Conditions
- Existing Conditions Plus Project
- Existing Conditions Plus Near-Term Cumulative Projects
- Existing Conditions Plus Near-Term Cumulative Projects Plus Project
- Buildout Conditions (2030)
- Buildout Conditions Plus Project

Local Transportation Assessment (LTA)

A Local Transportation Assessment (LTA) may be required instead of a Local Transportation Study depending on the size of the project. A helps the City monitor development impacts on the transportation network and is similar to a Local Transportation Study(LTS). The main difference between the two studies is a Local Transportation Assessment (LTA) analyzes fewer scenarios than a Local Transportation Study (LTS). A Local Transportation Assessment (LTA) will be required if a project is less than 1,000 ADT but is anticipated to influence the surrounding environment.

A Local Transportation Assessment (LTA) will be required to analyze the following scenarios based on the thresholds for identified for the project’s ADT.

- **A project that generates between 200-500 ADT will be required to analyze existing conditions and existing conditions plus project.**

-
- **A project that generates between 500-1,000 ADT will be required to analyze existing conditions, existing conditions plus project, existing conditions plus near-term cumulative projects, and existing conditions plus near-term cumulative projects plus project.**

Transportation Modes to be Included for Discussion in the LTS/LTA

Pedestrian:

- The LTS/LTA shall include pedestrian infrastructure available including any opportunities or deficiencies such as path obstructions or missing sidewalk for ½ mile walking distance from project pedestrian access points.
- All pedestrian facilities directly connected to project access points or adjacent to the project development, extending in each direction to the nearest intersection with a classified roadway or connection with a Class I path
- Facilities connecting to transit stops within two blocks of the project
- Only facilities on the side of the project or along the walking route to transit stop
- Additional geographic areas may be included in certain cases to address special cases such as schools or retail centers

Bicycle:

- The LTS/LTA shall include a discussion of bicycle infrastructure available including any opportunities or deficiencies such as bike lanes, bike buffers, or bike boxes. This section must also include discussion of what is planned based on City and regional documentation. The extents are as follows:
 - All roadways adjacent to the project, extending in each direction to the nearest intersection with a classified roadway or with a Class I path
 - Both directions of travel should be evaluated

Transit:

- The LTS/LTA shall identify any transit stops or routes existing and planned near the project site. This section shall also include a discussion and evaluation of transit stop amenities within ½ mile of each pedestrian access point.

Vehicle:

All signalized intersections and signalized project driveways shall be analyzed if:

- The project will add 50 or more peak hour (final cumulative) trips in either direction

All unsignalized intersections and unsignalized project driveways shall be analyzed if:

- The project will add 50 or more peak hour (final cumulative) trips in either direction

All freeway ramp intersections and signalized project driveways shall be analyzed if:

- The project will add 20 or more peak hour (final cumulative) trips in either direction

Intersection Level of Service analysis should be conducted using the Highway Capacity Manual (HCM) Methodology. For signalized intersections, the methodology described in the HCM for signalized intersections is used. With this methodology, the average control delay per vehicle is estimated for each lane group and aggregated for each approach and for the intersection as a whole. The relationship between control delay per vehicle and LOS for signalized intersections is summarized in **Table 10**.

Table 10 – HCM Level of Service Description for Signalized Intersections

Level of Service	Description of Traffic Conditions	Control Delay (sec/veh)
A	Insignificant delays: no approach phase is fully utilized and no vehicle waits longer than one red indication	≤ 10
B	Minimal delays: an occasional approach phase is fully utilized. Drivers begin to feel restricted.	> 10 – 20
C	Acceptable delays: major approach phase may become fully utilized. Most drivers feel somewhat restricted.	> 25 – 35
D	Tolerable delays: Drivers may wait through more than one red indication. Queues may develop but dissipate rapidly without excessive delays.	> 35 – 55
E	Significant delays: Volumes approaching capacity. Vehicles may wait through several cycles and long vehicle queues form upstream.	> 55 – 80
F	Excessive delays: Represents conditions at capacity, with extremely long delays. Queues may block upstream intersections.	> 80

Source: Highway Capacity Manual, Transportation Research Board, 2010.

For unsignalized intersections, the methodology described in the HCM for unsignalized intersections is used. With this methodology, LOS is related to the control delay for each stop-controlled movement. The relationship between control delay per vehicle and LOS for unsignalized intersections is summarized in **Table 11**.

Table 11 – HCM Level of Service Description for Unsignalized Intersections

Level of Service	Description of Traffic Conditions	Control Delay (sec/veh)
A	No delay for stop-controlled approaches.	≤ 10
B	Operations with minor delay.	> 10 – 15
C	Operations with moderate delays.	> 15 – 25
D	Operations with some delays.	> 25 – 35
E	Operations with high delays and long queues.	> 35 – 50
F	Operation with extreme congestion, with very high delays and long queues unacceptable to most drivers.	> 50

Source: Highway Capacity Manual, Transportation Research Board, 2010.

Table 12 provides guidance on the levels of ADT that can be accommodated on various types of roadways, based on level of service.

Table 12 – Circulation Element Roadway Classification LOS & Capacity

Class	Lanes	Cross Section ⁽¹⁾	Level of Service (LOS)				
			A	B	C	D	E
Expressway	6	102/160 122/200	30,000	42,000	60,000	70,000	80,000
Expressway	4	102/160 122/200	25,000	35,000	50,000	55,000	60,000
Prime Arterial	6	104/124	25,000	35,000	50,000	55,000	60,000
6-Lane Major Arterial	6	104/124	20,000	28,000	40,000	45,000	50,000
5-Lane Major Arterial ⁽²⁾	5	102/122	17,500	24,500	35,000	40,000	45,000
4-Lane Major Arterial	4	80/100	15,000	21,000	30,000	35,000	40,000
Secondary Collector (4 lanes with 2-way left turn lane)	4	64/84	10,000	14,000	20,000	25,000	30,000
Secondary Collector (4 lanes without 2-way left-turn lane, with left turn pockets)	4	54/74, 60/80	9,000	13,000	18,000	22,000	25,000
Collector (commercial fronting, 2-lanes with 2-way left turn lane) ⁽³⁾	2	50/70	5,000	7,000	10,000	13,000	15,000
Collector (residential streets in the Circulation Element or industrial fronting)	2	40/60, 50/70	4,000	5,500	7,500	9,000	10,000
Local Street (residential streets NOT in the Circulation Element)	1	36/56, 40/60	–	–	2,400	–	–

(1) Cross sections are listed as curd-to-curb width/total right of way width, in feet.

(2) Vandegrift Boulevard is the only Circulation Element roadway designated as a 5-lane Major Arterial. It is not intended that other roadways be build to 5-lane Major Arterial standards.

(3) This capacity will also be assumed for a two-lane one-way collector.

Table 13 indicates when a project's effect on the roadway system is considered to justify the need for roadway improvements. That is, if a project's traffic effect causes the values in this table to be exceeded, roadway improvements should be considered as follows on a case by case basis:

- Improvements should be consistent with the General Plan
- Improvements for transit, bike and pedestrian facilities should be given priority in Transit Priority Areas or Smart Growth Opportunity Areas as identified by SANDAG.
- Projects in Transit Priority Areas or Smart Growth Opportunity Areas as identified by SANDAG, that are consistent with the General Plan at the time of project application, should not be denied due to the inability to provide roadway improvements (i.e. existing right of way is constrained, etc.)

Table 13 – Determination of the Need for Roadway Improvements

Level of Service with Project*	Allowable Change Due to Project Effect**					
	Freeways		Roadway Segments		Intersections	Ramp Metering
	V/C	Speed (MPH)	V/C	Speed (MPH)	Delay (Sec.)	Delay (Min.)
E & F (or ramp meter delays above 15 min)	0.01	1	0.02	1	2	2

12.0 TRANSPORTATION DEMAND MANAGEMENT (TDM) STRATEGIES

In general, the goal of City Staff is to help Oceanside increase connectivity and level of comfort for pedestrians, bicyclists, and transit users. Project improvements may come from the City’s adopted General Plan or other City policies that help improve the overall quality of life for the community. **Table 14** identifies some TDM improvement measures that may be considered for a project.

Table 14 – Potential TDM Improvement Measures

Potential TDM Measures	
Transit Facilities	Telecommuting
Bike Facilities	Rideshare Programs
Walkability	Flex-time
Carpool Incentives	Parking Cash-Out
Subsidized Transit Passes	Shuttle Service

A measure that is not listed may be considered if the mitigation is appropriately applied and reasonable. Additional improvement measures may be identified as future technologies and policies evolve or with consultation by City Staff.

Appendix D

Excerpts from the *Oceanside General Plan Circulation Element*, Sept 2012

OCEANSIDE GENERAL PLAN CIRCULATION ELEMENT UPDATE

City of Oceanside, California

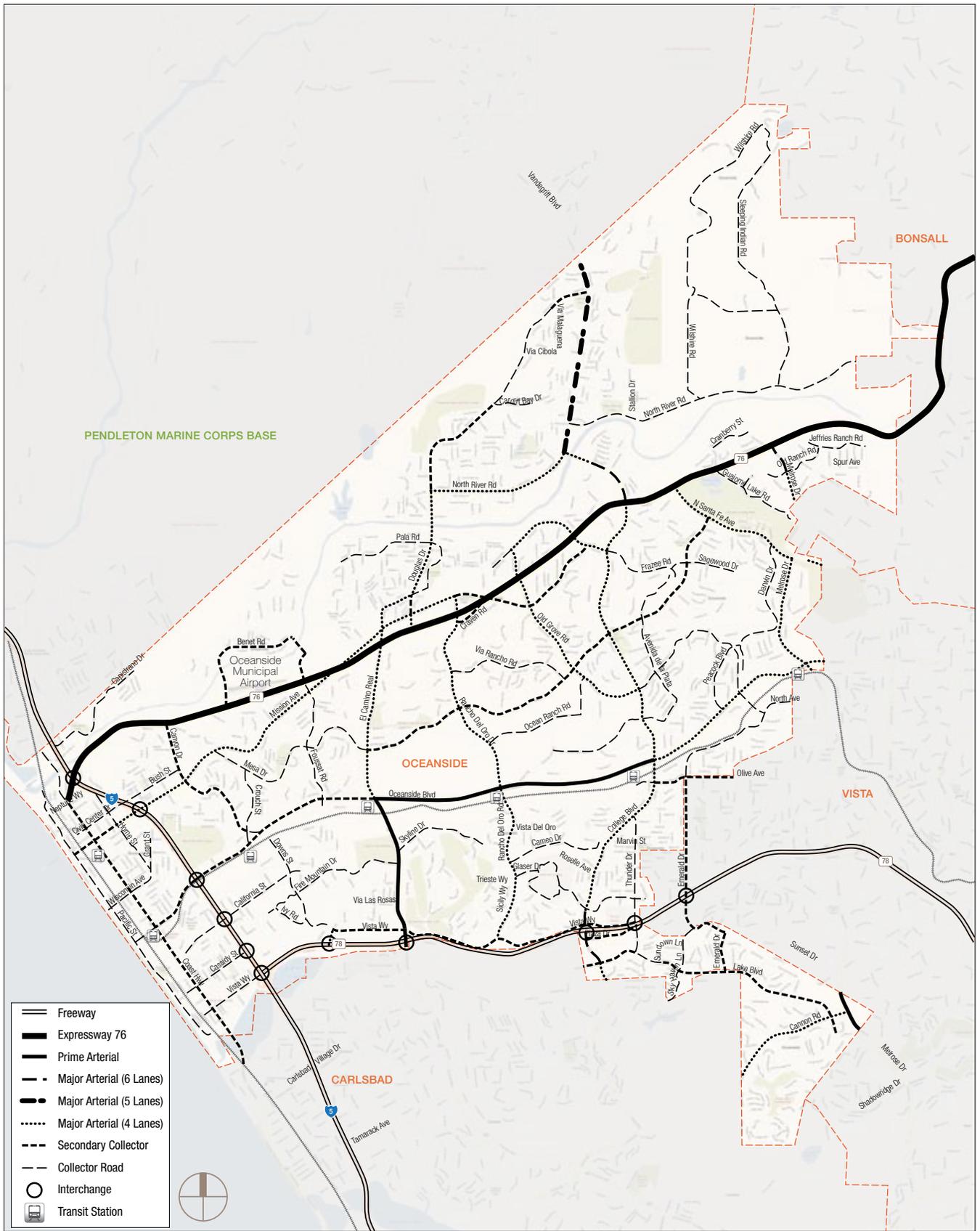
Prepared for
City of Oceanside
Transportation Engineering Division
300 North Coast Highway
Oceanside, CA 92054

Prepared by



701 B Street, Suite 1810
San Diego, CA 92101

September 2012



Not to Scale



Existing Roadway Classifications

Appendix E

Count Data and Signal Timing Sheets



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: SR-76
E/W: I-5 SB Ramps

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 SB Ramps Eastbound			I-5 SB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	61	46	240	87	0	0	0	15	0	0	64	513
7:15 AM	0	62	55	318	95	0	0	0	15	0	0	72	617
7:30 AM	0	66	58	260	113	0	0	0	7	0	0	78	582
7:45 AM	0	72	53	234	126	0	0	0	10	0	0	100	595
8:00 AM	0	80	49	248	111	0	0	0	16	0	0	80	584
8:15 AM	0	74	63	247	127	0	0	0	10	0	0	63	584
8:30 AM	0	76	55	276	119	0	0	0	8	0	0	62	596
8:45 AM	0	63	43	237	118	0	0	0	9	0	0	79	549
TOTAL VOLUMES:	0	554	422	2060	896	0	0	0	90	0	0	598	4620

AM Peak Hr Begins at: 7:15 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	280	215	1060	445	0	0	0	48	0	0	330	2378

PEAK HR FACTOR:	0.959	0.911	0.750	0.825	0.964
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Bicycle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 SB Ramps Eastbound			I-5 SB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	2	0	0	0	0	0	0	0	0	0	0	2

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	0	0	0	0	0	0	0	0	0	0	0	0

Pedestrian Counts

	SR-76 North Leg			SR-76 South Leg			I-5 SB Ramps East Leg			I-5 SB Ramps West Leg			TOTAL
7:00 AM	0			0			0			0			0
7:15 AM	0			0			0			0			0
7:30 AM	0			0			0			0			0
7:45 AM	0			0			0			0			0
8:00 AM	0			0			0			0			0
8:15 AM	0			0			0			0			0
8:30 AM	0			0			0			0			0
8:45 AM	0			0			0			0			0
TOTAL VOLUMES:	0			0			0			0			0

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	0	0	0	0	0



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: SR-76
 E/W: I-5 SB Ramps

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 SB Ramps Eastbound			I-5 SB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	117	64	168	132	0	0	0	26	0	0	161	668
4:15 PM	0	115	52	148	155	0	0	0	18	0	0	159	647
4:30 PM	0	119	61	178	169	0	0	0	16	0	0	149	692
4:45 PM	0	128	63	168	155	0	0	0	26	0	0	189	729
5:00 PM	0	121	71	165	196	0	0	0	24	0	0	176	753
5:15 PM	0	92	46	164	157	0	0	0	18	0	0	172	649
5:30 PM	0	117	50	178	137	0	0	0	24	0	0	150	656
5:45 PM	0	100	54	125	152	0	0	0	19	0	0	144	594
TOTAL VOLUMES:	0	909	461	1294	1253	0	0	0	171	0	0	1300	5388

PM Peak Hr Begins at: 430 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	460	241	675	677	0	0	0	84	0	0	686	2823

PEAK HR FACTOR:	0.913			0.936			0.808			0.907			0.937
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Bicycle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 SB Ramps Eastbound			I-5 SB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0	0	0	0	0	0	0	0	0

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	0	0	0	0	0	0	0	0	0	0	0

Pedestrian Counts

	SR-76 North Leg	SR-76 South Leg	I-5 SB Ramps East Leg	I-5 SB Ramps West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	0	0
4:30 PM	0	0	0	0	0
4:45 PM	0	0	0	0	0
5:00 PM	0	0	0	0	0
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	0	0
5:45 PM	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	0	0	0	0	0



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: SR-76
E/W: I-5 NB Ramps

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 NB Ramps Eastbound			I-5 NB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	110	25	230	316	0	0	0	0	12	0	152	845
7:15 AM	0	124	24	208	389	0	0	0	0	12	0	147	904
7:30 AM	0	131	24	263	359	0	0	0	0	20	0	148	945
7:45 AM	0	152	16	222	324	0	0	0	0	24	0	149	887
8:00 AM	0	133	25	188	309	0	0	0	0	25	0	171	851
8:15 AM	0	118	27	166	359	0	0	0	0	29	0	194	893
8:30 AM	0	122	21	187	349	0	0	0	0	35	0	164	878
8:45 AM	0	128	14	168	275	0	0	0	0	20	0	134	739
TOTAL VOLUMES:	0	1018	176	1632	2680	0	0	0	0	177	0	1259	6942

AM Peak Hr Begins at: 715 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	540	89	881	1381	0	0	0	0	81	0	615	3587

PEAK HR FACTOR:	0.936	0.909	0.000	0.888	0.949
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Bicycle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 NB Ramps Eastbound			I-5 NB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	2	0	0	0	0	0	0	0	0	0	0	2

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	0	0	0	0	0	0	0	0	0	0	0	0

Pedestrian Counts

	SR-76 North Leg	SR-76 South Leg	I-5 NB Ramps East Leg	I-5 NB Ramps West Leg	TOTAL
7:00 AM	0	0	0	0	0
7:15 AM	0	0	0	0	0
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	0	0	0	0
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	0	0	0	0	0



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: SR-76
 E/W: I-5 NB Ramps

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:15 PM to 5:15 PM

Vehicle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 NB Ramps Eastbound			I-5 NB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	239	36	87	237	0	0	0	0	63	0	178	840
4:15 PM	0	248	32	88	271	0	0	0	0	49	0	196	884
4:30 PM	0	258	22	96	253	0	0	0	0	64	0	208	901
4:45 PM	0	283	23	102	254	0	0	0	0	52	0	213	927
5:00 PM	0	265	24	109	275	0	0	0	0	70	0	211	954
5:15 PM	0	242	20	100	234	0	0	0	0	67	0	208	871
5:30 PM	0	234	24	98	215	0	0	0	0	55	0	224	850
5:45 PM	0	226	27	87	206	0	0	0	0	59	0	199	804
TOTAL VOLUMES:	0	1995	208	767	1945	0	0	0	0	479	0	1637	7031

PM Peak Hr Begins at: 4:15 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	1054	101	395	1053	0	0	0	0	235	0	828	3666

PEAK HR FACTOR:	0.944			0.943			0.000			0.946			0.961
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Bicycle Counts

	SR-76 Northbound			SR-76 Southbound			I-5 NB Ramps Eastbound			I-5 NB Ramps Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0	0	0	0	0	0	0	0	0

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	0	0	0	0	0	0	0	0	0	0	0

Pedestrian Counts

	SR-76 North Leg	SR-76 South Leg	I-5 NB Ramps East Leg	I-5 NB Ramps West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	0	0
4:30 PM	0	0	0	0	0
4:45 PM	0	0	0	0	0
5:00 PM	0	0	0	0	0
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	0	0
5:45 PM	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	0	0	0	0	0



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Loretta Street
E/W: SR-76

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	Loretta Street Northbound			Loretta Street Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	3	0	4	0	0	0	0	267	4	1	542	0	821
7:15 AM	13	0	8	0	0	0	0	256	2	5	589	0	873
7:30 AM	15	0	8	0	0	0	0	247	5	2	596	0	873
7:45 AM	7	0	7	0	0	0	0	306	7	3	538	0	868
8:00 AM	8	0	3	0	0	0	0	301	9	2	517	0	840
8:15 AM	9	0	9	0	0	0	0	289	5	2	512	0	826
8:30 AM	6	0	4	0	0	0	0	278	7	1	525	0	821
8:45 AM	9	0	2	0	0	0	0	263	2	1	440	0	717
TOTAL VOLUMES:	70	0	45	0	0	0	0	2207	41	17	4259	0	6639

AM Peak Hr Begins at: 7:15 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	43	0	26	0	0	0	0	1110	23	12	2240	0	3454

PEAK HR FACTOR:	0.750	0.000	0.905	0.941	0.989
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Bicycle Counts

	Loretta Street Northbound			Loretta Street Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	1	0	0	0	0	0	0	0	0	0	0	0	1
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:45 AM	1	0	0	0	0	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	0	0	0	0	0	1	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	1	1	0	0	0	2
TOTAL VOLUMES:	2	0	0	0	0	0	0	2	1	0	0	0	5

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	1	0	0	0	0	0	0	0	0	0	0	0	1

Pedestrian Counts

	Loretta Street North Leg	Loretta Street South Leg	SR-76 East Leg	SR-76 West Leg	TOTAL
7:00 AM	0	0	0	0	0
7:15 AM	0	0	0	0	0
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	0	1	0	1
8:45 AM	0	0	0	1	1
TOTAL VOLUMES:	0	0	1	1	2

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	0	0	0	0	0



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Loretta Street
 E/W: SR-76

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:15 PM to 5:15 PM

Vehicle Counts

	Loretta Street Northbound			Loretta Street Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	5	0	1	0	0	0	0	432	14	6	308	0	766
4:15 PM	5	0	1	0	0	0	0	440	19	6	343	0	814
4:30 PM	3	0	1	0	0	0	0	465	20	6	350	0	845
4:45 PM	12	0	5	0	0	0	0	477	21	5	331	0	851
5:00 PM	5	0	5	0	0	0	0	494	13	3	385	0	905
5:15 PM	5	0	4	0	0	0	0	461	14	1	324	0	809
5:30 PM	6	0	2	0	0	0	0	452	19	7	317	0	803
5:45 PM	5	0	5	0	0	0	0	453	10	5	293	0	771
TOTAL VOLUMES:	46	0	24	0	0	0	0	3674	130	39	2651	0	6564

PM Peak Hr Begins at: 4:15 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	25	0	12	0	0	0	0	1876	73	20	1409	0	3415

PEAK HR FACTOR:	0.544			0.000			0.961			0.921			0.943
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Bicycle Counts

	Loretta Street Northbound			Loretta Street Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0	0	0	0	0	0	0	0	0

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	0	0	0	0	0	0	0	0	0	0	0

Pedestrian Counts

	Loretta Street North Leg		Loretta Street South Leg		SR-76 East Leg		SR-76 West Leg		TOTAL
4:00 PM	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	0	0	0	0	0	0	0
4:30 PM	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0
5:00 PM	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0	0	0	0	0

PEAK VOLUMES:	North Leg		South Leg		East Leg		West Leg		TOTAL
	0	0	0	0	0	0	0	0	0



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Canyon Drive
 E/W: SR-76

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
 Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	Canyon Drive Northbound			Canyon Drive Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	5	0	21	0	0	0	0	263	0	20	516	0	825
7:15 AM	6	0	18	0	0	0	0	267	1	10	602	0	904
7:30 AM	3	0	19	0	0	0	0	264	5	24	590	0	905
7:45 AM	5	0	21	0	0	0	0	295	6	20	525	0	872
8:00 AM	6	0	17	0	0	0	0	312	2	20	532	0	889
8:15 AM	1	0	13	0	0	0	0	306	2	16	493	0	831
8:30 AM	5	0	27	0	0	0	0	262	5	20	498	0	817
8:45 AM	4	0	22	0	0	0	0	259	6	21	437	0	749
TOTAL VOLUMES:	35	0	158	0	0	0	0	2228	27	151	4193	0	6792

AM Peak Hr Begins at: 715 AM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	20	0	75	0	0	0	0	1138	14	74	2249	0	3570

PEAK HR FACTOR:													
	0.913		0.000			0.917		0.946		0.986			

Bicycle Counts

	Canyon Drive Northbound			Canyon Drive Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	1	0	0	0	0	0	0	0	0	0	1
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	1	0	0	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	0	1	0	0	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	1	0	0	0	0	1
TOTAL VOLUMES:	0	0	4	0	0	0	0	1	0	0	0	0	5

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	2	0	0	0	0	0	0	0	0	0	2

Pedestrian Counts

	Canyon Drive North Leg	Canyon Drive South Leg	SR-76 East Leg	SR-76 West Leg	TOTAL
7:00 AM	0	0	0	0	0
7:15 AM	0	0	0	0	0
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	1	1	0	2
8:15 AM	0	0	0	0	0
8:30 AM	0	0	1	0	1
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	0	1	2	0	3

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	0	1	1	0	2



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Canyon Drive
 E/W: SR-76

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Canyon Drive Northbound			Canyon Drive Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	3	0	22	0	0	0	0	404	35	34	297	0	795
4:15 PM	1	0	29	0	0	0	0	447	8	24	334	0	843
4:30 PM	1	0	25	0	0	0	0	437	10	21	372	0	866
4:45 PM	2	0	21	0	0	0	0	449	14	49	336	0	871
5:00 PM	1	0	22	0	0	0	0	494	16	32	371	0	936
5:15 PM	3	0	23	0	0	0	0	446	8	34	337	0	851
5:30 PM	4	0	24	0	0	0	0	449	9	34	321	0	841
5:45 PM	2	0	26	0	0	0	0	433	7	18	304	0	790
TOTAL VOLUMES:	17	0	192	0	0	0	0	3559	107	246	2672	0	6793

PM Peak Hr Begins at: 4:30 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	7	0	91	0	0	0	0	1826	48	136	1416	0	3524

PEAK HR FACTOR:	0.942			0.000			0.919			0.963			0.941
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Bicycle Counts

	Canyon Drive Northbound			Canyon Drive Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:15 PM	0	0	1	0	0	0	0	0	0	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	2	1	0	0	0	0	0	0	0	0	0	3

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	2	0	0	0	0	0	0	0	0	0	0	2

Pedestrian Counts

	Canyon Drive North Leg	Canyon Drive South Leg	SR-76 East Leg	SR-76 West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	0	0
4:30 PM	0	0	0	0	0
4:45 PM	0	2	0	0	2
5:00 PM	0	0	0	0	0
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	0	0
5:45 PM	0	0	0	0	0
TOTAL VOLUMES:	0	2	0	0	2

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	0	2	0	0	2



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Benet Road
E/W: SR-76

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	16	0	11	1	3	59	18	228	23	5	430	2	796
7:15 AM	14	1	19	5	1	61	21	252	30	6	540	1	951
7:30 AM	12	0	10	1	1	73	16	260	23	8	547	1	952
7:45 AM	15	1	10	2	8	73	26	250	37	7	445	3	877
8:00 AM	9	2	7	0	2	41	35	261	39	9	490	2	897
8:15 AM	19	3	5	3	3	49	22	282	28	10	436	4	864
8:30 AM	14	0	7	1	0	34	26	224	35	14	457	4	816
8:45 AM	12	4	5	5	3	33	21	245	26	19	424	1	798
TOTAL VOLUMES:	111	11	74	18	21	423	185	2002	241	78	3769	18	6951

AM Peak Hr Begins at: 715 AM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	50	4	46	8	12	248	98	1023	129	30	2022	7	3677

PEAK HR FACTOR:	0.735			0.807			0.933			0.926			0.966
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	0	0	0	0	0	1	0	0	0	0	1
7:15 AM	0	0	0	0	0	0	0	0	1	0	0	0	1
7:30 AM	0	0	0	0	0	0	0	0	1	0	0	0	1
7:45 AM	0	0	0	0	0	0	0	0	1	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:15 AM	0	0	0	0	3	0	0	0	0	0	0	0	3
8:30 AM	0	1	0	0	0	0	0	1	0	0	0	0	2
8:45 AM	0	2	0	0	0	0	0	1	0	0	0	0	3
TOTAL VOLUMES:	0	3	0	0	3	0	0	3	3	0	0	0	12

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	0	0	0	0	0	0	3	0	0	0	3

Pedestrian Counts

	Benet Road North Leg		Benet Road South Leg		SR-76 East Leg		SR-76 West Leg		TOTAL
7:00 AM	0		0		1		0		1
7:15 AM	0		0		0		0		0
7:30 AM	0		0		1		0		1
7:45 AM	0		0		0		0		0
8:00 AM	0		0		0		0		0
8:15 AM	0		0		0		0		0
8:30 AM	0		0		1		0		1
8:45 AM	0		0		0		0		0
TOTAL VOLUMES:	0		0		3		0		3

PEAK VOLUMES:	North Leg		South Leg		East Leg		West Leg		TOTAL
	0		0		1		0		1



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Benet Road
 E/W: SR-76

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	24	1	13	5	2	24	36	378	28	7	289	6	813
4:15 PM	30	4	13	12	9	26	47	412	26	11	292	2	884
4:30 PM	44	9	17	7	2	23	46	402	30	14	328	6	928
4:45 PM	23	7	21	3	3	22	35	431	20	8	354	4	931
5:00 PM	33	3	28	1	0	29	49	465	14	5	330	1	958
5:15 PM	29	2	25	2	4	43	50	413	24	8	295	1	896
5:30 PM	22	3	16	3	0	19	52	408	21	3	295	0	842
5:45 PM	23	2	20	3	2	26	46	384	18	6	273	4	807
TOTAL VOLUMES:	228	31	153	36	22	212	361	3293	181	62	2456	24	7059

PM Peak Hr Begins at: 430 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	129	21	91	13	9	117	180	1711	88	35	1307	12	3713

PEAK HR FACTOR:	0.861			0.709			0.937			0.925			0.969
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	2
4:15 PM	0	0	0	0	1	0	0	1	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	1	0	0	3	0	0	0	0	0	0	0	4
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	1	0	0	0	0	0	0	0	1
5:30 PM	0	1	0	0	2	0	0	0	0	0	0	0	3
5:45 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	4	0	0	8	0	0	1	0	0	0	0	13

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	1	0	0	4	0	0	0	0	0	0	0	5

Pedestrian Counts

	Benet Road North Leg	Benet Road South Leg	SR-76 East Leg	SR-76 West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	2	0	2	0	4
4:30 PM	0	0	1	0	1
4:45 PM	1	0	3	0	4
5:00 PM	0	0	2	0	2
5:15 PM	0	0	1	0	1
5:30 PM	0	0	2	0	2
5:45 PM	1	0	5	0	6
TOTAL VOLUMES:	4	0	16	0	20

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	1	0	7	0	8



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Foussat Road
E/W: SR-76

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	Foussat Road Northbound			Foussat Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	35	7	19	17	21	9	15	214	10	25	462	2	836
7:15 AM	16	5	25	30	17	17	10	251	10	46	490	8	925
7:30 AM	27	13	22	53	19	6	5	246	14	34	510	18	967
7:45 AM	44	16	22	41	26	8	5	231	14	31	442	11	891
8:00 AM	29	20	15	24	23	12	13	243	15	32	433	34	893
8:15 AM	30	19	30	16	31	9	7	213	19	45	482	14	915
8:30 AM	25	26	20	21	24	21	12	199	23	30	392	23	816
8:45 AM	25	9	13	17	21	12	15	211	15	38	427	14	817
TOTAL VOLUMES:	231	115	166	219	182	94	82	1808	120	281	3638	124	7060

AM Peak Hr Begins at: 715 AM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	116	54	84	148	85	43	33	971	53	143	1875	71	3676

PEAK HR FACTOR:													
	0.774			0.885			0.975			0.929			0.950

Bicycle Counts

	Foussat Road Northbound			Foussat Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	1	1	0	2	0	0	0	0	0	0	0	4
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	1	0	0	0	0	0	0	0	1
7:45 AM	0	1	0	0	2	0	0	0	0	0	0	0	3
8:00 AM	0	1	1	0	2	0	0	0	0	1	0	0	5
8:15 AM	0	0	0	0	0	0	0	0	0	1	0	0	1
8:30 AM	0	1	1	0	0	0	0	0	0	0	0	0	2
8:45 AM	0	0	0	0	0	0	0	1	0	0	0	0	1
TOTAL VOLUMES:	0	4	3	0	7	0	0	1	0	2	0	0	17

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	2	1	0	5	0	0	0	0	1	0	0	9

Pedestrian Counts

	Foussat Road North Leg	Foussat Road South Leg	SR-76 East Leg	SR-76 West Leg	TOTAL
7:00 AM	0	0	1	0	1
7:15 AM	0	0	0	0	0
7:30 AM	1	0	0	0	1
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	1	0	1
8:30 AM	0	0	0	0	0
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	1	0	2	0	3

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	1	0	0	0	1



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Foussat Road
 E/W: SR-76

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Foussat Road Northbound			Foussat Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	14	29	27	18	13	4	1	388	40	17	268	24	843
4:15 PM	16	35	30	19	17	8	9	412	41	32	321	22	962
4:30 PM	24	30	35	18	22	10	7	368	22	22	306	17	881
4:45 PM	16	31	35	11	19	5	5	455	36	22	371	24	1030
5:00 PM	24	31	37	14	12	7	5	419	37	24	268	23	901
5:15 PM	14	32	29	13	17	4	10	481	32	30	291	38	991
5:30 PM	15	37	41	13	20	4	5	360	28	26	309	17	875
5:45 PM	16	33	29	19	18	6	3	397	37	17	264	17	856
TOTAL VOLUMES:	139	258	263	125	138	48	45	3280	273	190	2398	182	7339

PM Peak Hr Begins at: 430 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	78	124	136	56	70	26	27	1723	127	98	1236	102	3803

PEAK HR FACTOR:	0.918			0.760			0.897			0.861			0.923
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Bicycle Counts

	Foussat Road Northbound			Foussat Road Southbound			SR-76 Eastbound			SR-76 Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:30 PM	0	3	0	0	0	0	0	0	0	0	0	0	3
4:45 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
5:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	2	0	0	0	0	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	8	0	0	3	0	0	0	0	0	0	0	11

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	6	0	0	1	0	0	0	0	0	0	0	7

Pedestrian Counts

	Foussat Road North Leg		Foussat Road South Leg		SR-76 East Leg		SR-76 West Leg		TOTAL
	North	South	North	South	East	West	East	West	
4:00 PM	0	0	0	0	1	0	0	0	1
4:15 PM	0	0	0	0	1	0	0	0	1
4:30 PM	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	1	0	0	0	1
5:00 PM	0	0	0	0	0	0	0	0	0
5:15 PM	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	0	0	0	0	0
5:45 PM	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	3	0	0	0	3

PEAK VOLUMES:	North Leg		South Leg		East Leg		West Leg		TOTAL
	0	0	0	0	1	0	0	0	1



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Benet Road
E/W: Airport Road

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:00 AM to 8:00 AM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			Airport Road Eastbound			Airport Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	14	7	0	57	0	0	0	0	4	0	4	86
7:15 AM	0	19	6	1	65	0	0	0	0	6	0	5	102
7:30 AM	0	13	2	2	73	0	0	0	0	8	0	3	101
7:45 AM	0	31	6	2	74	0	0	0	0	8	0	4	125
8:00 AM	0	28	5	1	39	0	0	0	0	4	0	3	80
8:15 AM	0	24	7	1	48	0	0	0	0	5	0	5	90
8:30 AM	0	19	7	2	32	0	0	0	0	2	0	4	66
8:45 AM	0	22	6	0	42	0	0	0	0	1	0	0	71
TOTAL VOLUMES:	0	170	46	9	430	0	0	0	0	38	0	28	721

AM Peak Hr Begins at: 700 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	77	21	5	269	0	0	0	0	26	0	16	414

PEAK HR FACTOR:	0.662	0.901	0.000	0.875	0.828
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			Airport Road Eastbound			Airport Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	2	0	0	0	0	0	0	0	0	0	2
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	0	0	0	0	0	0	0	0	0	0	1	1
7:45 AM	0	0	0	1	0	0	0	0	0	0	0	0	1
8:00 AM	0	0	0	0	0	0	0	0	0	0	0	1	1
8:15 AM	0	0	0	0	3	0	0	0	0	0	0	0	3
8:30 AM	0	0	0	0	1	0	0	0	0	0	0	2	3
8:45 AM	0	2	0	0	0	0	0	0	0	0	0	0	2
TOTAL VOLUMES:	0	2	2	1	4	0	0	0	0	0	0	4	13

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	0	2	1	0	0	0	0	0	0	0	1	4

Pedestrian Counts

	Benet Road North Leg	Benet Road South Leg	Airport Road East Leg	Airport Road West Leg	TOTAL
7:00 AM	0	1	0	0	1
7:15 AM	1	0	0	0	1
7:30 AM	0	0	1	0	1
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	0	1	0	1
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	1	1	2	0	4

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	1	1	1	0	3



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Benet Road
 E/W: Airport Road

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			Airport Road Eastbound			Airport Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	41	3	2	30	0	0	0	0	5	0	2	83
4:15 PM	0	49	1	2	35	0	0	0	0	8	0	2	97
4:30 PM	0	51	4	1	27	0	0	0	0	3	0	4	90
4:45 PM	0	46	3	1	22	0	0	0	0	5	0	1	79
5:00 PM	0	44	5	2	23	0	0	0	0	5	0	2	81
5:15 PM	0	52	2	1	42	0	0	0	0	4	0	4	105
5:30 PM	0	52	5	1	18	0	0	0	0	4	0	1	81
5:45 PM	0	45	6	3	22	0	0	0	0	6	1	4	87
TOTAL VOLUMES:	0	380	29	13	219	0	0	0	0	40	1	20	703

PM Peak Hr Begins at: 4:30 PM

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	193	14	5	114	0	0	0	0	17	0	11	354

PEAK HR FACTOR:	0.945			0.692			0.000			0.875			0.845
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			Airport Road Eastbound			Airport Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
4:15 PM	0	0	0	0	0	0	0	0	0	0	0	2	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	1	0	3	0	0	0	0	0	0	0	4
5:00 PM	0	0	0	0	0	0	0	0	0	0	0	1	1
5:15 PM	0	0	0	1	1	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	2	1	0	0	0	0	0	0	0	3
5:45 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	2	1	3	5	0	0	0	0	0	0	3	14

PEAK VOLUMES:	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
	0	0	1	1	4	0	0	0	0	0	0	1	7

Pedestrian Counts

	Benet Road North Leg	Benet Road South Leg	Airport Road East Leg	Airport Road West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	0	0
4:30 PM	0	0	1	0	1
4:45 PM	0	0	1	0	1
5:00 PM	0	0	1	0	1
5:15 PM	1	0	1	0	2
5:30 PM	0	2	2	0	4
5:45 PM	0	1	2	0	3
TOTAL VOLUMES:	1	3	8	0	12

PEAK VOLUMES:	North Leg	South Leg	East Leg	West Leg	TOTAL
	1	0	4	0	5



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Benet Road
 E/W: Eddy Jones

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
 Peak Hour: 7:15 AM to 8:15 AM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			Eddy Jones Eastbound			Eddy Jones Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	18	0	0	56	0	0	0	0	0	0	0	74
7:15 AM	0	22	0	1	63	0	0	0	0	0	0	0	86
7:30 AM	0	16	0	0	81	0	0	0	0	0	0	0	97
7:45 AM	0	23	1	0	62	0	0	0	0	1	0	1	88
8:00 AM	0	43	0	0	47	0	0	0	0	0	0	0	90
8:15 AM	0	29	0	0	43	0	0	0	0	0	0	0	72
8:30 AM	0	22	0	0	38	0	0	0	0	0	0	0	60
8:45 AM	0	23	0	0	43	0	0	0	0	0	0	0	66
TOTAL VOLUMES:	0	196	1	1	433	0	0	0	0	1	0	1	633

AM Peak Hr Begins at: 7:15 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	104	1	1	253	0	0	0	0	1	0	1	361

PEAK HR FACTOR:	0.610	0.784	0.000	0.250	0.930
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			Eddy Jones Eastbound			Eddy Jones Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
7:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
7:45 AM	0	0	0	0	1	0	0	0	0	0	0	0	1
8:00 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:15 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
8:30 AM	0	3	0	0	1	0	0	0	0	0	0	0	4
8:45 AM	0	2	0	0	0	0	0	0	0	0	0	0	2
TOTAL VOLUMES:	0	7	0	0	2	0	0	0	0	0	0	0	9

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	2	0	0	1	0	0	0	0	0	0	0	3

Pedestrian Counts

	Benet Road North Leg	Benet Road South Leg	Eddy Jones East Leg	Eddy Jones West Leg	TOTAL
7:00 AM	1	0	1	0	2
7:15 AM	2	1	2	0	5
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	2	0	0	2
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	3	3	3	0	9

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	2	1	2	0	5



PO Box 1178
 Corona, CA 92880
 951-268-6268

Location: Oceanside
 N/S: Benet Road
 E/W: Eddy Jones

Date: 10/6/2021
 Day: WEDNESDAY
 Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
 Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Benet Road Northbound			Benet Road Southbound			Eddy Jones Eastbound			Eddy Jones Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	42	0	0	33	0	0	0	0	0	0	0	75
4:15 PM	0	49	0	0	34	0	0	0	0	0	0	0	83
4:30 PM	0	56	0	0	27	0	0	0	0	0	0	0	83
4:45 PM	0	49	0	0	23	0	0	0	0	1	0	0	73
5:00 PM	0	46	0	0	25	0	0	0	0	0	0	0	71
5:15 PM	0	55	0	0	45	0	0	0	0	0	0	0	100
5:30 PM	0	53	0	0	20	0	0	0	0	0	0	0	73
5:45 PM	0	45	0	0	27	0	0	0	0	0	0	0	72
TOTAL VOLUMES:	0	395	0	0	234	0	0	0	0	1	0	0	630

PM Peak Hr Begins at: 430 PM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	206	0	0	120	0	0	0	0	1	0	0	327

PEAK HR FACTOR:	0.920	0.667	0.000	0.250	0.818
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Bicycle Counts

	Benet Road Northbound			Benet Road Southbound			Eddy Jones Eastbound			Eddy Jones Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	0	2	0	0	2	0	0	0	0	0	0	0	4
4:15 PM	0	2	0	0	0	0	0	0	0	0	0	0	2
4:30 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
4:45 PM	0	0	0	0	3	0	0	0	0	0	0	0	3
5:00 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
5:15 PM	0	0	0	0	2	0	0	0	0	0	0	0	2
5:30 PM	0	0	0	0	3	0	0	0	0	0	0	0	3
5:45 PM	0	1	0	0	0	0	0	0	0	0	0	0	1
TOTAL VOLUMES:	0	6	0	0	10	0	0	0	0	0	0	0	16

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	0	1	0	0	5	0	0	0	0	0	0	0	6

Pedestrian Counts

	Benet Road North Leg	Benet Road South Leg	Eddy Jones East Leg	Eddy Jones West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	2	0	0	2
4:30 PM	0	0	1	0	1
4:45 PM	1	0	1	0	2
5:00 PM	0	0	1	0	1
5:15 PM	0	0	1	0	1
5:30 PM	0	0	0	0	0
5:45 PM	0	0	1	0	1
TOTAL VOLUMES:	1	2	5	0	8

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	1	0	4	0	5



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Foussat Road
E/W: Alex Road

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 7:00 AM to 9:00 AM
Peak Hour: 7:30 AM to 8:30 AM

Vehicle Counts

	Foussat Road Northbound			Foussat Road Southbound			Alex Road Eastbound			Alex Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	0	24	0	0	46	1	0	0	1	0	0	0	72
7:15 AM	0	23	0	0	61	0	0	0	3	0	0	0	87
7:30 AM	4	30	0	0	89	0	0	0	0	0	0	0	123
7:45 AM	1	32	0	0	61	0	1	0	2	0	0	0	97
8:00 AM	1	64	0	0	57	2	1	0	2	0	0	0	127
8:15 AM	4	38	0	0	61	1	0	0	1	0	0	0	105
8:30 AM	3	56	0	0	57	0	0	0	1	0	0	0	117
8:45 AM	2	35	0	0	56	0	0	0	2	0	0	0	95
TOTAL VOLUMES:	15	302	0	0	488	4	2	0	12	0	0	0	823

AM Peak Hr Begins at: 730 AM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	10	164	0	0	268	3	2	0	5	0	0	0	452

PEAK HR FACTOR:	0.669	0.761	0.583	0.000	0.890
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Bicycle Counts

	Foussat Road Northbound			Foussat Road Southbound			Alex Road Eastbound			Alex Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
7:00 AM	1	2	0	0	2	0	0	0	0	0	0	0	5
7:15 AM	0	0	0	0	0	0	0	0	1	0	0	0	1
7:30 AM	0	1	0	0	2	0	0	0	0	0	0	0	3
7:45 AM	1	0	0	0	1	0	0	0	0	0	0	0	2
8:00 AM	0	1	0	0	2	0	0	0	0	0	0	0	3
8:15 AM	0	0	0	0	1	0	0	0	0	0	0	0	1
8:30 AM	0	1	0	0	0	0	0	0	0	0	0	0	1
8:45 AM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	2	5	0	0	8	0	0	0	1	0	0	0	16

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	1	2	0	0	6	0	0	0	0	0	0	0	9

Pedestrian Counts

	Foussat Road North Leg	Foussat Road South Leg	Alex Road East Leg	Alex Road West Leg	TOTAL
7:00 AM	0	0	0	0	0
7:15 AM	0	0	0	0	0
7:30 AM	0	0	0	0	0
7:45 AM	0	0	0	0	0
8:00 AM	0	0	0	0	0
8:15 AM	0	0	0	0	0
8:30 AM	0	0	0	0	0
8:45 AM	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	0	0	0	0	0



PO Box 1178
Corona, CA 92880
951-268-6268

Location: Oceanside
N/S: Foussat Road
E/W: Alex Road

Date: 10/6/2021
Day: WEDNESDAY
Project # 143-21552

TURNING MOVEMENT COUNT

Count Period: 4:00 PM to 6:00 PM
Peak Hour: 4:30 PM to 5:30 PM

Vehicle Counts

	Foussat Road Northbound			Foussat Road Southbound			Alex Road Eastbound			Alex Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	5	50	0	0	36	1	0	0	4	0	0	0	96
4:15 PM	9	54	0	0	37	1	1	0	1	0	0	0	103
4:30 PM	7	39	0	0	44	0	1	0	3	0	0	0	94
4:45 PM	3	62	0	0	35	0	0	0	2	0	0	0	102
5:00 PM	4	52	0	0	29	1	0	0	1	0	0	0	87
5:15 PM	6	73	0	0	32	1	1	0	2	0	0	0	115
5:30 PM	9	49	0	0	28	0	0	0	5	0	0	0	91
5:45 PM	6	47	0	0	34	0	0	0	6	0	0	0	93
TOTAL VOLUMES:	49	426	0	0	275	4	3	0	24	0	0	0	781

PM Peak Hr Begins at: 430 PM

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	20	226	0	0	140	2	2	0	8	0	0	0	398

PEAK HR FACTOR:	0.778	0.807	0.625	0.000	0.865
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Bicycle Counts

	Foussat Road Northbound			Foussat Road Southbound			Alex Road Eastbound			Alex Road Westbound			TOTAL
	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	
4:00 PM	2	1	0	0	0	0	0	0	0	0	0	0	3
4:15 PM	1	0	0	0	0	0	0	0	0	0	0	0	1
4:30 PM	1	0	0	0	0	0	0	0	0	0	0	0	1
4:45 PM	1	1	0	0	0	0	0	0	1	0	0	0	3
5:00 PM	0	1	0	0	1	0	0	0	0	0	0	0	2
5:15 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
5:30 PM	0	0	0	0	1	0	0	0	1	0	0	0	2
5:45 PM	0	0	0	0	0	0	0	0	0	0	0	0	0
TOTAL VOLUMES:	5	3	0	0	2	0	0	0	2	0	0	0	12

	NL	NT	NR	SL	ST	SR	EL	ET	ER	WL	WT	WR	TOTAL
PEAK VOLUMES:	2	2	0	0	1	0	0	0	1	0	0	0	6

Pedestrian Counts

	Foussat Road North Leg	Foussat Road South Leg	Alex Road East Leg	Alex Road West Leg	TOTAL
4:00 PM	0	0	0	0	0
4:15 PM	0	0	0	0	0
4:30 PM	0	0	0	0	0
4:45 PM	0	0	0	0	0
5:00 PM	0	0	0	0	0
5:15 PM	0	0	0	0	0
5:30 PM	0	0	0	0	0
5:45 PM	0	0	0	0	0
TOTAL VOLUMES:	0	0	0	0	0

	North Leg	South Leg	East Leg	West Leg	TOTAL
PEAK VOLUMES:	0	0	0	0	0



City of Oceanside
 Benet Road
 B/ Eddy Jones - State Route 76

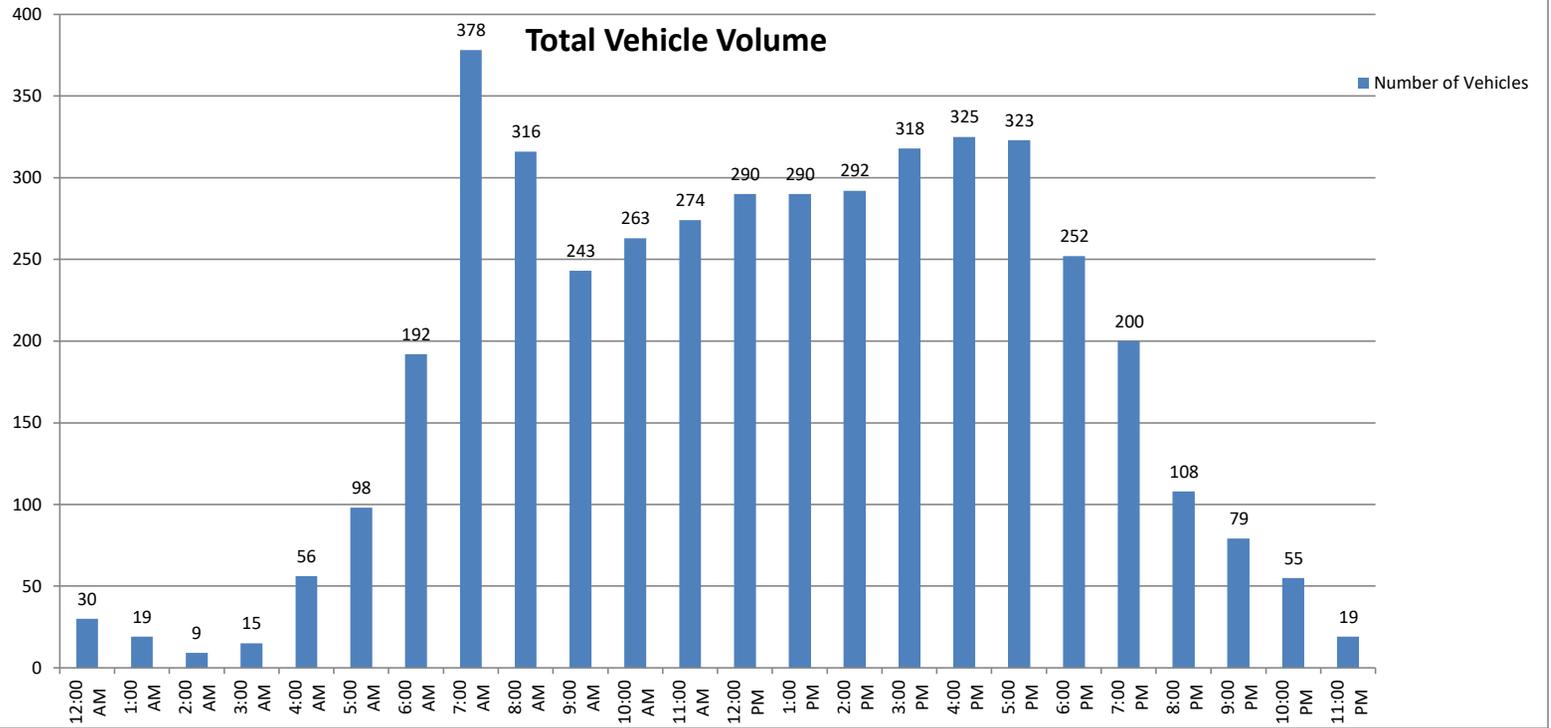
File Name 001
 Site Code: 143-21552
 24 Hour Directional Volume Count

Date: 10/6/2021	Northbound				Southbound				Combined Totals	
	15 Minute Totals		Hourly Totals		15 Minute Totals		Hourly Totals		Morning	Afternoon
Time	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00	6	33			4	38				
12:15	4	33			5	37				
12:30	4	47			2	36				
12:45	2	35	16	148	3	31	14	142	30	290
1:00	3	46			0	38				
1:15	3	38			1	41				
1:30	2	32			0	32				
1:45	4	34	12	150	6	29	7	140	19	290
2:00	2	42			0	28				
2:15	2	36			0	40				
2:30	1	24			3	43				
2:45	1	50	6	152	0	29	3	140	9	292
3:00	1	62			1	24				
3:15	1	45			4	27				
3:30	2	52			3	41				
3:45	1	43	5	202	2	24	10	116	15	318
4:00	3	51			8	37				
4:15	3	53			5	25				
4:30	6	47			7	34				
4:45	3	50	15	201	21	28	41	124	56	325
5:00	5	49			8	24				
5:15	7	47			12	28				
5:30	7	62			20	27				
5:45	17	54	36	212	22	32	62	111	98	323
6:00	14	38			30	36				
6:15	4	60			30	24				
6:30	9	35			44	18				
6:45	19	27	46	160	42	14	146	92	192	252
7:00	14	39			72	12				
7:15	16	29			70	16				
7:30	28	40			74	20				
7:45	29	32	87	140	75	12	291	60	378	200
8:00	33	20			53	10				
8:15	39	14			64	12				
8:30	33	22			32	4				
8:45	23	21	128	77	39	5	188	31	316	108
9:00	31	17			32	7				
9:15	27	12			32	8				
9:30	20	14			34	5				
9:45	27	11	105	54	40	5	138	25	243	79
10:00	21	14			36	3				
10:15	45	8			30	3				
10:30	22	3			52	7				
10:45	23	12	111	37	34	5	152	18	263	55
11:00	29	7			36	0				
11:15	39	6			42	1				
11:30	26	4			37	1				
11:45	37	0	131	17	28	0	143	2	274	19
Totals	698	1550			1195	1001				
Combined Totals		2248				2196				
ADT										4444
AM Peak Hour	745	AM			700	AM				
Volume	134				291					
P.H.F.	0.859				0.970					
PM Peak Hour		530	PM			2430	PM			
Volume		214				146				
P.H.F.		0.863				0.890				
Percentage	31.0%	69.0%			54.4%	45.6%				



24 Hour Volume Plot
Benet Road
B/ Eddy Jones - State Route 76
 10/6/2021

Start Time	10/6/2021
12:00 AM	30
1:00 AM	19
2:00 AM	9
3:00 AM	15
4:00 AM	56
5:00 AM	98
6:00 AM	192
7:00 AM	378
8:00 AM	316
9:00 AM	243
10:00 AM	263
11:00 AM	274
12:00 PM	290
1:00 PM	290
2:00 PM	292
3:00 PM	318
4:00 PM	325
5:00 PM	323
6:00 PM	252
7:00 PM	200
8:00 PM	108
9:00 PM	79
10:00 PM	55
11:00 PM	19
Total	4444



Volumes represent the combined totals for both directions



City of Oceanside
 Foussat Road
 B/ Alex Road - State Route 76

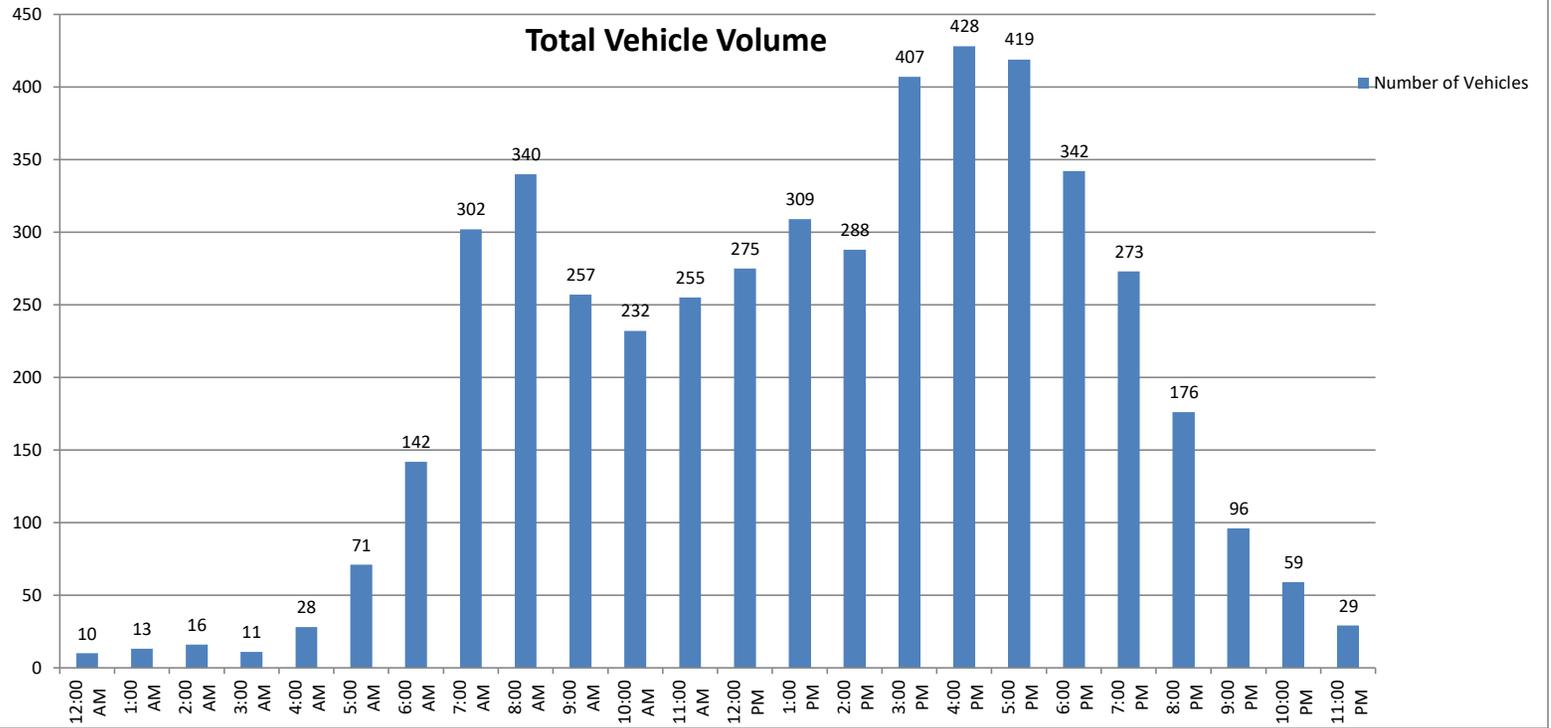
File Name 002
 Site Code: 143-21552
 24 Hour Directional Volume Count

Date: 10/6/2021	Northbound				Southbound				Combined Totals	
	15 Minute Totals		Hourly Totals		15 Minute Totals		Hourly Totals		Morning	Afternoon
Time	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon	Morning	Afternoon
12:00	2	27			0	31				
12:15	4	32			1	43				
12:30	0	43			2	37				
12:45	1	31	7	133	0	31	3	142	10	275
1:00	4	43			1	36				
1:15	1	35			3	27				
1:30	2	37			1	39				
1:45	0	49	7	164	1	43	6	145	13	309
2:00	3	33			3	40				
2:15	1	33			1	29				
2:30	3	29			3	41				
2:45	1	35	8	130	1	48	8	158	16	288
3:00	0	63			1	46				
3:15	1	59			1	52				
3:30	1	46			4	36				
3:45	0	59	2	227	3	46	9	180	11	407
4:00	1	54			0	39				
4:15	1	64			4	54				
4:30	4	58			8	45				
4:45	2	62	8	238	8	52	20	190	28	428
5:00	3	69			13	43				
5:15	1	57			10	53				
5:30	1	61			16	42				
5:45	8	65	13	252	19	29	58	167	71	419
6:00	4	50			11	32				
6:15	11	57			18	28				
6:30	9	51			39	38				
6:45	17	36	41	194	33	50	101	148	142	342
7:00	18	48			45	39				
7:15	15	44			49	34				
7:30	20	33			69	17				
7:45	21	41	74	166	65	17	228	107	302	273
8:00	44	33			63	16				
8:15	37	24			41	20				
8:30	40	25			47	23				
8:45	26	28	147	110	42	7	193	66	340	176
9:00	27	25			35	14				
9:15	23	14			35	3				
9:30	34	14			42	10				
9:45	28	10	112	63	33	6	145	33	257	96
10:00	22	13			36	3				
10:15	33	9			35	16				
10:30	22	5			32	2				
10:45	28	8	105	35	24	3	127	24	232	59
11:00	39	4			34	4				
11:15	28	5			30	4				
11:30	20	3			33	3				
11:45	36	3	123	15	35	3	132	14	255	29
Totals	647	1727			1030	1374				
Combined Totals		2374				2404				
ADT										4778
AM Peak Hour	800	AM			715	AM				
Volume	147				246					
P.H.F.	0.835				0.891					
PM Peak Hour		415	PM			415	PM			
Volume		253				194				
P.H.F.		0.917				0.898				
Percentage	27.3%	72.7%			42.8%	57.2%				



24 Hour Volume Plot
Foussat Road
B/ Alex Road - State Route 76
 10/6/2021

Start Time	10/6/2021
12:00 AM	10
1:00 AM	13
2:00 AM	16
3:00 AM	11
4:00 AM	28
5:00 AM	71
6:00 AM	142
7:00 AM	302
8:00 AM	340
9:00 AM	257
10:00 AM	232
11:00 AM	255
12:00 PM	275
1:00 PM	309
2:00 PM	288
3:00 PM	407
4:00 PM	428
5:00 PM	419
6:00 PM	342
7:00 PM	273
8:00 PM	176
9:00 PM	96
10:00 PM	59
11:00 PM	29
Total	4778



Volumes represent the combined totals for both directions

Report Segment	Dist	Route	Cou nty	Postmile	Description	Back Peak Hour	Back Peak Month	Back AADT	Ahead Peak Hour	Ahead Peak Month	Ahead AADT
CALTRANS 2015 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3700	52000	49500
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET	3700	52000	49500	4200	56000	48000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE	4200	56000	48000	4350	55000	50000
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	4350	55000	50000	4450	56000	50000
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	4450	56000	50000	4700	60000	55000
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	4700	60000	55000	4250	51000	48000
CALTRANS 2016 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3650	54000	50000
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET	3650	54000	50000	3950	53000	45000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE	3950	53000	45000	4050	51000	46500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	4050	51000	46500	4150	52000	46500
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	4150	52000	46500	4350	56000	51000
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	4350	56000	51000	3950	47500	44500
CALTRANS 2017 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3700	55000	51000
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET	3700	55000	51000	3950	53000	45000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE	3700	49500	42000	4050	51000	46500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	3750	47500	43000	3850	48500	43000
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	3850	48500	43000	4050	52000	47500
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	4050	52000	47500	3650	44000	41500
CALTRANS 2018 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5	0	0	0	3700	55000	51000
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET	3700	55000	51000	3950	53000	45000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE	3700	49500	42000	4050	51000	46500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	3750	47500	43000	3850	48500	43000
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	3850	48500	43000	4050	52000	47500
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	4050	52000	47500	3650	44000	41500
CALTRANS 2019 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3650	53000	50000
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET	3650	54000	50000	3950	53000	45000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE	3650	48500	41000	4050	51000	46500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	3700	46500	42000	3750	47500	42000
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	3750	47500	42000	3950	51000	46500
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	3950	51000	46500	3600	43000	40500
CALTRANS 2020 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3650	49500	44500
4	11	076	SD R	0.486	OCEANSIDE, LORETTA ST	3250	48000	44500	3500	47000	40000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DR	3250	43000	36500	3600	45500	41500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	3300	41500	37500	3350	42500	37500
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	3350	42500	37500	3500	45500	41500
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	3500	45500	41500	3200	38500	36000
CALTRANS 2021 VOLUMES											
3	11	076	SD R	0.000	JCT. RTE. 5				3650	51000	46500
4	11	076	SD R	0.486	OCEANSIDE, LORETTA ST	3400	50000	46500	3650	49000	42000
5	11	076	SD R	1.156	OCEANSIDE, CANYON DR	3400	45000	38000	3750	47500	43500
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD	3450	43500	39000	3500	44500	39000
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD	3500	44500	39000	3650	47500	43500
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL	3650	47500	43500	3350	40000	37500
Maximum from above 7 years											
3	11	076	SD R	0.000	JCT. RTE. 5				Maximum from above	51000	
4	11	076	SD R	0.486	OCEANSIDE, LORETTA STREET				Maximum from above	48000	
5	11	076	SD R	1.156	OCEANSIDE, CANYON DRIVE				Maximum from above	50000	
	11	076	SD R	1.767	OCEANSIDE, BENET ROAD				Maximum from above	50000	
6	11	076	SD R	2.145	OCEANSIDE, AIRPORT ROAD				Maximum from above	55000	
7	11	076	SD R	3.389	OCEANSIDE, EL CAMINO REAL				Maximum from above	48000	

INTERVAL	PHASE TIMING								PRE-EMPTION	F										
	1	2	3	4	5	6	7	8		9	E	FLAGS	1	2	3	4	5	6	7	8
0 WALK	1	1							CLK RST	EV SEL	0	PERMIT	1	2		4				
1 DONT WALK	1	1								RR1 CLR	15	RED LOCK	1	2						1
2 MIN GREEN	5	5								EVA DLY	0	YEL LOCK								2
3 TYPE 3 DET	0	0								EVA CLR	5	V RECALL								3
4 ADD/VEH	0.0	0.0								EVB DLY	0	P RECALL								4
5 PASSAGE	2.0	2.0								EVB CLR	5	PED PHASES								5
6 MAX GAP	2.0	2.0								EVC DLY	0	RT OLA								6
7 MIN GAP	2.0	2.0								EVC CLR	5	RT OLB								7
8 MAX EXT	45	35								EVD DLY	0	DEL ENTRY								8
9 MAX 2									YR	EVD CLR	5	MAX 2 PHASES								9
A MAX 3									NO	MAX EV	255	LAG PHASES	READ ONLY							
B									DAY	RR2 CLR	15	RED REST	1	2						B
C REDUCE BY	0.0	0.0							DOM			REST IN-WALK								C
D EVERY	1.0	1.0							HR			MAX 3 PHASES								D
E YELLOW	3.7	4.8							MIN			YEL START UP		2						E
F RED	2.0	2.0							SEC			FIRST PHASE	1							F
3.5 PED KING FT													1	2	3	4	5	6	7	8
BIKE KING FT		38																		

FOC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5

FCO	3
FC1	3
FC2	10
FCA	0.0
FCB	0.0
FCC	0.0
FCD	0.0

FD0 TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FD8 OS SEEKING	1

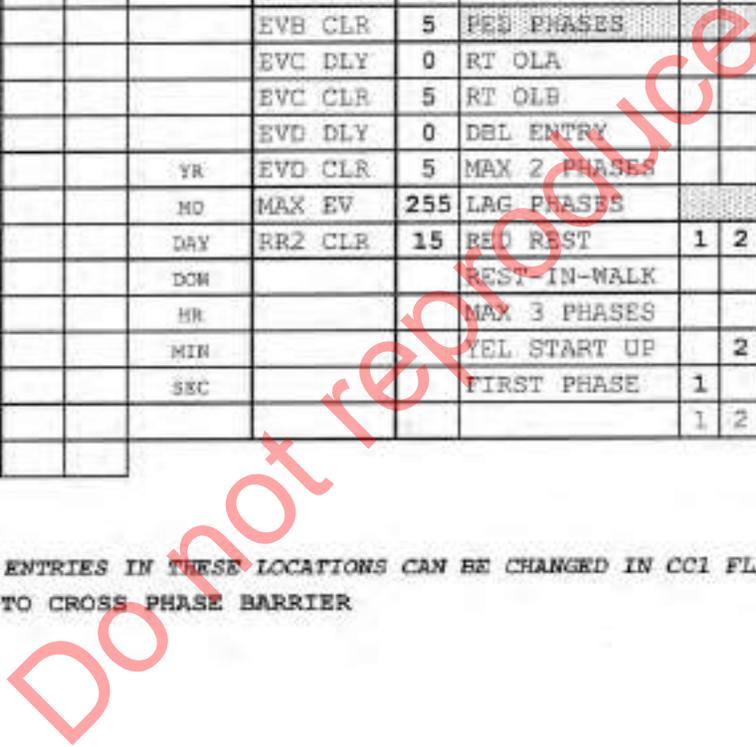
CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:

FZ 2 BIKE = 3 sec

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CC1 FLASH ONLY

FZ 4 PERMITTED TO ALLOW CONTROLLER TO CROSS PHASE BARRIER



		CONTROL PLANS									Y-COORD			LAG PHASE	FLAGS										
		1	2	3	4	5	6	7	8	9		C	D	E	F	1	2	3	4	5	6	7	8		
0	CYCLE LENGTH														LAG FZ FREE		2								0
1	FZ1 GRN FCTR													GAPOUT CP1	LAG FZ CP 1										1
2														GAPOUT CP2	LAG FZ CP 2										2
3	FZ3 GRN FCTR													GAPOUT CP3	LAG FZ CP 3										3
4	FZ4 GRN FCTR										PERM TIME			GAPOUT CP4	LAG FZ CP 4										4
5	FZ5 GRN FCTR										LAG OFFSET			GAPOUT CP5	LAG FZ CP 5										5
6											FORCE OFF			GAPOUT CP6	LAG FZ CP 6										6
7	FZ7 GRN FCTR										LONG GRN			GAPOUT CP7	LAG FZ CP 7										7
8	FZ8 GRN FCTR										NO GREEN			GAPOUT CP8	LAG FZ CP 8										8
9	MULTI CYCLE													GAPOUT CP9	LAG FZ CP 9										9
A	OFFSET A										OFFSET				LAG C COORD										A
B	OFFSET B														LAG D COORD										B
C	OFFSET C														COORD PHASES		2				6				C
D	FZ 3 EXT																								D
E	FZ 7 EXT																								E
F	OFFSET INTRPT																								F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP
 CO4 LAST CP
 CO7 TRNSMT CP
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

SYSTEM MASTER:
 RTE 76X @
 RTE 5 SB

FEATURE	OFF	ON	LOCATION	OFF	ON
1			1		
2			2		2
3			3		
4			4		
5			5		
6			6		
7			7		
8			8		

COO = 2

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1									CP 1								
2	CP 2									CP 2									CP 2								
3	CP 3									CP 3									CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8									CP 8								
9	CP 9									CP 9									CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											

	E	FLAGS								F	FLAGS							
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8
0										CODE 4								
1										CODE 5								
2										C-RECALL								
3										D-RECALL								
4										EXCLUSIVE								
5										Z PED	2							
6										S PED						6		
7										4 PED			4					
8										S PED								8
9																		
A	OLA NOT									OLA ON								
B	OLB NOT									OLB ON								
C	OLC NOT									OLC ON								
D	OLD NOT									OLD ON								
E																		
F																		

LAST POWER FAILURE REGISTER

HOUR = D-A-E
 MINUTE = D-B-E
 DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES
 (CALL ACTIVE LIGHTS)
 RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES
 (CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOUR = D-A-F
 MINUTE = D-B-F
 DAY = D-C-F

D-E-E = C8 VERSION NUMBER
 D-E-F = LITHIUM BATTERY CONDITION
 84 = BAD
 85 = GOOD

TIME OF DAY ACTIVITY TABLE											
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS											
	HR	MIN	ACT	ON/OFF	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0											
1											
2											
3											
4											
5											
6											
7											
8											
9											
A											
B											
C											
D											
E											
F											

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

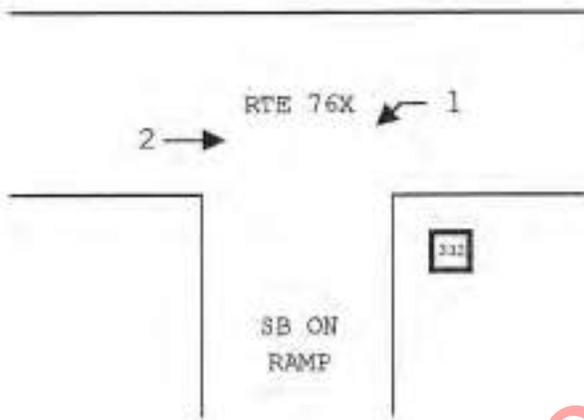
- 8 ENERGIZE AUX OUTPUT-YELLOW
- 9 TIME OF DAY MAX RECALL (1ST SELECT)
- A TRAFFIC ACT. MAX 2 OPERATION
- B TIME OF DAY MAX RECALL (2ND SELECT)
- C YELLOW YIELD COORDINATION
- D YELLOW YIELD COORDINATION
- E TIME OF DAY FREE OPERATION
- F FLASHING OPERATION

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

DATE: 11/1/95

LOCATION: RTE 76 EXPRESSWAY @ RTE 5 SB ON RAMP

CONFLICT MONITOR PROGRAM



Do not reproduce

INTERVAL	PHASE TIMING								9	PRE-EMPTION		F										
	1	2	3	4	5	6	7	8		E	FLAGS	1	2	3	4	5	6	7	8			
0 WALK	1	1				1		1	CLK RST	EV SEL	0	PERMIT	1	2				6		8	0	
1 DONT WALK	1	1				1		1		RR1 CLR	15	RED LOCK	1							8	1	
2 MIN GREEN	5	5				5		5		EVA DLY	0	YEL LOCK									2	
3 TYPE 3 DET	0	0				255		0		EVA CLR	5	V RECALL		2				6			3	
4 ADD/VEH	0.0	0.0				0.0		0.0		EVB DLY	0	P RECALL									4	
5 PASSAGE	2.0	2.0				6.3		2.0		EVB CLR	5	PED PHASES									5	
6 MAX GAP	2.0	2.0				8.3		2.0		EVC DLY	0	RT OLA									6	
7 MIN GAP	2.0	2.0				3.0		2.0		EVC CLR	5	RT OLB									7	
8 MAX EXT	25	50				50		25		EVD DLY	0	DBL ENTRY									8	
9 MAX 2	20	30				80			YR	EVD CLR	5	MAX 2 PHASES	1	2					6		9	
A MAX 3									MO	MAX EV	255	LAG PHASES	READ ONLY								A	
B									DAY	RR2 CLR	15	RED REST										B
C REDUCE BY	0.0	0.0				0.1		0.0	DOW			REST-IN-WALK										C
D EVERY	1.0	1.0				0.5		1.0	HR			MAX 3 PHASES										D
E YELLOW	3.7	5.5				5.5		4.1	MIN			YEL START UP		2				6				E
F RED	2.0	2.5				2.5		2.0	SEC			FIRST PHASE									8	F
1.5 PED XING FT													1	2	3	4	5	6	7	8		
BIKE XING FT		68				25																

FOC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5

FCO	3
FC1	3
FC2	10
FCA	0.0
FCH	0.0
FCC	0.0
FCD	0.0

FDO TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FD8 OS SEEKING	1

CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:

- OLA = FZ 8
- FZ 2 BIKE = 4 sec
- FZ 6 BIKE = 1 sec

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CCI FLASH ONLY

Do not reproduce

	CONTROL PLANS									Y-COORD			LAG PHASE	FLAGS										
	1	2	3	4	5	6	7	8	9		C	D	E	F	1	2	3	4	5	6	7	8		
0 CYCLE LENGTH														LAG FZ FREE		2					6		8	0
1 FZ1 GRN FCTR													GAPOUT CP1	LAG FZ CP 1										1
2													GAPOUT CP2	LAG FZ CP 2										2
3 FZ3 GRN FCTR													GAPOUT CP3	LAG FZ CP 3										3
4 FZ4 GRN FCTR													PERM TIME	LAG FZ CP 4										4
5 FZ5 GRN FCTR													LAG OFFSET	LAG FZ CP 5										5
6													FORCE OFF	LAG FZ CP 6										6
7 FZ7 GRN FCTR													LONG GRN	LAG FZ CP 7										7
8 FZ8 GRN FCTR													NO GREEN	LAG FZ CP 8										8
9 MULTI CYCLE													GAPOUT CP9	LAG FZ CP 9										9
A OFFSET A													OFFSET	LAG C COORD										A
B OFFSET B														LAG D COORD										B
C OFFSET C														COORD FAZES		2					6			C
D FZ 3 EXT																								D
E FZ 7 EXT																								E
F OFFSET INTRPT																								F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP **SYSTEM MASTER:**
 CO4 LAST CP **RTE 76X @**
 CO7 TRNSMT CP **RTE 5 SB**
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

FEATURE	OFF	ON	LOCATION	OFF	ON
1					1
2					2
3					
4					
5					
6					
7					
8					

COO = 3

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1									CP 1								
2	CP 2									CP 2									CP 2								
3	CP 3									CP 3									CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8									CP 8								
9	CP 9									CP 9									CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											

	E	FLAGS								F	FLAGS								
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8	
0										CODE 4	1								0
1										CODE 5									1
2										C-RECALL									2
3										D-RECALL									3
4										EXCLUSIVE									4
5										2 PED		2							5
6										6 PED						6			6
7										4 PED				4					7
8										8 PED								8	8
9																			9
A	OLA NOT									OLA ON									A
B	OLB NOT									OLB ON									B
C	OLC NOT									OLC ON									C
D	OLD NOT									OLD ON									D
E																			E
F																			F

LAST POWER FAILURE REGISTER

HOUR = D-A-E
 MINUTE = D-B-E
 DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES
 (CALL ACTIVE LIGHTS)
 RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES
 (CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOUR = D-A-F
 MINUTE = D-B-F
 DAY = D-C-F

D-E-E = C8 VERSION NUMBER
 D-E-F = LITHIUM BATTERY CONDITION
 84 = BAD
 85 = GOOD

TIME OF DAY ACTIVITY TABLE											
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS											
	HR	MIN	ACT	ON/ OFF	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0	05	30	4	ON	1	2	3	4	5	6	7
1	05	31	2	ON	1	2	3	4	5	6	7
2	14	30	2		1	2	3	4	5	6	7
3	14	31	4		1	2	3	4	5	6	7
4											
5											
6											
7											
8											
9											
A											
B											
C											
D											
E											
F											

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

CONTROL PLAN TIME OF DAY											
9+EVENT+HR+MIN+CP+OS+E+DOW											
	HR	MIN	CP	OS	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0											
1											
2											
3											
4											
5											
6											
7											
8											
9											
A											
B											
C											
D											
E											
F											

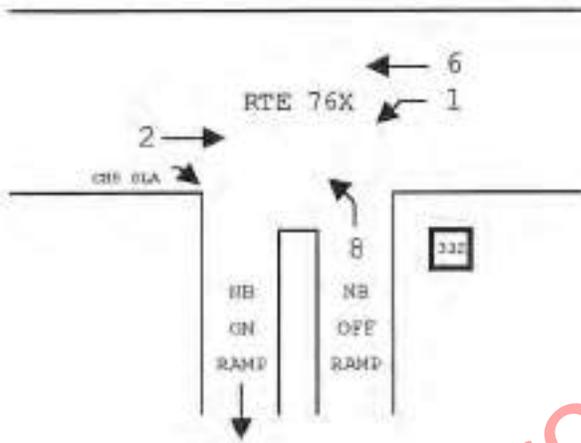
- 8 ENERGIZE AUX OUTPUT-YELLOW
- 9 TIME OF DAY MAX RECALL (1ST SELECT)
- A TRAFFIC ACT. MAX 2 OPERATION
- B TIME OF DAY MAX RECALL (2ND SELECT)
- C YELLOW YIELD COORDINATION
- D YELLOW YIELD COORDINATION
- E TIME OF DAY FREE OPERATION
- F FLASHING OPERATION

CONTROL PLAN TIME OF DAY											
9+EVENT+HR+MIN+CP+OS+E+DOW											
	HR	MIN	CP	OS	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0											
1											
2											
3											
4											
5											
6											
7											
8											
9											
A											
B											
C											
D											
E											
F											

DATE: 11/1/95

LOCATION: RTE 76 EXPRESSWAY @ RTE 5 NB RAMPS

CONFLICT MONITOR PROGRAM



Do not reproduce

INTERVAL	PHASE TIMING								9	PRE-EMPTION E	F										
	1	2	3	4	5	6	7	8			FLAGS	1	2	3	4	5	6	7	8		
0 WALK	1	1				1		7	CLK RST	EV SEL	0	PERMIT	1	2			6		8	0	
1 DONT WALK	1	1				1		25		RR1 CLR	15	RED LOCK	1						8	1	
2 MIN GREEN	9	25				25		10		EVA DLY	0	YEL LOCK								2	
3 TYPE 3 DET	0	255				255		0		EVA CLR	5	V RECALL		2			6			3	
4 ADD/VEH	0.0	0.0				0.0		0.0		EVB DLY	0	P RECALL								4	
5 PASSAGE	2.0	6.3				6.3		2.0		EVB CLR	5	PEU PHASES							8	5	
6 MAX GAP	2.0	8.3				8.3		2.0		EVC DLY	0	RT OLA								6	
7 MIN GAP	2.0	3.5				3.5		2.0		EVC CLR	5	RT OLB								7	
8 MAX EXT	20	50				50		24		EVD DLY	0	DBL ENTRY								8	
9 MAX 2		80				80		10	YR	EVD CLR	5	MAX 2 PHASES		2			6		8	9	
A MAX 3									NO	MAX EV	255	LAG PHASES	READ ONLY								A
B									DAY	RR2 CLR	15	RED REST								B	
C REDUCE BY	0.0	0.1				0.1		0.0	DOM			REST IN-WALK								C	
D EVERY	1.0	1.0				1.0		1.0	HR			MAX 3 PHASES								D	
E YELLOW	3.7	5.5				5.5		4.1	MIN			YEL START UP		2			6			E	
F RED	2.0	2.5				2.5		2.0	SEC			FIRST PHASE							8	F	
3.5 PED XING FT													1	2	3	4	5	6	7	8	
BIKE XING FT	103	70				60		132													

FOC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5

FCO	3
FC1	3
FC2	10
FCA	0.0
FCB	0.0
FCC	0.0
FCD	0.0

FD0 TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FD8 DS SEEKING	1

CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:

OLA = FZ 8

OLB = FZ 1

FZ 2 BIKE = 4 sec

FZ 6 BIKE = 3 sec

FZ 8 BIKE = 10 sec

FZ 8 BIKE PB = use pedestrian calculation

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CCI FLASH ONLY

Do not reproduce

		CONTROL PLANS									Y-COORD			LAG PHASE	FLAGS										
		1	2	3	4	5	6	7	8	9		C	D	E	F	1	2	3	4	5	6	7	8		
0	CYCLE LENGTH	160	160	160	160	160	160		200	180					LAG FZ FREE		2					6	8	0	
1	FZ1 GRN FCTR	20	20	20	20	20	20		20	20				GAPOUT CP1	1	LAG FZ CP 1	1						6	8	1
2														GAPOUT CP2	1	LAG FZ CP 2	1						6	8	2
3	FZ3 GRN FCTR	0	0	0	0	0	0		0	0				GAPOUT CP3	0	LAG FZ CP 3		2					6	8	3
4	FZ4 GRN FCTR	0	0	0	0	0	0		0	0	PERM TIME			GAPOUT CP4	0	LAG FZ CP 4		2					6	8	4
5	FZ5 GRN FCTR	0	0	0	0	0	0		0	0	LAG OFFSET			GAPOUT CP5	1	LAG FZ CP 5	1						6	8	5
6											FORCE OFF			GAPOUT CP6	0	LAG FZ CP 6		2					6	8	6
7	FZ7 GRN FCTR	0	0	0	0	0	0		0	0	LONG GRN			GAPOUT CP7		LAG FZ CP 7									7
8	FZ8 GRN FCTR	34	34	34	34	34	34		34	34	NO GREEN			GAPOUT CP8	1	LAG FZ CP 8	1						6	8	8
9	MULTI CYCLE	0	0	0	0	0	0		0	0				GAPOUT CP9	0	LAG FZ CP 9		2					6	8	9
A	OFFSET A	0	0	0	0	0	0		0	0	OFFSET					LAG C COORD									A
B	OFFSET B	0	0	0	0	0	0		0	0						LAG D COORD									B
C	OFFSET C	0	0	0	0	0	0		0	0						COORD PHASES		2					6		C
D	FZ 3 EXT																								D
E	FZ 7 EXT																								E
F	OFFSET INTRPT																								F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP
 CO4 LAST CP
 CO7 TRNSMT CP
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

SYSTEM MASTER:
 RTE 76X @
 COLLEGE BLVD

FEATURE	OFF	ON	LOCATION	OFF	ON
1					
2					
3				4	
4					
5					
6					
7					
8					

COO = 4

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1									CP 1								
2	CP 2									CP 2									CP 2								
3	CP 3									CP 3									CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8									CP 8								
9	CP 9									CP 9									CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											

	E	FLAGS								F	FLAGS								
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8	
0										CODE 4									0
1										CODE 5									1
2										C-RECALL									2
3										D-RECALL									3
4										EXCLUSIVE									4
5										2 PED		2							5
6										6 PED						6			6
7										4 PED				4					7
8										8 PED								8	8
9																			9
A	OLA NOT									OLA ON									A
B	OLB NOT									OLB ON									B
C	OLC NOT									OLC ON									C
D	OLD NOT									OLD ON									D
E																			E
F																			F

LAST POWER FAILURE REGISTER

HOOR = D-A-E

MINUTE = D-B-E

DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES

(CALL ACTIVE LIGHTS)

RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES

(CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOOR = D-A-F

MINUTE = D-B-F

DAY = D-C-F

D-E-E = C8 VERSION NUMBER

D-E-F = LITHIUM BATTERY CONDITION

84 = BAD

85 = GOOD

TIME OF DAY ACTIVITY TABLE											
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS											
	HR	MIN	ACT	ON/OFF	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0	05	30	2	ON	1	2	3	4	5	6	7
1	21	00	2		1	2	3	4	5	6	7
2											
3											
4											
5											
6											
7											
8											
9											
A											
B											
C	20	00	E	ON	1	2	3	4	5	6	7
D	05	30	E			2	3	4	5	6	
E											
F	09	00	E		1						7

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	1	A		2	3	4	5	6		
1												
2	09	00	2	A	1	2	3	4	5	6	7	
3	14	00	3	A	1	2	3	4	5	6	7	
4	14	45	9	A		2	3	4	5	6		
5	18	30	3	A		2	3	4	5	6		
6	20	00	E		1	2	3	4	5	6	7	
7												
8												
9												
A												
B												
C												
D												
E												
F												

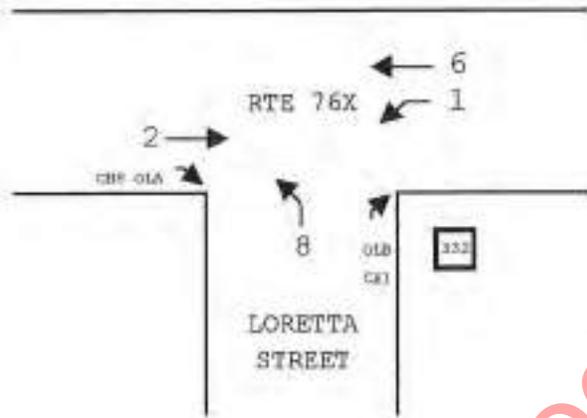
- 8 ENERGIZE AUX OUTPUT-YELLOW
- 9 TIME OF DAY MAX RECALL (1ST SELECT)
- A TRAFFIC ACT. MAX 2 OPERATION
- B TIME OF DAY MAX RECALL (2ND SELECT)
- C YELLOW YIELD COORDINATION
- D YELLOW YIELD COORDINATION
- E TIME OF DAY FREE OPERATION
- F FLASHING OPERATION

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

DATE: 11/1/95

LOCATION: RTE 76 EXPRESSWAY @ LORETTA STREET

CONFLICT MONITOR PROGRAM



Do not reproduce

INTERVAL	PHASE TIMING								9	PRE-EMPTION		F										
	1	2	3	4	5	6	7	8		E	0	FLAGS	1	2	3	4	5	6	7	8		
0 WALK	1	7				1		7	CLK RST	EV SEL	0	PERMIT	1	2				6		8	0	
1 DONT WALK	1	19				1		26		RR1 CLR	15	RED LOCK	1							8	1	
2 MIN GREEN	10	25				25		9		EVA DLY	0	YEL LOCK									2	
3 TYPE 3 DET	0	255				255		0		EVA CLR	5	V RECALL		2			6				3	
4 ADD/VEH	0.0	0.0				0.0		0.0		EVB DLY	0	P RECALL									4	
5 PASSAGE	2.0	6.3				6.3		2.0		EVB CLR	5	PED PHASES		2						8	5	
6 MAX GAP	4.0	8.3				8.3		2.0		EVC DLY	0	RT OLA									6	
7 MIN GAP	1.0	3.5				3.5		2.0		EVC CLR	5	RT OLB									7	
8 MAX EXT	30	50				50		21		EVD DLY	0	DBL ENTRY									8	
9 MAX 2		80				80		11	YR	EVD CLR	5	MAX 2 PHASES		2			6		8	9		
A MAX 3									MO	MAX EV	255	LAG PHASES	READ ONLY								A	
B									DAY	RR2 CLR	15	RED REST										B
C REDUCE BY	0.1	0.1				0.1		0.0	DOW			REST-IN-WALK										C
D EVERY	1.0	1.0				1.0		1.0	HR			MAX 3 PHASES										D
E YELLOW	3.7	5.5				5.5		4.1	MIN			YEL START UP		2			6					E
F RED	2.0	2.5				2.5		2.0	SEC			FIRST PHASE									8	F
3.5 PED KING FT		92						117						1	2	3	4	5	6	7	8	
BIKE KING FT	117	91				65		141														

FOC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5

FC0	3
FC1	3
FC2	10
FCA	0.0
FCB	0.0
FCC	0.0
FCD	0.0

FD0 TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FD6 OS SEEKING	1

CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:

OLA = FZ 8

OLB = FZ 1

FZ 2 BIKE = 6 sec

FZ 6 BIKE = 4 sec

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CC1 FLASH ONLY



Do not replicate

		CONTROL PLANS								Y-COORD		LAG PHASE	FLAGS										
		1	2	3	4	5	6	7	8	9	C	D	E	F	1	2	3	4	5	6	7	8	
0	CYCLE LENGTH	160	160	160	160	160	160		200	180					LAG F2 FREE		2				6	8	0
1	FZ1 GRN FCTR	30	30	30	30	30	30		35	35			GAPOUT CP1	0	LAG F2 CP 1		2				6	8	1
2													GAPOUT CP2	0	LAG F2 CP 2		2				6	8	2
3	FZ3 GRN FCTR	0	0	0	0	0	0		0	0			GAPOUT CP3	0	LAG F2 CP 3		2				6	8	3
4	FZ4 GRN FCTR	0	0	0	0	0	0		0	0	PERM TIME		GAPOUT CP4	1	LAG F2 CP 4	1					6	8	4
5	FZ5 GRN FCTR	0	0	0	0	0	0		0	0	LAG OFFSET		GAPOUT CP5	0	LAG F2 CP 5		2				6	8	5
6											FORCE OFF		GAPOUT CP6	1	LAG F2 CP 6	1					6	8	6
7	FZ7 GRN FCTR	0	0	0	0	0	0		0	0	LONG GRN		GAPOUT CP7		LAG F2 CP 7								7
8	FZ8 GRN FCTR	31	31	31	31	31	31		31	31	NO GREEN		GAPOUT CP8	0	LAG F2 CP 8		2				6	8	8
9	MULTI CYCLE	0	0	0	0	0	0		0	0			GAPOUT CP9	1	LAG F2 CP 9	1					6	8	9
A	OFFSET A	141	19	44	83	26	76		44	82	OFFSET				LAG C COORD								A
B	OFFSET B	141	19	44	83	26	76		44	82					LAG D COORD								B
C	OFFSET C	141	19	44	83	26	76		44	82					COORD FAZES		2				6		C
D	F2 3 EXT																						D
E	F2 7 EXT																						E
F	OFFSET INTRPT																						F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP
 CO4 LAST CP
 CO7 TRNSMT CP
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

SYSTEM MASTER:
 RTE 76X @
 COLLEGE BLVD

FEATURE	OFF	ON	LOCATION	OFF	ON
1					1
2					
3					4
4					
5					
6					
7					
8					

COO - 5

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1									CP 1								
2	CP 2									CP 2									CP 2								
3	CP 3									CP 3									CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8									CP 8								
9	CP 9									CP 9									CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											
		1	2	3	4	5	6	7	8		1	2	3	4	5	6	7	8		1	2	3	4	5	6	7	8

	E	FLAGS								F	FLAGS							
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8
0										CODE 4								
1										CODE 5								
2										C-RECALL								
3										D-RECALL								
4										EXCLUSIVE								
5										2 PED	2							
6										6 PED				6				
7										4 PED			4					
8										6 PED							8	
9																		
A	OLA NOT									OLA ON								
B	OLE NOT									OLE ON								
C	OLC NOT									OLC ON								
D	OLD NOT									OLD ON								
E																		
F																		
		1	2	3	4	5	6	7	8		1	2	3	4	5	6	7	8

LAST POWER FAILURE REGISTER

HOUR = D-A-E
 MINUTE = D-B-E
 DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES
 (CALL ACTIVE LIGHTS)
 RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES
 (CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOUR = D-A-F
 MINUTE = D-B-F
 DAY = D-C-F

D-E-E = C8 VERSION NUMBER
 D-E-F = LITHIUM BATTERY CONDITION
 84 = BAD
 85 = GOOD

TIME OF DAY ACTIVITY TABLE												
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS												
	HR	MIN	ACT	ON/OFF	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	2	ON	1	2	3	4	5	6	7	
1	21	00	2		1	2	3	4	5	6	7	
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C	20	00	E	ON	1	2	3	4	5	6	7	
D	05	30	E			2	3	4	5	6		
E												
F	09	00	E		1							7

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	1	A		2	3	4	5	6		
1												
2	09	00	2	A	1	2	3	4	5	6	7	
3	14	00	3	A	1	2	3	4	5	6	7	
4	14	45	9	A		2	3	4	5	6		
5	18	30	3	A		2	3	4	5	6		
6	20	00	E		1	2	3	4	5	6	7	
7												
8												
9												
A												
B												
C												
D												
E												
F												

8 ENERGIZE AUX OUTPUT-YELLOW

9 TIME OF DAY MAX RECALL (1ST SELECT)

A TRAFFIC ACT, MAX 2 OPERATION

B TIME OF DAY MAX RECALL (2ND SELECT)

C YELLOW YIELD COORDINATION

D YELLOW YIELD COORDINATION

E TIME OF DAY FREE OPERATION

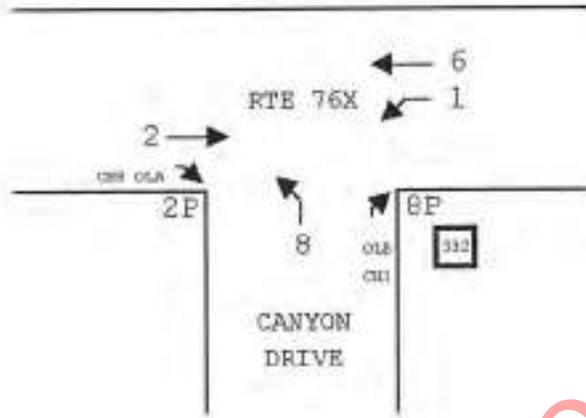
F FLASHING OPERATION

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

DATE: 5/20/97

LOCATION: RTE 76 EXPRESSWAY @ CANYON DRIVE

CONFLICT MONITOR PROGRAM



Do not reproduce

INTERVAL	PHASE TIMING								9	PRE-EMPTION E	F											
	1	2	3	4	5	6	7	8			FLAGS	1	2	3	4	5	6	7	8			
0 WALK	1	1	1	1	1	1	1	7	CLK RST	EV SEL	0	PERMIT	1	2	3	4	5	6	7	8	0	
1 DONT WALK	1	1	1	1	1	1	1	37		RR1 CLR	15	RED LOCK									1	
2 MIN GREEN	12	25	12	11	13	25	12	12		EVA DLY	0	YEL LOCK				4					2	
3 TYPE 3 DET	0	255	0	0	0	255	0	0		EVA CLR	5	V RECALL		2				6			3	
4 ADD/VEH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		EVB DLY	0	P RECALL									4	
5 PASSAGE	2.0	6.3	2.0	2.0	3.0	6.3	3.0	2.0		EVB CLR	5	PED PHASES								8	5	
6 MAX GAP	2.0	8.3	2.0	2.0	5.0	8.3	5.0	2.0		EVC DLY	0	RT OLA									6	
7 MIN GAP	2.0	3.5	2.0	2.0	2.0	3.5	2.0	2.0		EVC CLR	5	RT OLB									7	
8 MAX EXT	18	50	18	24	27	50	28	23		EVD DLY	0	DBL ENTRY				4				8	8	
9 MAX 2	8	80	3	4	2	80	0	3	YR	EVD CLR	5	MAX 2 PHASES	1	2	3	4			6	7	8	9
A MAX 3	3	80		4		80		3	MO	MAX EV	255	LAG PHASES	READ ONLY								A	
B									DAY	RR2 CLR	15	RED REST										B
C REDUCE BY	0.0	0.1	0.0	0.0	0.1	0.1	0.1	0.0	DOM			REST-IN-WALK										C
D EVERY	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	HR			MAX 3 PHASES	1	2		4			6		8	D
E YELLOW	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1	MIN			YEL START UP		2					6			E
F RED	2.0	2.5	2.0	2.0	2.0	2.5	2.0	2.0	SEC			FIRST PHASE				3					7	F
3.5 PED XING FT								150					1	2	3	4	5	6	7	8		
BIKE XING FT	160	164	160	149	164	154	149	156														

FUC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5

FCO	3
FC1	3
FC2	10
FCA	0.0
FCB	0.0
FCC	0.0
FCD	0.0

FD0 TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FD8 OS SEEKING	1

CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:
 OLA = FE 5
 FE 2 BIKE = 10 sec
 FE 6 BIKE = 10 sec

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CC1 FLASH ONLY

Do not reproduce

		CONTROL PLANS									Y-COORD		LAG PHASE	FLAGS											
		1	2	3	4	5	6	7	8	9		C	D	E	F	1	2	3	4	5	6	7	8		
0	CYCLE LENGTH	160	160	160	160	160	160		200	180					LAG F2 FREE		2		4		6		8	0	
1	FZ1 GRN FCTR	15	15	15	15	15	15		15	15				GAPOUT CP1	1	LAG F2 CP 1	1		3		6		8	1	
2														GAPOUT CP2	1	LAG F2 CP 2	1		3		6		8	2	
3	FZ3 GRN FCTR	18	18	18	18	18	18		18	18				GAPOUT CP3	1	LAG F2 CP 3		2	3		5			8	3
4	FZ4 GRN FCTR	15	15	15	15	15	15		15	15	PERM TIME			GAPOUT CP4	1	LAG F2 CP 4		2	3		5			8	4
5	FZ5 GRN FCTR	15	25	40	25	25	40		40	40	LAG OFFSET			GAPOUT CP5	1	LAG F2 CP 5	1		3			6		8	5
6											FORCE OFF			GAPOUT CP6	1	LAG F2 CP 6		2	3		5			8	6
7	FZ7 GRN FCTR	15	15	15	15	15	15		15	15	LONG GRN			GAPOUT CP7		LAG F2 CP 7									7
8	FZ8 GRN FCTR	44	44	44	44	44	44		44	44	NO GREEN			GAPOUT CP8	1	LAG F2 CP 8		2	3		5			8	8
9	MULTI CYCLE	0	0	0	0	0	0		0	0				GAPOUT CP9	1	LAG F2 CP 9		2	3		5			8	9
A	OFFSET A	69	68	37	76	75	51		38	47	OFFSET					LAG C COORD									A
B	OFFSET B	69	68	37	76	75	51		38	47						LAG D COORD									B
C	OFFSET C	69	68	37	76	75	51		38	47						COORD PHASES		2				6			C
D	FZ 3 EXT																								D
E	FZ 7 EXT	10	10	20	10	10	20		20	20															E
F	OFFSET INTRPT																								F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP SYSTEM MASTER:
 CO4 LAST CP RTE 76X @
 CO7 TRNSMT CP COLLEGE BLVD
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

FEATURE	OFF	ON	LOCATION	OFF	ON
1					
2					2
3					4
4					
5					
6					
7					
8					

COO = 6

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1	1								CP 1								
2	CP 2									CP 2	1								CP 2								
3	CP 3									CP 3				5					CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8				5					CP 8								
9	CP 9									CP 9	1	3	5	7	8				CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											

	E	FLAGS								F	FLAGS							
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8
0										CODE 4								
1										CODE 5								
2										C-RECALL								
3										D-RECALL								
4										EXCLUSIVE								
5										2 PED	2							
6										6 PED					6			
7										4 PED				4				
8										8 PED								8
9																		
A	OLA NOT									OLA ON								
B	OLB NOT									OLB ON								
C	OLC NOT									OLC ON								
D	OLD NOT									OLD ON								
E																		
F																		

LAST POWER FAILURE REGISTER

HOUR = D-A-E
 MINUTE = D-B-E
 DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES
 (CALL ACTIVE LIGHTS)
 RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES
 (CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOUR = D-A-F
 MINUTE = D-B-F
 DAY = D-C-F

D-E-E = CB VERSION NUMBER
 D-E-F = LITHIUM BATTERY CONDITION
 84 = BAD
 85 = GOOD

TIME OF DAY ACTIVITY TABLE												
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS												
	HR	MIN	ACT	ON/OFF	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	3	ON	1	2	3	4	5	6	7	
1	06	00	2	ON		2	3	4	5	6		
2	08	00	2			2	3	4	5	6		
3	21	00	3		1	2	3	4	5	6	7	
4												
5												
6												
7												
8												
9												
A												
B												
C	20	00	E	ON	1	2	3	4	5	6	7	
D	05	30	E			2	3	4	5	6		
E												
F	09	00	E		1						7	

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	1	A		2	3	4	5	6		
1												
2	09	00	2	A	1	2	3	4	5	6	7	
3	14	00	3	A	1	2	3	4	5	6	7	
4	14	45	9	A		2	3	4	5	6		
5	18	30	3	A		2	3	4	5	6		
6	20	00	E		1	2	3	4	5	6	7	
7												
8												
9												
A												
B												
C												
D												
E												
F												

8 ENERGIZE AUX OUTPUT-YELLOW

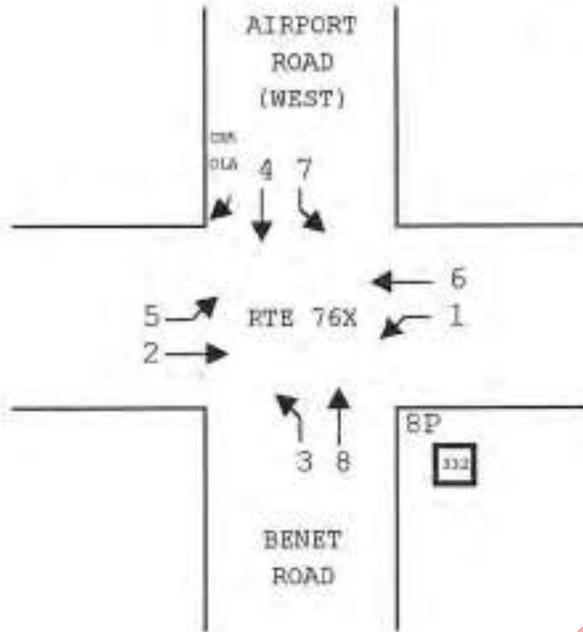
- 9 TIME OF DAY MAX RECALL (1ST SELECT)
- A TRAFFIC ACT. MAX 2 OPERATION
- B TIME OF DAY MAX RECALL (2ND SELECT)
- C YELLOW YIELD COORDINATION
- D YELLOW YIELD COORDINATION
- E TIME OF DAY FREE OPERATION
- F FLASHING OPERATION

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

DATE: 11/1/95

LOCATION: RTE 76 EXPRESSWAY @ BENET ROAD - AIRPORT ROAD (WEST)

CONFLICT MONITOR PROGRAM



Do not reproduce

INTERVAL	PHASE TIMING								9	PRE-EMPTION	S	F									
	1	2	3	4	5	6	7	8				FLAGS	1	2	3	4	5	6	7	8	
0 WALK	1	1	1	1	1	7	1	7	CLK RST	EV SEL	0	PERMIT	1	2	3	4	5	6	7	8	0
1 DONT WALK	1	1	1	1	1	33	1	39		RR1 CLR	15	RED LOCK				5					1
2 MIN GREEN	12	25	13	13	13	25	13	12		EVA DLY	0	YEL LOCK									2
3 TYPE 3 DET	0	255	0	0	0	255	0	0		EVA CLR	5	V RECALL		2			6				3
4 ADD/VEH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		EVB DLY	0	F RECALL									4
5 PASSAGE	1.5	6.3	1.5	1.5	1.5	6.3	1.5	1.5		EVB CLR	5	PED PHASES					6		8		5
6 MAX GAP	1.5	6.3	1.5	1.5	1.5	6.3	1.5	1.5		EVC DLY	0	RT OLA									6
7 MIN GAP	1.5	3.5	1.5	1.5	1.5	3.5	1.5	1.5		EVC CLR	5	RT CLB									7
8 MAX EXT	13	50	22	17	7	50	12	18		EVD DLY	0	DBL ENTRY			4				8		8
9 MAX 2		80				80		0	YR	EVD CLR	5	MAX 2 PHASES		2			6		8		9
A MAX 3									NO	MAX EV	255	LAG PHASES	READ ONLY								A
B									DAY	RR2 CLR	15	PED REST									B
C REDUCE BY	0.0	0.1	0.0	0.0	0.0	0.1	0.0	0.0	DOW			REST IN-WALK									C
D EVERY	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	HR			MAX 3 PHASES									D
E YELLOW	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1	MIN			YEL START UP		2			6				E
F RED	2.0	2.5	2.0	2.0	2.0	2.5	2.0	2.0	SEC			FIRST PHASE			3				7		F
15 PED XING FT						142		156					1	2	3	4	5	6	7	8	
BIKE XING FT	150	163	162	169	171	151	163	155													

FOC LONG FAILURE	
FOD SHORT FAILURE	
FOE	0
FOF	5
FC0	3
FC1	3
FC2	10
FCA	0.0
FCB	0.0
FCC	0.0
FCD	0.0

FDO TB SELECT	1
FD3 PED SELECT	0
FD4 7 WIRE	0
FD5 PERMISSIVE	0
FDB OS SEEKING	1

CO5 FLASH TYPE	1
CC2 DOWNLOAD	1

NOTES:

FZ 2 BIKE = 10 sec
 FZ 6 BIKE = 10 sec

ENTRIES IN THESE LOCATIONS CAN BE CHANGED IN CC1 FLASH ONLY



Do not reproduce

		CONTROL PLANS									Y-COORD			LAG PHASE	FLAGE									
		1	2	3	4	5	6	7	8	9		C	D	E	F	1	2	3	4	5	6	7	8	
0	CYCLE LENGTH	160	160	160	160	160	160		200	180					LAG FZ FREE		2		4		6		8	0
1	FZ1 GRN FCTR	15	15	15	15	15	15		15	15				GAPOUT CP1	1	LAG FZ CP 1	2		4	5			8	1
2														GAPOUT CP2	1	LAG FZ CP 2	2		4	5			8	2
3	FZ3 GRN FCTR	16	16	16	16	16	16		16	16				GAPOUT CP3	1	LAG FZ CP 3	2		4	5			8	3
4	FZ4 GRN FCTR	14	14	14	14	14	14		14	14	PERM TIME			GAPOUT CP4	1	LAG FZ CP 4	1		4		6		8	4
5	FZ5 GRN FCTR	15	15	15	15	15	15		15	15	LAG OFFSET			GAPOUT CP5	1	LAG FZ CP 5	2		4	5			8	5
6											FORCE OFF			GAPOUT CP6	1	LAG FZ CP 6	1		4		6		8	6
7	FZ7 GRN FCTR	13	13	13	13	13	13		13	13	LONG GRN			GAPOUT CP7		LAG FZ CP 7								7
8	FZ8 GRN FCTR	46	46	46	46	46	46		46	46	NO GREEN			GAPOUT CP8	1	LAG FZ CP 8	1		4		6		8	8
9	MULTI CYCLE	0	0	0	0	0	0		0	0				GAPOUT CP9	1	LAG FZ CP 9	1		4		6		8	9
A	OFFSET A	53	69	120	9	75	148		135	151	OFFSET					LAG C COORD								A
B	OFFSET B	53	69	120	9	75	148		135	151						LAG D COORD								B
C	OFFSET C	53	69	120	9	75	148		135	151						COORD PHASES	2				6			C
D	FZ 3 EXT																							D
E	FZ 7 EXT	3	3	3	3	3	3		3	3														E
F	OFFSET INTRPT																							F

CO1 MANUAL CP
 CO2 MASTER CP
 CO3 CURRENT CP **SYSTEM MASTER:**
 CO4 LAST CP **RTE 76 X @**
 CO7 TRNSMT CP **COLLEGE BLVD**
 COD MANUAL OFFSET
 CAO LOCAL CYCLE TIMER
 CBO MASTER CYCLE TIMER
 CAA LOCAL OFFSET
 CBA MASTER OFFSET

FEATURE	OFF	ON	LOCATION	OFF	ON
1			1		
2			2		
3			3		
4			4		B
5			5		
6			6		
7			7		
8			8		

COO - 8

CCB/CDB OFFSET TIMER
 CCC/CDC LAG GREEN TIMER
 CCD/CDD FORCE OFF TIMER
 CCE/CDE LONG GREEN TIMER
 CCF/CDF NO GREEN TIMER

	D	FLAGS								E	FLAGS								F	FLAGS							
	MAX	1	2	3	4	5	6	7	8	MIN	1	2	3	4	5	6	7	8	PED	1	2	3	4	5	6	7	8
0	RCL									RCL									RCL								
1	CP 1									CP 1									CP 1								
2	CP 2									CP 2									CP 2								
3	CP 3									CP 3									CP 3								
4	CP 4									CP 4									CP 4								
5	CP 5									CP 5									CP 5								
6	CP 6									CP 6									CP 6								
7	CP 7									CP 7									CP 7								
8	CP 8									CP 8	1								CP 8								
9	CP 9									CP 9	1								CP 9								
A																			RCL 1								
B																			RCL 2								
C																											
D																											
E																											
F																											

	E	FLAGS								F	FLAGS								
	FUNCTION	1	2	3	4	5	6	7	8	FUNCTION	1	2	3	4	5	6	7	8	
0										CODE 4									0
1										CODE 5									1
2										C-RECALL									2
3										D-RECALL									3
4										EXCLUSIVE									4
5										2 PED	2								5
6										6 PED						6			6
7										4 PED			4						7
8										8 PED								8	8
9																			9
A	OLA NOT									OLA ON									A
B	OLE NOT									OLE ON									B
C	OLC NOT									OLC ON									C
D	OLD NOT									OLD ON									D
E																			E
F																			F

LAST POWER FAILURE REGISTER

HOUR = D-A-E
 MINUTE = D-B-E
 DAY = D-C-E

RCL 1 = TIME OF DAY MAX RECALL (1ST SELECT) PHASES
 (CALL ACTIVE LIGHTS)
 RCL 2 = TIME OF DAY MAX RECALL (2ND SELECT) PHASES
 (CALL ACTIVE LIGHTS)

LAST FLASH TIME REGISTER

HOUR = D-A-F
 MINUTE = D-B-F
 DAY = D-C-F

D-E-E = C8 VERSION NUMBER
 D-E-F = LITHIUM BATTERY CONDITION
 84 = BAD
 85 = GOOD

TIME OF DAY ACTIVITY TABLE											
7+EVENT+HR+MIN+ACT+"E"+ON/OFF+DOW LTS											
	HR	MIN	ACT	ON/OFF	S	M	T	W	T	F	S
					1	2	3	4	5	6	7
0	05	30	2	ON	1	2	3	4	5	6	7
1	21	00	2		1	2	3	4	5	6	7
2											
3											
4											
5											
6											
7											
8											
9											
A											
B											
C	20	00	E	ON	1	2	3	4	5	6	7
D	05	30	E			2	3	4	5	6	
E											
F	09	00	E		1						7

ACTIVITY CODE

- 1 TYPE OF MAX TERMINATION
- 2 MAX 2
- 3 MAX 3
- 4 COND SERV (1ST SELECT)
- 5 COND SERV (2ND SELECT)
- 6 ENERGIZE AUX OUTPUT-RED
- 7 ENERGIZE AUX OUTPUT-GREEN

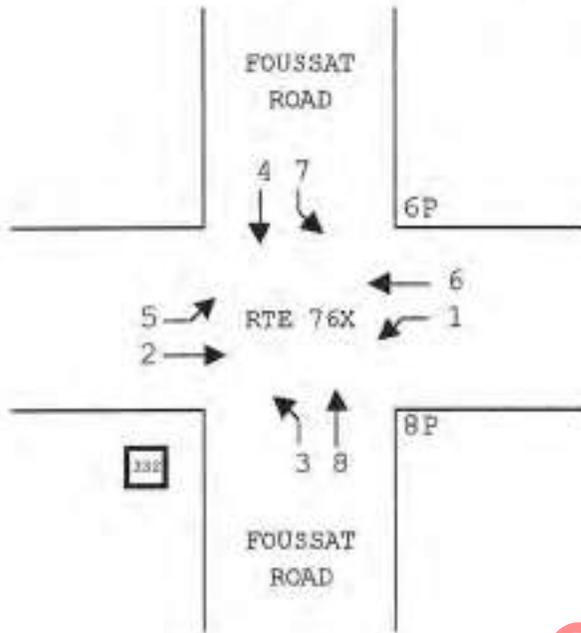
CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0	05	30	1	A		2	3	4	5	6		
1												
2	09	00	2	A	1	2	3	4	5	6	7	
3	14	00	3	A	1	2	3	4	5	6	7	
4	14	45	9	A		2	3	4	5	6		
5	18	30	3	A		2	3	4	5	6		
6	20	00	E		1	2	3	4	5	6	7	
7												
8												
9												
A												
B												
C												
D												
E												
F												

ACTIVITY CODE

- 8 ENERGIZE AUX OUTPUT-YELLOW
- 9 TIME OF DAY MAX RECALL (1ST SELECT)
- A TRAFFIC ACT. MAX 2 OPERATION
- B TIME OF DAY MAX RECALL (2ND SELECT)
- C YELLOW YIELD COORDINATION
- D YELLOW YIELD COORDINATION
- E TIME OF DAY FREE OPERATION
- F FLASHING OPERATION

CONTROL PLAN TIME OF DAY												
9+EVENT+HR+MIN+CP+OS+E+DOW												
	HR	MIN	CP	OS	S	M	T	W	T	F	S	
					1	2	3	4	5	6	7	
0												
1												
2												
3												
4												
5												
6												
7												
8												
9												
A												
B												
C												
D												
E												
F												

CONFLICT MONITOR PROGRAM



Do not reproduce

Appendix F

Existing Intersection LOS Worksheets

AM Existing
1: SR-76 & I-5 SB Ramp

Timings



Lane Group	NBT	NBR	SBL	SBT
Lane Configurations	↑↑	↑	↑↑	
Traffic Volume (vph)	280	215	1060	445
Future Volume (vph)	280	215	1060	445
Turn Type	NA	Perm	Prot	NA
Protected Phases	2		1	6
Permitted Phases		2		
Detector Phase	2	2	1	6
Switch Phase				
Minimum Initial (s)	5.0	5.0	5.0	4.0
Minimum Split (s)	22.8	22.8	10.7	20.0
Total Split (s)	31.0	31.0	49.0	80.0
Total Split (%)	38.8%	38.8%	61.3%	100.0%
Yellow Time (s)	4.8	4.8	3.7	3.5
All-Red Time (s)	2.0	2.0	2.0	0.5
Lost Time Adjust (s)	0.0	0.0	0.0	
Total Lost Time (s)	6.8	6.8	5.7	
Lead/Lag	Lag	Lag	Lead	
Lead-Lag Optimize?	Yes	Yes	Yes	
Recall Mode	Min	Min	None	Min
Act Effct Green (s)	32.4	32.4	35.1	0.0
Actuated g/C Ratio	0.40	0.40	0.44	0.00
v/c Ratio	0.21	0.35	0.76	no cap
Control Delay	17.4	15.1	22.2	
Queue Delay	0.0	0.0	0.0	
Total Delay	17.4	15.1	22.2	Error
LOS	B	B	C	F
Approach Delay	16.4			Err
Approach LOS	B			F

Intersection Summary

Cycle Length: 80	
Actuated Cycle Length: 80	
Natural Cycle: 50	
Control Type: Actuated-Uncoordinated	
Maximum v/c Ratio: Err	
Intersection Signal Delay: Err	Intersection LOS: F
Intersection Capacity Utilization 39.5%	ICU Level of Service A
Analysis Period (min) 15	

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

AM Existing
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↑	↑↑	
Traffic Volume (veh/h)	0	0	280	215	1060	445
Future Volume (veh/h)	0	0	280	215	1060	445
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			304	234	1152	484
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			862	384	1562	0
Arrive On Green			0.24	0.24	0.45	0.83
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			304	234	1152	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			2.9	5.4	11.2	0.0
Cycle Q Clear(g_c), s			2.9	5.4	11.2	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			862	384	1562	0
V/C Ratio(X)			0.35	0.61	0.74	0.00
Avail Cap(c_a), veh/h			2102	938	3657	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			12.8	13.8	9.2	0.0
Incr Delay (d2), s/veh			0.2	1.6	0.7	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.0	1.7	3.0	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			13.1	15.3	9.9	0.0
LnGrp LOS			B	B	A	A
Approach Vol, veh/h			538			1152
Approach Delay, s/veh			14.1			9.9
Approach LOS			B			A
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	24.2	16.7				40.9
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	13.2	7.4				0.0
Green Ext Time (p_c), s	5.3	2.6				0.0

Intersection Summary

HCM 6th Ctrl Delay			11.2			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing
2: SR-76 & I-5 NB Ramp

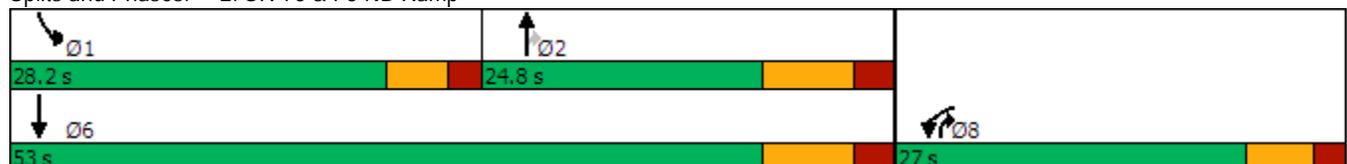
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	81	540	89	881	1381
Future Volume (vph)	81	540	89	881	1381
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	0.99	0.79	0.11	0.99	0.75
Control Delay	42.9	39.0	7.4	57.7	16.4
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	42.9	39.0	7.4	57.7	16.4
LOS	D	D	A	E	B
Approach Delay	42.9	34.6			32.5
Approach LOS	D	C			C

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 75
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 0.99
 Intersection Signal Delay: 34.9
 Intersection LOS: C
 Intersection Capacity Utilization 99.0%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Existing
2: SR-76 & I-5 NB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	81	615	540	89	881	1381
Future Volume (veh/h)	81	615	540	89	881	1381
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1900	1870	1870	1870	1870
Adj Flow Rate, veh/h	88	668	587	97	958	1501
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	0	2	2	2	2
Cap, veh/h	0	0	1436	458	1166	3017
Arrive On Green	0.00	0.00	0.40	0.40	0.34	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	587	97	958	1501
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	6.2	2.5	13.5	5.8
Cycle Q Clear(g_c), s	0.0	0.0	6.2	2.5	13.5	5.8
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1436	458	1166	3017
V/C Ratio(X)	0.00	0.00	0.41	0.21	0.82	0.50
Avail Cap(c_a), veh/h	0	0	1436	458	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	11.3	14.3	16.1	1.0
Incr Delay (d2), s/veh	0.0	0.0	0.9	1.1	3.1	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	2.2	0.7	5.1	0.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	12.1	15.3	19.2	1.6
LnGrp LOS	A	A	B	B	B	A
Approach Vol, veh/h	0		684			2459
Approach Delay, s/veh	0.0		12.6			8.5
Approach LOS			B			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	23.6	29.4			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	15.5	8.2			7.8	0.0
Green Ext Time (p_c), s	2.4	2.8			16.3	0.0

Intersection Summary

HCM 6th Ctrl Delay		9.4
HCM 6th LOS		A

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing
3: Loretta St & SR-76

Timings

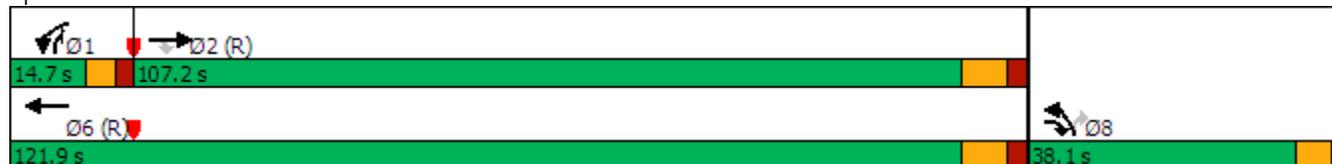


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	1110	23	12	2240	43	26
Future Volume (vph)	1110	23	12	2240	43	26
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effect Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	0.53	0.02	0.13	0.97	0.13	0.06
Control Delay	17.5	0.5	66.0	36.5	53.9	12.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	17.5	0.5	66.0	36.5	53.9	12.9
LOS	B	A	E	D	D	B
Approach Delay	17.2			36.6	38.6	
Approach LOS	B			D	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.97
 Intersection Signal Delay: 30.3
 Intersection Capacity Utilization 82.0%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service E

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

AM Existing
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (veh/h)	1110	23	12	2240	43	26
Future Volume (veh/h)	1110	23	12	2240	43	26
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1207	25	13	2435	47	28
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2315	1350	44	2530	356	356
Arrive On Green	0.65	0.65	0.02	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1207	25	13	2435	47	28
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	28.7	0.4	1.1	100.3	3.5	2.2
Cycle Q Clear(g_c), s	28.7	0.4	1.1	100.3	3.5	2.2
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2315	1350	44	2530	356	356
V/C Ratio(X)	0.52	0.02	0.30	0.96	0.13	0.08
Avail Cap(c_a), veh/h	2315	1350	100	2530	356	356
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.45	0.45	1.00	1.00
Uniform Delay (d), s/veh	14.7	1.8	76.7	21.1	52.6	49.0
Incr Delay (d2), s/veh	0.8	0.0	1.7	6.1	0.8	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.8	0.3	0.6	40.5	1.7	0.9
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	15.6	1.8	78.3	27.2	53.4	49.4
LnGrp LOS	B	A	E	C	D	D
Approach Vol, veh/h	1232			2448	75	
Approach Delay, s/veh	15.3			27.4	51.9	
Approach LOS	B			C	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	9.6	112.3			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.1	30.7			102.3	5.5
Green Ext Time (p_c), s	0.0	13.2			10.7	0.2

Intersection Summary

HCM 6th Ctrl Delay	23.9
HCM 6th LOS	C

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing
4: Canyon Dr & SR-76

Timings

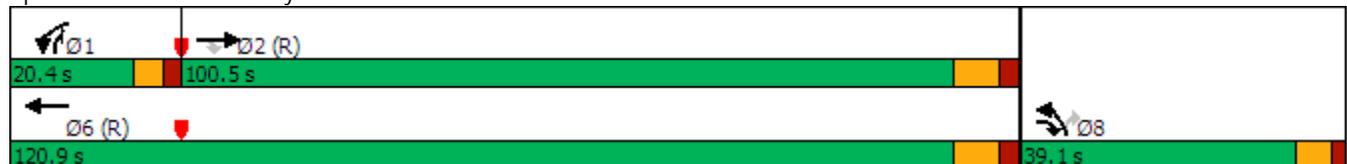


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↘	↗
Traffic Volume (vph)	1138	14	74	2249	20	75
Future Volume (vph)	1138	14	74	2249	20	75
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.5	39.1	20.4	120.9	39.1	20.4
Total Split (%)	62.8%	24.4%	12.8%	75.6%	24.4%	12.8%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effect Green (s)	113.9	135.7	12.5	132.1	13.8	32.4
Actuated g/C Ratio	0.71	0.85	0.08	0.83	0.09	0.20
v/c Ratio	0.49	0.01	0.58	0.84	0.07	0.22
Control Delay	21.0	0.9	59.7	17.6	63.3	16.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	21.0	0.9	59.7	17.6	63.3	16.6
LOS	C	A	E	B	E	B
Approach Delay	20.7			18.9	26.5	
Approach LOS	C			B	C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20.4 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.84
 Intersection Signal Delay: 19.7
 Intersection Capacity Utilization 81.4%
 Analysis Period (min) 15
 Intersection LOS: B
 ICU Level of Service D

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

AM Existing
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↗↖	↗
Traffic Volume (veh/h)	1138	14	74	2249	20	75
Future Volume (veh/h)	1138	14	74	2249	20	75
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1237	15	80	2445	22	82
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2677	1293	108	3020	215	195
Arrive On Green	0.75	0.75	0.06	0.85	0.06	0.06
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1237	15	80	2445	22	82
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	21.1	0.3	7.1	53.0	1.0	7.7
Cycle Q Clear(g_c), s	21.1	0.3	7.1	53.0	1.0	7.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2677	1293	108	3020	215	195
V/C Ratio(X)	0.46	0.01	0.74	0.81	0.10	0.42
Avail Cap(c_a), veh/h	2677	1293	164	3020	713	423
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.84	0.84	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	7.5	2.7	73.9	5.8	70.8	64.9
Incr Delay (d2), s/veh	0.5	0.0	0.9	0.2	0.2	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	7.8	0.1	3.3	15.0	0.4	3.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	7.9	2.8	74.8	6.0	71.0	66.4
LnGrp LOS	A	A	E	A	E	E
Approach Vol, veh/h	1252			2525	104	
Approach Delay, s/veh	7.9			8.2	67.3	
Approach LOS	A			A	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	15.4	128.5			144.0	16.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	9.1	23.1			55.0	9.7
Green Ext Time (p_c), s	0.1	13.7			43.7	0.3

Intersection Summary

HCM 6th Ctrl Delay			9.7			
HCM 6th LOS			A			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing
5: Benet Rd & SR-76

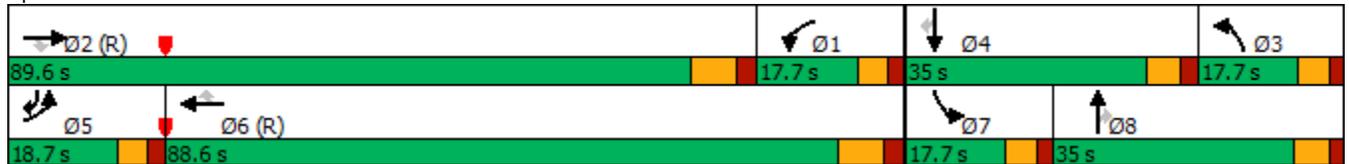
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	98	1023	129	30	2022	7	50	4	46	8	12	248
Future Volume (vph)	98	1023	129	30	2022	7	50	4	46	8	12	248
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	35.0	35.0	17.7	20.0	18.7
Total Split (s)	18.7	89.6	89.6	17.7	88.6	88.6	17.7	35.0	35.0	17.7	35.0	18.7
Total Split (%)	11.7%	56.0%	56.0%	11.1%	55.4%	55.4%	11.1%	21.9%	21.9%	11.1%	21.9%	11.7%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	None	None	None	Min	None
Act Effct Green (s)	14.5	96.7	96.7	12.0	90.7	90.7	18.3	31.5	31.5	12.0	11.0	25.9
Actuated g/C Ratio	0.09	0.60	0.60	0.08	0.57	0.57	0.11	0.20	0.20	0.08	0.07	0.16
v/c Ratio	0.67	0.52	0.14	0.25	1.10	0.01	0.55	0.01	0.11	0.07	0.10	0.74
Control Delay	89.5	31.0	11.9	78.8	81.3	0.0	75.9	52.0	0.5	70.2	71.9	35.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	89.5	31.0	11.9	78.8	81.3	0.0	75.9	52.0	0.5	70.2	71.9	35.9
LOS	F	C	B	E	F	A	E	D	A	E	E	D
Approach Delay		33.6			81.0			52.5			38.6	
Approach LOS		C			F			D			D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 145
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.10
 Intersection Signal Delay: 60.9
 Intersection LOS: E
 Intersection Capacity Utilization 97.4%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Existing
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	98	1023	129	30	2022	7	50	4	46	8	12	248
Future Volume (veh/h)	98	1023	129	30	2022	7	50	4	46	8	12	248
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	107	1112	140	33	2198	8	111	4	50	9	13	270
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	143	1812	808	175	1925	859	133	362	307	44	264	352
Arrive On Green	0.08	1.00	0.51	0.10	1.00	0.54	0.07	0.19	0.19	0.02	0.14	0.24
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	107	1112	140	33	2198	8	111	4	50	9	13	270
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	9.4	0.0	7.6	2.7	0.0	0.4	9.8	0.3	3.2	0.8	1.0	20.1
Cycle Q Clear(g_c), s	9.4	0.0	7.6	2.7	0.0	0.4	9.8	0.3	3.2	0.8	1.0	20.1
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	143	1812	808	175	1925	859	133	362	307	44	264	352
V/C Ratio(X)	0.75	0.61	0.17	0.19	1.14	0.01	0.84	0.01	0.16	0.20	0.05	0.77
Avail Cap(c_a), veh/h	145	1812	808	175	1925	859	134	362	307	134	338	414
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.88	0.88	0.88	0.22	0.22	0.22	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.0	0.0	21.1	66.3	0.0	16.9	73.1	52.1	31.8	76.5	59.4	31.3
Incr Delay (d2), s/veh	16.7	1.4	0.4	0.1	65.3	0.0	34.7	0.0	0.2	2.3	0.1	7.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.0	0.3	3.0	1.3	17.5	0.1	5.8	0.1	1.7	0.4	0.5	7.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	88.7	1.4	21.5	66.4	65.3	16.9	107.8	52.2	32.0	78.7	59.5	38.5
LnGrp LOS	F	A	C	E	F	B	F	D	C	E	E	D
Approach Vol, veh/h		1359			2239			165			292	
Approach Delay, s/veh		10.3			65.2			83.5			40.6	
Approach LOS		B			E			F			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	23.7	89.6	18.0	28.7	18.6	94.7	9.7	37.1				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 82	12.0	* 29	* 13	80.6	* 12	28.9				
Max Q Clear Time (g_c+I1), s	4.7	9.6	11.8	22.1	11.4	2.4	2.8	5.2				
Green Ext Time (p_c), s	0.0	12.3	0.0	0.6	0.0	44.7	0.0	0.1				

Intersection Summary

HCM 6th Ctrl Delay	45.8
HCM 6th LOS	D

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	
Traffic Volume (veh/h)	33	971	53	143	1875	71	116	54	84	148	85	43
Future Volume (veh/h)	33	971	53	143	1875	71	116	54	84	148	85	43
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	36	1055	58	155	2038	77	126	59	91	161	92	47
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	224	2145	957	259	2180	973	280	414	128	281	289	227
Arrive On Green	0.13	1.00	1.00	0.07	0.61	0.61	0.08	0.08	0.08	0.08	0.08	0.08
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	36	1055	58	155	2038	77	126	59	91	161	92	47
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.5	0.0	0.0	7.0	83.1	3.2	5.6	1.7	9.0	7.2	3.9	2.5
Cycle Q Clear(g_c), s	1.5	0.0	0.0	7.0	83.1	3.2	5.6	1.7	9.0	7.2	3.9	2.5
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	224	2145	957	259	2180	973	280	414	128	281	289	227
V/C Ratio(X)	0.16	0.49	0.06	0.60	0.93	0.08	0.45	0.14	0.71	0.57	0.32	0.21
Avail Cap(c_a), veh/h	281	2145	957	272	2180	973	281	1468	456	281	1022	802
HCM Platoon Ratio	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.87	0.87	0.87	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	65.8	0.0	0.0	71.7	28.0	12.6	70.1	68.4	71.7	70.8	69.3	68.7
Incr Delay (d2), s/veh	0.3	0.7	0.1	3.3	9.0	0.2	1.1	0.2	7.0	2.8	0.6	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.7	0.2	0.0	3.2	36.9	1.2	2.5	0.8	3.9	3.3	1.8	0.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	66.0	0.7	0.1	75.0	37.0	12.7	71.3	68.5	78.7	73.7	70.0	69.1
LnGrp LOS	E	A	A	E	D	B	E	E	E	E	E	E
Approach Vol, veh/h		1149			2270			276			300	
Approach Delay, s/veh		2.7			38.8			73.1			71.8	
Approach LOS		A			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	104.6	18.7	19.1	16.1	106.2	18.7	19.1				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	62.9	* 13	46.0	* 13	62.5	* 13	46.0				
Max Q Clear Time (g_c+I1), s	9.0	2.0	7.6	5.9	3.5	85.1	9.2	11.0				
Green Ext Time (p_c), s	0.1	10.7	0.2	0.8	0.0	0.0	0.2	0.7				
Intersection Summary												
HCM 6th Ctrl Delay				33.3								
HCM 6th LOS				C								
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

AM Existing
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	1.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	26	16	77	21	5	269
Future Vol, veh/h	26	16	77	21	5	269
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	28	17	84	23	5	292

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	223	96	0	0	107	0
Stage 1	96	-	-	-	-	-
Stage 2	127	-	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	753	960	-	-	1483	-
Stage 1	891	-	-	-	-	-
Stage 2	846	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	750	960	-	-	1483	-
Mov Cap-2 Maneuver	750	-	-	-	-	-
Stage 1	891	-	-	-	-	-
Stage 2	843	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.7	0	0.1
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	818	1483
HCM Lane V/C Ratio	-	-	0.056	0.004
HCM Control Delay (s)	-	-	9.7	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Existing
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	1	104	1	1	253
Future Vol, veh/h	1	1	104	1	1	253
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	1	113	1	1	275

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	391	114	0	0	114
Stage 1	114	-	-	-	-
Stage 2	277	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	613	939	-	-	1475
Stage 1	911	-	-	-	-
Stage 2	770	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	612	939	-	-	1475
Mov Cap-2 Maneuver	612	-	-	-	-
Stage 1	911	-	-	-	-
Stage 2	769	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.9	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	741	1475
HCM Lane V/C Ratio	-	-	0.003	0.001
HCM Control Delay (s)	-	-	9.9	7.4
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0	0

LOS Engineering, Inc.

Intersection						
Int Delay, s/veh	0.4					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	2	5	10	164	268	3
Future Vol, veh/h	2	5	10	164	268	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	200	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	5	11	178	291	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	493	293	294	0	-	0
Stage 1	293	-	-	-	-	-
Stage 2	200	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	535	746	1268	-	-	-
Stage 1	757	-	-	-	-	-
Stage 2	834	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	530	746	1268	-	-	-
Mov Cap-2 Maneuver	530	-	-	-	-	-
Stage 1	750	-	-	-	-	-
Stage 2	834	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.5	0.5	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1268	-	668	-	-
HCM Lane V/C Ratio	0.009	-	0.011	-	-
HCM Control Delay (s)	7.9	-	10.5	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

PM Existing
1: SR-76 & I-5 SB Ramp

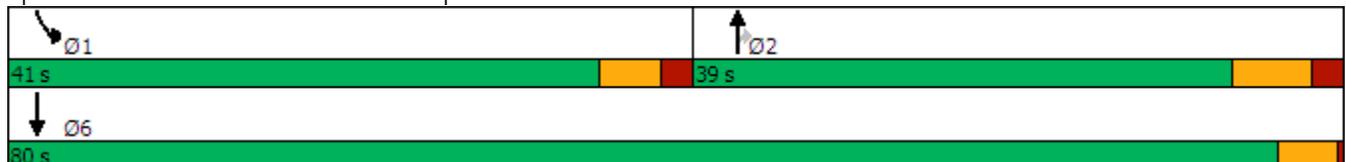
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	460	241	675	677
Future Volume (vph)	0	460	241	675	677
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	44.2	44.2	23.3	0.0
Actuated g/C Ratio	0.00	0.55	0.55	0.29	0.00
v/c Ratio	no cap	0.26	0.28	0.74	no cap
Control Delay		10.5	6.7	29.9	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	10.5	6.7	29.9	Error
LOS	F	B	A	C	F
Approach Delay	Err	9.2			Err
Approach LOS	F	A			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 40
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection Capacity Utilization 41.1%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service A

Splits and Phases: 1: SR-76 & I-5 SB Ramp



PM Existing
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↗↗	
Traffic Volume (veh/h)	0	686	460	241	675	677
Future Volume (veh/h)	0	686	460	241	675	677
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			500	262	734	736
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1139	508	1115	0
Arrive On Green			0.32	0.32	0.32	0.81
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			500	262	734	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			3.9	4.7	6.4	0.0
Cycle Q Clear(g_c), s			3.9	4.7	6.4	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1139	508	1115	0
V/C Ratio(X)			0.44	0.52	0.66	0.00
Avail Cap(c_a), veh/h			3266	1457	3482	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			9.4	9.7	10.2	0.0
Incr Delay (d2), s/veh			0.3	0.8	0.7	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.1	1.3	1.8	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			9.7	10.5	10.9	0.0
LnGrp LOS			A	B	B	A
Approach Vol, veh/h			762			734
Approach Delay, s/veh			10.0			10.9
Approach LOS			A			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	17.0	18.0				35.0
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	8.4	6.7				0.0
Green Ext Time (p_c), s	2.9	4.5				0.0

Intersection Summary

HCM 6th Ctrl Delay			10.4			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing
2: SR-76 & I-5 NB Ramp

Timings

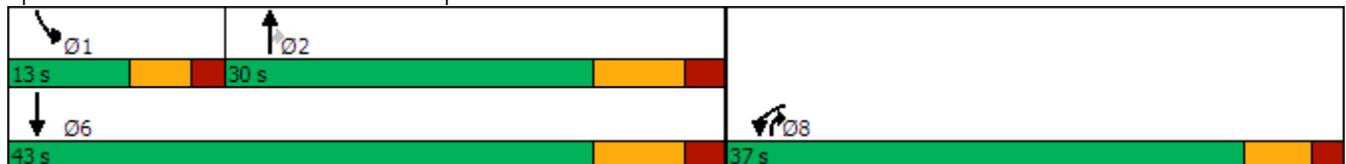
	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↘	↑↑	↘	↘↘	↑↑
Traffic Volume (vph)	235	1054	101	395	1053
Future Volume (vph)	235	1054	101	395	1053
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.45	1.18	0.09	1.37	0.74
Control Delay	231.5	119.8	2.6	217.5	22.3
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	231.5	119.8	2.6	217.5	22.3
LOS	F	F	A	F	C
Approach Delay	231.5	109.5			75.5
Approach LOS	F	F			E

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 130
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.45
 Intersection Signal Delay: 131.4
 Intersection Capacity Utilization 121.0%
 Analysis Period (min) 15

Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

PM Existing
2: SR-76 & I-5 NB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	235	828	1054	101	395	1053
Future Volume (veh/h)	235	828	1054	101	395	1053
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1900	1870	1870	1870	1870
Adj Flow Rate, veh/h	255	900	1146	110	429	1145
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	0	2	2	2	2
Cap, veh/h	0	0	1836	594	570	2893
Arrive On Green	0.00	0.00	0.52	0.52	0.16	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1146	110	429	1145
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	9.9	2.0	5.1	3.8
Cycle Q Clear(g_c), s	0.0	0.0	9.9	2.0	5.1	3.8
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1836	594	570	2893
V/C Ratio(X)	0.00	0.00	0.62	0.19	0.75	0.40
Avail Cap(c_a), veh/h	0	0	1836	594	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	7.4	9.0	17.1	1.1
Incr Delay (d2), s/veh	0.0	0.0	1.6	0.7	5.3	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	2.8	0.5	2.2	0.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	9.0	9.7	22.4	1.5
LnGrp LOS	A	A	A	A	C	A
Approach Vol, veh/h	0		1256			1574
Approach Delay, s/veh	0.0		9.1			7.2
Approach LOS			A			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.8	30.2			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	7.1	11.9			5.8	0.0
Green Ext Time (p_c), s	0.0	5.8			10.1	0.0

Intersection Summary

HCM 6th Ctrl Delay		8.0
HCM 6th LOS		A

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing
3: Loretta St & SR-76

Timings



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙	↗
Traffic Volume (vph)	1876	73	20	1409	25	12
Future Volume (vph)	1876	73	20	1409	25	12
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	105.1	147.5	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.66	0.92	0.06	0.71	0.20	0.29
v/c Ratio	0.88	0.05	0.22	0.61	0.08	0.03
Control Delay	29.0	0.3	87.1	7.6	52.9	18.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	29.0	0.3	87.1	7.6	52.9	18.6
LOS	C	A	F	A	D	B
Approach Delay	28.0			8.7	41.7	
Approach LOS	C			A	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.88
 Intersection Signal Delay: 20.1
 Intersection Capacity Utilization 71.9%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service C

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

PM Existing
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵	↵
Traffic Volume (veh/h)	1876	73	20	1409	25	12
Future Volume (veh/h)	1876	73	20	1409	25	12
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2039	79	22	1532	27	13
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2278	1333	63	2530	356	373
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2039	79	22	1532	27	13
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	77.3	1.3	1.9	34.9	2.0	1.0
Cycle Q Clear(g_c), s	77.3	1.3	1.9	34.9	2.0	1.0
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2278	1333	63	2530	356	373
V/C Ratio(X)	0.89	0.06	0.35	0.61	0.08	0.03
Avail Cap(c_a), veh/h	2278	1333	100	2530	356	373
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.84	0.84	1.00	1.00
Uniform Delay (d), s/veh	24.2	2.1	75.4	11.7	52.0	47.2
Incr Delay (d2), s/veh	6.0	0.1	2.8	0.9	0.4	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	33.2	1.1	0.9	13.7	0.9	0.4
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	30.1	2.2	78.2	12.6	52.4	47.4
LnGrp LOS	C	A	E	B	D	D
Approach Vol, veh/h	2118			1554	40	
Approach Delay, s/veh	29.1			13.5	50.8	
Approach LOS	C			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	11.3	110.6			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.9	79.3			36.9	4.0
Green Ext Time (p_c), s	0.0	15.9			20.7	0.1

Intersection Summary

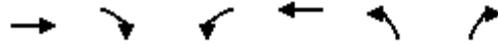
HCM 6th Ctrl Delay	22.8
HCM 6th LOS	C

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing
4: Canyon Dr & SR-76

Timings

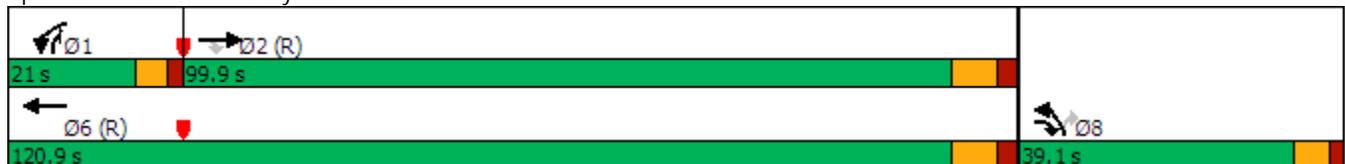


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙↙	↙
Traffic Volume (vph)	1826	48	136	1416	7	91
Future Volume (vph)	1826	48	136	1416	7	91
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	99.9	39.1	21.0	120.9	39.1	21.0
Total Split (%)	62.4%	24.4%	13.1%	75.6%	24.4%	13.1%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effct Green (s)	108.4	130.2	18.0	132.1	13.8	37.9
Actuated g/C Ratio	0.68	0.81	0.11	0.83	0.09	0.24
v/c Ratio	0.83	0.04	0.75	0.53	0.03	0.26
Control Delay	36.5	2.4	60.5	14.7	61.0	44.6
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	36.5	2.4	60.5	14.7	61.0	44.6
LOS	D	A	E	B	E	D
Approach Delay	35.6			18.7	45.8	
Approach LOS	D			B	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.83
 Intersection Signal Delay: 28.5
 Intersection Capacity Utilization 82.8%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service E

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Existing
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑↑	↑
Traffic Volume (veh/h)	1826	48	136	1416	7	91
Future Volume (veh/h)	1826	48	136	1416	7	91
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1985	52	148	1539	8	99
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2530	1239	168	2992	242	261
Arrive On Green	0.71	0.71	0.09	0.84	0.07	0.07
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1985	52	148	1539	8	99
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	58.3	1.2	13.1	19.3	0.3	8.9
Cycle Q Clear(g_c), s	58.3	1.2	13.1	19.3	0.3	8.9
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2530	1239	168	2992	242	261
V/C Ratio(X)	0.78	0.04	0.88	0.51	0.03	0.38
Avail Cap(c_a), veh/h	2530	1239	170	2992	713	477
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.37	0.37	0.52	0.52	1.00	1.00
Uniform Delay (d), s/veh	15.0	3.9	71.5	3.5	69.4	59.6
Incr Delay (d2), s/veh	1.0	0.0	22.7	0.3	0.1	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	22.7	0.5	7.1	5.7	0.2	3.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	16.0	4.0	94.3	3.9	69.4	60.5
LnGrp LOS	B	A	F	A	E	E
Approach Vol, veh/h	2037			1687	107	
Approach Delay, s/veh	15.7			11.8	61.2	
Approach LOS	B			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	20.8	121.9			142.7	17.3
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	91.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	15.1	60.3			21.3	10.9
Green Ext Time (p_c), s	0.0	22.0			21.3	0.3

Intersection Summary

HCM 6th Ctrl Delay	15.2
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	180	1711	88	35	1307	12	129	21	91	13	9	117
Future Volume (veh/h)	180	1711	88	35	1307	12	129	21	91	13	9	117
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	196	1860	96	38	1421	13	140	23	99	14	10	127
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	2143	956	109	1985	885	162	265	225	62	160	303
Arrive On Green	0.11	0.60	0.60	0.06	0.56	0.56	0.09	0.14	0.14	0.03	0.09	0.09
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	196	1860	96	38	1421	13	140	23	99	14	10	127
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	69.8	4.1	3.3	47.1	0.6	12.4	1.7	9.1	1.2	0.8	11.3
Cycle Q Clear(g_c), s	16.9	69.8	4.1	3.3	47.1	0.6	12.4	1.7	9.1	1.2	0.8	11.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	2143	956	109	1985	885	162	265	225	62	160	303
V/C Ratio(X)	1.04	0.87	0.10	0.35	0.72	0.01	0.86	0.09	0.44	0.23	0.06	0.42
Avail Cap(c_a), veh/h	188	2143	956	134	1985	885	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.50	0.50	0.50	0.77	0.77	0.77	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	26.5	13.4	72.1	26.0	15.7	71.7	59.7	62.9	75.1	67.3	56.9
Incr Delay (d2), s/veh	57.7	2.6	0.1	1.5	1.7	0.0	19.0	0.1	1.4	1.8	0.2	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	10.8	29.6	1.5	1.6	20.3	0.2	6.6	0.8	3.8	0.6	0.4	4.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	129.2	29.1	13.5	73.5	27.7	15.7	90.7	59.8	64.2	77.0	67.4	57.8
LnGrp LOS	F	C	B	E	C	B	F	E	E	E	E	E
Approach Vol, veh/h		2152			1472			262			151	
Approach Delay, s/veh		37.5			28.8			78.0			60.2	
Approach LOS		D			C			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.5	104.5	20.3	19.8	22.6	97.4	11.3	28.8				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.3	71.8	14.4	13.3	18.9	49.1	3.2	11.1				
Green Ext Time (p_c), s	0.0	0.0	0.2	0.4	0.0	8.0	0.0	0.4				

Intersection Summary

HCM 6th Ctrl Delay	37.8
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  				
Traffic Volume (veh/h)	27	1723	127	98	1236	102	78	124	136	56	70	26
Future Volume (veh/h)	27	1723	127	98	1236	102	78	124	136	56	70	26
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	29	1873	138	107	1343	111	85	135	148	61	76	28
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	203	2053	916	257	2108	940	274	575	178	262	388	304
Arrive On Green	0.06	0.58	0.58	0.07	0.59	0.59	0.08	0.11	0.11	0.08	0.11	0.11
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	29	1873	138	107	1343	111	85	135	148	61	76	28
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.3	75.3	6.4	4.7	39.5	4.9	3.7	3.9	14.6	2.7	3.1	1.4
Cycle Q Clear(g_c), s	1.3	75.3	6.4	4.7	39.5	4.9	3.7	3.9	14.6	2.7	3.1	1.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	203	2053	916	257	2108	940	274	575	178	262	388	304
V/C Ratio(X)	0.14	0.91	0.15	0.42	0.64	0.12	0.31	0.23	0.83	0.23	0.20	0.09
Avail Cap(c_a), veh/h	309	2053	916	287	2108	940	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.38	0.38	0.38	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.5	30.2	15.6	70.7	21.3	14.2	69.5	64.7	69.5	69.6	64.9	64.1
Incr Delay (d2), s/veh	0.1	3.2	0.1	1.1	1.5	0.3	0.6	0.2	9.4	0.4	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.6	32.4	2.4	2.2	16.8	1.9	1.7	1.7	6.4	1.2	1.4	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	71.6	33.4	15.8	71.8	22.8	14.5	70.1	64.9	78.9	70.0	65.1	64.3
LnGrp LOS	E	C	B	E	C	B	E	E	E	E	E	E
Approach Vol, veh/h		2040			1561			368			165	
Approach Delay, s/veh		32.7			25.5			71.7			66.8	
Approach LOS		C			C			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.6	100.4	18.4	23.6	15.1	102.9	17.8	24.1				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	6.7	77.3	5.7	5.1	3.3	41.5	4.7	16.6				
Green Ext Time (p_c), s	0.1	0.0	0.1	0.6	0.0	9.8	0.1	1.4				
Intersection Summary												
HCM 6th Ctrl Delay											34.8	
HCM 6th LOS											C	
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Existing
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	17	11	193	14	5	114
Future Vol, veh/h	17	11	193	14	5	114
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	12	210	15	5	124

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	278	218	0	0	225
Stage 1	218	-	-	-	-
Stage 2	60	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	705	821	-	-	1342
Stage 1	788	-	-	-	-
Stage 2	915	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	702	821	-	-	1342
Mov Cap-2 Maneuver	702	-	-	-	-
Stage 1	788	-	-	-	-
Stage 2	911	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10	0	0.3
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	744	1342
HCM Lane V/C Ratio	-	-	0.041	0.004
HCM Control Delay (s)	-	-	10	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0

LOS Engineering, Inc.

PM Existing
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	1	0	206	0	0	120
Future Vol, veh/h	1	0	206	0	0	120
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	0	224	0	0	130

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	354	224	0	0	224	0
Stage 1	224	-	-	-	-	-
Stage 2	130	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	644	815	-	-	1345	-
Stage 1	813	-	-	-	-	-
Stage 2	896	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	644	815	-	-	1345	-
Mov Cap-2 Maneuver	644	-	-	-	-	-
Stage 1	813	-	-	-	-	-
Stage 2	896	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.6	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	644	1345
HCM Lane V/C Ratio	-	-	0.002	-
HCM Control Delay (s)	-	-	10.6	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

LOS Engineering, Inc.

Intersection						
Int Delay, s/veh	0.6					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	2	8	20	226	140	2
Future Vol, veh/h	2	8	20	226	140	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	200	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	2	9	22	246	152	2

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	443	153	154	0	-	0
Stage 1	153	-	-	-	-	-
Stage 2	290	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	572	893	1426	-	-	-
Stage 1	875	-	-	-	-	-
Stage 2	759	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	563	893	1426	-	-	-
Mov Cap-2 Maneuver	563	-	-	-	-	-
Stage 1	862	-	-	-	-	-
Stage 2	759	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.6	0.6	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1426	-	799	-	-
HCM Lane V/C Ratio	0.015	-	0.014	-	-
HCM Control Delay (s)	7.6	-	9.6	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0	-	0	-	-

Appendix G

ITE 11th Edition Trip Generation

Land Use: 140

Manufacturing

Description

A manufacturing facility is an area where the primary activity is the conversion of raw materials or parts into finished products. Size and type of activity may vary substantially from one facility to another. In addition to the actual production of goods, a manufacturing facility typically has an office and may provide space for warehouse, research, and associated functions. General light industrial (Land Use 110) and industrial park (Land Use 130) are related uses.

Additional Data

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

The sites were surveyed in the 1980s, the 1990s, the 2000s, and the 2010s in Alberta (CAN), California, Minnesota, Missouri, New Jersey, New York, Oregon, Pennsylvania, South Dakota, Texas, Vermont, Washington, and West Virginia.

Source Numbers

177, 179, 184, 241, 357, 384, 418, 443, 583, 598, 611, 728, 747, 875, 879, 940, 969, 1067, 1068, 1082

Manufacturing (140)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 53

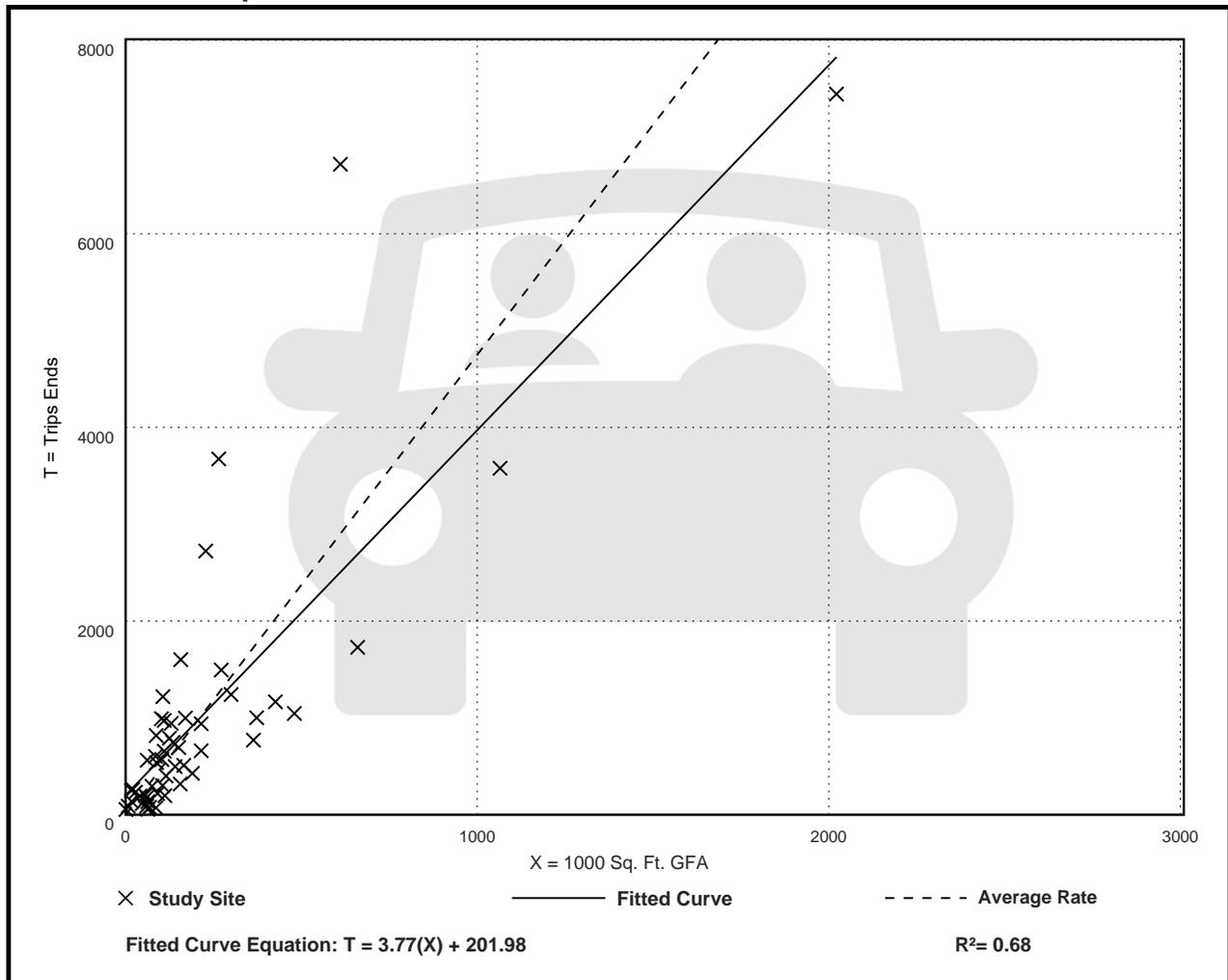
Avg. 1000 Sq. Ft. GFA: 208

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
4.75	0.83 - 49.50	3.20

Data Plot and Equation



Manufacturing (140)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 48

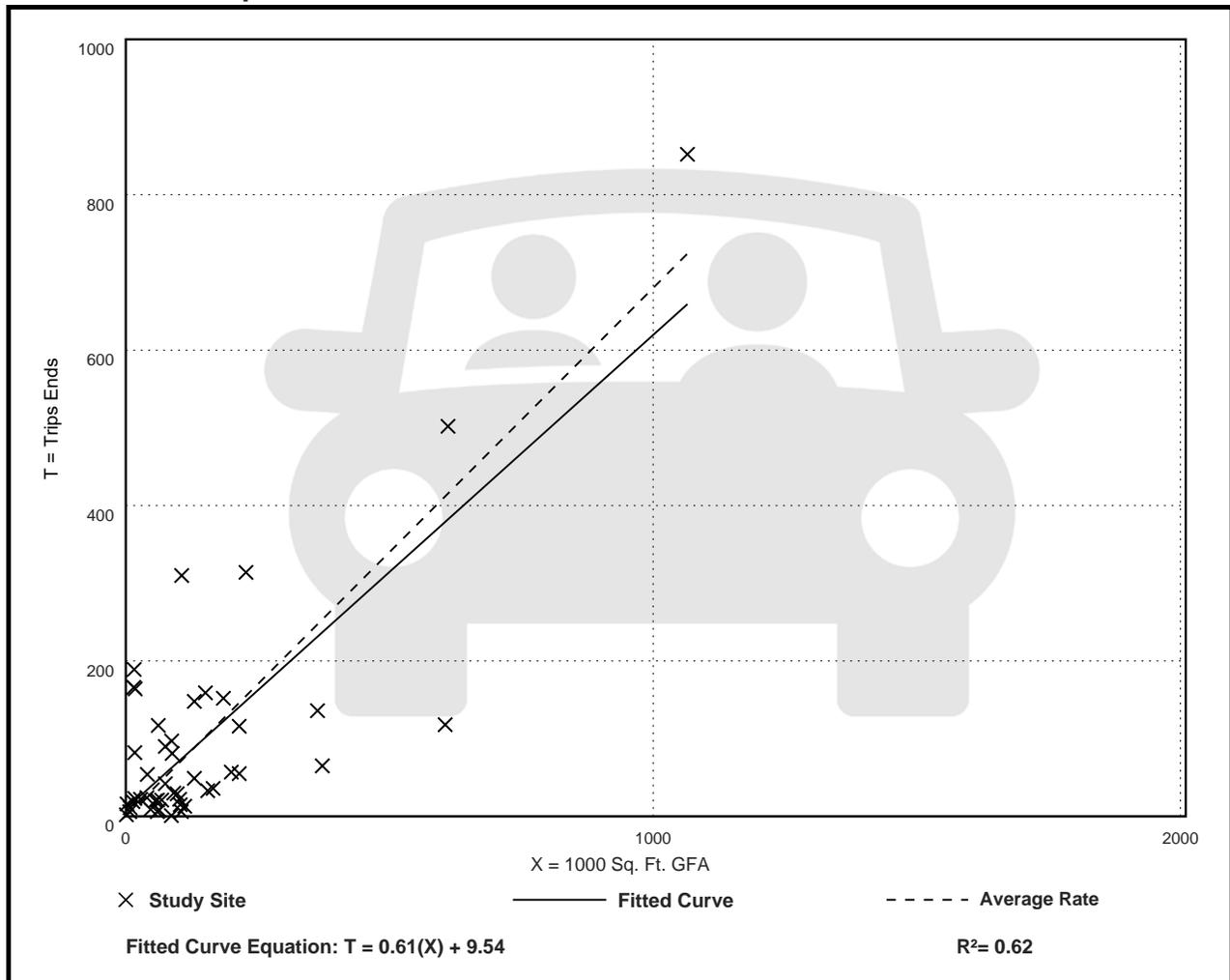
Avg. 1000 Sq. Ft. GFA: 138

Directional Distribution: 76% entering, 24% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.68	0.01 - 11.93	1.03

Data Plot and Equation



Manufacturing (140)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 55

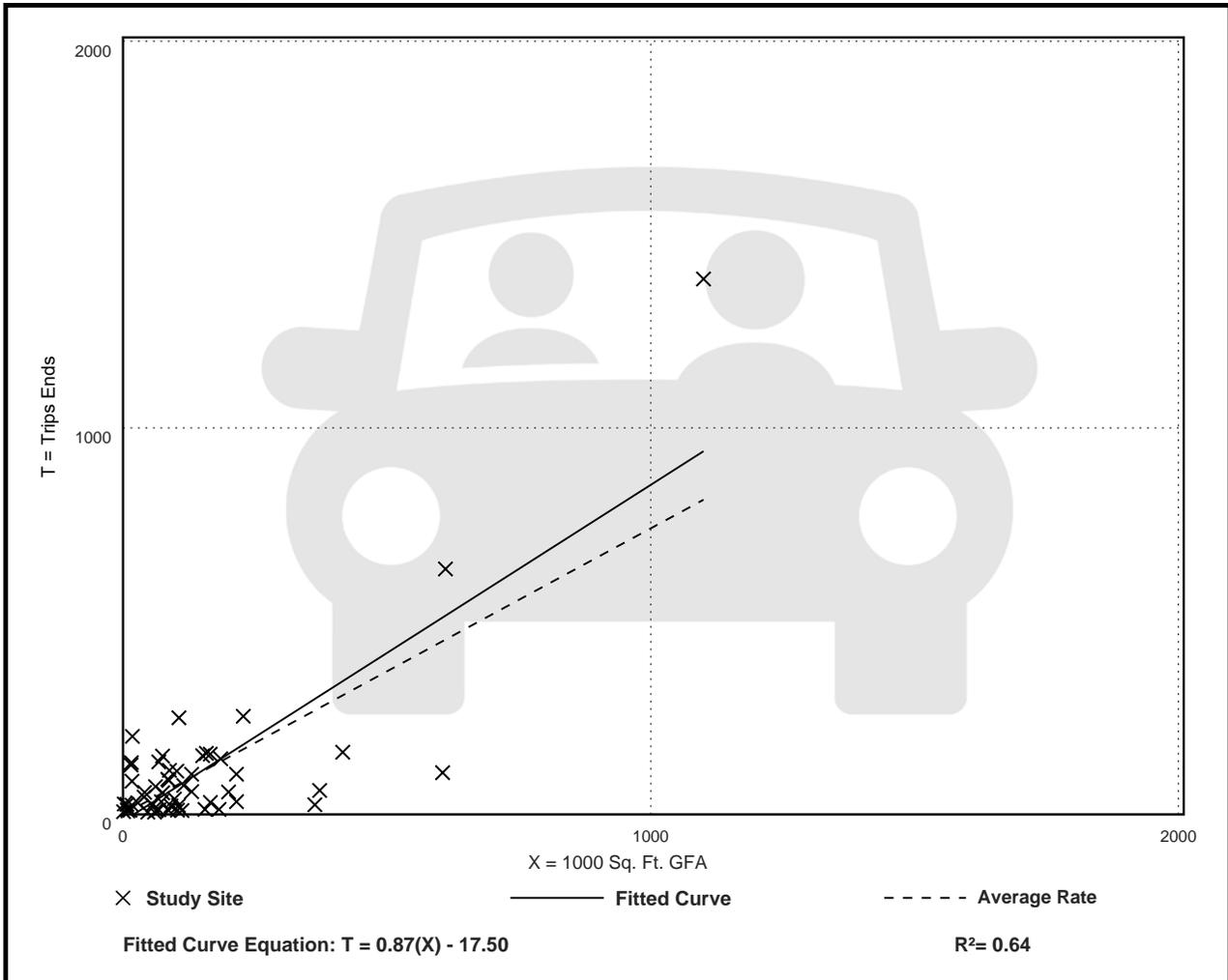
Avg. 1000 Sq. Ft. GFA: 142

Directional Distribution: 31% entering, 69% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.74	0.07 - 11.37	0.93

Data Plot and Equation



Manufacturing (140)

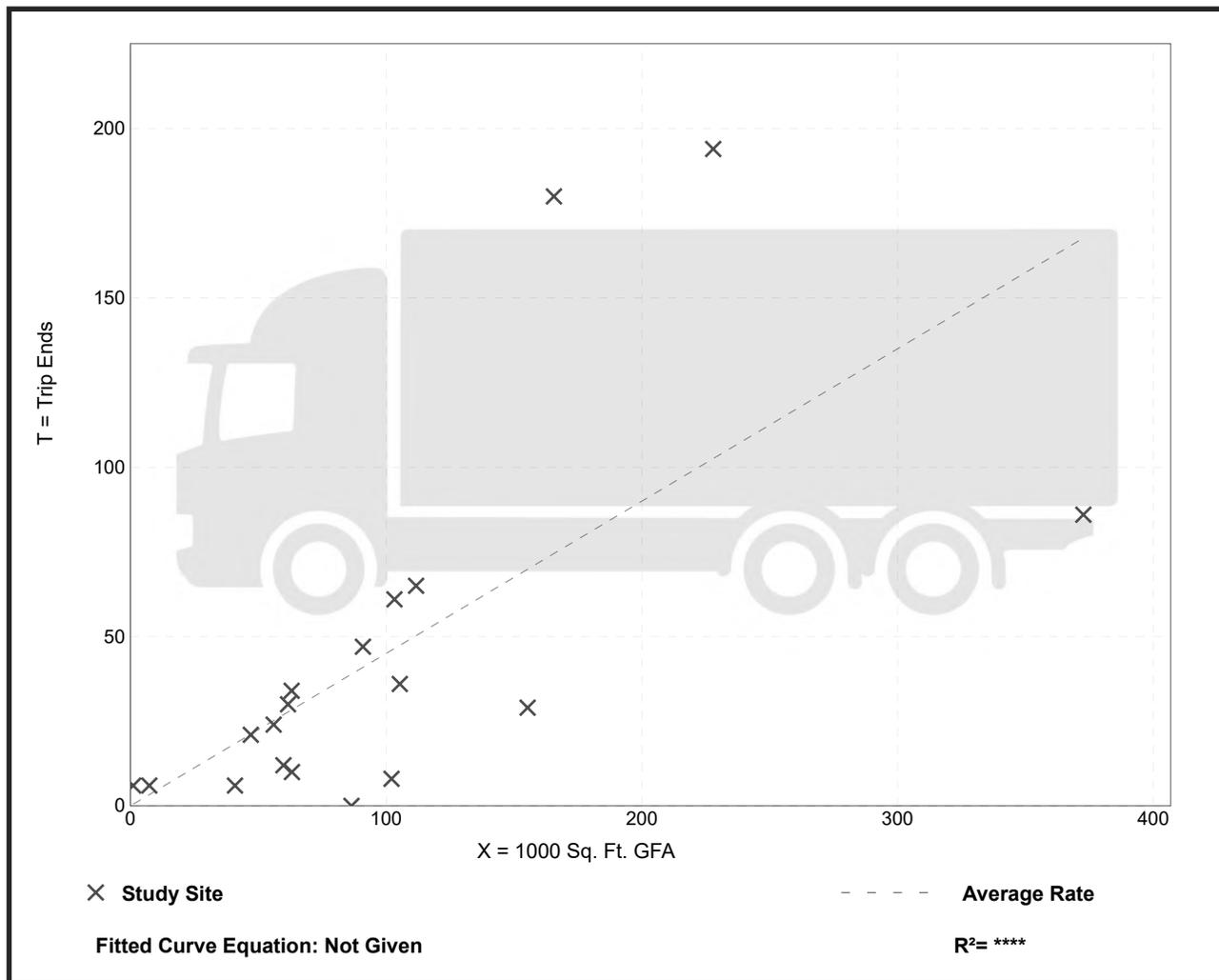
Truck Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban
Number of Studies: 19
Avg. 1000 Sq. Ft. GFA: 101
Directional Distribution: 50% entering, 50% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.45	0.00 - 5.50	0.34

Data Plot and Equation



Manufacturing (140)

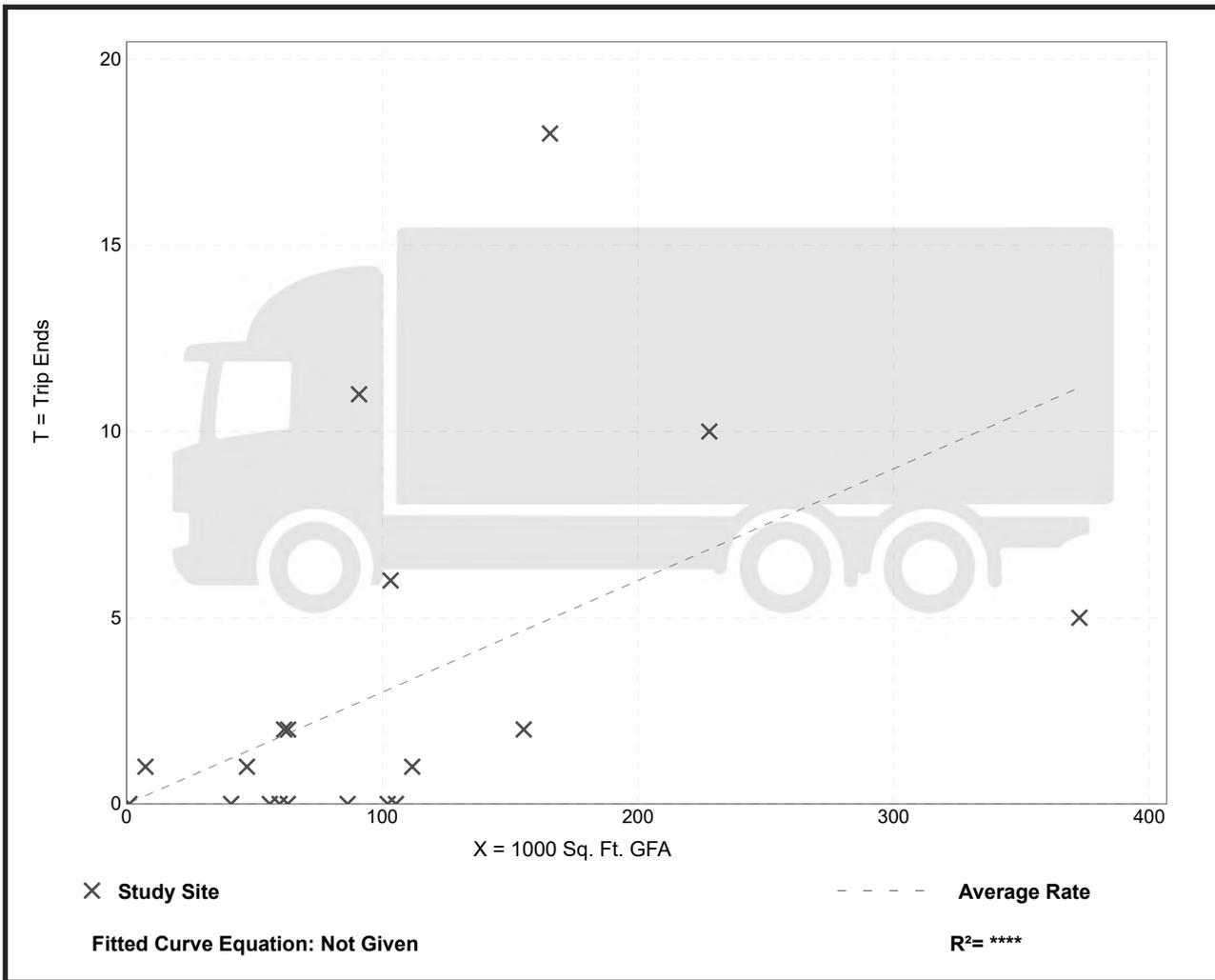
Truck Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban
 Number of Studies: 19
 Avg. 1000 Sq. Ft. GFA: 101
 Directional Distribution: 56% entering, 44% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.03	0.00 - 0.14	0.04

Data Plot and Equation



Manufacturing (140)

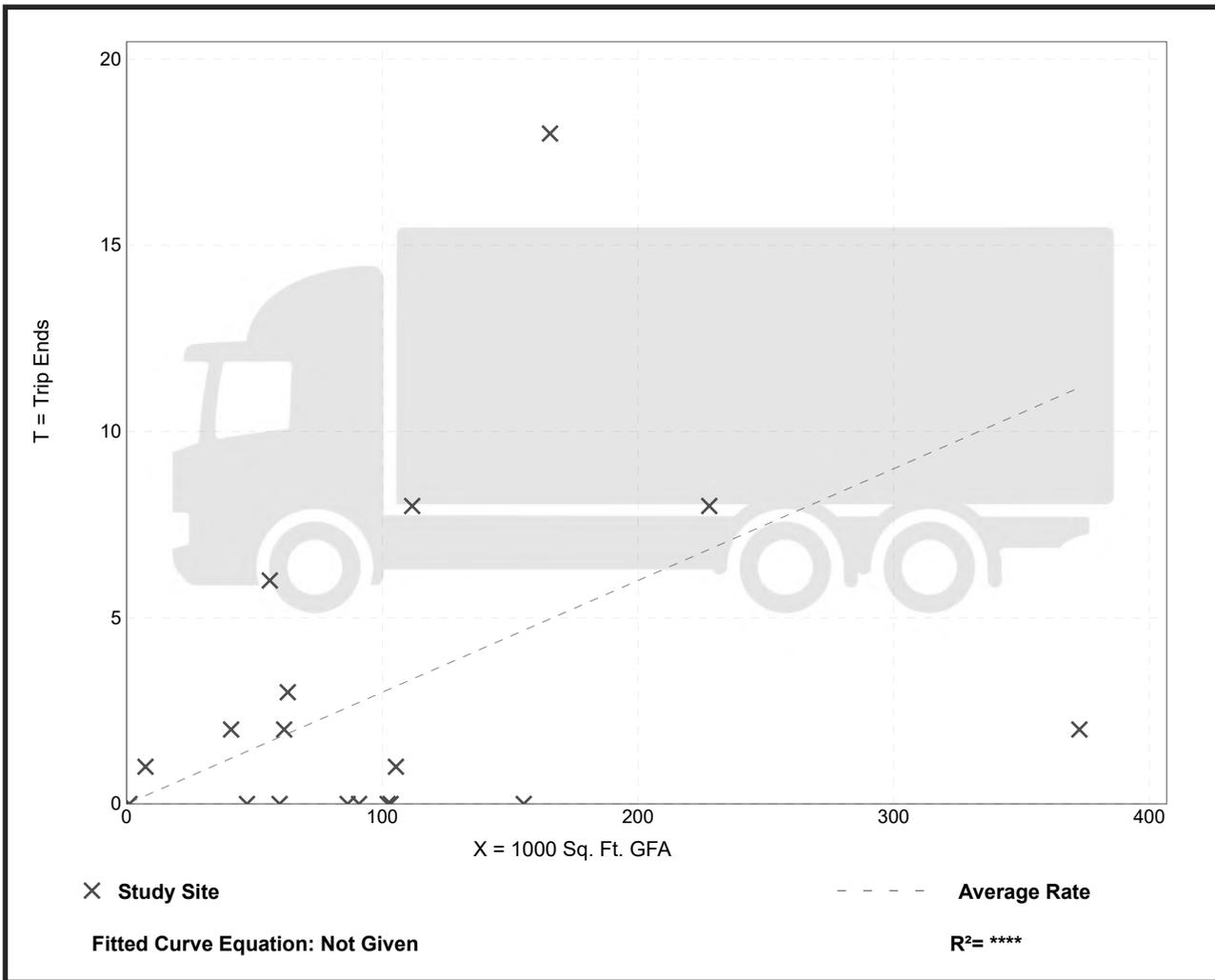
Truck Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban
 Number of Studies: 18
 Avg. 1000 Sq. Ft. GFA: 103
 Directional Distribution: 41% entering, 59% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.03	0.00 - 0.14	0.04

Data Plot and Equation



Land Use: 154

High-Cube Transload and Short-Term Storage Warehouse

Description

A high-cube warehouse (HCW) is a building that typically has at least 200,000 gross square feet of floor area, has a ceiling height of 24 feet or more, and is used primarily for the storage and/or consolidation of manufactured goods (and to a lesser extent, raw materials) prior to their distribution to retail locations or other warehouses. A typical HCW has a high level of on-site automation and logistics management. The automation and logistics enable highly-efficient processing of goods through the HCW. A high-cube warehouse can be free-standing or located in an industrial park.

The HCWs included in this land use include transload and short-term storage facilities. A transload facility has the primary function of consolidation and distribution of pallet loads (or larger) for manufacturers, wholesalers, or retailers. A transload facility typically has little storage duration, high throughput, and its operations are high efficiency. A short-term HCW is a distribution facility often with custom/special features built into the structure for the movement of large volumes of freight with only short-term storage of products.

Some limited assembly and repackaging may occur within the facility.

A high-cube warehouse may contain a mezzanine. In a HCW setting, a mezzanine is a free-standing, semi-permanent structure that is commonly supported by structural steel columns and that is lined with racks or shelves. The gross floor area (GFA) values for the study sites in the database for this land use do NOT include the floor area of the mezzanine. The GFA values represent only the permanent ground-floor square footage.

The amount of office/employee welfare space that is provided within a HCW can be highly variable but is typically an insignificant portion of the overall building square footage. Within the trip generation database, common values are between 3,000 and 5,000 square feet for a Cold Storage HCW and between 5,000 and 10,000 square feet for Transload, Fulfillment Center, and Parcel Hub HCW (all of which are less than one percent of total GFA for a site). Therefore, for the trip generation data plots, any office space that is part of the normal operation of the warehouse is included in the total GFA.

Warehousing (Land Use 150), high-cube fulfillment center warehouse (Land Use 155), high-cube parcel hub warehouse (Land Use 156), and high-cube cold storage warehouse (Land Use 157) are related land uses.

The number of dock doors at a HCW is a potential independent variable. Future data submissions should include that information.

Additional Data

The High-Cube Warehouse/Distribution Center-related land uses underwent specialized consideration through a commissioned study titled “High-Cube Warehouse Vehicle Trip Generation Analysis,” published in October 2016. The results of this study are posted on the ITE website at <http://library.ite.org/pub/a3e6679a-e3a8-bf38-7f29-2961becdd498>.

The technical appendices provide supporting information on time-of-day distributions for this land use. The appendices can be accessed through either the ITETripGen web app or the trip generation resource page on the ITE website (<https://www.ite.org/technical-resources/topics/trip-and-parking-generation/>).

The sites were surveyed in the 1980s, the 2000s, and the 2010s in Alberta (CAN), California, Florida, Michigan, New Jersey, Texas, and Washington.

Source Numbers

331, 605, 619, 642, 645, 649, 739, 750, 752, 903, 904, 941, 942, 943, 969

High-Cube Transload and Short-Term Storage Warehouse (154)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday

Setting/Location: General Urban/Suburban

Number of Studies: 91

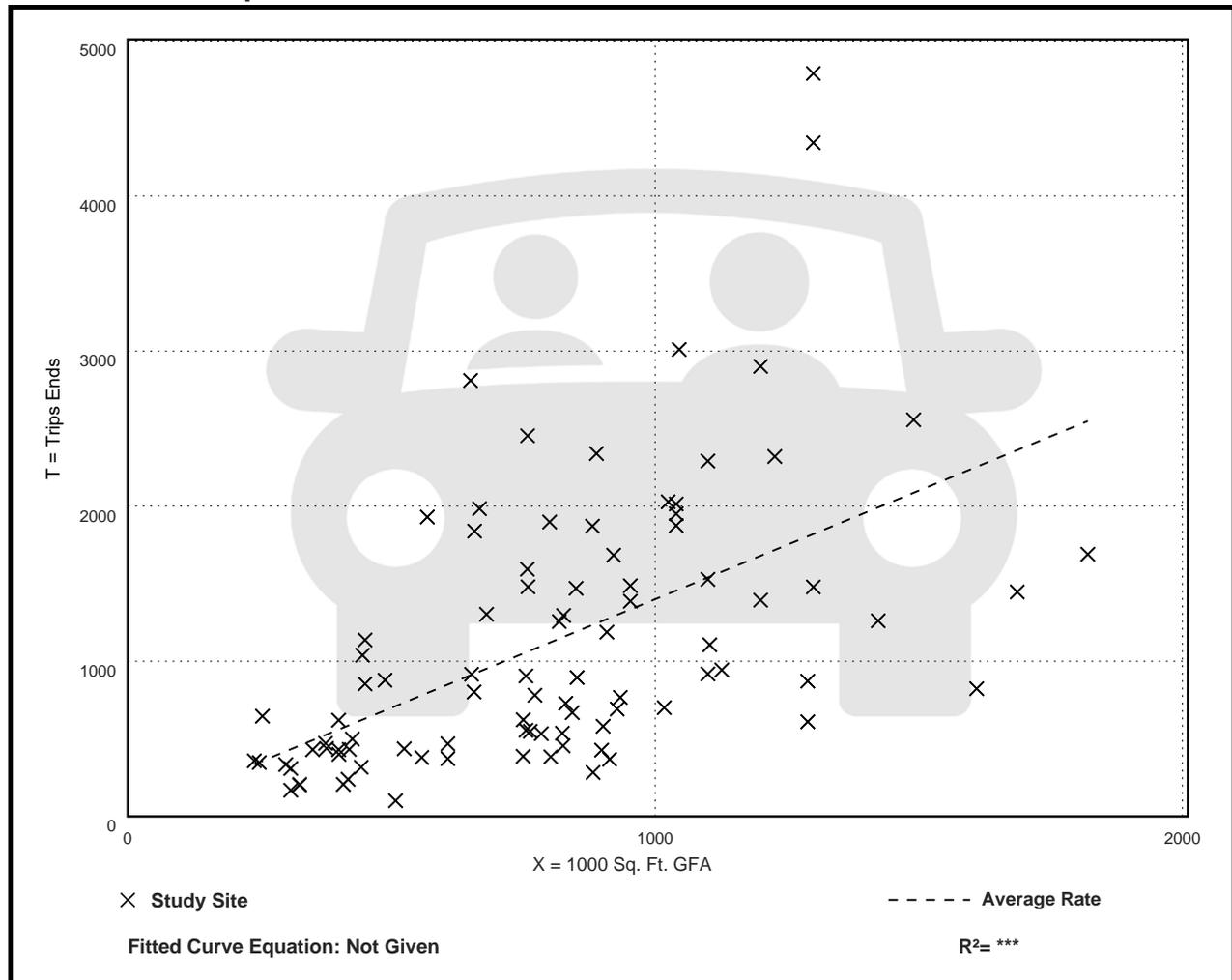
Avg. 1000 Sq. Ft. GFA: 798

Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
1.40	0.20 - 4.32	0.86

Data Plot and Equation



High-Cube Transload and Short-Term Storage Warehouse (154)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 7 and 9 a.m.

Setting/Location: General Urban/Suburban

Number of Studies: 102

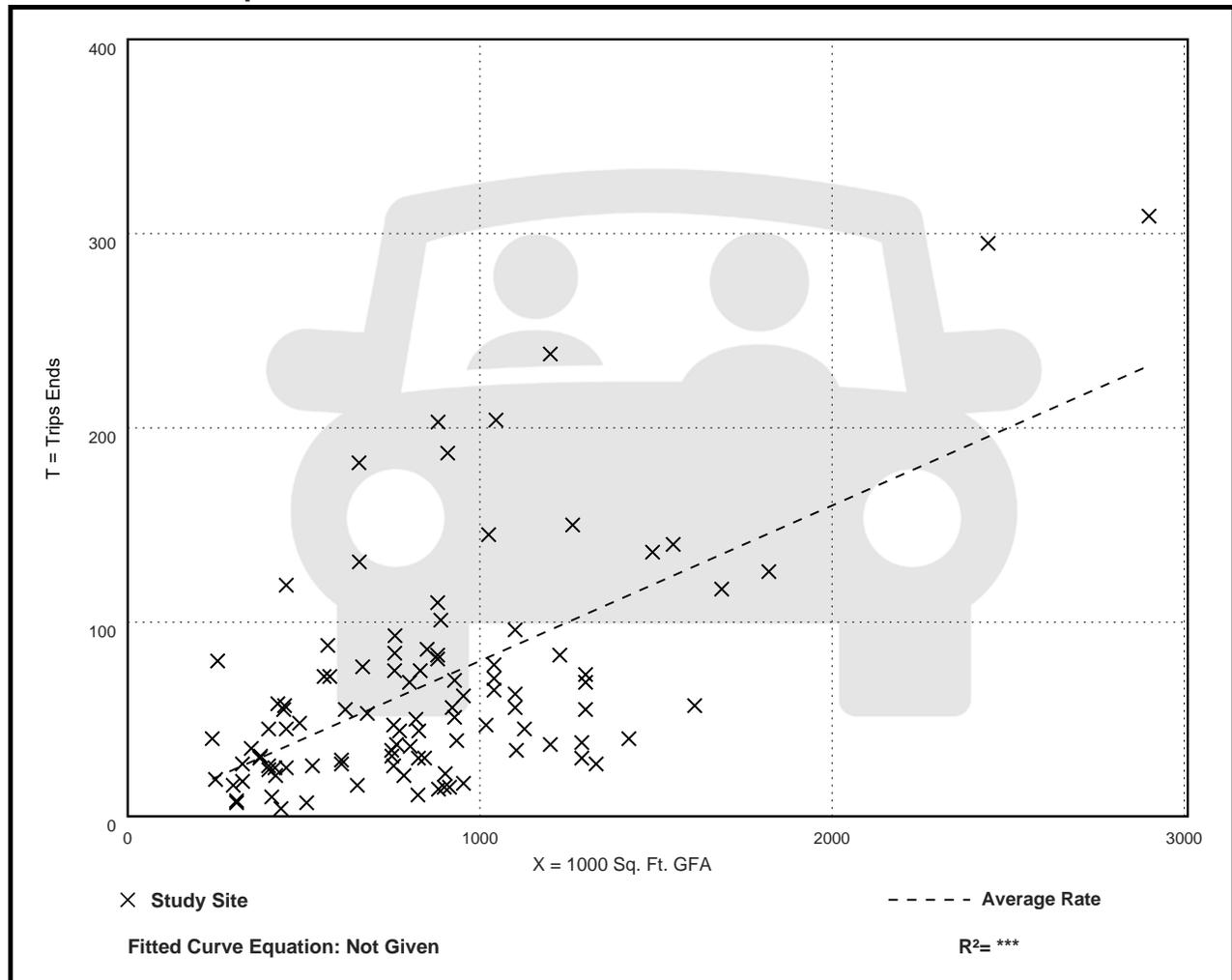
Avg. 1000 Sq. Ft. GFA: 846

Directional Distribution: 77% entering, 23% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.08	0.01 - 0.31	0.05

Data Plot and Equation



High-Cube Transload and Short-Term Storage Warehouse (154)

Vehicle Trip Ends vs: 1000 Sq. Ft. GFA

On a: Weekday,

Peak Hour of Adjacent Street Traffic,

One Hour Between 4 and 6 p.m.

Setting/Location: General Urban/Suburban

Number of Studies: 103

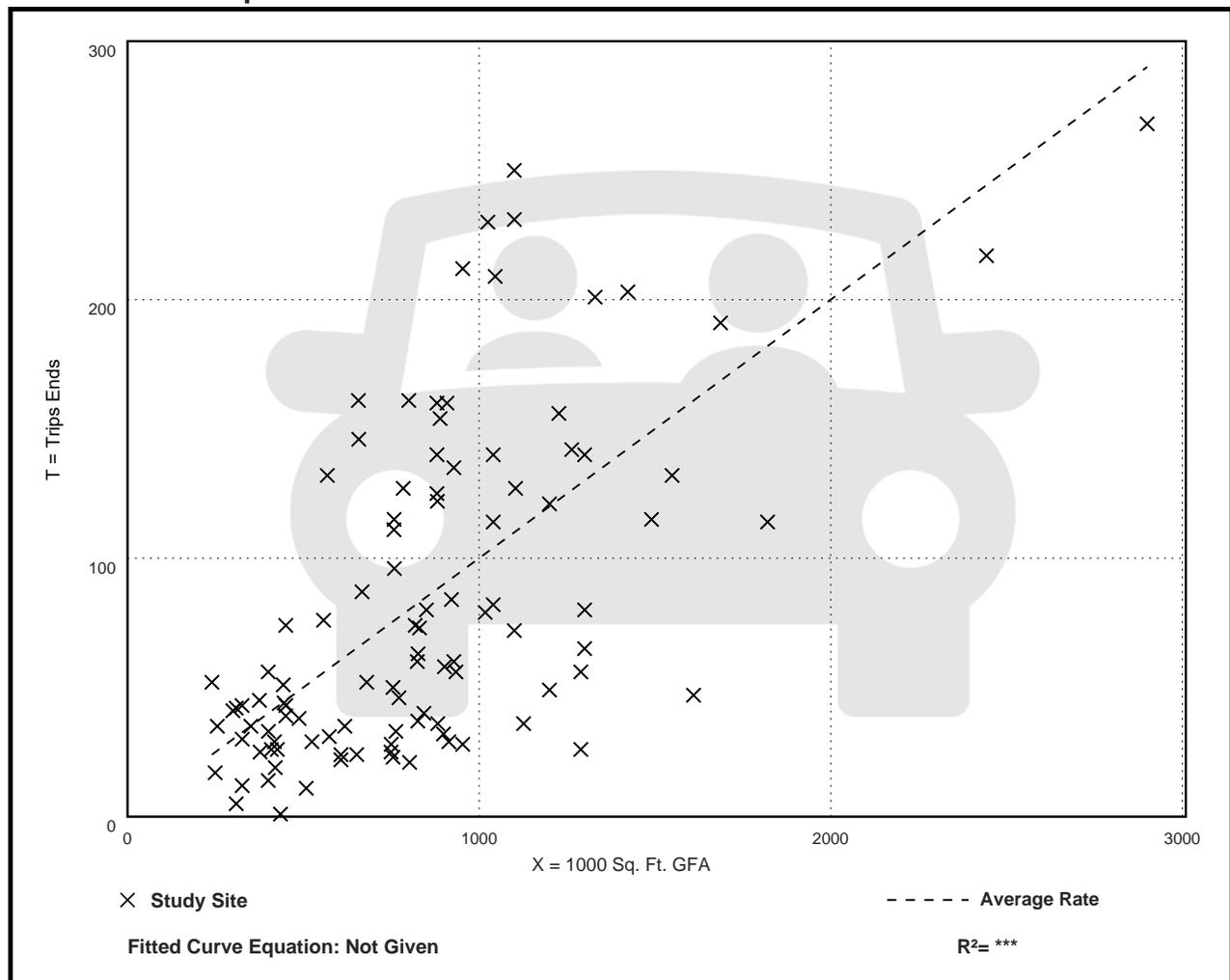
Avg. 1000 Sq. Ft. GFA: 840

Directional Distribution: 28% entering, 72% exiting

Vehicle Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.10	0.00 - 0.25	0.06

Data Plot and Equation



High-Cube Transload and Short-Term Storage Warehouse (154)

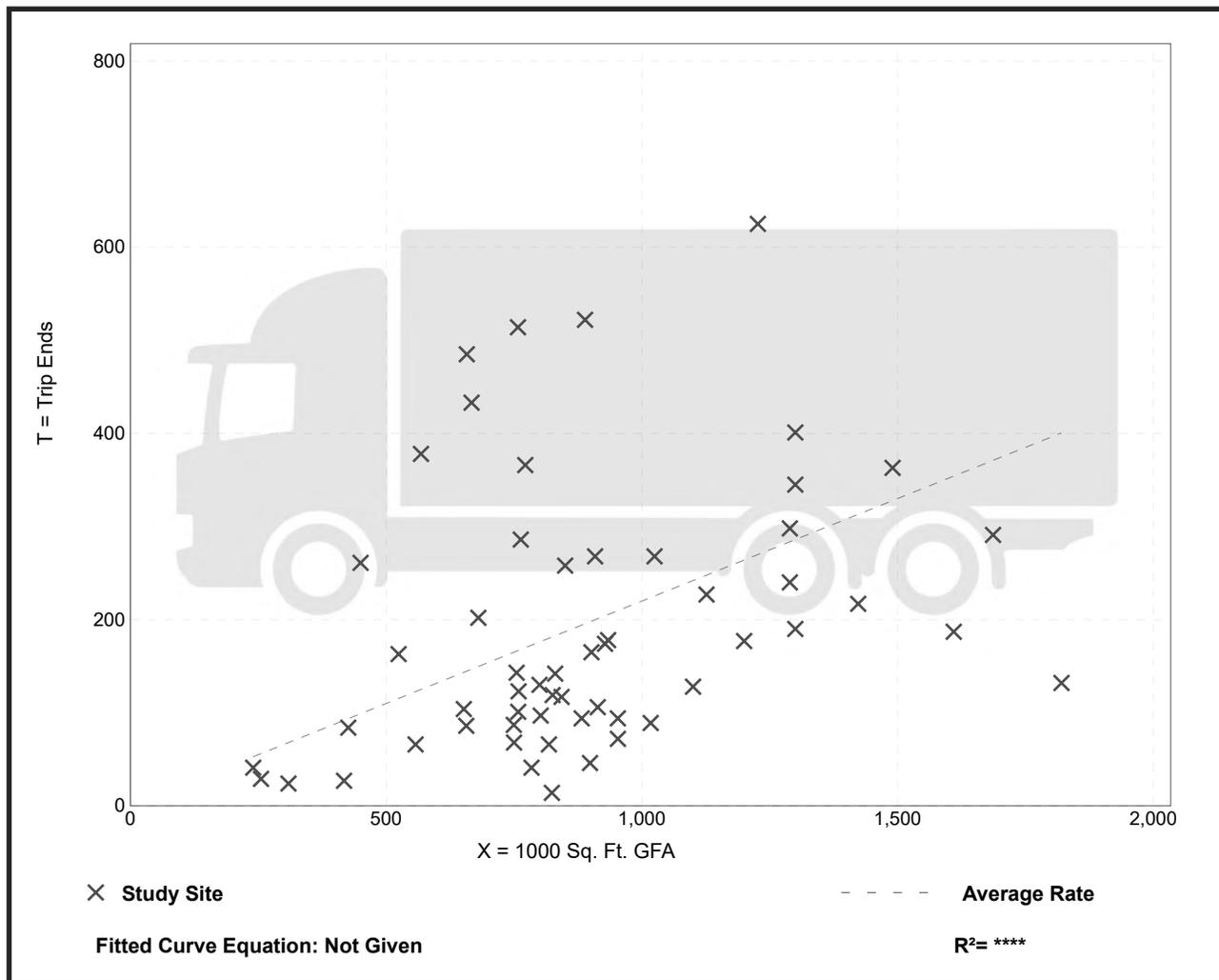
Truck Trip Ends vs: 1000 Sq. Ft. GFA
 On a: Weekday

Setting/Location: General Urban/Suburban
 Number of Studies: 57
 Avg. 1000 Sq. Ft. GFA: 892
 Directional Distribution: 50% entering, 50% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.22	0.02 - 0.74	0.16

Data Plot and Equation



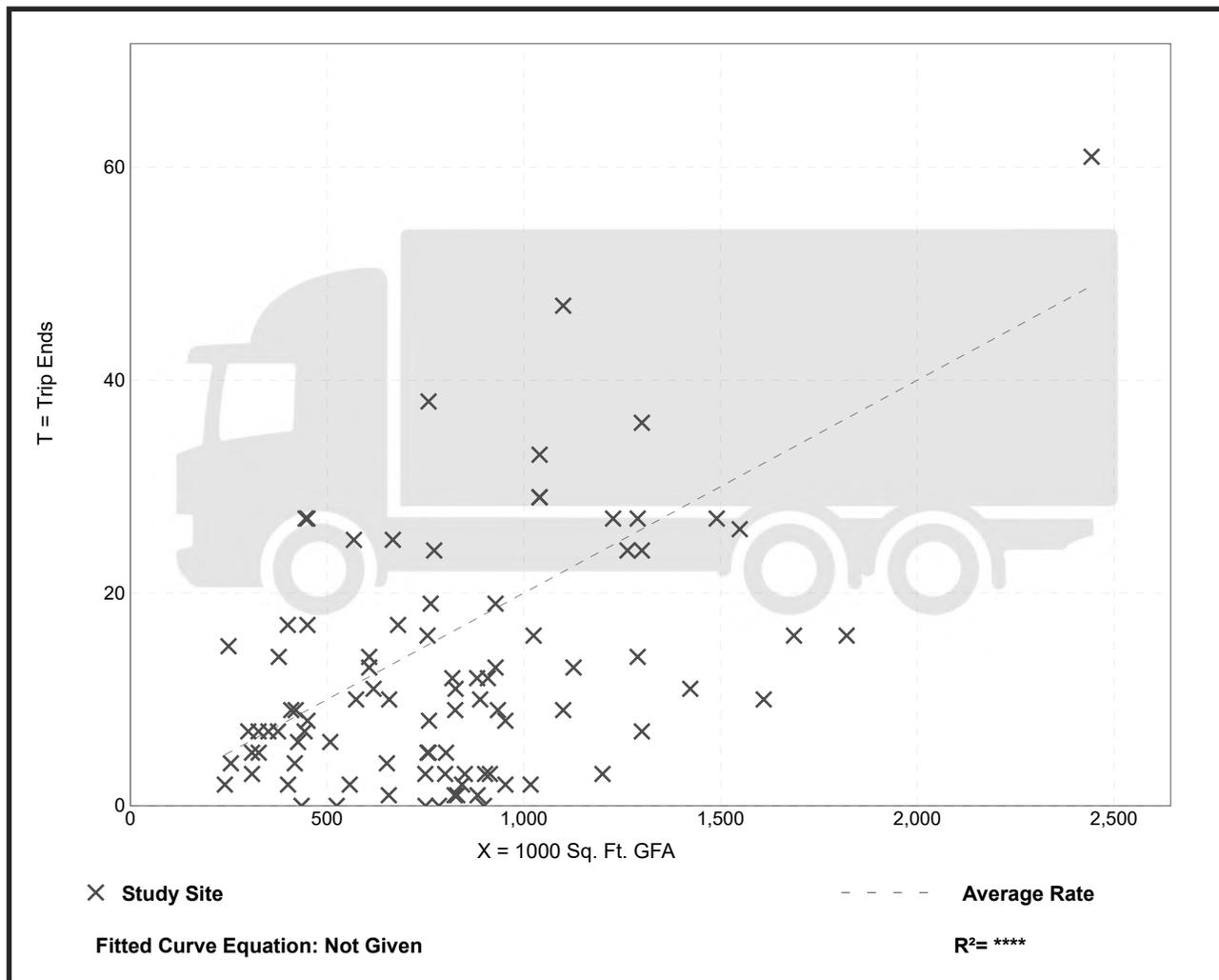
High-Cube Transload and Short-Term Storage Warehouse (154)

Truck Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 7 and 9 a.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 90
 Avg. 1000 Sq. Ft. GFA: 812
 Directional Distribution: 49% entering, 51% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.02	0.00 - 0.06	0.01

Data Plot and Equation



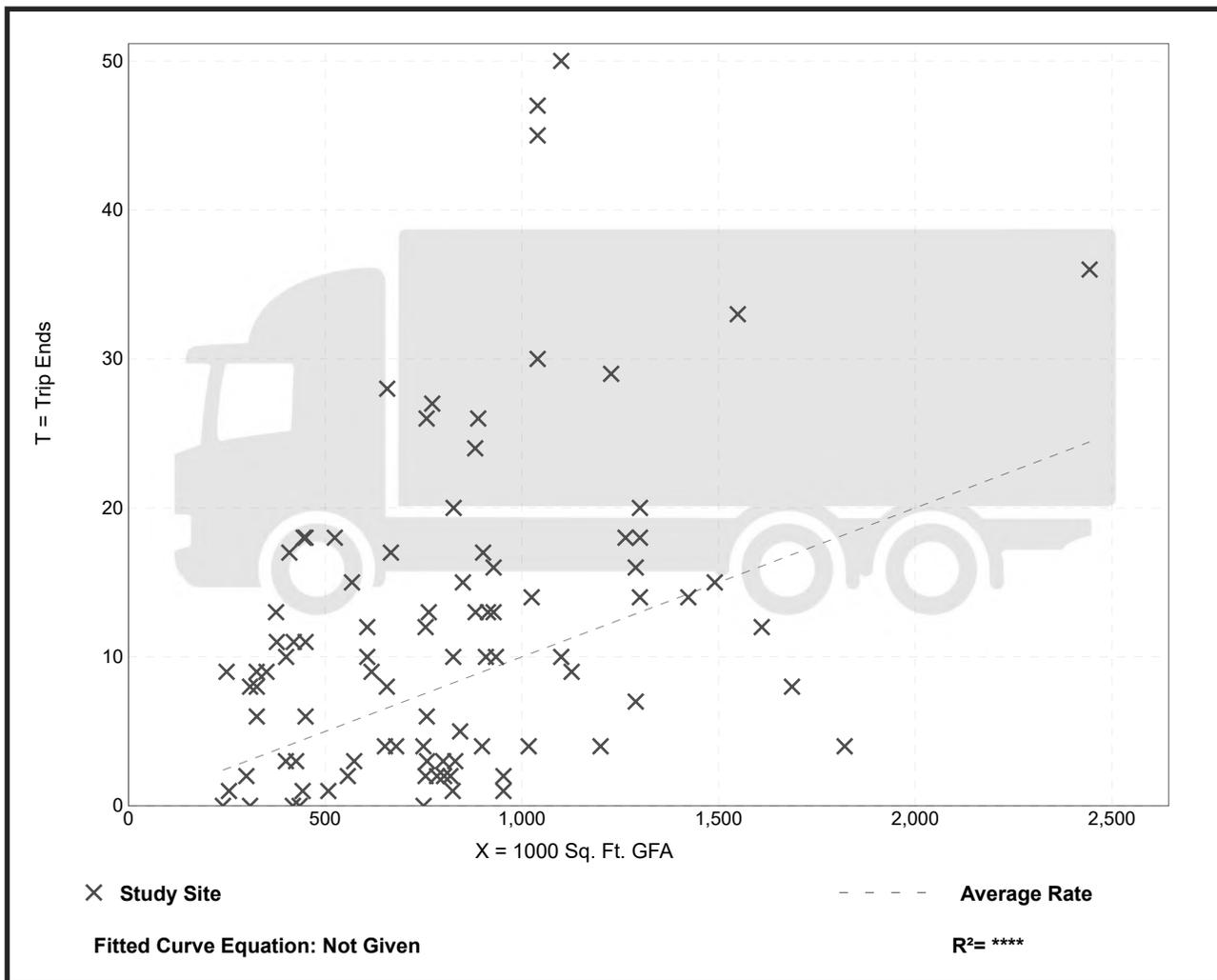
High-Cube Transload and Short-Term Storage Warehouse (154)

Truck Trip Ends vs: 1000 Sq. Ft. GFA
On a: Weekday,
Peak Hour of Adjacent Street Traffic,
One Hour Between 4 and 6 p.m.
Setting/Location: General Urban/Suburban
 Number of Studies: 91
 Avg. 1000 Sq. Ft. GFA: 807
 Directional Distribution: 47% entering, 53% exiting

Truck Trip Generation per 1000 Sq. Ft. GFA

Average Rate	Range of Rates	Standard Deviation
0.01	0.00 - 0.05	0.01

Data Plot and Equation



Appendix H

Existing + Project Intersection LOS Worksheets

AM Existing + Project
1: SR-76 & I-5 SB Ramp

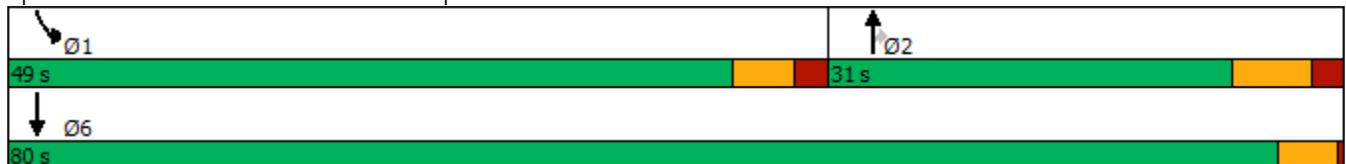
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	280	215	1072	445
Future Volume (vph)	0	280	215	1072	445
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		31.0	31.0	49.0	80.0
Total Split (%)		38.8%	38.8%	61.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	32.1	32.1	35.4	0.0
Actuated g/C Ratio	0.00	0.40	0.40	0.44	0.00
v/c Ratio	no cap	0.21	0.35	0.77	no cap
Control Delay		17.5	15.4	22.2	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	17.5	15.4	22.2	Error
LOS	F	B	B	C	F
Approach Delay	Err	16.6			Err
Approach LOS	F	B			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 55
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection Capacity Utilization 39.5%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service A

Splits and Phases: 1: SR-76 & I-5 SB Ramp



AM Existing + Project
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	330	280	215	1072	445
Future Volume (veh/h)	0	330	280	215	1072	445
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			304	234	1165	484
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			859	383	1574	0
Arrive On Green			0.24	0.24	0.46	0.84
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			304	234	1165	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			2.9	5.4	11.4	0.0
Cycle Q Clear(g_c), s			2.9	5.4	11.4	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			859	383	1574	0
V/C Ratio(X)			0.35	0.61	0.74	0.00
Avail Cap(c_a), veh/h			2085	930	3627	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			13.0	13.9	9.2	0.0
Incr Delay (d2), s/veh			0.2	1.6	0.7	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.0	1.8	3.1	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			13.2	15.5	9.9	0.0
LnGrp LOS			B	B	A	A
Approach Vol, veh/h			538			1165
Approach Delay, s/veh			14.2			9.9
Approach LOS			B			A
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	24.5	16.8				41.3
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	13.4	7.4				0.0
Green Ext Time (p_c), s	5.4	2.6				0.0

Intersection Summary

HCM 6th Ctrl Delay			11.3			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Project
2: SR-76 & I-5 NB Ramp

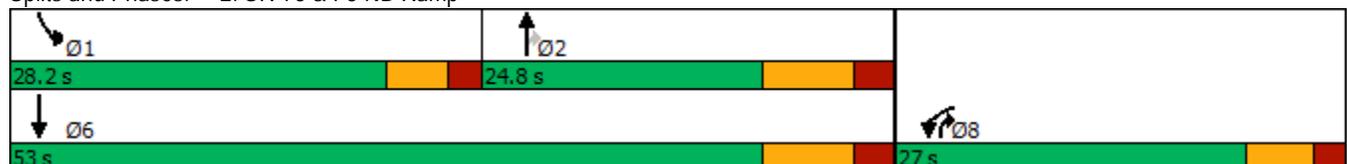
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↘	↑↑	↘	↘↘	↑↑
Traffic Volume (vph)	81	576	89	893	1393
Future Volume (vph)	81	576	89	893	1393
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	0.99	0.84	0.11	1.01	0.76
Control Delay	42.9	42.5	7.5	61.0	16.6
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	42.9	42.5	7.5	61.0	16.6
LOS	D	D	A	E	B
Approach Delay	42.9	37.8			33.9
Approach LOS	D	D			C

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 75
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.01
 Intersection Signal Delay: 36.3
 Intersection LOS: D
 Intersection Capacity Utilization 100.4%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Existing + Project
2: SR-76 & I-5 NB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	81	615	576	89	893	1393
Future Volume (veh/h)	81	615	576	89	893	1393
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	88	668	626	97	971	1514
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1425	453	1177	3017
Arrive On Green	0.00	0.00	0.40	0.40	0.34	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	626	97	971	1514
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	6.8	2.5	13.7	5.9
Cycle Q Clear(g_c), s	0.0	0.0	6.8	2.5	13.7	5.9
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1425	453	1177	3017
V/C Ratio(X)	0.00	0.00	0.44	0.21	0.82	0.50
Avail Cap(c_a), veh/h	0	0	1425	453	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	11.5	14.4	16.0	1.1
Incr Delay (d2), s/veh	0.0	0.0	1.0	1.1	3.2	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	2.4	0.7	5.1	0.3
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	12.5	15.5	19.3	1.7
LnGrp LOS	A	A	B	B	B	A
Approach Vol, veh/h	0		723			2485
Approach Delay, s/veh	0.0		12.9			8.5
Approach LOS			B			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	23.8	29.2			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	15.7	8.8			7.9	0.0
Green Ext Time (p_c), s	2.4	2.8			16.5	0.0

Intersection Summary

HCM 6th Ctrl Delay		9.5
HCM 6th LOS		A

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Project
3: Loretta St & SR-76

Timings

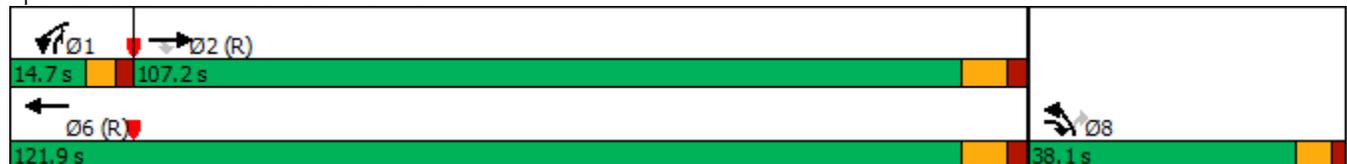


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙	↙
Traffic Volume (vph)	1182	23	12	2264	43	26
Future Volume (vph)	1182	23	12	2264	43	26
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	0.57	0.02	0.13	0.98	0.13	0.06
Control Delay	18.2	0.5	66.8	29.3	53.9	12.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	18.2	0.5	66.8	29.3	53.9	12.9
LOS	B	A	E	C	D	B
Approach Delay	17.9			29.5	38.6	
Approach LOS	B			C	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.98
 Intersection Signal Delay: 25.7
 Intersection Capacity Utilization 82.7%
 Analysis Period (min) 15
 Intersection LOS: C
 ICU Level of Service E

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

AM Existing + Project
3: Loretta St & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵	↵
Traffic Volume (veh/h)	1182	23	12	2264	43	26
Future Volume (veh/h)	1182	23	12	2264	43	26
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1285	25	13	2461	47	28
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2315	1350	44	2530	356	356
Arrive On Green	0.65	0.65	0.02	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1285	25	13	2461	47	28
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	31.6	0.4	1.1	103.8	3.5	2.2
Cycle Q Clear(g_c), s	31.6	0.4	1.1	103.8	3.5	2.2
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2315	1350	44	2530	356	356
V/C Ratio(X)	0.55	0.02	0.30	0.97	0.13	0.08
Avail Cap(c_a), veh/h	2315	1350	100	2530	356	356
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.43	0.43	1.00	1.00
Uniform Delay (d), s/veh	15.2	1.8	76.7	21.6	52.6	49.0
Incr Delay (d2), s/veh	1.0	0.0	1.6	7.0	0.8	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	13.0	0.3	0.5	42.1	1.7	0.9
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	16.2	1.8	78.2	28.6	53.4	49.4
LnGrp LOS	B	A	E	C	D	D
Approach Vol, veh/h	1310			2474	75	
Approach Delay, s/veh	15.9			28.8	51.9	
Approach LOS	B			C	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	9.6	112.3			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.1	33.6			105.8	5.5
Green Ext Time (p_c), s	0.0	14.7			7.6	0.2
Intersection Summary						
HCM 6th Ctrl Delay			24.9			
HCM 6th LOS			C			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

AM Existing + Project
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↗↖	↗
Traffic Volume (veh/h)	1210	14	74	2273	20	75
Future Volume (veh/h)	1210	14	74	2273	20	75
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1315	15	80	2471	22	82
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2677	1293	108	3020	215	195
Arrive On Green	0.75	0.75	0.06	0.85	0.06	0.06
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1315	15	80	2471	22	82
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	23.2	0.3	7.1	54.9	1.0	7.7
Cycle Q Clear(g_c), s	23.2	0.3	7.1	54.9	1.0	7.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2677	1293	108	3020	215	195
V/C Ratio(X)	0.49	0.01	0.74	0.82	0.10	0.42
Avail Cap(c_a), veh/h	2677	1293	164	3020	713	423
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.81	0.81	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	7.7	2.7	73.9	5.9	70.8	64.9
Incr Delay (d2), s/veh	0.5	0.0	0.9	0.2	0.2	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	8.6	0.1	3.3	15.5	0.4	3.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	8.2	2.8	74.8	6.2	71.0	66.4
LnGrp LOS	A	A	E	A	E	E
Approach Vol, veh/h	1330			2551	104	
Approach Delay, s/veh	8.2			8.3	67.3	
Approach LOS	A			A	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	15.4	128.5			144.0	16.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	9.1	25.2			56.9	9.7
Green Ext Time (p_c), s	0.1	15.2			43.2	0.3

Intersection Summary

HCM 6th Ctrl Delay			9.8			
HCM 6th LOS			A			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Project
5: Benet Rd & SR-76

Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	170	1023	129	30	2022	9	50	10	46	9	14	272
Future Volume (vph)	170	1023	129	30	2022	9	50	10	46	9	14	272
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	35.0	35.0	17.7	20.0	18.7
Total Split (s)	22.0	99.6	99.6	17.7	95.3	95.3	17.7	35.0	35.0	17.7	35.0	22.0
Total Split (%)	12.9%	58.6%	58.6%	10.4%	56.1%	56.1%	10.4%	20.6%	20.6%	10.4%	20.6%	12.9%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	None	None	None	Min	None
Act Effect Green (s)	25.7	112.6	112.6	12.0	95.4	95.4	15.8	22.1	22.1	12.0	11.2	38.4
Actuated g/C Ratio	0.15	0.66	0.66	0.07	0.56	0.56	0.09	0.13	0.13	0.07	0.07	0.23
v/c Ratio	0.69	0.47	0.13	0.27	1.11	0.01	0.33	0.05	0.15	0.08	0.12	0.65
Control Delay	81.2	17.4	2.3	80.7	91.5	0.0	75.5	64.1	0.9	75.6	77.5	35.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	81.2	17.4	2.3	80.7	91.5	0.0	75.5	64.1	0.9	75.6	77.5	35.2
LOS	F	B	A	F	F	A	E	E	A	E	E	D
Approach Delay		24.2			90.9			42.0			38.4	
Approach LOS		C			F			D			D	

Intersection Summary

Cycle Length: 170
 Actuated Cycle Length: 170
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 145
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.11
 Intersection Signal Delay: 62.1
 Intersection LOS: E
 Intersection Capacity Utilization 98.9%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Existing + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	170	1023	129	30	2022	9	50	10	46	9	14	272
Future Volume (veh/h)	170	1023	129	30	2022	9	50	10	46	9	14	272
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	185	1112	140	33	2198	10	54	11	50	10	15	296
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	171	1915	854	138	1898	847	116	362	307	47	285	394
Arrive On Green	0.10	1.00	0.54	0.08	1.00	0.53	0.07	0.19	0.19	0.03	0.15	0.25
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	185	1112	140	33	2198	10	54	11	50	10	15	296
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.3	0.0	7.6	3.0	0.0	0.5	5.0	0.8	3.6	0.9	1.2	23.6
Cycle Q Clear(g_c), s	16.3	0.0	7.6	3.0	0.0	0.5	5.0	0.8	3.6	0.9	1.2	23.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	171	1915	854	138	1898	847	116	362	307	47	285	394
V/C Ratio(X)	1.08	0.58	0.16	0.24	1.16	0.01	0.47	0.03	0.16	0.21	0.05	0.75
Avail Cap(c_a), veh/h	171	1915	854	138	1898	847	126	362	307	126	318	421
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.85	0.85	0.85	0.37	0.37	0.37	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	76.8	0.0	19.8	73.7	0.0	18.6	76.6	55.6	36.6	81.0	61.5	32.2
Incr Delay (d2), s/veh	87.4	1.1	0.4	0.3	73.6	0.0	2.9	0.0	0.2	2.2	0.1	6.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.7	0.3	3.0	1.4	19.4	0.2	2.4	0.4	1.8	0.5	0.6	9.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	164.2	1.1	20.2	74.0	73.6	18.6	79.5	55.7	36.9	83.2	61.6	39.1
LnGrp LOS	F	A	C	E	F	B	E	E	D	F	E	D
Approach Vol, veh/h		1437			2241			115			321	
Approach Delay, s/veh		24.0			73.4			58.7			41.5	
Approach LOS		C			E			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	21.2	99.6	17.2	32.0	22.0	98.8	10.2	39.0				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 92	12.0	* 29	* 16	87.3	* 12	28.9				
Max Q Clear Time (g_c+I1), s	5.0	9.6	7.0	25.6	18.3	2.5	2.9	5.6				
Green Ext Time (p_c), s	0.0	12.3	0.0	0.4	0.0	46.6	0.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	53.2
HCM 6th LOS	D

Notes

User approved pedestrian interval to be less than phase max green.
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Project
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	
Traffic Volume (veh/h)	33	972	53	143	1877	104	116	60	84	160	87	43
Future Volume (veh/h)	33	972	53	143	1877	104	116	60	84	160	87	43
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	33	972	53	143	1877	104	116	60	84	160	87	43
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	216	2145	957	259	2189	977	279	413	128	281	289	227
Arrive On Green	0.06	0.60	0.60	0.07	0.62	0.62	0.08	0.08	0.08	0.08	0.08	0.08
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	33	972	53	143	1877	104	116	60	84	160	87	43
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.4	23.9	2.2	6.4	68.8	4.3	5.1	1.7	8.2	7.1	3.7	2.3
Cycle Q Clear(g_c), s	1.4	23.9	2.2	6.4	68.8	4.3	5.1	1.7	8.2	7.1	3.7	2.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	216	2145	957	259	2189	977	279	413	128	281	289	227
V/C Ratio(X)	0.15	0.45	0.06	0.55	0.86	0.11	0.42	0.15	0.66	0.57	0.30	0.19
Avail Cap(c_a), veh/h	281	2145	957	272	2189	977	281	1468	456	281	1022	802
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.89	0.89	0.89	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.0	17.3	13.0	71.4	25.0	12.6	69.9	68.4	71.4	70.8	69.2	68.6
Incr Delay (d2), s/veh	0.3	0.6	0.1	2.2	4.6	0.2	1.0	0.2	5.6	2.7	0.6	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.7	10.0	0.8	2.9	29.6	1.6	2.3	0.8	3.6	3.3	1.7	0.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	71.3	17.9	13.1	73.6	29.6	12.8	70.9	68.6	76.9	73.6	69.8	69.0
LnGrp LOS	E	B	B	E	C	B	E	E	E	E	E	E
Approach Vol, veh/h		1058			2124			260			290	
Approach Delay, s/veh		19.3			31.7			72.3			71.7	
Approach LOS		B			C			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	104.6	18.6	19.1	15.7	106.6	18.7	19.0				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	62.9	* 13	46.0	* 13	62.5	* 13	46.0				
Max Q Clear Time (g_c+I1), s	8.4	25.9	7.1	5.7	3.4	70.8	9.1	10.2				
Green Ext Time (p_c), s	0.1	8.9	0.1	0.7	0.0	0.0	0.2	0.7				
Intersection Summary												
HCM 6th Ctrl Delay				34.2								
HCM 6th LOS				C								
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	T		T		T	
Traffic Vol, veh/h	26	16	157	21	5	297
Future Vol, veh/h	26	16	157	21	5	297
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	28	17	171	23	5	323

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	322	183	0	0	194
Stage 1	183	-	-	-	-
Stage 2	139	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	668	859	-	-	1378
Stage 1	816	-	-	-	-
Stage 2	834	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	665	859	-	-	1378
Mov Cap-2 Maneuver	665	-	-	-	-
Stage 1	816	-	-	-	-
Stage 2	831	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.3	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	728	1378
HCM Lane V/C Ratio	-	-	0.063	0.004
HCM Control Delay (s)	-	-	10.3	7.6
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

AM Existing + Project
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↘↗		↑	↗		↘
Traffic Vol, veh/h	28	0	104	80	0	253
Future Vol, veh/h	28	0	104	80	0	253
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	50	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	0	113	87	0	275

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	388	113	0	0	200
Stage 1	113	-	-	-	-
Stage 2	275	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	616	940	-	-	1372
Stage 1	912	-	-	-	-
Stage 2	771	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	616	940	-	-	1372
Mov Cap-2 Maneuver	616	-	-	-	-
Stage 1	912	-	-	-	-
Stage 2	771	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.1	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	616	1372
HCM Lane V/C Ratio	-	-	0.049	-
HCM Control Delay (s)	-	-	11.1	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

Intersection						
Int Delay, s/veh	1.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	2	18	49	164	268	3
Future Vol, veh/h	2	18	49	164	268	3
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	200	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	38	53	178	291	3

Major/Minor	Minor2	Major1	Major2			
Conflicting Flow All	577	293	294	0	-	0
Stage 1	293	-	-	-	-	-
Stage 2	284	-	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-	-
Pot Cap-1 Maneuver	478	746	1268	-	-	-
Stage 1	757	-	-	-	-	-
Stage 2	764	-	-	-	-	-
Platoon blocked, %				-	-	-
Mov Cap-1 Maneuver	458	746	1268	-	-	-
Mov Cap-2 Maneuver	458	-	-	-	-	-
Stage 1	725	-	-	-	-	-
Stage 2	764	-	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	10.5	1.8	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1268	-	702	-	-
HCM Lane V/C Ratio	0.042	-	0.059	-	-
HCM Control Delay (s)	8	-	10.5	-	-
HCM Lane LOS	A	-	B	-	-
HCM 95th %tile Q(veh)	0.1	-	0.2	-	-

PM Existing + Project
1: SR-76 & I-5 SB Ramp

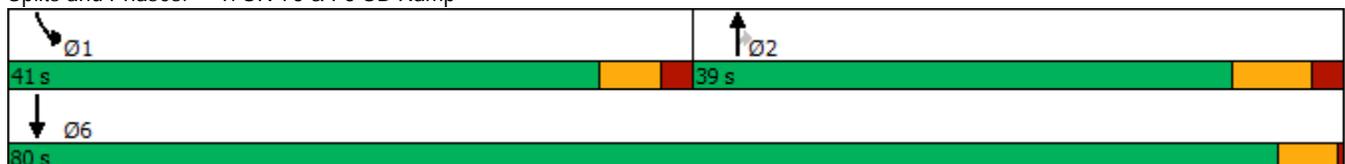
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	460	241	711	677
Future Volume (vph)	0	460	241	711	677
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	43.2	43.2	24.3	0.0
Actuated g/C Ratio	0.00	0.54	0.54	0.30	0.00
v/c Ratio	no cap	0.26	0.29	0.74	no cap
Control Delay		11.2	7.7	29.3	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	11.2	7.7	29.3	Error
LOS	F	B	A	C	F
Approach Delay	Err	10.0			Err
Approach LOS	F	A			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 40
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection Capacity Utilization 41.1%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service A

Splits and Phases: 1: SR-76 & I-5 SB Ramp



PM Existing + Project
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	686	460	241	711	677
Future Volume (veh/h)	0	686	460	241	711	677
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			500	262	773	736
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1127	502	1154	0
Arrive On Green			0.32	0.32	0.33	0.81
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			500	262	773	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			4.0	4.8	6.9	0.0
Cycle Q Clear(g_c), s			4.0	4.8	6.9	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1127	502	1154	0
V/C Ratio(X)			0.44	0.52	0.67	0.00
Avail Cap(c_a), veh/h			3195	1425	3406	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			9.7	10.0	10.2	0.0
Incr Delay (d2), s/veh			0.3	0.8	0.7	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.2	1.3	2.0	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			10.0	10.8	10.9	0.0
LnGrp LOS			A	B	B	A
Approach Vol, veh/h			762			773
Approach Delay, s/veh			10.3			10.9
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	17.7	18.2				35.8
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	8.9	6.8				0.0
Green Ext Time (p_c), s	3.1	4.5				0.0

Intersection Summary

HCM 6th Ctrl Delay	10.6
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Project
2: SR-76 & I-5 NB Ramp

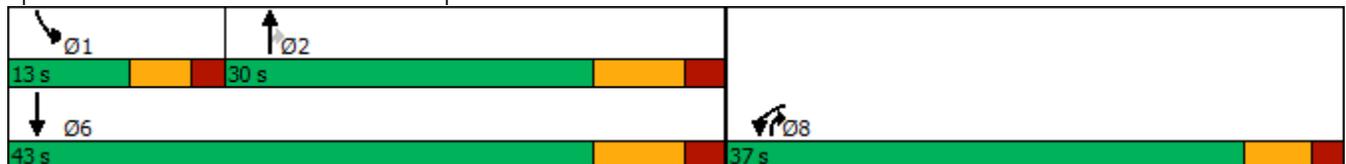
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	235	1070	101	431	1089
Future Volume (vph)	235	1070	101	431	1089
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effect Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.45	1.20	0.09	1.50	0.76
Control Delay	231.5	126.8	2.6	268.7	23.1
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	231.5	126.8	2.6	268.7	23.1
LOS	F	F	A	F	C
Approach Delay	231.5	116.1			92.7
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 140
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.50
 Intersection Signal Delay: 139.3
 Intersection LOS: F
 Intersection Capacity Utilization 122.4%
 ICU Level of Service H
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

PM Existing + Project
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	235	828	1070	101	431	1089
Future Volume (veh/h)	235	828	1070	101	431	1089
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	255	900	1163	110	468	1184
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1818	586	587	2893
Arrive On Green	0.00	0.00	0.51	0.51	0.17	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1163	110	468	1184
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	10.2	2.0	5.6	4.0
Cycle Q Clear(g_c), s	0.0	0.0	10.2	2.0	5.6	4.0
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1818	586	587	2893
V/C Ratio(X)	0.00	0.00	0.64	0.19	0.80	0.41
Avail Cap(c_a), veh/h	0	0	1818	586	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	7.6	9.2	17.1	1.1
Incr Delay (d2), s/veh	0.0	0.0	1.7	0.7	7.6	0.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	3.0	0.5	2.5	0.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	9.4	9.9	24.8	1.5
LnGrp LOS	A	A	A	A	C	A
Approach Vol, veh/h	0		1273			1652
Approach Delay, s/veh	0.0		9.4			8.1
Approach LOS			A			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	13.0	30.0			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	7.6	12.2			6.0	0.0
Green Ext Time (p_c), s	0.0	5.8			10.5	0.0
Intersection Summary						
HCM 6th Ctrl Delay			8.7			
HCM 6th LOS			A			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Existing + Project
3: Loretta St & SR-76

Timings

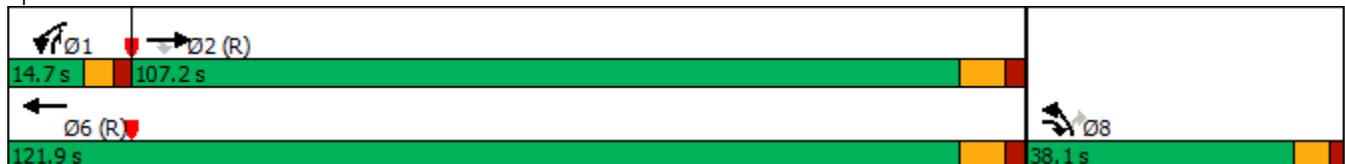


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵	↵
Traffic Volume (vph)	1909	73	20	1481	25	12
Future Volume (vph)	1909	73	20	1481	25	12
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	105.1	147.5	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.66	0.92	0.06	0.71	0.20	0.29
v/c Ratio	0.89	0.05	0.22	0.64	0.08	0.03
Control Delay	30.2	0.3	88.1	6.7	52.9	20.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	30.2	0.3	88.1	6.7	52.9	20.2
LOS	C	A	F	A	D	C
Approach Delay	29.1			7.8	42.3	
Approach LOS	C			A	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 140
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.89
 Intersection Signal Delay: 20.2
 Intersection LOS: C
 Intersection Capacity Utilization 72.9%
 ICU Level of Service C
 Analysis Period (min) 15

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

PM Existing + Project
3: Loretta St & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑	↑
Traffic Volume (veh/h)	1909	73	20	1481	25	12
Future Volume (veh/h)	1909	73	20	1481	25	12
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2075	79	22	1610	27	13
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2278	1333	63	2530	356	373
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2075	79	22	1610	27	13
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	80.6	1.3	1.9	38.2	2.0	1.0
Cycle Q Clear(g_c), s	80.6	1.3	1.9	38.2	2.0	1.0
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2278	1333	63	2530	356	373
V/C Ratio(X)	0.91	0.06	0.35	0.64	0.08	0.03
Avail Cap(c_a), veh/h	2278	1333	100	2530	356	373
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.82	0.82	1.00	1.00
Uniform Delay (d), s/veh	24.8	2.1	75.4	12.1	52.0	47.2
Incr Delay (d2), s/veh	6.9	0.1	2.7	1.0	0.4	0.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	34.8	1.1	0.9	14.9	0.9	0.4
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	31.6	2.2	78.2	13.2	52.4	47.4
LnGrp LOS	C	A	E	B	D	D
Approach Vol, veh/h	2154			1632	40	
Approach Delay, s/veh	30.6			14.0	50.8	
Approach LOS	C			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	11.3	110.6			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.9	82.6			40.2	4.0
Green Ext Time (p_c), s	0.0	13.8			22.7	0.1
Intersection Summary						
HCM 6th Ctrl Delay			23.7			
HCM 6th LOS			C			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Existing + Project
4: Canyon Dr & SR-76

Timings

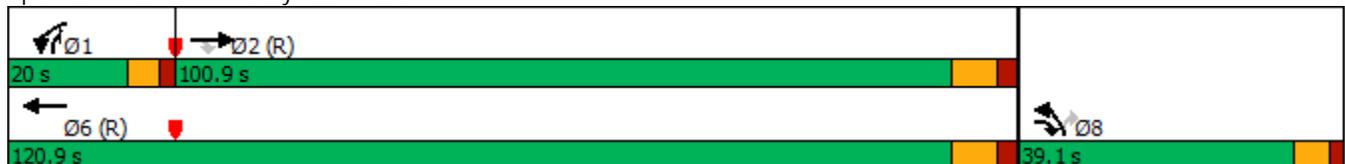


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓↓	↓
Traffic Volume (vph)	1859	48	136	1488	7	91
Future Volume (vph)	1859	48	136	1488	7	91
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.9	39.1	20.0	120.9	39.1	20.0
Total Split (%)	63.1%	24.4%	12.5%	75.6%	24.4%	12.5%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effct Green (s)	107.9	129.7	18.5	132.1	13.8	38.4
Actuated g/C Ratio	0.67	0.81	0.12	0.83	0.09	0.24
v/c Ratio	0.85	0.04	0.72	0.55	0.03	0.26
Control Delay	36.9	2.3	56.6	16.4	61.0	44.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	36.9	2.3	56.6	16.4	61.0	44.4
LOS	D	A	E	B	E	D
Approach Delay	36.0			19.8	45.7	
Approach LOS	D			B	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.85
 Intersection Signal Delay: 29.0
 Intersection LOS: C
 Intersection Capacity Utilization 83.7%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Existing + Project
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑↑	↑
Traffic Volume (veh/h)	1859	48	136	1488	7	91
Future Volume (veh/h)	1859	48	136	1488	7	91
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2021	52	148	1617	8	99
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2547	1247	159	2991	243	253
Arrive On Green	0.72	0.72	0.09	0.84	0.07	0.07
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	2021	52	148	1617	8	99
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	59.8	1.2	13.2	21.2	0.3	9.0
Cycle Q Clear(g_c), s	59.8	1.2	13.2	21.2	0.3	9.0
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2547	1247	159	2991	243	253
V/C Ratio(X)	0.79	0.04	0.93	0.54	0.03	0.39
Avail Cap(c_a), veh/h	2547	1247	159	2991	713	469
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.34	0.34	0.43	0.43	1.00	1.00
Uniform Delay (d), s/veh	14.9	3.8	72.4	3.7	69.3	60.3
Incr Delay (d2), s/veh	0.9	0.0	29.6	0.3	0.1	1.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	23.1	0.5	7.4	6.3	0.2	3.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	15.8	3.8	101.9	4.0	69.4	61.2
LnGrp LOS	B	A	F	A	E	E
Approach Vol, veh/h	2073			1765	107	
Approach Delay, s/veh	15.5			12.2	61.9	
Approach LOS	B			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	20.0	122.7			142.7	17.3
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 14	92.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	15.2	61.8			23.2	11.0
Green Ext Time (p_c), s	0.0	22.2			23.7	0.3

Intersection Summary

HCM 6th Ctrl Delay	15.3
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Project
5: Benet Rd & SR-76

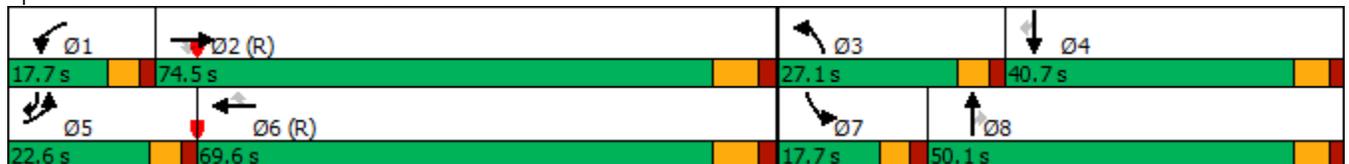
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	213	1711	88	35	1307	13	129	23	91	15	15	189
Future Volume (vph)	213	1711	88	35	1307	13	129	23	91	15	15	189
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	22.6	74.5	74.5	17.7	69.6	69.6	27.1	50.1	50.1	17.7	40.7	22.6
Total Split (%)	14.1%	46.6%	46.6%	11.1%	43.5%	43.5%	16.9%	31.3%	31.3%	11.1%	25.4%	14.1%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	31.7	92.9	92.9	12.0	69.7	69.7	17.4	31.8	31.8	12.0	15.7	53.5
Actuated g/C Ratio	0.20	0.58	0.58	0.08	0.44	0.44	0.11	0.20	0.20	0.08	0.10	0.33
v/c Ratio	0.66	0.91	0.10	0.29	0.92	0.02	0.73	0.07	0.25	0.12	0.09	0.34
Control Delay	58.8	51.4	8.1	74.1	44.0	0.0	89.9	51.3	7.2	71.5	62.0	19.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	58.8	51.4	8.1	74.1	44.0	0.0	89.9	51.3	7.2	71.5	62.0	19.4
LOS	E	D	A	E	D	A	F	D	A	E	E	B
Approach Delay		50.3			44.4			55.2			25.8	
Approach LOS		D			D			E			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 22.6 (14%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.92
 Intersection Signal Delay: 47.1
 Intersection LOS: D
 Intersection Capacity Utilization 87.6%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

PM Existing + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	213	1711	88	35	1307	13	129	23	91	15	15	189
Future Volume (veh/h)	213	1711	88	35	1307	13	129	23	91	15	15	189
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	232	1860	96	38	1421	14	140	25	99	16	16	205
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	1985	885	109	1827	815	162	342	290	68	243	373
Arrive On Green	0.11	0.56	0.56	0.06	0.51	0.51	0.09	0.18	0.18	0.04	0.13	0.13
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	232	1860	96	38	1421	14	140	25	99	16	16	205
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	77.6	4.6	3.3	51.8	0.7	12.4	1.8	8.7	1.4	1.2	18.2
Cycle Q Clear(g_c), s	16.9	77.6	4.6	3.3	51.8	0.7	12.4	1.8	8.7	1.4	1.2	18.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	1985	885	109	1827	815	162	342	290	68	243	373
V/C Ratio(X)	1.23	0.94	0.11	0.35	0.78	0.02	0.86	0.07	0.34	0.24	0.07	0.55
Avail Cap(c_a), veh/h	188	1985	885	134	1827	815	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.47	0.47	0.47	0.77	0.77	0.77	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	32.7	16.6	72.1	31.5	19.1	71.7	54.2	57.0	74.7	61.1	53.7
Incr Delay (d2), s/veh	124.9	5.3	0.1	1.5	2.6	0.0	19.0	0.1	0.7	1.8	0.1	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	14.3	34.2	1.7	1.6	22.8	0.3	6.6	0.9	3.6	0.7	0.6	7.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	196.4	38.0	16.7	73.5	34.1	19.1	90.7	54.2	57.7	76.4	61.2	55.0
LnGrp LOS	F	D	B	E	C	B	F	D	E	E	E	D
Approach Vol, veh/h		2188			1473			264			237	
Approach Delay, s/veh		53.9			34.9			74.9			56.8	
Approach LOS		D			C			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	15.5	97.4	20.3	26.9	22.6	90.3	11.8	35.3				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.3	79.6	14.4	20.2	18.9	53.8	3.4	10.7				
Green Ext Time (p_c), s	0.0	0.0	0.2	0.6	0.0	5.5	0.0	0.4				

Intersection Summary

HCM 6th Ctrl Delay	48.7
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Project
6: Foussat Rd & SR-76

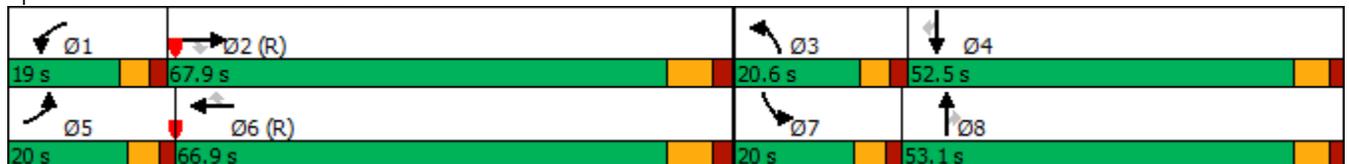
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	27	1725	127	98	1237	117	78	127	136	91	76	26
Future Volume (vph)	27	1725	127	98	1237	117	78	127	136	91	76	26
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	20.0	67.9	67.9	19.0	66.9	66.9	20.6	53.1	53.1	20.0	52.5	52.5
Total Split (%)	12.5%	42.4%	42.4%	11.9%	41.8%	41.8%	12.9%	33.2%	33.2%	12.5%	32.8%	32.8%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.0	89.7	89.7	12.2	96.4	96.4	13.0	19.6	19.6	13.0	19.6	19.6
Actuated g/C Ratio	0.08	0.56	0.56	0.08	0.60	0.60	0.08	0.12	0.12	0.08	0.12	0.12
v/c Ratio	0.10	0.95	0.15	0.41	0.63	0.13	0.31	0.22	0.46	0.36	0.19	0.06
Control Delay	55.9	40.5	17.2	75.3	25.5	4.2	72.4	62.0	11.7	73.4	61.2	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	55.9	40.5	17.2	75.3	25.5	4.2	72.4	62.0	11.7	73.4	61.2	0.3
LOS	E	D	B	E	C	A	E	E	B	E	E	A
Approach Delay		39.1			27.1			44.3			58.8	
Approach LOS		D			C			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.95
 Intersection Signal Delay: 36.1
 Intersection LOS: D
 Intersection Capacity Utilization 85.4%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



LOS Engineering, Inc.

PM Existing + Project
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	 
Traffic Volume (veh/h)	27	1725	127	98	1237	117	78	127	136	91	76	26
Future Volume (veh/h)	27	1725	127	98	1237	117	78	127	136	91	76	26
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	29	1875	138	107	1345	127	85	138	148	99	83	28
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	203	2037	909	257	2092	933	274	576	179	277	404	317
Arrive On Green	0.06	0.57	0.57	0.07	0.59	0.59	0.08	0.11	0.11	0.08	0.11	0.11
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	29	1875	138	107	1345	127	85	138	148	99	83	28
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.3	76.3	6.5	4.7	40.1	5.7	3.7	3.9	14.6	4.3	3.4	1.4
Cycle Q Clear(g_c), s	1.3	76.3	6.5	4.7	40.1	5.7	3.7	3.9	14.6	4.3	3.4	1.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	203	2037	909	257	2092	933	274	576	179	277	404	317
V/C Ratio(X)	0.14	0.92	0.15	0.42	0.64	0.14	0.31	0.24	0.83	0.36	0.21	0.09
Avail Cap(c_a), veh/h	309	2037	909	287	2092	933	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.38	0.38	0.38	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.5	30.8	16.0	70.7	21.8	14.7	69.5	64.7	69.5	69.7	64.4	63.5
Incr Delay (d2), s/veh	0.1	3.5	0.1	1.1	1.5	0.3	0.6	0.2	9.4	0.8	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.6	32.9	2.5	2.2	17.1	2.2	1.7	1.7	6.4	2.0	1.6	0.5
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	71.6	34.4	16.1	71.8	23.3	15.0	70.1	64.9	78.8	70.4	64.6	63.6
LnGrp LOS	E	C	B	E	C	B	E	E	E	E	E	E
Approach Vol, veh/h		2042			1579			371			210	
Approach Delay, s/veh		33.7			25.9			71.7			67.2	
Approach LOS		C			C			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.6	99.7	18.4	24.3	15.1	102.2	18.5	24.1				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	6.7	78.3	5.7	5.4	3.3	42.1	6.3	16.6				
Green Ext Time (p_c), s	0.1	0.0	0.1	0.6	0.0	9.7	0.1	1.4				

Intersection Summary

HCM 6th Ctrl Delay	35.8
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Project
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			W
Traffic Vol, veh/h	17	11	229	14	5	195
Future Vol, veh/h	17	11	229	14	5	195
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	18	12	249	15	5	212

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	352	257	0	0	264
Stage 1	257	-	-	-	-
Stage 2	95	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	644	781	-	-	1299
Stage 1	757	-	-	-	-
Stage 2	878	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	641	781	-	-	1299
Mov Cap-2 Maneuver	641	-	-	-	-
Stage 1	757	-	-	-	-
Stage 2	874	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.5	0	0.2
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	690	1299
HCM Lane V/C Ratio	-	-	0.044	0.004
HCM Control Delay (s)	-	-	10.5	7.8
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0

LOS Engineering, Inc.

PM Existing + Project
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	2.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔		↑	↑		↔
Traffic Vol, veh/h	82	0	206	36	0	120
Future Vol, veh/h	82	0	206	36	0	120
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	50	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	89	0	224	39	0	130

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	354	224	0	0	263	0
Stage 1	224	-	-	-	-	-
Stage 2	130	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	644	815	-	-	1301	-
Stage 1	813	-	-	-	-	-
Stage 2	896	-	-	-	-	-
Platoon blocked, %			-	-		
Mov Cap-1 Maneuver	644	815	-	-	1301	-
Mov Cap-2 Maneuver	644	-	-	-	-	-
Stage 1	813	-	-	-	-	-
Stage 2	896	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.5	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	644	1301
HCM Lane V/C Ratio	-	-	0.138	-
HCM Control Delay (s)	-	-	11.5	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.5	0

LOS Engineering, Inc.

Intersection						
Int Delay, s/veh	2.5					
Movement	EBL	EBR	NBL	NBT	SBT	SBR
Lane Configurations						
Traffic Vol, veh/h	2	48	38	226	140	2
Future Vol, veh/h	2	48	38	226	140	2
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	200	-	-	-
Veh in Median Storage, #	0	-	-	0	0	-
Grade, %	0	-	-	0	0	-
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	4	104	41	246	152	2

Major/Minor	Minor2	Major1		Major2	
Conflicting Flow All	481	153	154	0	0
Stage 1	153	-	-	-	-
Stage 2	328	-	-	-	-
Critical Hdwy	6.42	6.22	4.12	-	-
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	2.218	-	-
Pot Cap-1 Maneuver	544	893	1426	-	-
Stage 1	875	-	-	-	-
Stage 2	730	-	-	-	-
Platoon blocked, %				-	-
Mov Cap-1 Maneuver	528	893	1426	-	-
Mov Cap-2 Maneuver	528	-	-	-	-
Stage 1	850	-	-	-	-
Stage 2	730	-	-	-	-

Approach	EB	NB	SB
HCM Control Delay, s	9.7	1.1	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBL	NBT	EBLn1	SBT	SBR
Capacity (veh/h)	1426	-	869	-	-
HCM Lane V/C Ratio	0.029	-	0.125	-	-
HCM Control Delay (s)	7.6	-	9.7	-	-
HCM Lane LOS	A	-	A	-	-
HCM 95th %tile Q(veh)	0.1	-	0.4	-	-

Appendix I

Cumulative Project Traffic Volumes and Assignments

Cumulative project traffic volumes are summarized on the next two pages with sources included after the summary tables.

Cumulative Projects	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
1) SR-76/I-SB Cumulative AM Sum:	0	83	106	104	140	0	0	0	0	0	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18		5		11	70							
Ocean Kamp		18		60	24							
N. River Farms		5		25	5							
Airport Rd Industrial				0								
Alta Oceanside		28	56		36							
Liberty RV and Boat Storage												
Mission Flats		1		3	2							
Modera Neptune and Melrose		26	50		3							
Nitto Denko				1								
Rio Rockwell				4								
Cumulative PM Sum:	(0)	(72)	(47)	(98)	(169)	0	0	0	0	0	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18		(5)		(15)	(43)							
Ocean Kamp		(36)		(63)	(25)							
N. River Farms		(5)		(10)	(5)							
Airport Rd Industrial				(4)								
Alta Oceanside		(13)	(26)		(83)							
Liberty RV and Boat Storage												
Mission Flats		(3)		(1)	(1)							
Modera Neptune and Melrose		(10)	(21)		(12)							
Nitto Denko				(4)								
Rio Rockwell				(1)								
2) SR-76/I-5 NB Cumulative AM Sum:	0	121	27	82	215	0	0	0	0	24	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18		15		10	81							
Ocean Kamp		59		54	85							
N. River Farms		10		10	30							
Airport Rd Industrial		4		1	0							
Alta Oceanside		14	14		6							
Liberty RV and Boat Storage				1								
Mission Flats		2		5	5							
Modera Neptune and Melrose		13	13		3							
Nitto Denko		4			1							
Rio Rockwell				1	4					24		
Cumulative PM Sum:	(0)	(195)	(22)	(86)	(203)	0	0	0	0	(55)	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18		(19)		(14)	(58)							
Ocean Kamp		(117)		(57)	(88)							
N. River Farms		(40)		(5)	(20)							
Airport Rd Industrial		(1)		(4)	(4)							
Alta Oceanside		(7)	(7)		(14)							
Liberty RV and Boat Storage				(1)								
Mission Flats		(6)		(2)	(2)							
Modera Neptune and Melrose		(5)	(15)		(12)							
Nitto Denko				(3)	(4)							
Rio Rockwell					(1)					(55)		
3) SR-76/Loretta St Cumulative AM Sum:	0	0	5	0	0	0	0	196	0	14	297	0
Cumulative projects 1,4,5,7,12,14,15,17,18								27			91	
Ocean Kamp			5					104		6	139	
N. River Farms								15		8	40	
Airport Rd Industrial								8			1	
Alta Oceanside								14			6	
Liberty RV and Boat Storage								1			1	
Mission Flats								5			10	
Modera Neptune and Melrose								13			3	
Nitto Denko								8			1	
Rio Rockwell								1			5	
Cumulative PM Sum:	(0)	(0)	(9)	(0)	(0)	(0)	(0)	(328)	(0)	(12)	(289)	(0)
Cumulative projects 1,4,5,7,12,14,15,17,18								(35)			(72)	
Ocean Kamp			(9)					(207)		(6)	(145)	
N. River Farms								(50)		(6)	(25)	
Airport Rd Industrial								(2)			(8)	
Alta Oceanside								(7)			(14)	
Liberty RV and Boat Storage								(1)			(1)	
Mission Flats								(15)			(4)	
Modera Neptune and Melrose								(5)			(12)	
Nitto Denko								(1)			(7)	
Rio Rockwell								(5)			(1)	
4) SR-76/Canyon Dr Cumulative AM Sum:	0	0	14	0	0	0	0	201	0	26	304	0
Cumulative projects 1,4,5,7,12,14,15,17,18								27			91	
Ocean Kamp			14					109		18	145	
N. River Farms								15		8	40	
Airport Rd Industrial								8			2	
Alta Oceanside								14			6	
Liberty RV and Boat Storage								1			1	
Mission Flats								5			10	
Modera Neptune and Melrose								13			3	

Cumulative Projects	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
Nitto Denko								8				1
Rio Rockwell								1				5
Cumulative PM Sum:	(0)	(0)	(27)	(0)	(0)	(0)	(0)	(336)	(0)	(25)	(295)	(0)
Cumulative projects 1,4,5,7,12,14,15,17,18								(35)			(72)	
Ocean Kamp			(27)					(216)		(19)	(151)	
N. River Farms								(50)		(6)	(25)	
Airport Rd Industrial								(1)			(8)	
Alta Oceanside								(7)			(14)	
Liberty RV and Boat Storage								(1)			(1)	
Mission Flats								(15)			(4)	
Modera Neptune and Melrose								(5)			(12)	
Nitto Denko								(1)			(7)	
Rio Rockwell								(5)			(1)	
5) SR-76/Benet Rd Cumulative AM Sum:	2	0	2	5	0	1	8	197	9	17	318	6
Cumulative projects 1,4,5,7,12,14,15,17,18								27			91	
Ocean Kamp				5				122			163	6
N. River Farms								15		8	40	
Airport Rd Industrial						1	8					
Alta Oceanside								14			6	
Liberty RV and Boat Storage	1		1					5	1	1	10	
Mission Flats								13			3	
Modera Neptune and Melrose												
Nitto Denko	1		1						8	8		
Rio Rockwell								1			5	
Cumulative PM Sum:	(8)	(0)	(8)	(9)	(0)	(8)	(2)	(361)	(2)	(8)	(298)	(6)
Cumulative projects 1,4,5,7,12,14,15,17,18								(35)			(72)	
Ocean Kamp				(9)				(244)			(170)	(6)
N. River Farms								(50)		(6)	(25)	
Airport Rd Industrial						(8)	(2)					
Alta Oceanside								(7)			(14)	
Liberty RV and Boat Storage	(1)		(1)						(1)	(1)		
Mission Flats								(15)			(4)	
Modera Neptune and Melrose								(5)			(12)	
Nitto Denko	(7)		(7)						(1)	(1)		
Rio Rockwell								(5)			(1)	
6) SR-76/Foussat Rd Cumulative AM Sum:	14	60	32	54	77	175	132	47	31	35	134	41
Cumulative projects 1,4,5,7,12,14,15,17,18		5	26		5				27	27	65	
Ocean Kamp		55		54	72	175	132					41
N. River Farms			5					15		8	40	
Airport Rd Industrial								1			8	
Alta Oceanside	2							10	4		4	
Liberty RV and Boat Storage								1			1	
Mission Flats	12		1					5				
Modera Neptune and Melrose								13			3	
Nitto Denko								1			8	
Rio Rockwell								1			5	
Cumulative PM Sum:	(8)	(134)	(51)	(70)	(101)	(226)	(305)	(96)	(37)	(41)	(92)	(95)
Cumulative projects 1,4,5,7,12,14,15,17,18		(7)	(34)		(7)				(35)	(35)	(38)	
Ocean Kamp		(127)		(70)	(94)	(226)	(305)					(95)
N. River Farms			(10)					(50)		(6)	(25)	
Airport Rd Industrial								(8)			(2)	
Alta Oceanside	(2)							(5)	(2)		(12)	
Liberty RV and Boat Storage								(1)			(1)	
Mission Flats	(6)		(7)					(15)				
Modera Neptune and Melrose								(5)			(12)	
Nitto Denko								(7)			(1)	
Rio Rockwell								(5)			(1)	
7) Benet Rd/Airport Rd Cumulative AM Sum:	0	6	8	0	5	0	0	0	0	1	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18												
Ocean Kamp		6			5							
N. River Farms												
Airport Rd Industrial			8							1		
Alta Oceanside												
Liberty RV and Boat Storage												
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												
Cumulative PM Sum:	(0)	(6)	(2)	(0)	(9)	(0)	(0)	(0)	(0)	(8)	(0)	(0)
Cumulative projects 1,4,5,7,12,14,15,17,18												
Ocean Kamp		(6)			(9)							
N. River Farms												
Airport Rd Industrial			(2)							(8)		
Alta Oceanside												
Liberty RV and Boat Storage												

Cumulative Projects	NBL	NBT	NBR	SBL	SBT	SBR	EBL	EBT	EBR	WBL	WBT	WBR
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												
8) Benet Rd/Eddy Jones Way Cumulative AM Sum:	0	6	0	0	5	0	0	0	0	0	0	0
Cumulative projects 1,4,5,7,12,14,15,17,18												
Ocean Kamp		6			5							
N. River Farms												
Airport Rd Industrial												
Alta Oceanside												
Liberty RV and Boat Storage												
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												
Cumulative PM Sum:	(0)	(6)	(0)	(0)	(9)	(0)	(0)	(0)	(0)	(0)	(0)	(0)
Cumulative projects 1,4,5,7,12,14,15,17,18												
Ocean Kamp		(6)			(9)							
N. River Farms												
Airport Rd Industrial												
Alta Oceanside												
Liberty RV and Boat Storage												
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												
9) Foussat/Alex Rd Cumulative AM Sum:	0	141	91	32	186	0	0	4	0	121	6	43
Cumulative projects 1,4,5,7,12,14,15,17,18		5			5							
Ocean Kamp		136	91	32	181			4		121	6	43
N. River Farms												
Airport Rd Industrial												
Alta Oceanside												
Liberty RV and Boat Storage												
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												
Cumulative PM Sum:	(0)	(323)	(211)	(74)	(241)	(0)	(0)	(11)	(0)	(156)	(8)	(55)
Cumulative projects 1,4,5,7,12,14,15,17,18		(7)			(7)							
Ocean Kamp		(316)	(211)	(74)	(234)			(11)		(156)	(8)	(55)
N. River Farms												
Airport Rd Industrial												
Alta Oceanside												
Liberty RV and Boat Storage												
Mission Flats												
Modera Neptune and Melrose												
Nitto Denko												
Rio Rockwell												

	Cumulative C ADT	1,4,5,7,12 14,15,17,18	Ocean Kamp	N. River Farms	Airport Ind. Liberty RV	Alta Oceanside	Mission Flats Neptune Mel.	Nitto Rio Rock
<u>Benet Road</u>								
1) Eddy Jones to SR-76	190	0	190	0	0	0	0	0
<u>Foussat Rd</u>								
2) Alex Rd to SR-76	9770	250	9520	0	0	0	0	0
<u>State Route 76</u>								
3) I-5 to Loretta St	8450	2260	4380	1000	100	250	397	63
4) Loretta St to Canyon Dr	8640	2260	4570	1000	100	250	397	63
5) Canyon Dr to Benet Rd	9210	2260	5140	1000	100	250	397	63
6) Benet Rd to Foussat Rd	9590	2260	5520	1000	100	250	397	63
7) East of Foussat Rd	7003	2260	2670	1070	100	200	640	63

**Traffic Impact Analysis
Alta Oceanside, City of Oceanside**

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~~OCTOBER 2019~~ MARCH 2020

Traffic Impact Analysis – Alta Oceanside

3 PROJECT TRAFFIC

This section documents the trip generation, distribution, and assignment of project traffic.

3.1 Trip Generation

Trip generation estimates for the proposed project are based on daily and AM and PM peak hour trip generation rates obtained from the SANDAG (*Not So*) *Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region* (2002). Trip generation estimates for the project are based on the trip generation rates for multi-family residential land uses as well as the sit-down, high turnover restaurant land use. The trip rates for the residential units were classified as multi-family >20 du/acre. The use of trip rates for the sit-down, high turnover restaurant land use is a conservative rate, as it represents the highest retail trip generator that could be developed on the project site. In addition, the square footage of the retail use was conservatively assumed to be 5,800 SF, and pending the final site plan, the retail use would be slightly smaller at 5,422 SF. Other retail uses that could utilize the ground floor retail space would generate less traffic. Trip generation rates and resulting trip generation estimates for the project are summarized in Table 7.

**Table 7
Project Trip Generation for Alta Oceanside Mixed-Use Project**

SANDAG Trip Generation Rates									
Land Use	Size/Unit		Daily	AM Peak Hour			PM Peak Hour		
				% In	% Out	Total	% In	% Out	Total
Residential Apartments (multi-family >20du/a)	per DU		6	20%	80%	8%	70%	30%	9%
Restaurant - Sit-Down, High Turnover	per TSF		160	50%	50%	8%	60%	40%	8%
Trip Generation									
Apartment Units	309	DU	1,854	30	118	148	117	50	167
High-Turnover Restaurant	5.80	TSF	928	37	37	74	44	30	74
Total Trip Generation			2,782	67	155	222	161	80	241
<i>Restaurant pass-by trips (10% Daily/AM, 20% PM)¹</i>			0	0	0	0	-9	-6	-15
<i>Vehicle Trip Reduction (10%)²</i>			-278	-7	-16	-23	-16	-8	-24
Total NET Trip Generation			2,504	60	139	199	136	66	202

Notes: TSF = 1000 square feet; DU = Dwelling Unit

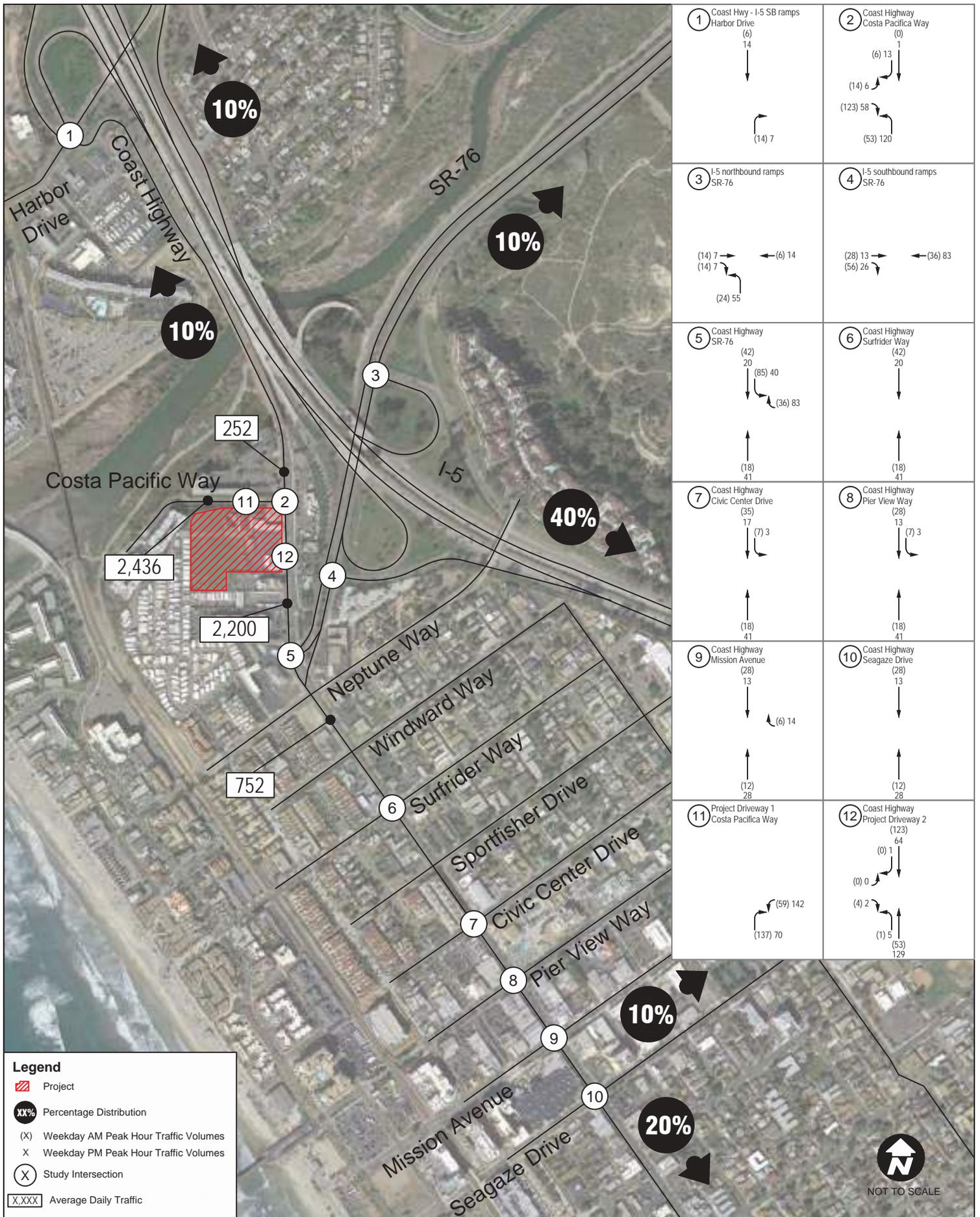
Trip rates from the SANDAG 2002.

¹ SANDAG does not provide daily and AM peak hour pass-by percentages. Daily and AM pass-by percentages were assumed to be one-half of the PM Peak Hour pass by percentages.

² Consistent with SANDAG's regional "smart growth" policies, vehicle trip reductions were applied for mixed-use developments.

As shown in the Table 7, the project would generate 2,782 daily trips, 222 AM peak hour trips (67 inbound and 155 outbound), and 241 trips during the PM peak hour (161 inbound and 80 outbound).

However, applying the allowed trip reduction methods for pass-by trips (existing traffic that would pass-by the retail/restaurant use on their primary trips) and internal trip capture (trips that would



<p>① Coast Hwy - I-5 SB ramps Harbor Drive (6) 14 ↓ (14) 7 ↙</p>	<p>② Coast Highway Costa Pacific Way (0) 1 ↓ (6) 13 (14) 6 (123) 58 ↘ (53) 120</p>
<p>③ I-5 northbound ramps SR-76 (14) 7 (14) 7 ↘ (24) 55</p>	<p>④ I-5 southbound ramps SR-76 (28) 13 (56) 26 ↘ (36) 83</p>
<p>⑤ Coast Highway SR-76 (42) 20 (85) 40 ↘ (36) 83</p>	<p>⑥ Coast Highway Surf Rider Way (42) 20 ↓ (18) 41</p>
<p>⑦ Coast Highway Civic Center Drive (35) 17 (7) 3 ↘</p>	<p>⑧ Coast Highway Pier View Way (28) 13 (7) 3 ↘</p>
<p>⑨ Coast Highway Mission Avenue (28) 13 ↘ (6) 14</p>	<p>⑩ Coast Highway Seagaze Drive (28) 13 ↓ (12) 28</p>
<p>⑪ Project Driveway 1 Costa Pacific Way (59) 142 (137) 70</p>	<p>⑫ Coast Highway Project Driveway 2 (123) 64 (0) 1 (0) 0 (4) 2 (1) 5 (53) 129</p>

Source: Google Maps, 03/2018

FIGURE 6
Project Trip Distribution and Assignment
Page 171 of 340
Alta Oceanside Project

LOCAL TRANSPORTATION STUDY
OCEAN KAMP PROJECT
Oceanside, California
July 14, 2021

LLG Ref. 3-19-3145

**TABLE 7-2
WEEKDAY PROJECT TRIP GENERATION**

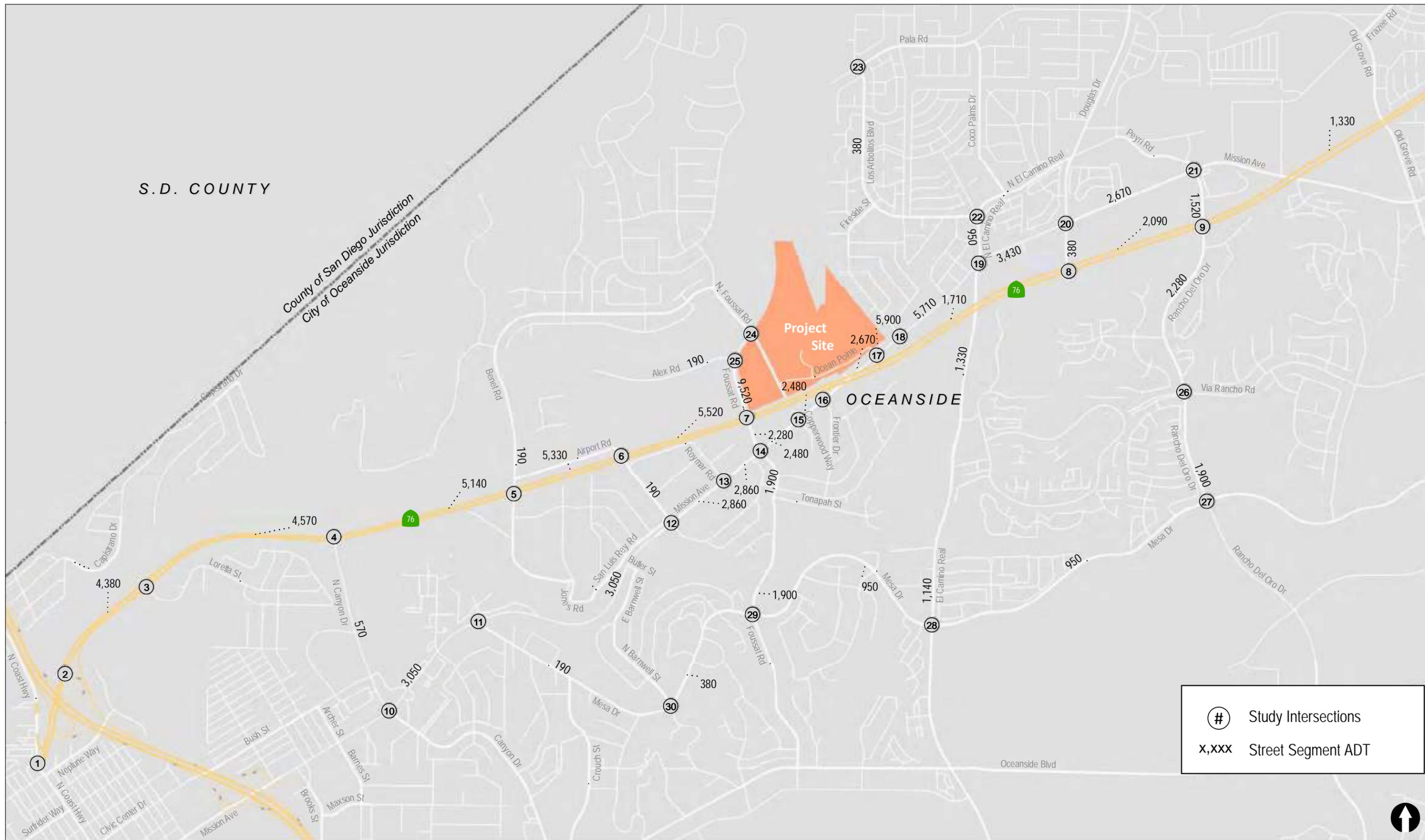
Land Use	Size	Daily Trip Ends (ADTs)		AM Peak Hour					PM Peak Hour				
		Rate ^a	Volume	% of ADT	In:Out Split	Volume			% of ADT	In:Out Split	Volume		
						In	Out	Total			In	Out	Total
Hotel (w/ convention facilities / restaurant)	300 Rooms	10 / Room	3,000	6%	60:40	108	72	180	8%	60:40	144	96	240
Multi-Family Residential	700 Units	8 / DU	5,600	8%	20:80	90	358	448	10%	70:30	392	168	560
Retail / Commercial Center	126 KSF	80 / KSF	10,080	4%	60:40	242	161	403	10%	50:50	504	504	1,008
<i>Pass-by Credit (30% PM peak)</i>	-	-	-	-	-	-	-	-	-	-	-151	-151	-302
Surf Lagoon / Resort Pass Guests ^b	1 Site	-	360	-	-	13	13	26	-	-	13	13	26
Proposed Project Weekday Total			19,040			453	604	1,057			902	630	1,532
Proposed Project Weekday Total (Driveway Trips) ^c			19,040			453	604	1,057			1,053	781	1,834

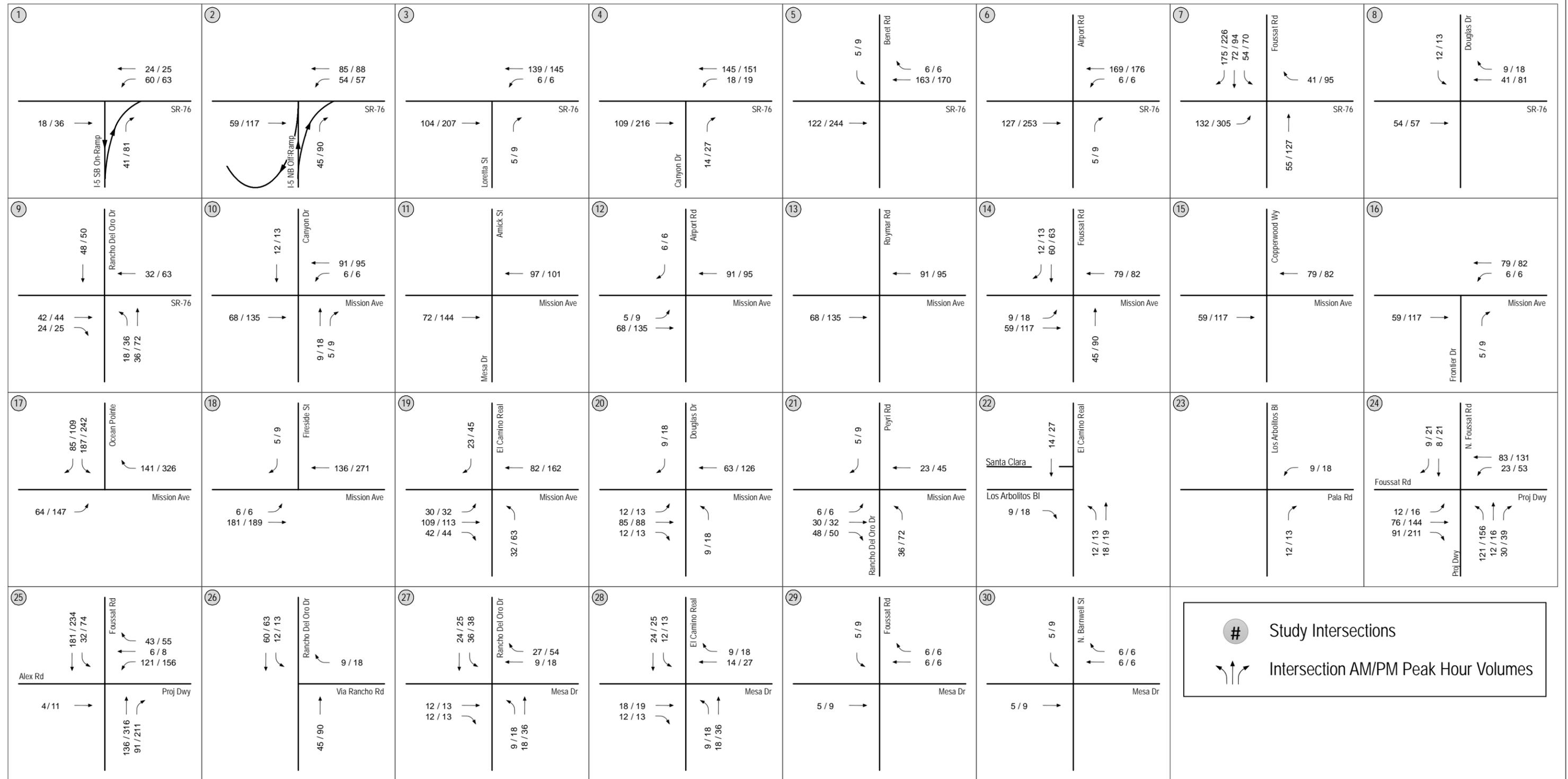
Footnotes:

- a. Rates based on SANDAG's (Not So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002 except where noted.
- b. 120 Surf Lagoon guests and 50 Report Pass guests expected daily. See *Appendix E* for Trip Generation Calculations.
- c. Driveway trip calculations do not include pass-by credits.

General Notes:

1. ADT = Average daily traffic





Study Intersections
 ↑ Intersection AM/PM Peak Hour Volumes



**TABLE 6-1
CUMULATIVE PROJECTS**

Project Name	Type of Development	Project Size	ADT	AM Peak Hour Trips	PM Peak Hour Trips
1. Oceanpointe Development	Multi-Family Residential	158 dwelling units	1,264	101	126
2. El Corazon Phases 1-3	Residential Retail Arena Aquatic Center	340 dwelling units 158,000 SF 6,000 seats 3,500 SF	15,498	715	1,534
3. Oceanside + Melrose	Residential Restaurant Commercial Office	313 dwelling units 10,000 SF 10,000 SF	4,059	340	360
4. North River Road Residential Subdivision (Kawano-Nagata)	Residential	400 dwelling units	3,200	256	320
5. Onpoint Oceanside	Gas Station & Car Wash Food Mart Retail Fast Food Restaurant High-Turn Over Restaurant	12 pumps 3,000 SF 7,980 SF 2,500 SF 2,320 SF	5,068	213	273
6. Airport Hotel	Hotel	86 rooms	688	34	48
7. Villas at Mission San Luis Rey	Retirement Community	222 units	694	32	49
8. Concordia Collection at Cypress Point	Residential	54 dwelling units	540	42	53

Figure 6-1 shows the locations of the cumulative projects. **Figures 6-2a** and **6-2b** depict the Cumulative Projects daily traffic volumes and AM / PM peak hour traffic volumes, respectively.

Appendix L includes additional information on the Cumulative Projects.

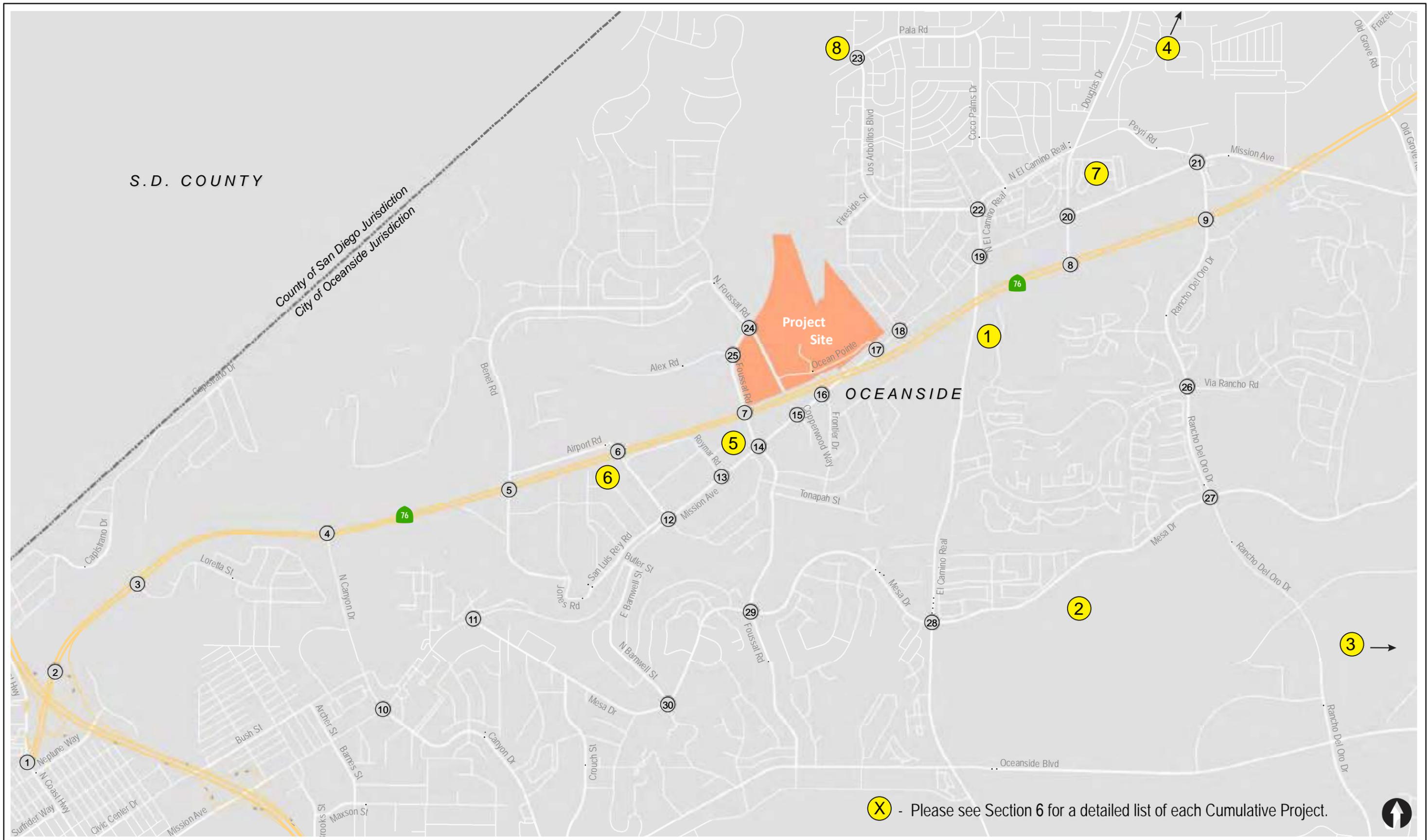


Figure 6-1

Cumulative Projects Location Map

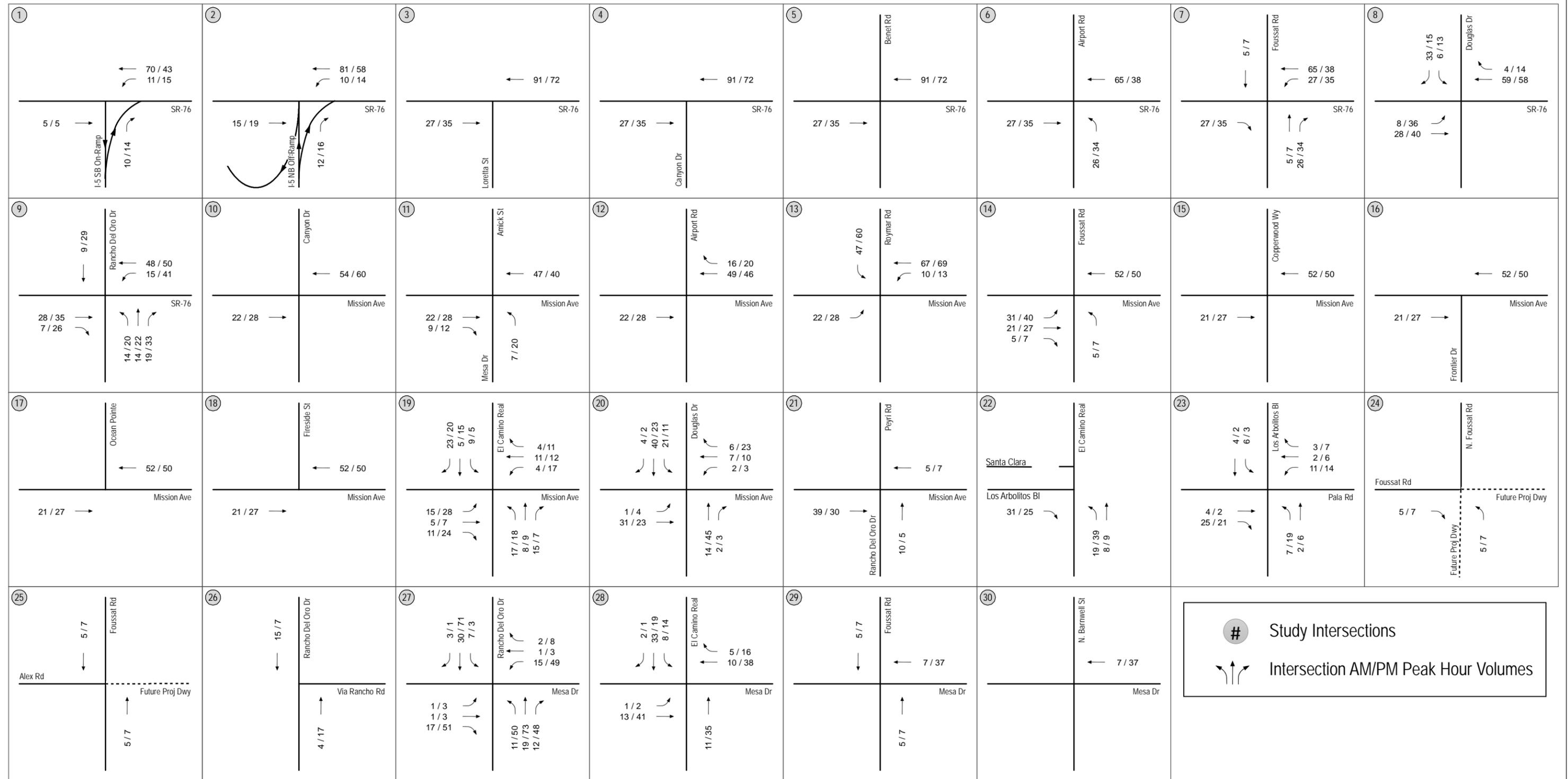


Figure 6-2b
 Near-Term Cumulative Projects Traffic Volumes

TRANSPORTATION IMPACT ANALYSIS
NORTH RIVER FARMS
Oceanside, California
July 23, 2018

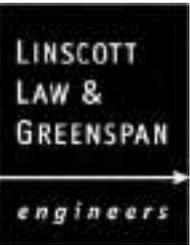
LLG Ref. 3-16-2596

**TABLE 7-1
PROJECT TRIP GENERATION**

Land Use	Size	Daily Trip Ends (ADTs)		AM Peak Hour						PM Peak Hour						
		Rate ^a	Volume	% of ADT ^a	In:Out		Volume		% of ADT ^a	In:Out		Volume				
					Split	In	Out	Total		Split	In	Out	Total			
RESIDENTIAL TRIPS																
UNIT TYPE																
-	Single-Family Detached and Multi-Family Attached (≥ 20 DU per acre)	130 DU	6 /DU	780	8%	20%	80%	12	50	62	9%	70%	30%	49	21	70
-	Single-Family Detached (≤ 20 DU per acre)	250 DU	8 /DU	2,000	8%	20%	80%	32	128	160	10%	70%	30%	140	60	200
-	Single-Family Detached (≤ 6 DU per acre)	309 DU	10 /DU	3,090	8%	30%	70%	74	173	247	10%	70%	30%	216	93	309
A	RESIDENTIAL TRIP GENERATION	689 DU	—	5,870	—	—	—	118	351	469	—	—	—	405	174	579
NON-RESIDENTIAL COMPONENT																
B	Commercial (C + D)	25 KSF	40 /KSF	1,000	3%	60%	40%	18	12	30	9%	50%	50%	45	45	90
C	Primary External Trips ^d	90%	—	900	—	—	—	18	12	30	9%	50%	50%	40	40	80
D	Pass-by External Trips ^d	10%	—	100	—	—	—	—	—	—	9%	50%	50%	5	5	10
E	Restaurant ^b (F + G)	5 KSF	100 /KSF	500	1%	60%	40%	3	2	5	8%	70%	30%	28	12	40
F	Primary External Trips ^d	90%	—	450	—	—	—	3	2	5	8%	70%	30%	25	11	36
G	Pass-by External Trips ^d	10%	—	50	—	—	—	—	—	—	8%	70%	30%	3	1	4
H	Farm ^c	30 acres	2 /acre	60	0.26	43%	57%	7	9	16	0.45	57%	43%	15	12	27
I	Hotel ^e	100 rooms	9 /room	900	8%	40%	60%	29	43	72	9%	60%	40%	49	32	81
J	Subtotal Primary Trips (Residential + Primary Commercial Trips + Hotel Trips) (A + C + F + H + I)			8,180	—	—	—	175	417	592	—	—	—	534	269	803
K	Mixed Use Reduction ^d (J x 5%)	5%	—	(409)	—	—	—	(9)	(21)	(30)	—	—	—	(27)	(13)	(40)
L	TOTAL PRIMARY TRIPS (J - K)			7,771	—	—	—	166	396	562	—	—	—	507	256	763
M	TOTAL DRIVEWAY TRIPS (A + B + E + H + I) - K			7,921	—	—	—	166	396	562	—	—	—	515	262	777

Footnotes:

- Rates based on SANDAG's (Not So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002
- Restaurant uses "Quality Restaurant" rate from SANDAG.
- For Farm, SANDAG "agriculture" rate applied. For peak splits, ITE 818 "Nursery (Wholesale)" rate applied.
- Primary trips, pass-by trips, and mixed-use credit percentages sourced to SANDAG.
- For Hotel, SANDAG "hotel" rate reduced from 10 ADT/room to 9 ADT/room since the "hotel" rate includes trips generated by convention facilities, which are not proposed by the Project.



TRAFFIC IMPACT ANALYSIS
OCEANSIDE RIO ROCKWELL PROJECT
Oceanside, California
March 2, 2020

LLG Ref. 3-18-2911

Prepared by:
Amelia Giacalone
Transportation Planner III

Under the Supervision of:
John Boarman, P. E.
Principal

**Linscott Law &
Greenspan, Engineers**
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San Diego, CA 92111
858 300 8800 T
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EXECUTIVE SUMMARY

The Oceanside Rio Rockwell Project (“Project”) proposes the development of 50 single family units and 54 attached town homes in the north-east quadrant of the Old Grove Road / Frazee Road intersection in the City of Oceanside. The Project also proposes the construction of a single-lane roundabout at the intersection of Old Grove Road / Frazee Road.

The total project is calculated to generate 932 ADT with 75 AM peak hour trips (19 inbound / 56 outbound) and 93 PM peak hour trips (65 inbound / 28 outbound).

Near-term conditions include ten cumulative development projects selected in coordination with City of Oceanside staff.

The Nichols Elementary School is located immediately to the west of the Project site. Due to the high traffic activity during School dismissal, a peak hour analysis of the nearby Old Grove Road / Frazee Road intersection at School dismissal between 3:00 and 4:00 PM is also included, in addition to the traditional 7:00 to 9:00 AM and 4:00 to 6:00 PM peak period analyses. *Section 10* includes the results of this analysis.

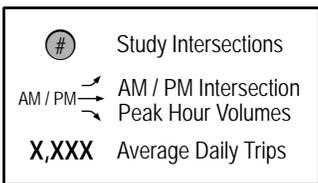
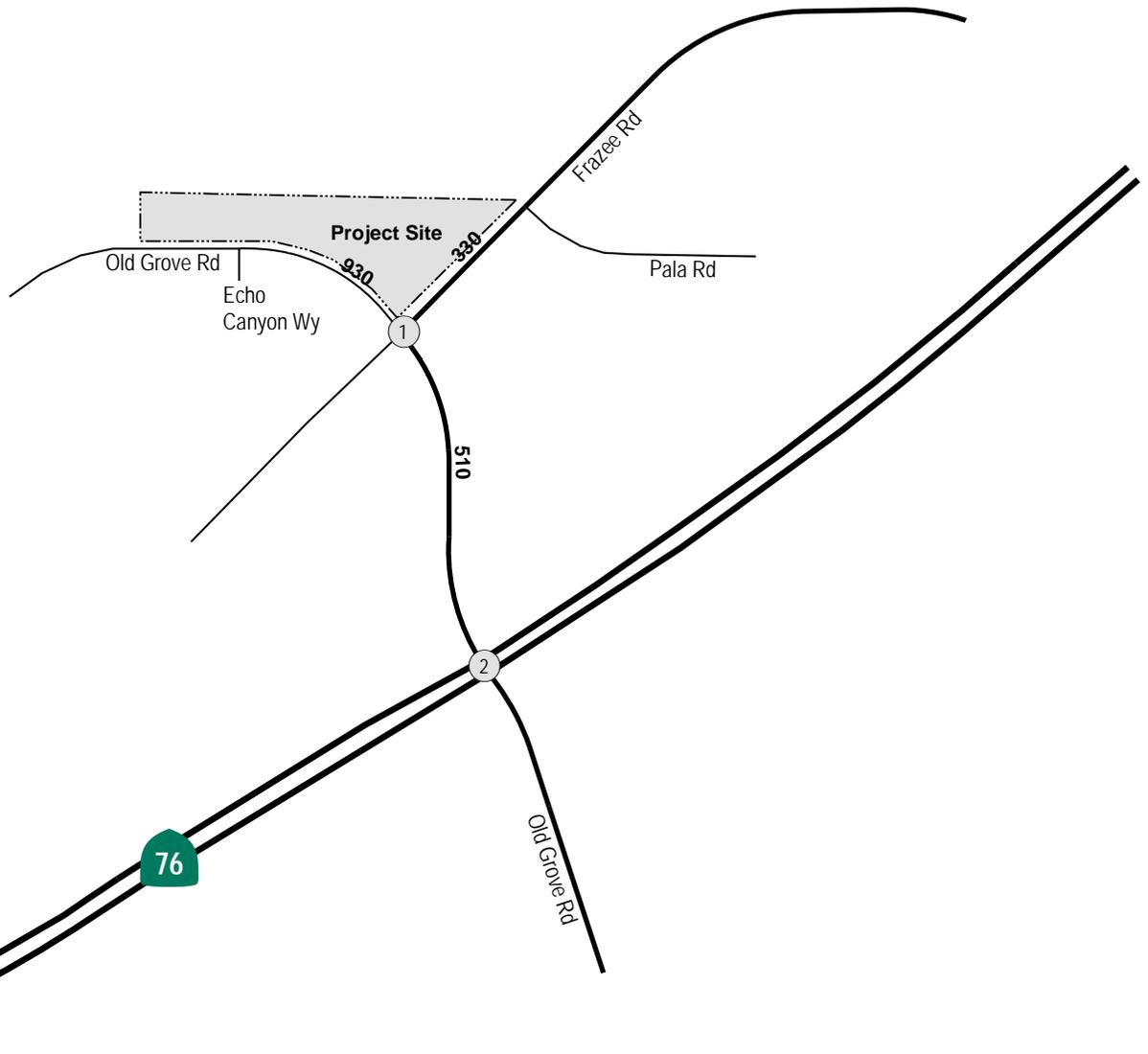
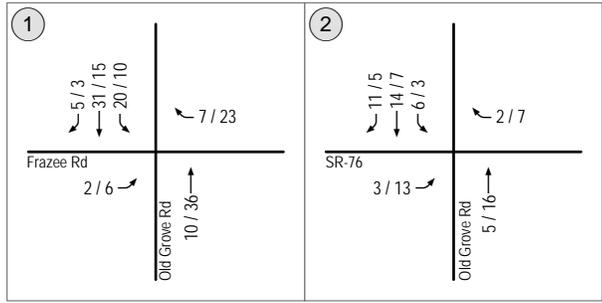
The project has no direct or cumulative impacts and therefore no mitigation measures are required.

**TABLE 7-1
TRIP GENERATION**

Land Use	Size		Daily Trip Ends (ADTs)		AM Peak Hour					PM Peak Hour				
			Rate ^a	Volume	% of ADT ^a	In : Out Split	Volume			% of ADT	In : Out Split	Volume		
							In	Out	Total			In	Out	Total
Detached Single Family Homes	50	DU	10 /DU	500	8%	30:70	12	28	40	10%	70:30	35	15	50
Attached Town Homes	54	DU	8/ DU	432	8%	20:80	7	28	35	10%	70:30	30	13	43
Total	104	DU	-	932	-	-	19	56	75	-	-	65	28	93

Footnotes:

a. Rates are based on SANDAG's (Not So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002.





LOCAL TRANSPORTATION STUDY

MODERA NEPTUNE

Oceanside, California
December 12, 2022

LLG Ref. 3-22-3668

Linscott Law &
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San Diego, CA 92117
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EXECUTIVE SUMMARY

Linscott, Law & Greenspan, Engineers (LLG) has prepared the following Local Transportation Study (LTS) to determine and evaluate the potential effects to the local roadway system due to the proposed Modera Neptune project, consistent with the City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, July 2020. This City document provides guidance for the preparation of a Local Transportation Study (LTS) to identify any off-site infrastructure improvements in the project vicinity that may be triggered with the development of the project as well as to analyze site access and circulation and evaluate the local multi-model network available to serve to project.

PROJECT DESCRIPTION

The Project proposes to demolish the existing 62 room (key) Motel and reconstruct new 62 hotel guestrooms and 360 residential units in a new 8-story building. The primary vehicular access to the site is provided via a driveway on Neptune Way.

The Project is calculated to generate 1,966 net daily trips with 157 trips during the AM peak hour (32 inbound/ 126 outbound trips) and 177 trips during PM peak hour (124 inbound/ 53 outbound trips).

The LTS includes an analysis of the following intersections and street segments:

INTERSECTIONS

1. Harbor Drive / N Coast Hwy & I-5 SB Ramps
2. SR-76 / I-5 NB Ramps
3. SR-76 / I-5 SB ramps
4. SR-76 & N Coast Hwy
5. N Coast Hwy / Neptune Way
6. N Coast Hwy / Surfrider Way
7. N. Coast Highway / Civic Center Drive
8. N. Coast Highway / Pier View Way
9. N. Coast Highway / Mission Avenue
10. S. Coast Highway / Seagaze Drive

STREET SEGMENTS

N Coast Hwy

Harbor Drive to SR-76
SR-76 to Surfrider Way
Surfrider Way to Pier View Way
Pier View Way to Mission Avenue

Neptune Way

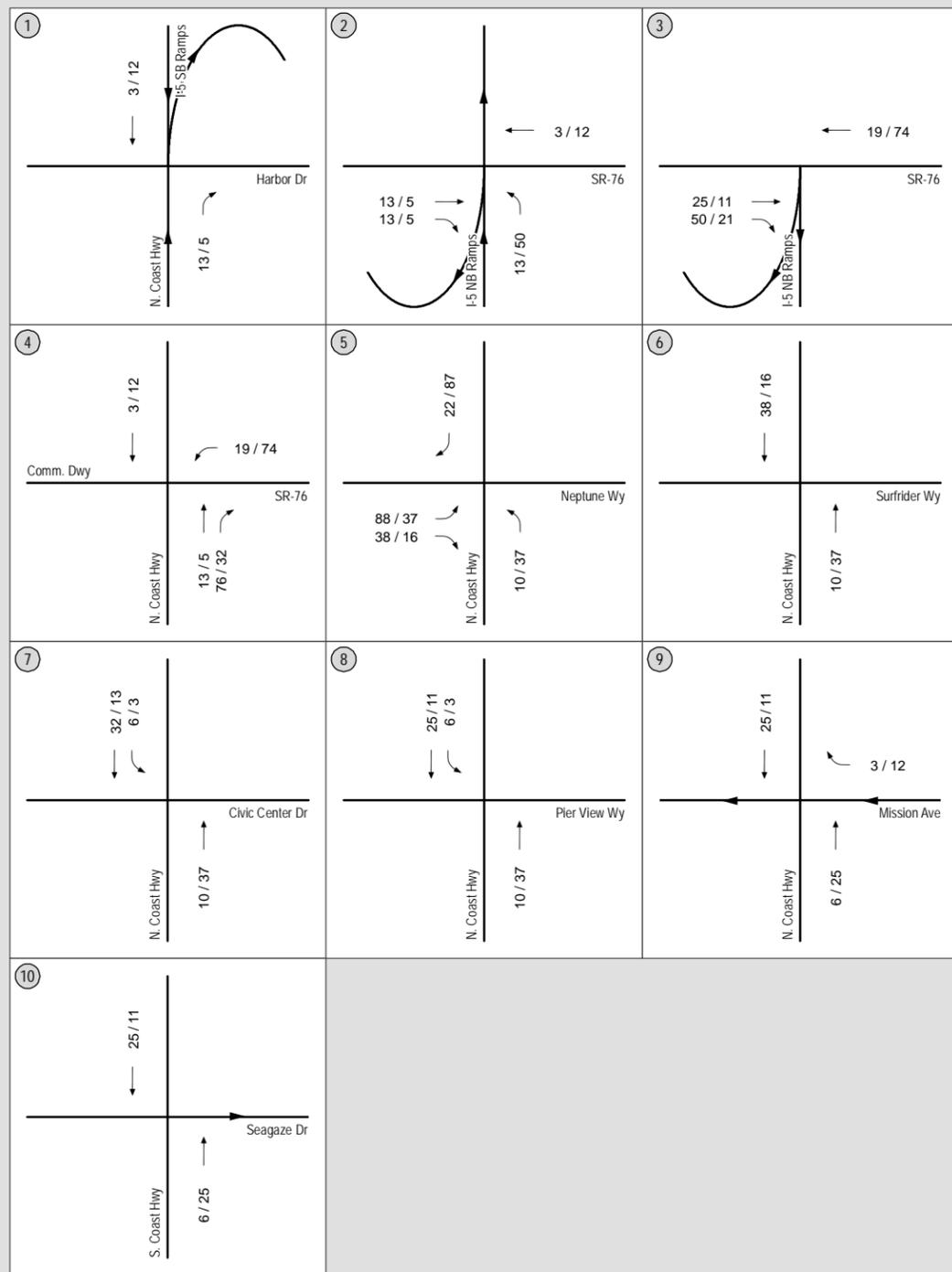
Tremont St to N Coast Hwy

**TABLE 8-1
PROJECT TRIP GENERATION**

Land Use	Size	Daily Trip Ends (ADTs)		AM Peak Hour						PM Peak Hour					
		Rate ^a	Volume	% of ADT	In:Out Split	Volume			% of ADT	In:Out Split	Volume				
						In	Out	Total			In	Out	Total		
<i>Proposed</i>															
Apartments	360 DU	6 /DU ^b	2,160	8%	20 : 80	35	138	173	9%	70 : 30	136	58	194		
Subtotal			2,160			35	138	173			136	58	194		
<i>Mode Split^c</i>															
Transit		4%	(86)			(1)	(6)	(7)			(8)	(2)	(8)		
Bicycle		3%	(63)			(1)	(4)	(5)			(6)	(2)	(6)		
Pedestrian		2%	(43)			(1)	(3)	(3)			(3)	(1)	(4)		
Total Trip Reduction			(194)			(3)	(12)	(16)			(12)	(5)	(17)		
<i>Net Trips</i>			1,966			32	126	157			124	53	177		

Footnotes:

- a. Rates are based on SANDAG's (Not So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, April 2002.
- b. Based on apartment rate of 6/DU.
- c. Percentage obtained from SANDAG's Jurisdictional Mode Choice Report.



Study Intersections

↑↑↑ Intersection AM / PM Peak Hour Volumes

XX,XXX Street Segment ADT

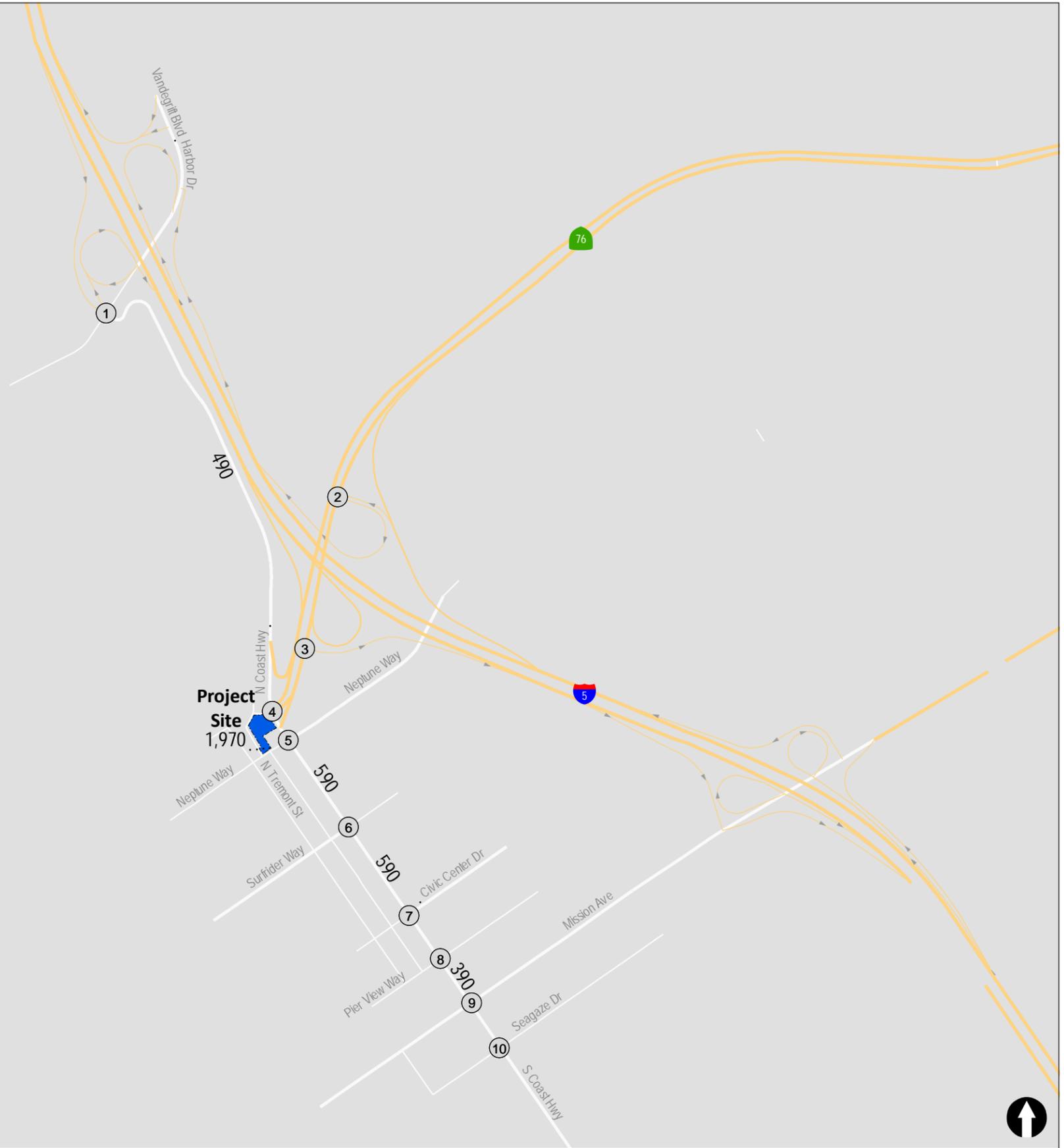


Figure 8-2

Project Traffic Volumes



LOCAL TRANSPORTATION STUDY

MODERA MELROSE

Oceanside, California

April 8, 2022

LLG Ref. 3-21-3419

Prepared by:
Narasimha Prasad
Senior Transportation Engineer

Under the Supervision of:
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Principal

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**TABLE 7-1
PROJECT TRIP GENERATION**

Use	Quantity	Daily Trip Ends (ADT) ^a		AM Peak Hour					PM Peak Hour				
		Rate ^b	Volume	% of ADT	In:Out Split	Volume			% of ADT	In:Out Split	Volume		
						In	Out	Total			In	Out	Total
Apartments	324 DU	6/ DU ^c	1,944	8%	2:8	31	125	156	9%	7:3	123	52	175
Retail ^c	2,338 SF	40/ KSF ^d	94	3%	6:4	2	1	3	9%	5:5	4	4	8
Total			2,038			33	126	159			127	56	183

Footnotes:

- a Average Daily Trips
- b Trip Generation Rate from the SANDAG's *Not So Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, 2002*.
- c Trip rates for Apartments used.
- d Trip rates for Strip Commercial used.



Figure 7-2

Project Traffic Volumes

**Traffic Impact Analysis
Mission Flats, City of Oceanside**

Prepared for:

REALM

1201 Dove Street, Suite 250
Newport Beach, California 92660
Contact: Todd Cadwell

Prepared by:

DUDEK

605 Third Street
Encinitas, California 92024
Contact: Dennis Pascua, Transportation Services Manager

FEBRUARY 2020

3 PROJECT TRAFFIC

This section documents the trip generation, distribution, and assignment of project traffic.

3.1 Trip Generation

Trip generation estimates for the proposed project are based on daily and AM and PM peak hour trip generation rates obtained from the SANDAG (*Not So*) *Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region* (2002). Trip generation estimates for the project are based on the trip generation rates for multi-family residential land uses as well as the specialty retail land use. The residential units could all be classified as multi-family >20 du/acre, therefore the trip rate for apartment use was utilized. No credit was used for existing structures on the site. Specialty retail trip rate was used for the proposed 4,200 square foot retail uses. It is noted that the project currently proposes 3,500 square-foot of commercial uses, but this analysis conservatively assumes the inclusion of 4,200 square feet. Trip generation rates and resulting trip generation estimates for the project are summarized in Table 6.

**Table 6
Project Trip Generation for Mission Flats**

SANDAG Trip Generation Rates									
Land Use	Size/Unit		Daily	AM Peak Hour			PM Peak Hour		
				% In	% Out	Total	% In	% Out	Total
Residential Apartments (multi-family >20du)	per DU		6	20%	80%	8%	70%	30%	9%
Specialty Retail/Strip Commercial	per TSF		40	60%	40%	3%	50%	50%	9%
Trip Generation									
Apartment Units in Main Building	137	DU	822	13	53	66	52	22	74
Commercial	4.2	TSF	168	3	2	5	8	7	15
Total Trip Generation			990	16	55	71	60	29	89

Notes: TSF = 1,000 square feet; DU = Dwelling Unit

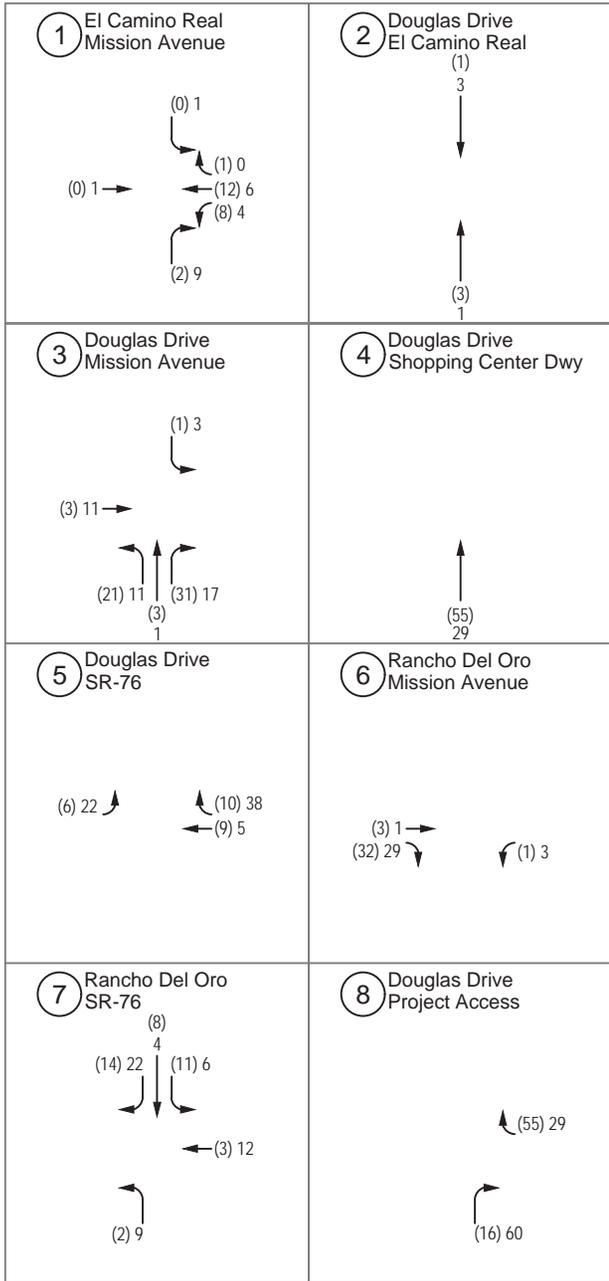
Trip rates from the SANDAG (Not So) Brief Guide of Vehicular Traffic Generation Rates for the San Diego Region, 2002.

As shown in the Table 6, the project would generate 990 daily trips, 71 AM peak hour trips (16 inbound and 55 outbound), and 89 trips during the PM peak hour (60 inbound and 29 outbound).

3.2 Trip Distribution and Assignment

Project trip distribution percentages were based on logical travel paths to commute corridors in the study area; review of trip distribution percentages in the TIA prepared for other proposed developments in the City. City staff approved the project’s trip distribution percentages prior to the initiation of the traffic analysis.

Jun 24, 2019 - 3:48pm mppopvic P:\3000.Environmental\11887 Jones Ranch Oceanside\DUDEK\WORK\PRODUCTS\Traffic\Graphics\AutoCAD Files\MissionFats_06.19.dwg Layout: Fig7-ProjTripAssign

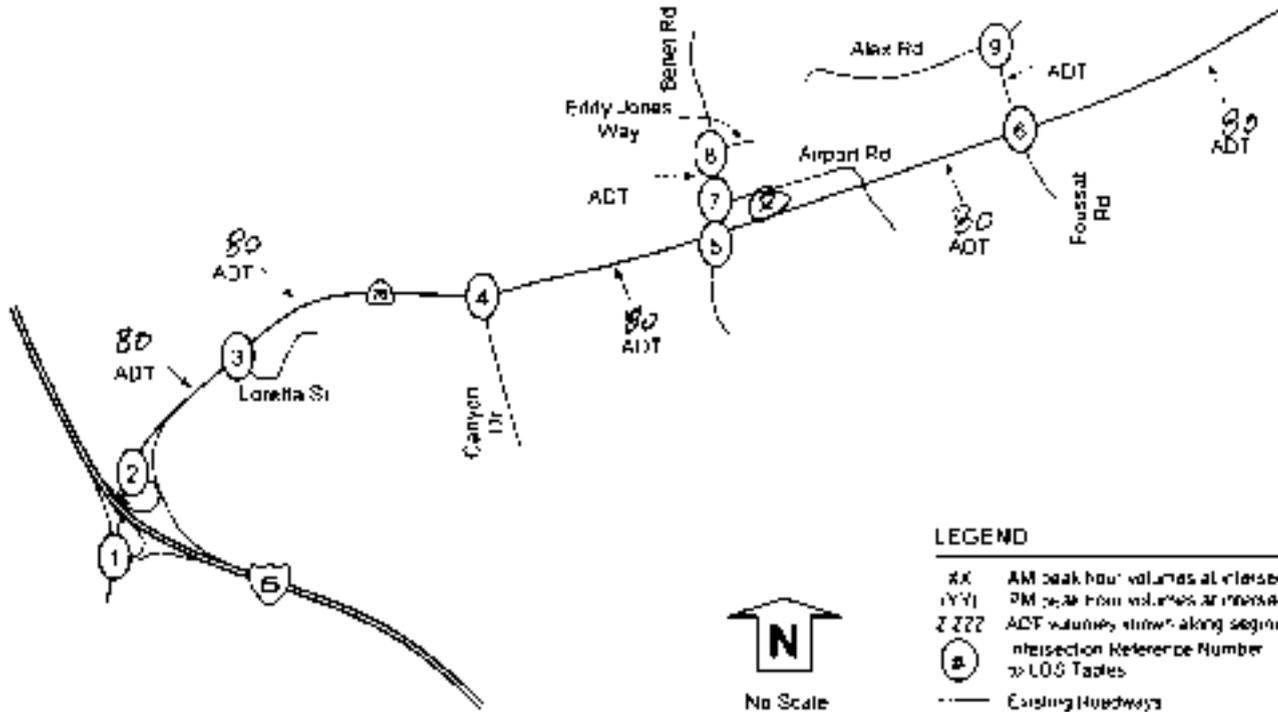


Source: Google Maps 2018

FIGURE 7
Project Trip Assignment

555 AIRPORT RD 20,007 SF INDUSTRIAL

AIRPORT RD INDUSTRIAL



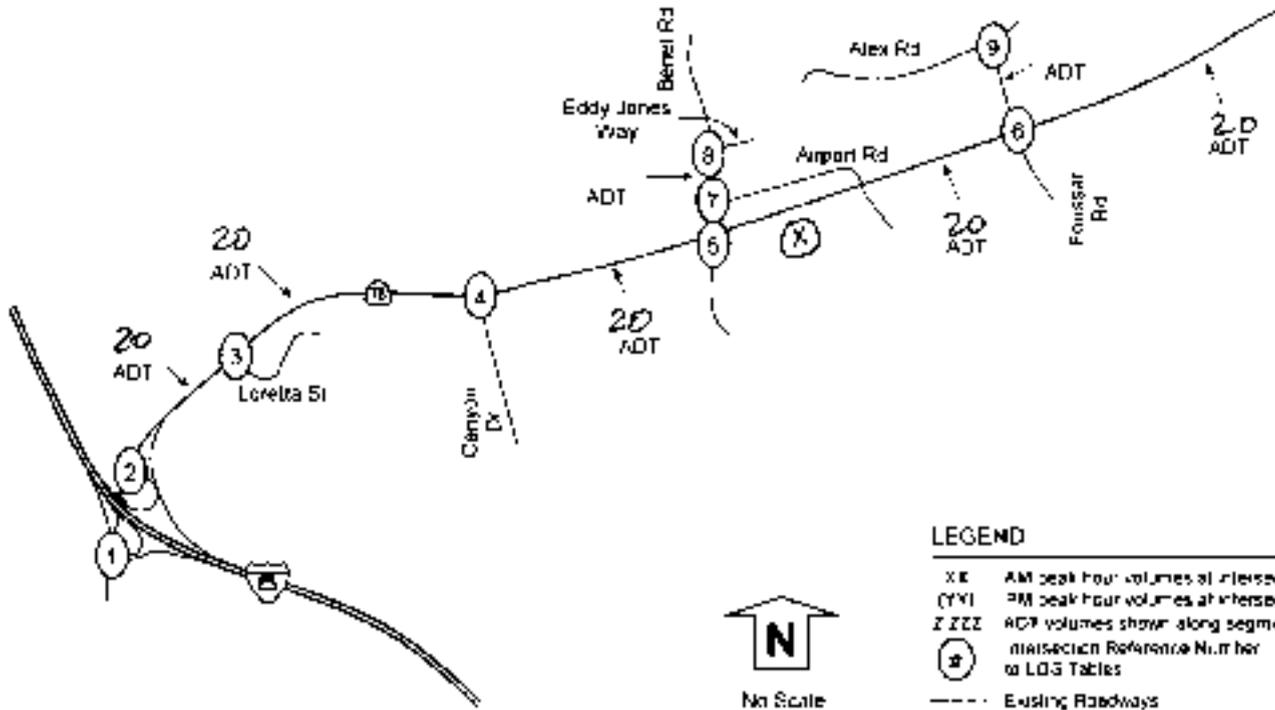
LEGEND

- XX AM peak hour volumes at intersections
- (YY) PM peak hour volumes at intersections
- Z ZZZ ADT volumes shown along segments
- (a) Intersection Reference Number > LOS Tables
- Existing Highways

<p>SR-76</p> <p>(1)</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>(2)</p> <p>I-5 NB Ramp</p>	<p>SR-76</p> <p>(3)</p> <p>Loretta St</p>
<p>SR-76</p> <p>(4)</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>(5)</p> <p>Benet Rd</p>	<p>SR-76</p> <p>(6)</p> <p>Foushat Rd</p>
<p>Benet Rd</p> <p>(7)</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>(8)</p> <p>Eddy Jones Way</p>	<p>Alex Rd</p> <p>(9)</p> <p>Foushat Rd</p> <p>Ocean Camp Hwy</p>

507 JONES RD

LIBERTY RV & BOAT STORAGE



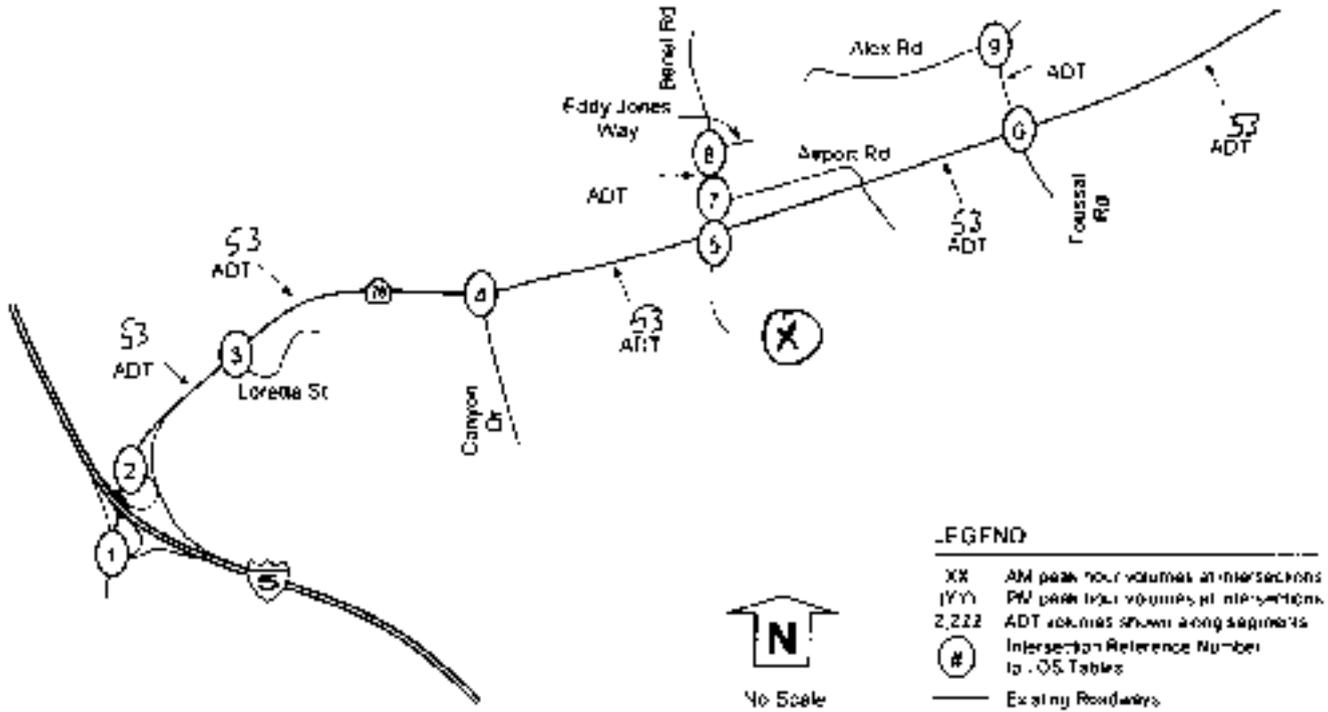
LEGEND

- X X AM peak hour volumes at intersections
- Y Y PM peak hour volumes at intersections
- Z Z Z ADT volumes shown along segments
- (X) Intersection Reference Number to LOS Tables
- Existing Roadways

<p>SR-75</p> <p>I-5 SB Ramp</p>	<p>SR-76</p> <p>I-5 NB Ramp</p>	<p>SR-76</p> <p>Loreita St</p>
<p>SR-76</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>Benet Rd</p>	<p>SR-76</p> <p>Fousgar Rd</p>
<p>Benet Rd</p> <p>Airport Rd</p>	<p>Benet Rd</p> <p>Eddy Jones Way</p>	<p>Alex Rd</p> <p>Fousgar Rd</p> <p>Ocean Kerrig Dwy</p>

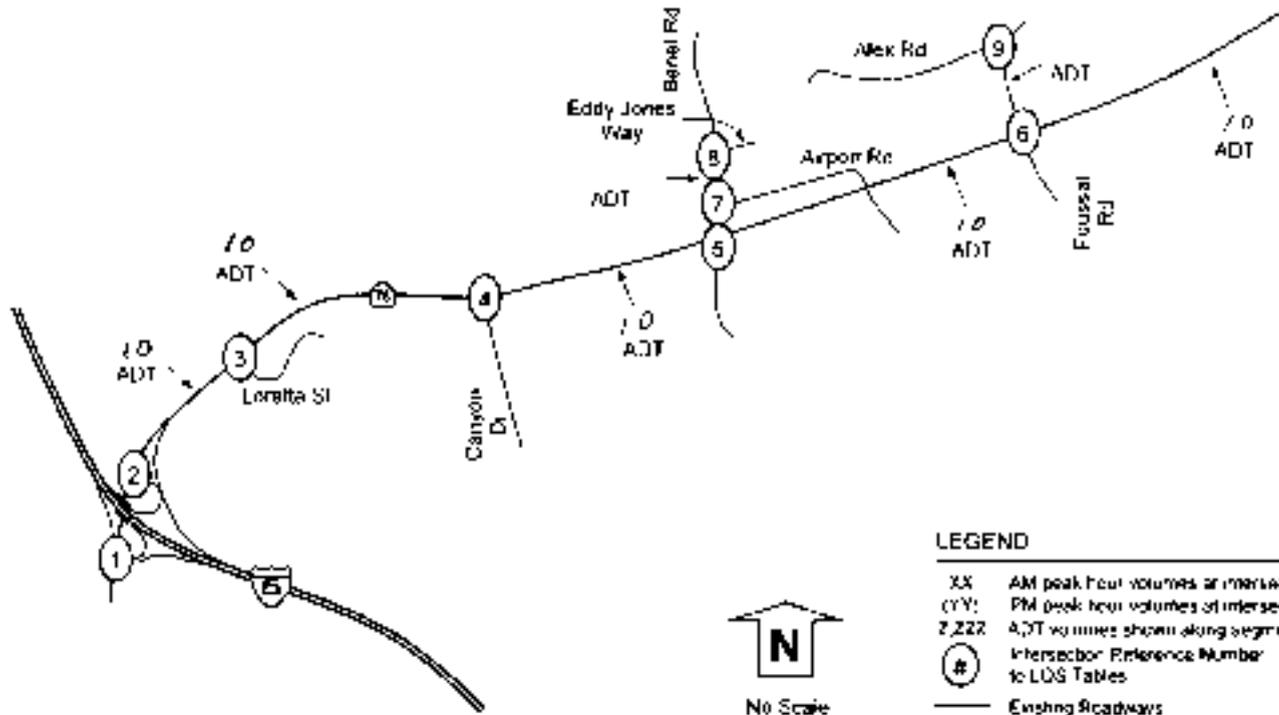
NITTO DENKO ADDITION

16,500 SF RESEARCH & DEVELOPMENT



<p>SR-76</p> <p>1.5 SB Ramp</p> <p>(1)</p>	<p>SR-76</p> <p>1.5 NB Ramp</p> <p>(2)</p> <p>(4)</p> <p>(3)</p>	<p>SR-76</p> <p>(1)</p> <p>(3)</p> <p>Lorena St</p>
<p>SR-76</p> <p>(1)</p> <p>(4)</p> <p>(7)</p> <p>Canyon Dr</p>	<p>SR-76</p> <p>(5)</p> <p>(3)</p> <p>(7)</p> <p>Bennet Rd</p>	<p>SR-76</p> <p>(7)</p> <p>(6)</p> <p>(1)</p> <p>Foussal Rd</p>
<p>Bennet Rd</p> <p>(7)</p> <p>Airport Rd</p>	<p>Bennet Rd</p> <p>(8)</p> <p>Eddy Jones Way</p>	<p>Alex Rd</p> <p>(9)</p> <p>Foussal Rd</p> <p>Ocean Kamp Dwy</p>

RIO ROCKWELL



LEGEND

- XX AM peak hour volumes at intersections
- YY: PM peak hour volumes at intersections
- Z,ZZZ ADT volumes shown along segments
- # Intersection Reference Number to LOS Tables
- Existing Roadways

SR-76 1,000 (1) I-5 SR Ramp	SR-76 1,000 (2) I-5 NB Ramp	SR-76 1,000 (3) Loretta St
SR-76 1,000 (4) Canyon Dr	SR-76 1,000 (5) Benet Rd	SR-76 1,000 (6) Fossil Rd
Benet Rd 1,000 (7) Airport Rd	Benet Rd 1,000 (8) Eddy Jones Way	Alex Rd 1,000 (9) Fossil Rd Ocean Kamp Drwy

Appendix J

Existing + Cumulative Intersection LOS Worksheets

AM Existing + Cumulative
1: SR-76 & I-5 SB Ramp

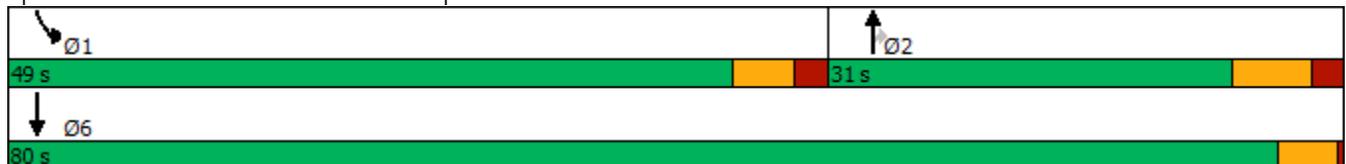
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	363	321	1164	585
Future Volume (vph)	0	363	321	1164	585
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		31.0	31.0	49.0	80.0
Total Split (%)		38.8%	38.8%	61.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	30.0	30.0	37.5	0.0
Actuated g/C Ratio	0.00	0.38	0.38	0.47	0.00
v/c Ratio	no cap	0.30	0.56	0.79	no cap
Control Delay		19.5	22.6	21.5	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	19.5	22.6	21.5	Error
LOS	F	B	C	C	F
Approach Delay	Err	21.0			Err
Approach LOS	F	C			F

Intersection Summary

Cycle Length: 80	
Actuated Cycle Length: 80	
Natural Cycle: 60	
Control Type: Actuated-Uncoordinated	
Maximum v/c Ratio: Err	
Intersection Signal Delay: Err	Intersection LOS: F
Intersection Capacity Utilization 46.0%	ICU Level of Service A
Analysis Period (min) 15	

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

AM Existing + Cumulative
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↑	↑↑	
Traffic Volume (veh/h)	0	330	363	321	1164	585
Future Volume (veh/h)	0	330	363	321	1164	585
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			395	349	1265	636
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1056	471	1596	0
Arrive On Green			0.30	0.30	0.46	0.87
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			395	349	1265	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			4.6	10.3	16.1	0.0
Cycle Q Clear(g_c), s			4.6	10.3	16.1	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1056	471	1596	0
V/C Ratio(X)			0.37	0.74	0.79	0.00
Avail Cap(c_a), veh/h			1658	740	2885	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			14.4	16.4	11.8	0.0
Incr Delay (d2), s/veh			0.2	2.3	0.9	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.6	3.5	5.1	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			14.6	18.8	12.8	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			744			1265
Approach Delay, s/veh			16.6			12.8
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	29.7	22.2				51.9
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	18.1	12.3				0.0
Green Ext Time (p_c), s	5.8	3.1				0.0

Intersection Summary

HCM 6th Ctrl Delay			14.2			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative
2: SR-76 & I-5 NB Ramp

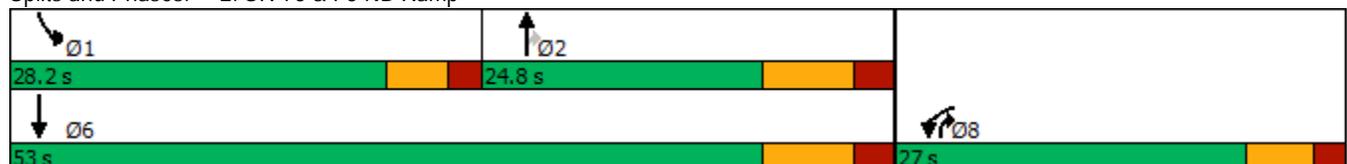
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↘	↑↑	↘	↘↘	↑↑
Traffic Volume (vph)	105	661	116	963	1596
Future Volume (vph)	105	661	116	963	1596
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	1.13	0.97	0.14	1.08	0.87
Control Delay	94.8	58.9	8.1	84.5	21.4
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	94.8	58.9	8.1	84.5	21.4
LOS	F	E	A	F	C
Approach Delay	94.8	51.3			45.1
Approach LOS	F	D			D

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 80
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.13
 Intersection Signal Delay: 55.1
 Intersection LOS: E
 Intersection Capacity Utilization 106.0%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Existing + Cumulative
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	105	615	661	116	963	1596
Future Volume (veh/h)	105	615	661	116	963	1596
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	114	668	718	126	1047	1735
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1357	423	1243	3017
Arrive On Green	0.00	0.00	0.38	0.38	0.36	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	718	126	1047	1735
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	8.3	3.4	14.8	7.6
Cycle Q Clear(g_c), s	0.0	0.0	8.3	3.4	14.8	7.6
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1357	423	1243	3017
V/C Ratio(X)	0.00	0.00	0.53	0.30	0.84	0.58
Avail Cap(c_a), veh/h	0	0	1357	423	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	12.7	15.5	15.6	1.2
Incr Delay (d2), s/veh	0.0	0.0	1.5	1.8	4.0	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	3.1	1.1	5.6	0.3
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	14.2	17.3	19.6	2.0
LnGrp LOS	A	A	B	B	B	A
Approach Vol, veh/h	0		844			2782
Approach Delay, s/veh	0.0		14.6			8.6
Approach LOS			B			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	24.8	28.2			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	16.8	10.3			9.6	0.0
Green Ext Time (p_c), s	2.3	2.8			19.6	0.0

Intersection Summary

HCM 6th Ctrl Delay			10.0			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative
3: Loretta St & SR-76

Timings

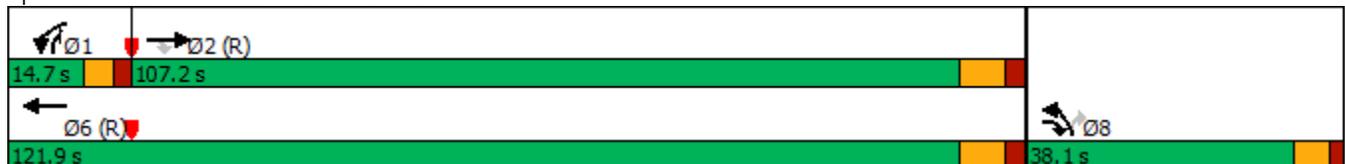


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙	↙
Traffic Volume (vph)	1306	23	26	2537	43	31
Future Volume (vph)	1306	23	26	2537	43	31
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	0.63	0.02	0.28	1.09	0.13	0.07
Control Delay	19.6	0.5	66.5	72.5	53.9	12.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	19.6	0.5	66.5	72.5	53.9	12.1
LOS	B	A	E	E	D	B
Approach Delay	19.3			72.5	36.4	
Approach LOS	B			E	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.09
 Intersection Signal Delay: 54.0
 Intersection Capacity Utilization 90.2%
 Analysis Period (min) 15
 Intersection LOS: D
 ICU Level of Service E

Splits and Phases: 3: Loretta St & SR-76

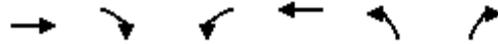


LOS Engineering, Inc.

AM Existing + Cumulative
3: Loretta St & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑	↑
Traffic Volume (veh/h)	1306	23	26	2537	43	31
Future Volume (veh/h)	1306	23	26	2537	43	31
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1420	25	28	2758	47	34
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2261	1325	71	2530	356	380
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1420	25	28	2758	47	34
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	38.7	0.4	2.5	113.9	3.5	2.7
Cycle Q Clear(g_c), s	38.7	0.4	2.5	113.9	3.5	2.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2261	1325	71	2530	356	380
V/C Ratio(X)	0.63	0.02	0.39	1.09	0.13	0.09
Avail Cap(c_a), veh/h	2261	1325	100	2530	356	380
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.22	0.22	1.00	1.00
Uniform Delay (d), s/veh	17.6	2.2	74.9	23.0	52.6	47.2
Incr Delay (d2), s/veh	1.3	0.0	0.8	42.4	0.8	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	16.1	0.3	1.1	58.4	1.7	1.1
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	19.0	2.2	75.7	65.5	53.4	47.7
LnGrp LOS	B	A	E	F	D	D
Approach Vol, veh/h	1445			2786	81	
Approach Delay, s/veh	18.7			65.6	51.0	
Approach LOS	B			E	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.1	109.8			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	4.5	40.7			115.9	5.5
Green Ext Time (p_c), s	0.0	17.2			0.0	0.2
Intersection Summary						
HCM 6th Ctrl Delay			49.6			
HCM 6th LOS			D			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

AM Existing + Cumulative
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↗↖	↗
Traffic Volume (veh/h)	1339	14	100	2553	20	89
Future Volume (veh/h)	1339	14	100	2553	20	89
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1455	15	109	2775	22	97
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2605	1274	130	2990	243	227
Arrive On Green	0.73	0.73	0.07	0.84	0.07	0.07
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1455	15	109	2775	22	97
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	29.6	0.3	9.7	90.4	1.0	8.9
Cycle Q Clear(g_c), s	29.6	0.3	9.7	90.4	1.0	8.9
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2605	1274	130	2990	243	227
V/C Ratio(X)	0.56	0.01	0.84	0.93	0.09	0.43
Avail Cap(c_a), veh/h	2605	1274	164	2990	713	442
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.75	0.75	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	9.6	3.1	73.3	9.2	69.6	62.6
Incr Delay (d2), s/veh	0.7	0.0	3.0	0.7	0.2	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	11.3	0.1	4.6	26.6	0.4	3.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	10.3	3.1	76.3	9.9	69.7	63.8
LnGrp LOS	B	A	E	A	E	E
Approach Vol, veh/h	1470			2884	119	
Approach Delay, s/veh	10.2			12.4	64.9	
Approach LOS	B			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	17.3	125.3			142.6	17.4
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	11.7	31.6			92.4	10.9
Green Ext Time (p_c), s	0.1	18.1			19.4	0.3

Intersection Summary

HCM 6th Ctrl Delay	13.1
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative
5: Benet Rd & SR-76

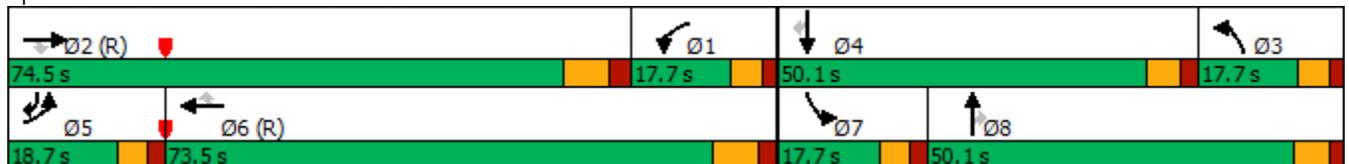
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	106	1220	138	47	2340	13	52	4	48	13	12	249
Future Volume (vph)	106	1220	138	47	2340	13	52	4	48	13	12	249
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	18.7	74.5	74.5	17.7	73.5	73.5	17.7	50.1	50.1	17.7	50.1	18.7
Total Split (%)	11.7%	46.6%	46.6%	11.1%	45.9%	45.9%	11.1%	31.3%	31.3%	11.1%	31.3%	11.7%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	15.2	99.6	99.6	12.0	92.9	92.9	18.8	25.1	25.1	12.0	11.2	27.9
Actuated g/C Ratio	0.10	0.62	0.62	0.08	0.58	0.58	0.12	0.16	0.16	0.08	0.07	0.17
v/c Ratio	0.69	0.60	0.14	0.39	1.24	0.01	0.28	0.01	0.13	0.11	0.10	0.71
Control Delay	69.5	26.0	7.6	91.4	147.0	0.0	64.1	52.2	0.7	71.1	71.8	33.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	69.5	26.0	7.6	91.4	147.0	0.0	64.1	52.2	0.7	71.1	71.8	33.7
LOS	E	C	A	F	F	A	E	D	A	E	E	C
Approach Delay		27.4			145.1			34.5			37.1	
Approach LOS		C			F			C			D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.24
 Intersection Signal Delay: 94.8
 Intersection LOS: F
 Intersection Capacity Utilization 106.3%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Existing + Cumulative
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	106	1220	138	47	2340	13	52	4	48	13	12	249
Future Volume (veh/h)	106	1220	138	47	2340	13	52	4	48	13	12	249
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	115	1326	150	51	2543	14	57	4	52	14	13	271
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	144	1477	659	347	1933	862	123	339	287	62	270	357
Arrive On Green	0.08	0.83	0.42	0.19	1.00	0.54	0.07	0.18	0.18	0.03	0.14	0.24
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	115	1326	150	51	2543	14	57	4	52	14	13	271
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	10.2	39.7	9.8	3.8	0.0	0.7	4.9	0.3	2.9	1.2	1.0	20.2
Cycle Q Clear(g_c), s	10.2	39.7	9.8	3.8	0.0	0.7	4.9	0.3	2.9	1.2	1.0	20.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	144	1477	659	347	1933	862	123	339	287	62	270	357
V/C Ratio(X)	0.80	0.90	0.23	0.15	1.32	0.02	0.46	0.01	0.18	0.23	0.05	0.76
Avail Cap(c_a), veh/h	145	1477	659	347	1933	862	134	514	436	134	514	564
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.80	0.80	0.80	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.3	11.2	30.2	53.4	0.0	16.8	71.6	53.7	23.8	75.1	59.0	31.3
Incr Delay (d2), s/veh	21.8	7.4	0.6	0.0	142.5	0.0	2.7	0.0	0.3	1.8	0.1	3.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	5.5	6.5	3.9	1.7	38.2	0.2	2.4	0.1	1.8	0.6	0.5	7.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	94.1	18.6	30.8	53.4	142.5	16.8	74.3	53.8	24.1	77.0	59.0	34.6
LnGrp LOS	F	B	C	D	F	B	E	D	C	E	E	C
Approach Vol, veh/h		1591			2608			113			298	
Approach Delay, s/veh		25.2			140.1			50.5			37.7	
Approach LOS		C			F			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	39.1	74.5	17.1	29.2	18.6	95.0	11.3	35.1				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 67	12.0	* 44	* 13	65.5	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.8	41.7	6.9	22.2	12.2	2.7	3.2	4.9				
Green Ext Time (p_c), s	0.0	12.1	0.0	1.0	0.0	49.0	0.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	91.6
HCM 6th LOS	F

Notes

User approved pedestrian interval to be less than phase max green.
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative
6: Foussat Rd & SR-76

Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	165	1018	84	178	2009	112	130	114	116	202	162	218
Future Volume (vph)	165	1018	84	178	2009	112	130	114	116	202	162	218
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	18.7	70.9	70.9	18.3	70.5	70.5	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (%)	11.7%	44.3%	44.3%	11.4%	44.1%	44.1%	11.7%	32.6%	32.6%	11.7%	32.6%	32.6%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.5	87.7	87.7	13.5	87.7	87.7	13.0	20.2	20.2	13.0	20.2	20.2
Actuated g/C Ratio	0.08	0.55	0.55	0.08	0.55	0.55	0.08	0.13	0.13	0.08	0.13	0.13
v/c Ratio	0.62	0.57	0.10	0.67	1.13	0.13	0.51	0.19	0.41	0.79	0.39	0.42
Control Delay	109.5	7.3	0.9	82.6	97.4	6.8	77.3	60.8	11.5	92.1	64.8	7.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	109.5	7.3	0.9	82.6	97.4	6.8	77.3	60.8	11.5	92.1	64.8	7.8
LOS	F	A	A	F	F	A	E	E	B	F	E	A
Approach Delay		20.2			91.8			50.8			53.0	
Approach LOS		C			F			D			D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 18.7 (12%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.13
 Intersection Signal Delay: 63.4
 Intersection LOS: E
 Intersection Capacity Utilization 108.8%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



AM Existing + Cumulative
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	
Traffic Volume (veh/h)	165	1018	84	178	2009	112	130	114	116	202	162	218
Future Volume (veh/h)	165	1018	84	178	2009	112	130	114	116	202	162	218
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	179	1107	91	193	2184	122	141	124	126	220	176	237
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	281	2046	913	259	2024	903	280	554	172	281	386	303
Arrive On Green	0.16	1.00	1.00	0.07	0.57	0.57	0.08	0.11	0.11	0.08	0.11	0.11
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	179	1107	91	193	2184	122	141	124	126	220	176	237
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	7.7	0.0	0.0	8.8	91.1	5.7	6.3	3.5	12.3	10.0	7.4	13.2
Cycle Q Clear(g_c), s	7.7	0.0	0.0	8.8	91.1	5.7	6.3	3.5	12.3	10.0	7.4	13.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	281	2046	913	259	2024	903	280	554	172	281	386	303
V/C Ratio(X)	0.64	0.54	0.10	0.74	1.08	0.14	0.50	0.22	0.73	0.78	0.46	0.78
Avail Cap(c_a), veh/h	281	2046	913	272	2024	903	281	1468	456	281	1022	802
HCM Platoon Ratio	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.79	0.79	0.79	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	64.8	0.0	0.0	72.5	34.4	16.1	70.4	65.2	69.1	72.1	66.9	69.5
Incr Delay (d2), s/veh	3.8	0.8	0.2	10.1	45.1	0.3	1.4	0.2	5.9	13.5	0.8	4.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.4	0.2	0.0	4.3	50.9	2.2	2.8	1.6	5.3	5.0	3.4	4.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	68.6	0.8	0.2	82.6	79.5	16.4	71.8	65.4	75.0	85.6	67.7	73.8
LnGrp LOS	E	A	A	F	F	B	E	E	E	F	E	E
Approach Vol, veh/h		1377			2499			391			633	
Approach Delay, s/veh		9.6			76.7			70.8			76.2	
Approach LOS		A			E			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	100.1	18.7	23.5	18.7	99.1	18.7	23.5				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	62.9	* 13	46.0	* 13	62.5	* 13	46.0				
Max Q Clear Time (g_c+I1), s	10.8	2.0	8.3	15.2	9.7	93.1	12.0	14.3				
Green Ext Time (p_c), s	0.1	11.7	0.2	2.2	0.2	0.0	0.1	1.2				

Intersection Summary

HCM 6th Ctrl Delay	57.3
HCM 6th LOS	E

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T			↑↑↑
Traffic Vol, veh/h	27	16	83	29	5	274
Future Vol, veh/h	27	16	83	29	5	274
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	29	17	90	32	5	298

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	235	106	0	0	122	0
Stage 1	106	-	-	-	-	-
Stage 2	129	-	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	742	948	-	-	1464	-
Stage 1	882	-	-	-	-	-
Stage 2	844	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	739	948	-	-	1464	-
Mov Cap-2 Maneuver	739	-	-	-	-	-
Stage 1	882	-	-	-	-	-
Stage 2	841	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.7	0	0.1
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	805	1464
HCM Lane V/C Ratio	-	-	0.058	0.004
HCM Control Delay (s)	-	-	9.7	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Existing + Cumulative
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	1	1	110	1	1	258
Future Vol, veh/h	1	1	110	1	1	258
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	1	120	1	1	280

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	403	121	0	0	121	0
Stage 1	121	-	-	-	-	-
Stage 2	282	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	603	930	-	-	1467	-
Stage 1	904	-	-	-	-	-
Stage 2	766	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	602	930	-	-	1467	-
Mov Cap-2 Maneuver	602	-	-	-	-	-
Stage 1	904	-	-	-	-	-
Stage 2	765	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.9	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	731	1467
HCM Lane V/C Ratio	-	-	0.003	0.001
HCM Control Delay (s)	-	-	9.9	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0	0

LOS Engineering, Inc.

AM Existing + Cumulative
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	6.8			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	19	186	442	531
Demand Flow Rate, veh/h	19	190	451	542
Vehicles Circulating, veh/h	674	354	44	153
Vehicles Exiting, veh/h	21	141	649	391
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	5.5	5.8	5.9	8.0
Approach LOS	A	A	A	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	19	190	451	542
Cap Entry Lane, veh/h	694	962	1319	1180
Entry HV Adj Factor	0.996	0.978	0.981	0.980
Flow Entry, veh/h	19	186	442	531
Cap Entry, veh/h	691	941	1294	1157
V/C Ratio	0.027	0.198	0.342	0.459
Control Delay, s/veh	5.5	5.8	5.9	8.0
LOS	A	A	A	A
95th %tile Queue, veh	0	1	2	2

LOS Engineering, Inc.

PM Existing + Cumulative
1: SR-76 & I-5 SB Ramp

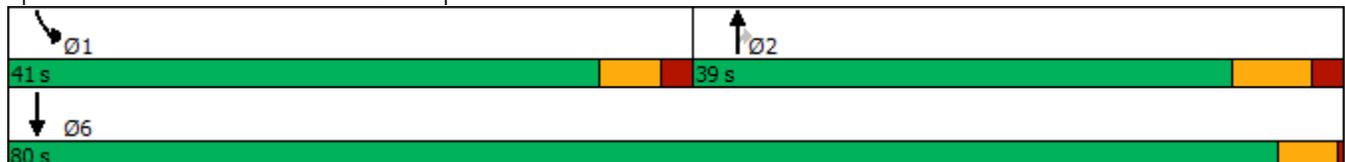
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	532	288	773	846
Future Volume (vph)	0	532	288	773	846
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	41.5	41.5	26.0	0.0
Actuated g/C Ratio	0.00	0.52	0.52	0.32	0.00
v/c Ratio	no cap	0.31	0.36	0.75	no cap
Control Delay		12.5	10.4	28.5	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	12.5	10.4	28.5	Error
LOS	F	B	B	C	F
Approach Delay	Err	11.8			Err
Approach LOS	F	B			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 45
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection Capacity Utilization 44.0%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service A

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

PM Existing + Cumulative
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	686	532	288	773	846
Future Volume (veh/h)	0	686	532	288	773	846
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			578	313	840	920
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1222	545	1192	0
Arrive On Green			0.34	0.34	0.34	0.83
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			578	313	840	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			5.1	6.5	8.4	0.0
Cycle Q Clear(g_c), s			5.1	6.5	8.4	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1222	545	1192	0
V/C Ratio(X)			0.47	0.57	0.70	0.00
Avail Cap(c_a), veh/h			2850	1271	3039	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			10.3	10.8	11.4	0.0
Incr Delay (d2), s/veh			0.3	1.0	0.8	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.6	1.9	2.6	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			10.6	11.7	12.2	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			891			840
Approach Delay, s/veh			11.0			12.2
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	19.5	20.6				40.1
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	10.4	8.5				0.0
Green Ext Time (p_c), s	3.4	5.3				0.0

Intersection Summary

HCM 6th Ctrl Delay			11.6			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative
2: SR-76 & I-5 NB Ramp

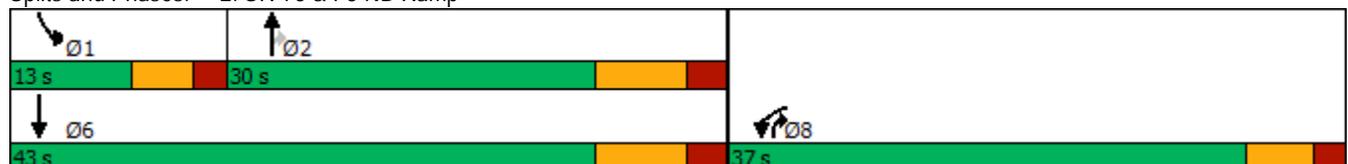
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	290	1249	123	481	1256
Future Volume (vph)	290	1249	123	481	1256
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.58	1.40	0.11	1.67	0.88
Control Delay	290.1	211.4	2.7	343.0	29.1
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	290.1	211.4	2.7	343.0	29.1
LOS	F	F	A	F	C
Approach Delay	290.1	192.7			116.0
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 150
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.67
 Intersection Signal Delay: 187.0
 Intersection Capacity Utilization 131.8%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



PM Existing + Cumulative
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	290	828	1249	123	481	1256
Future Volume (veh/h)	290	828	1249	123	481	1256
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	315	900	1358	134	523	1365
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1818	586	587	2893
Arrive On Green	0.00	0.00	0.51	0.51	0.17	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1358	134	523	1365
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	13.0	2.5	6.4	5.0
Cycle Q Clear(g_c), s	0.0	0.0	13.0	2.5	6.4	5.0
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1818	586	587	2893
V/C Ratio(X)	0.00	0.00	0.75	0.23	0.89	0.47
Avail Cap(c_a), veh/h	0	0	1818	586	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	8.3	9.3	17.5	1.2
Incr Delay (d2), s/veh	0.0	0.0	2.9	0.9	15.8	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	3.9	0.6	3.5	0.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	11.1	10.2	33.3	1.8
LnGrp LOS	A	A	B	B	C	A
Approach Vol, veh/h	0		1492			1888
Approach Delay, s/veh	0.0		11.1			10.5
Approach LOS			B			B
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	13.0	30.0			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	8.4	15.0			7.0	0.0
Green Ext Time (p_c), s	0.0	5.0			12.6	0.0

Intersection Summary

HCM 6th Ctrl Delay		10.7
HCM 6th LOS		B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative
3: Loretta St & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (veh/h)	2204	73	32	1698	25	21
Future Volume (veh/h)	2204	73	32	1698	25	21
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2396	79	35	1846	27	23
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2245	1319	79	2530	356	387
Arrive On Green	0.63	0.63	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2396	79	35	1846	27	23
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	101.1	1.4	3.1	49.8	2.0	1.8
Cycle Q Clear(g_c), s	101.1	1.4	3.1	49.8	2.0	1.8
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2245	1319	79	2530	356	387
V/C Ratio(X)	1.07	0.06	0.44	0.73	0.08	0.06
Avail Cap(c_a), veh/h	2245	1319	100	2530	356	387
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.73	0.73	1.00	1.00
Uniform Delay (d), s/veh	29.5	2.4	74.5	13.8	52.0	46.3
Incr Delay (d2), s/veh	39.8	0.1	2.8	1.4	0.4	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	53.6	1.2	1.5	19.5	0.9	0.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	69.3	2.5	77.3	15.2	52.4	46.6
LnGrp LOS	F	A	E	B	D	D
Approach Vol, veh/h	2475			1881	50	
Approach Delay, s/veh	67.1			16.4	49.8	
Approach LOS	E			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.8	109.1			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	5.1	103.1			51.8	4.0
Green Ext Time (p_c), s	0.0	0.0			28.5	0.1
Intersection Summary						
HCM 6th Ctrl Delay			45.3			
HCM 6th LOS			D			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Existing + Cumulative
4: Canyon Dr & SR-76

Timings

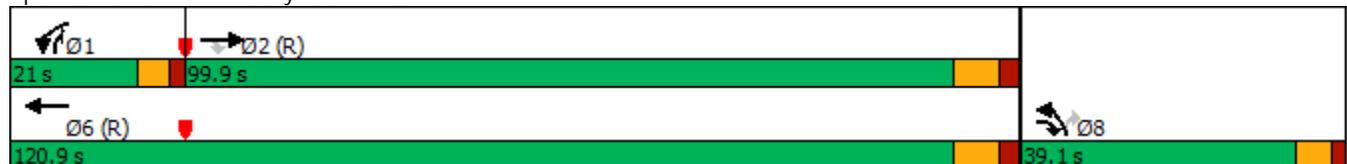


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↓
Traffic Volume (vph)	2162	48	161	1711	7	118
Future Volume (vph)	2162	48	161	1711	7	118
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	99.9	39.1	21.0	120.9	39.1	21.0
Total Split (%)	62.4%	24.4%	13.1%	75.6%	24.4%	13.1%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effect Green (s)	103.8	125.6	22.6	132.1	13.8	42.5
Actuated g/C Ratio	0.65	0.78	0.14	0.83	0.09	0.27
v/c Ratio	1.02	0.04	0.70	0.64	0.03	0.30
Control Delay	56.7	2.6	45.9	20.5	61.0	46.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	56.7	2.6	45.9	20.5	61.0	46.4
LOS	E	A	D	C	E	D
Approach Delay	55.5			22.7	47.3	
Approach LOS	E			C	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.02
 Intersection Signal Delay: 40.7
 Intersection LOS: D
 Intersection Capacity Utilization 92.7%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Existing + Cumulative
4: Canyon Dr & SR-76

	→	↘	↙	←	↖	↗
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↘	↑↑	↘↙	↘
Traffic Volume (veh/h)	2162	48	161	1711	7	118
Future Volume (veh/h)	2162	48	161	1711	7	118
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2350	52	175	1860	8	128
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2466	1237	170	2933	299	289
Arrive On Green	0.69	0.69	0.10	0.83	0.09	0.09
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	2350	52	175	1860	8	128
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	95.6	1.2	15.3	30.7	0.3	11.5
Cycle Q Clear(g_c), s	95.6	1.2	15.3	30.7	0.3	11.5
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2466	1237	170	2933	299	289
V/C Ratio(X)	0.95	0.04	1.03	0.63	0.03	0.44
Avail Cap(c_a), veh/h	2466	1237	170	2933	713	478
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.16	0.16	1.00	1.00
Uniform Delay (d), s/veh	22.1	4.0	72.3	5.1	66.9	58.2
Incr Delay (d2), s/veh	1.3	0.0	34.8	0.2	0.0	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	37.6	0.6	8.7	9.6	0.2	4.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	23.4	4.0	107.1	5.3	66.9	59.3
LnGrp LOS	C	A	F	A	E	E
Approach Vol, veh/h	2402			2035	136	
Approach Delay, s/veh	23.0			14.1	59.7	
Approach LOS	C			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	21.0	119.0			140.0	20.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	91.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	17.3	97.6			32.7	13.5
Green Ext Time (p_c), s	0.0	0.0			31.7	0.4
Intersection Summary						
HCM 6th Ctrl Delay			20.1			
HCM 6th LOS			C			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Existing + Cumulative
5: Benet Rd & SR-76

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	182	2072	90	43	1605	18	137	21	99	22	9	125
Future Volume (veh/h)	182	2072	90	43	1605	18	137	21	99	22	9	125
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	198	2252	98	47	1745	20	149	23	108	24	10	136
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	2091	932	117	1949	869	171	257	218	88	169	311
Arrive On Green	0.11	0.59	0.59	0.07	0.55	0.55	0.10	0.14	0.14	0.05	0.09	0.09
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	198	2252	98	47	1745	20	149	23	108	24	10	136
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	94.1	4.3	4.1	69.7	0.9	13.2	1.7	10.1	2.1	0.8	12.1
Cycle Q Clear(g_c), s	16.9	94.1	4.3	4.1	69.7	0.9	13.2	1.7	10.1	2.1	0.8	12.1
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	2091	932	117	1949	869	171	257	218	88	169	311
V/C Ratio(X)	1.05	1.08	0.11	0.40	0.90	0.02	0.87	0.09	0.50	0.27	0.06	0.44
Avail Cap(c_a), veh/h	188	2091	932	134	1949	869	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.12	0.12	0.12	0.54	0.54	0.54	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	32.9	14.5	71.7	32.1	16.5	71.3	60.3	63.9	73.3	66.5	56.5
Incr Delay (d2), s/veh	37.9	36.1	0.0	1.2	3.9	0.0	21.4	0.1	1.7	1.7	0.1	1.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9.7	49.6	1.6	1.9	30.5	0.4	7.1	0.8	4.2	1.0	0.4	5.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	109.5	69.1	14.5	72.9	36.0	16.6	92.8	60.4	65.6	75.0	66.7	57.5
LnGrp LOS	F	F	B	E	D	B	F	E	E	E	E	E
Approach Vol, veh/h		2548			1812			280			170	
Approach Delay, s/veh		70.1			36.7			79.6			60.5	
Approach LOS		E			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.2	102.1	21.1	20.6	22.6	95.7	13.6	28.1				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.1	96.1	15.2	14.1	18.9	71.7	4.1	12.1				
Green Ext Time (p_c), s	0.0	0.0	0.2	0.4	0.0	0.0	0.0	0.5				
Intersection Summary												
HCM 6th Ctrl Delay			57.8									
HCM 6th LOS			E									
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Existing + Cumulative
6: Foussat Rd & SR-76

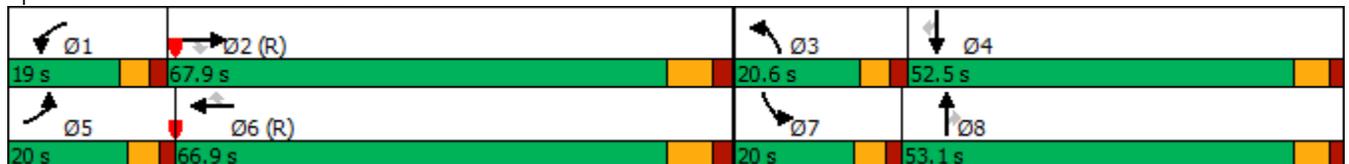
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	332	1819	164	139	1328	197	86	258	187	126	171	252
Future Volume (vph)	332	1819	164	139	1328	197	86	258	187	126	171	252
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	20.0	67.9	67.9	19.0	66.9	66.9	20.6	53.1	53.1	20.0	52.5	52.5
Total Split (%)	12.5%	42.4%	42.4%	11.9%	41.8%	41.8%	12.9%	33.2%	33.2%	12.5%	32.8%	32.8%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	25.3	87.6	87.6	12.9	75.2	75.2	13.0	20.7	20.7	13.3	21.0	21.0
Actuated g/C Ratio	0.16	0.55	0.55	0.08	0.47	0.47	0.08	0.13	0.13	0.08	0.13	0.13
v/c Ratio	0.66	1.02	0.19	0.55	0.87	0.25	0.33	0.42	0.55	0.48	0.40	0.45
Control Delay	61.5	53.0	18.3	78.4	44.5	4.2	73.0	64.6	13.6	76.1	64.3	7.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	61.5	53.0	18.3	78.4	44.5	4.2	73.0	64.6	13.6	76.1	64.3	7.5
LOS	E	D	B	E	D	A	E	E	B	E	E	A
Approach Delay		51.8			42.6			48.0			40.9	
Approach LOS		D			D			D			D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.02
 Intersection Signal Delay: 47.2
 Intersection LOS: D
 Intersection Capacity Utilization 102.7%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



LOS Engineering, Inc.

PM Existing + Cumulative
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 		
Traffic Volume (veh/h)	332	1819	164	139	1328	197	86	258	187	126	171	252
Future Volume (veh/h)	332	1819	164	139	1328	197	86	258	187	126	171	252
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	361	1977	178	151	1443	214	93	280	203	137	186	274
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	309	1889	842	259	1837	820	276	782	243	280	548	430
Arrive On Green	0.09	0.53	0.53	0.07	0.52	0.52	0.08	0.15	0.15	0.08	0.15	0.15
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	361	1977	178	151	1443	214	93	280	203	137	186	274
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	14.3	85.0	9.5	6.8	52.8	12.1	4.1	7.9	19.9	6.1	7.5	14.7
Cycle Q Clear(g_c), s	14.3	85.0	9.5	6.8	52.8	12.1	4.1	7.9	19.9	6.1	7.5	14.7
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	309	1889	842	259	1837	820	276	782	243	280	548	430
V/C Ratio(X)	1.17	1.05	0.21	0.58	0.79	0.26	0.34	0.36	0.84	0.49	0.34	0.64
Avail Cap(c_a), veh/h	309	1889	842	287	1837	820	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.8	37.5	19.8	71.6	31.4	21.6	69.6	60.7	65.8	70.3	60.4	63.5
Incr Delay (d2), s/veh	79.4	22.8	0.1	2.5	3.5	0.8	0.7	0.3	7.4	1.3	0.4	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9.8	42.1	3.6	3.1	23.5	4.8	1.8	3.5	8.6	2.8	3.4	5.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	152.3	60.3	19.8	74.1	34.9	22.3	70.3	61.0	73.2	71.7	60.7	65.0
LnGrp LOS	F	F	B	E	C	C	E	E	E	E	E	E
Approach Vol, veh/h		2516			1808			576			597	
Approach Delay, s/veh		70.6			36.7			66.8			65.2	
Approach LOS		E			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	93.0	18.5	30.8	20.0	90.7	18.7	30.6				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	8.8	87.0	6.1	16.7	16.3	54.8	8.1	21.9				
Green Ext Time (p_c), s	0.2	0.0	0.1	2.4	0.0	3.3	0.2	2.6				
Intersection Summary												
HCM 6th Ctrl Delay			58.5									
HCM 6th LOS			E									
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Existing + Cumulative
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	1.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T			↑↑↑
Traffic Vol, veh/h	25	11	199	16	5	123
Future Vol, veh/h	25	11	199	16	5	123
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	12	216	17	5	134

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	289	225	0	0	233
Stage 1	225	-	-	-	-
Stage 2	64	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	695	814	-	-	1333
Stage 1	782	-	-	-	-
Stage 2	911	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	692	814	-	-	1333
Mov Cap-2 Maneuver	692	-	-	-	-
Stage 1	782	-	-	-	-
Stage 2	907	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.2	0	0.3
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	725	1333
HCM Lane V/C Ratio	-	-	0.054	0.004
HCM Control Delay (s)	-	-	10.2	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

PM Existing + Cumulative
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	1	0	212	0	0	129
Future Vol, veh/h	1	0	212	0	0	129
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	1	0	230	0	0	140

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	370	230	0	0	230
Stage 1	230	-	-	-	-
Stage 2	140	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	630	809	-	-	1338
Stage 1	808	-	-	-	-
Stage 2	887	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	630	809	-	-	1338
Mov Cap-2 Maneuver	630	-	-	-	-
Stage 1	808	-	-	-	-
Stage 2	887	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	630	1338
HCM Lane V/C Ratio	-	-	0.002	-
HCM Control Delay (s)	-	-	10.7	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0	0

LOS Engineering, Inc.

PM Existing + Cumulative
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	10.7			
Intersection LOS	B			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	33	239	848	496
Demand Flow Rate, veh/h	33	243	865	506
Vehicles Circulating, veh/h	677	635	98	204
Vehicles Exiting, veh/h	33	328	612	674
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	5.7	9.3	12.7	8.2
Approach LOS	A	A	B	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	33	243	865	506
Cap Entry Lane, veh/h	692	722	1249	1121
Entry HV Adj Factor	0.993	0.983	0.980	0.980
Flow Entry, veh/h	33	239	848	496
Cap Entry, veh/h	687	710	1224	1098
V/C Ratio	0.048	0.337	0.693	0.452
Control Delay, s/veh	5.7	9.3	12.7	8.2
LOS	A	A	B	A
95th %tile Queue, veh	0	1	6	2

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Appendix K

Existing + Cumulative + Project Intersection LOS Worksheets

AM Existing + Cumulative + Project
1: SR-76 & I-5 SB Ramp

Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	363	321	1176	585
Future Volume (vph)	0	363	321	1176	585
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		31.0	31.0	49.0	80.0
Total Split (%)		38.8%	38.8%	61.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	29.8	29.8	37.7	0.0
Actuated g/C Ratio	0.00	0.37	0.37	0.47	0.00
v/c Ratio	no cap	0.30	0.56	0.79	no cap
Control Delay		19.7	22.9	21.5	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	19.7	22.9	21.5	Error
LOS	F	B	C	C	F
Approach Delay	Err	21.2			Err
Approach LOS	F	C			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 55
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection LOS: F
 Intersection Capacity Utilization 46.0%
 ICU Level of Service A
 Analysis Period (min) 15

Splits and Phases: 1: SR-76 & I-5 SB Ramp



AM Existing + Cumulative + Project
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	330	363	321	1176	585
Future Volume (veh/h)	0	330	363	321	1176	585
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			395	349	1278	636
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1052	469	1607	0
Arrive On Green			0.30	0.30	0.47	0.87
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			395	349	1278	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			4.6	10.4	16.4	0.0
Cycle Q Clear(g_c), s			4.6	10.4	16.4	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1052	469	1607	0
V/C Ratio(X)			0.38	0.74	0.80	0.00
Avail Cap(c_a), veh/h			1643	733	2858	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			14.6	16.6	11.9	0.0
Incr Delay (d2), s/veh			0.2	2.4	0.9	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.7	3.6	5.2	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			14.8	19.0	12.8	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			744			1278
Approach Delay, s/veh			16.8			12.8
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	30.0	22.3				52.3
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	18.4	12.4				0.0
Green Ext Time (p_c), s	5.9	3.1				0.0

Intersection Summary

HCM 6th Ctrl Delay	14.3
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
2: SR-76 & I-5 NB Ramp

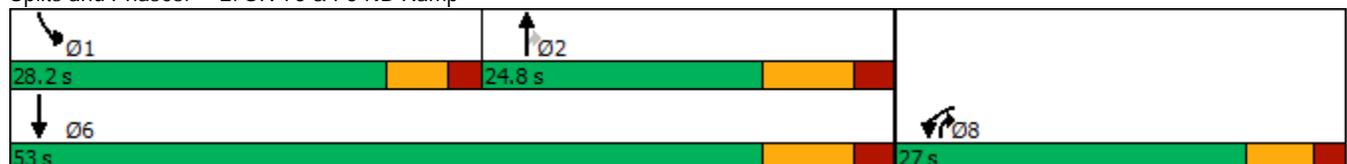
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↘	↑↑	↘	↘↘	↑↑
Traffic Volume (vph)	105	697	116	975	1608
Future Volume (vph)	105	697	116	975	1608
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	1.13	1.02	0.14	1.10	0.88
Control Delay	94.8	71.5	8.1	89.2	21.8
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	94.8	71.5	8.1	89.2	21.8
LOS	F	E	A	F	C
Approach Delay	94.8	62.4			47.3
Approach LOS	F	E			D

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 80
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.13
 Intersection Signal Delay: 58.6
 Intersection LOS: E
 Intersection Capacity Utilization 107.4%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Existing + Cumulative + Project
2: SR-76 & I-5 NB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	105	615	697	116	975	1608
Future Volume (veh/h)	105	615	697	116	975	1608
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	114	668	758	126	1060	1748
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1346	418	1254	3017
Arrive On Green	0.00	0.00	0.38	0.38	0.36	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	758	126	1060	1748
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	8.9	3.4	14.9	7.7
Cycle Q Clear(g_c), s	0.0	0.0	8.9	3.4	14.9	7.7
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1346	418	1254	3017
V/C Ratio(X)	0.00	0.00	0.56	0.30	0.85	0.58
Avail Cap(c_a), veh/h	0	0	1346	418	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	13.0	15.6	15.5	1.2
Incr Delay (d2), s/veh	0.0	0.0	1.7	1.8	4.2	0.8
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	3.3	1.1	5.7	0.3
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	14.7	17.5	19.7	2.0
LnGrp LOS	A	A	B	B	B	A
Approach Vol, veh/h	0		884			2808
Approach Delay, s/veh	0.0		15.1			8.7
Approach LOS			B			A
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	24.9	28.1			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	16.9	10.9			9.7	0.0
Green Ext Time (p_c), s	2.3	2.7			19.8	0.0

Intersection Summary

HCM 6th Ctrl Delay	10.2
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
3: Loretta St & SR-76

Timings



Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	1378	23	26	2561	43	31
Future Volume (vph)	1378	23	26	2561	43	31
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	0.66	0.02	0.28	1.11	0.13	0.07
Control Delay	20.6	0.5	66.8	73.2	53.9	12.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	20.6	0.5	66.8	73.2	53.9	12.1
LOS	C	A	E	E	D	B
Approach Delay	20.3			73.1	36.4	
Approach LOS	C			E	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.11
 Intersection Signal Delay: 54.2
 Intersection Capacity Utilization 90.9%
 Analysis Period (min) 15
 Intersection LOS: D
 ICU Level of Service E

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

AM Existing + Cumulative + Project
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑	↑
Traffic Volume (veh/h)	1378	23	26	2561	43	31
Future Volume (veh/h)	1378	23	26	2561	43	31
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1498	25	28	2784	47	34
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2261	1325	71	2530	356	380
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1498	25	28	2784	47	34
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	42.4	0.4	2.5	113.9	3.5	2.7
Cycle Q Clear(g_c), s	42.4	0.4	2.5	113.9	3.5	2.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2261	1325	71	2530	356	380
V/C Ratio(X)	0.66	0.02	0.39	1.10	0.13	0.09
Avail Cap(c_a), veh/h	2261	1325	100	2530	356	380
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.20	0.20	1.00	1.00
Uniform Delay (d), s/veh	18.3	2.2	74.9	23.0	52.6	47.2
Incr Delay (d2), s/veh	1.5	0.0	0.7	46.7	0.8	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	17.7	0.3	1.1	60.0	1.7	1.1
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	19.9	2.2	75.6	69.8	53.4	47.7
LnGrp LOS	B	A	E	F	D	D
Approach Vol, veh/h	1523			2812	81	
Approach Delay, s/veh	19.6			69.8	51.0	
Approach LOS	B			E	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.1	109.8			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	4.5	44.4			115.9	5.5
Green Ext Time (p_c), s	0.0	18.7			0.0	0.2

Intersection Summary

HCM 6th Ctrl Delay	52.2
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↖↗	↗
Traffic Volume (veh/h)	1411	14	100	2577	20	89
Future Volume (veh/h)	1411	14	100	2577	20	89
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1534	15	109	2801	22	97
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2605	1274	130	2990	243	227
Arrive On Green	0.73	0.73	0.07	0.84	0.07	0.07
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1534	15	109	2801	22	97
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	32.4	0.3	9.7	94.4	1.0	8.9
Cycle Q Clear(g_c), s	32.4	0.3	9.7	94.4	1.0	8.9
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2605	1274	130	2990	243	227
V/C Ratio(X)	0.59	0.01	0.84	0.94	0.09	0.43
Avail Cap(c_a), veh/h	2605	1274	164	2990	713	442
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.71	0.71	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	10.0	3.1	73.3	9.5	69.6	62.6
Incr Delay (d2), s/veh	0.7	0.0	3.0	0.8	0.2	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	12.4	0.1	4.6	27.9	0.4	3.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	10.7	3.1	76.3	10.3	69.7	63.8
LnGrp LOS	B	A	E	B	E	E
Approach Vol, veh/h	1549			2910	119	
Approach Delay, s/veh	10.7			12.7	64.9	
Approach LOS	B			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	17.3	125.3			142.6	17.4
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	11.7	34.4			96.4	10.9
Green Ext Time (p_c), s	0.1	19.7			15.8	0.3

Intersection Summary

HCM 6th Ctrl Delay	13.4
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
5: Benet Rd & SR-76

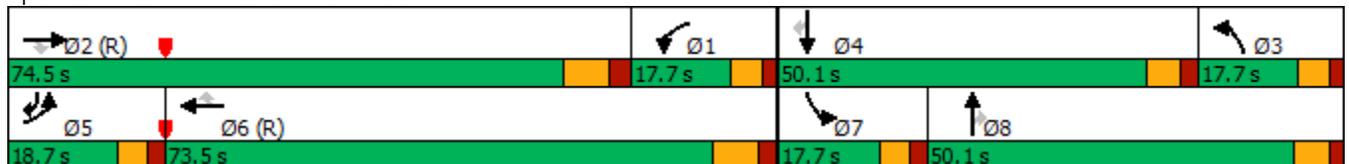
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	178	1220	138	47	2340	15	52	10	48	14	14	273
Future Volume (vph)	178	1220	138	47	2340	15	52	10	48	14	14	273
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	18.7	74.5	74.5	17.7	73.5	73.5	17.7	50.1	50.1	17.7	50.1	18.7
Total Split (%)	11.7%	46.6%	46.6%	11.1%	45.9%	45.9%	11.1%	31.3%	31.3%	11.1%	31.3%	11.7%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	28.5	99.6	99.6	12.0	79.6	79.6	18.8	25.1	25.1	12.0	11.2	41.2
Actuated g/C Ratio	0.18	0.62	0.62	0.08	0.50	0.50	0.12	0.16	0.16	0.08	0.07	0.26
v/c Ratio	0.61	0.60	0.14	0.39	1.45	0.02	0.28	0.04	0.13	0.11	0.12	0.59
Control Delay	58.5	25.6	6.9	91.4	239.1	0.0	64.1	53.3	0.7	71.3	72.2	27.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	58.5	25.6	6.9	91.4	239.1	0.0	64.1	53.3	0.7	71.3	72.2	27.1
LOS	E	C	A	F	F	A	E	D	A	E	E	C
Approach Delay		27.7			234.8			35.6			31.2	
Approach LOS		C			F			D			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.45
 Intersection Signal Delay: 142.5
 Intersection LOS: F
 Intersection Capacity Utilization 107.8%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Existing + Cumulative + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	178	1220	138	47	2340	15	52	10	48	14	14	273
Future Volume (veh/h)	178	1220	138	47	2340	15	52	10	48	14	14	273
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	193	1326	150	51	2543	16	57	11	52	15	15	297
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	145	1477	659	323	1885	841	123	360	305	65	295	378
Arrive On Green	0.08	0.83	0.42	0.18	1.00	0.53	0.07	0.19	0.19	0.04	0.16	0.26
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	193	1326	150	51	2543	16	57	11	52	15	15	297
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	13.0	39.7	9.8	3.9	0.0	0.8	4.9	0.8	2.9	1.3	1.1	22.2
Cycle Q Clear(g_c), s	13.0	39.7	9.8	3.9	0.0	0.8	4.9	0.8	2.9	1.3	1.1	22.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	145	1477	659	323	1885	841	123	360	305	65	295	378
V/C Ratio(X)	1.33	0.90	0.23	0.16	1.35	0.02	0.46	0.03	0.17	0.23	0.05	0.78
Avail Cap(c_a), veh/h	145	1477	659	323	1885	841	134	514	436	134	514	565
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.77	0.77	0.77	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.5	11.2	30.2	55.2	0.0	17.8	71.6	52.5	23.9	74.9	57.2	30.2
Incr Delay (d2), s/veh	181.7	7.1	0.6	0.0	157.5	0.0	2.7	0.0	0.3	1.8	0.1	4.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	13.3	6.4	3.9	1.8	41.2	0.3	2.4	0.4	1.8	0.6	0.5	8.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	255.2	18.4	30.8	55.2	157.5	17.8	74.3	52.5	24.2	76.7	57.3	34.4
LnGrp LOS	F	B	C	E	F	B	E	D	C	E	E	C
Approach Vol, veh/h		1669			2610			120			327	
Approach Delay, s/veh		46.9			154.7			50.6			37.4	
Approach LOS		D			F			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	37.1	74.5	17.1	31.3	18.7	92.9	11.5	36.9				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 67	12.0	* 44	* 13	65.5	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.9	41.7	6.9	24.2	15.0	2.8	3.3	4.9				
Green Ext Time (p_c), s	0.0	12.1	0.0	1.0	0.0	49.0	0.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	105.8
HCM 6th LOS	F

Notes

User approved pedestrian interval to be less than phase max green.
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
6: Foussat Rd & SR-76

Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	165	1019	84	178	2011	145	130	120	116	214	164	218
Future Volume (vph)	165	1019	84	178	2011	145	130	120	116	214	164	218
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	18.7	70.9	70.9	18.3	70.5	70.5	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (%)	11.7%	44.3%	44.3%	11.4%	44.1%	44.1%	11.7%	32.6%	32.6%	11.7%	32.6%	32.6%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.5	87.7	87.7	13.5	87.7	87.7	13.0	20.3	20.3	13.0	20.3	20.3
Actuated g/C Ratio	0.08	0.55	0.55	0.08	0.55	0.55	0.08	0.13	0.13	0.08	0.13	0.13
v/c Ratio	0.62	0.57	0.10	0.67	1.13	0.17	0.51	0.20	0.41	0.84	0.40	0.42
Control Delay	109.6	7.4	0.9	82.6	98.0	7.9	77.3	60.9	11.5	96.9	64.9	7.8
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	109.6	7.4	0.9	82.6	98.0	7.9	77.3	60.9	11.5	96.9	64.9	7.8
LOS	F	A	A	F	F	A	E	E	B	F	E	A
Approach Delay		20.2			91.2			51.0			55.5	
Approach LOS		C			F			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 18.7 (12%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.13
 Intersection Signal Delay: 63.6
 Intersection LOS: E
 Intersection Capacity Utilization 108.8%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



AM Existing + Cumulative + Project
6: Fousat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	
Traffic Volume (veh/h)	165	1019	84	178	2011	145	130	120	116	214	164	218
Future Volume (veh/h)	165	1019	84	178	2011	145	130	120	116	214	164	218
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	179	1108	91	193	2186	158	141	130	126	233	178	237
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	281	2046	913	259	2024	903	280	555	172	281	387	304
Arrive On Green	0.16	1.00	1.00	0.07	0.57	0.57	0.08	0.11	0.11	0.08	0.11	0.11
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	179	1108	91	193	2186	158	141	130	126	233	178	237
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	7.7	0.0	0.0	8.8	91.1	7.6	6.3	3.7	12.3	10.6	7.5	13.2
Cycle Q Clear(g_c), s	7.7	0.0	0.0	8.8	91.1	7.6	6.3	3.7	12.3	10.6	7.5	13.2
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	281	2046	913	259	2024	903	280	555	172	281	387	304
V/C Ratio(X)	0.64	0.54	0.10	0.74	1.08	0.18	0.50	0.23	0.73	0.83	0.46	0.78
Avail Cap(c_a), veh/h	281	2046	913	272	2024	903	281	1468	456	281	1022	802
HCM Platoon Ratio	2.00	2.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.79	0.79	0.79	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	64.8	0.0	0.0	72.5	34.4	16.5	70.4	65.2	69.1	72.4	66.9	69.4
Incr Delay (d2), s/veh	3.8	0.8	0.2	10.1	45.5	0.4	1.4	0.2	5.9	18.5	0.9	4.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.4	0.2	0.0	4.3	51.0	3.0	2.8	1.6	5.3	5.5	3.5	4.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	68.6	0.8	0.2	82.6	80.0	16.9	71.8	65.4	74.9	90.9	67.7	73.8
LnGrp LOS	E	A	A	F	F	B	E	E	E	F	E	E
Approach Vol, veh/h		1378			2537			397			648	
Approach Delay, s/veh		9.6			76.3			70.7			78.3	
Approach LOS		A			E			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	100.1	18.7	23.5	18.7	99.1	18.7	23.5				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	62.9	* 13	46.0	* 13	62.5	* 13	46.0				
Max Q Clear Time (g_c+I1), s	10.8	2.0	8.3	15.2	9.7	93.1	12.6	14.3				
Green Ext Time (p_c), s	0.1	11.7	0.2	2.2	0.2	0.0	0.0	1.3				

Intersection Summary

HCM 6th Ctrl Delay	57.6
HCM 6th LOS	E

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Existing + Cumulative + Project
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔		↔		↔↔↔	
Traffic Vol, veh/h	27	16	163	29	5	302
Future Vol, veh/h	27	16	163	29	5	302
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	29	17	177	32	5	328

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	334	193	0	0	209
Stage 1	193	-	-	-	-
Stage 2	141	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	659	848	-	-	1360
Stage 1	808	-	-	-	-
Stage 2	832	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	656	848	-	-	1360
Mov Cap-2 Maneuver	656	-	-	-	-
Stage 1	808	-	-	-	-
Stage 2	829	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.4	0	0.1
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	716	1360
HCM Lane V/C Ratio	-	-	0.065	0.004
HCM Control Delay (s)	-	-	10.4	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Existing + Cumulative + Project
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	28	0	110	80	0	258
Future Vol, veh/h	28	0	110	80	0	258
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	0	120	87	0	280

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	444	164	0	0	207	0
Stage 1	164	-	-	-	-	-
Stage 2	280	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	571	881	-	-	1364	-
Stage 1	865	-	-	-	-	-
Stage 2	767	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	571	881	-	-	1364	-
Mov Cap-2 Maneuver	571	-	-	-	-	-
Stage 1	865	-	-	-	-	-
Stage 2	767	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	571	1364
HCM Lane V/C Ratio	-	-	0.053	-
HCM Control Delay (s)	-	-	11.7	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Existing + Cumulative + Project
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	7.2			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	47	186	484	531
Demand Flow Rate, veh/h	48	190	494	542
Vehicles Circulating, veh/h	674	397	44	196
Vehicles Exiting, veh/h	64	141	678	391
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	6.0	6.1	6.3	8.6
Approach LOS	A	A	A	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	48	190	494	542
Cap Entry Lane, veh/h	694	920	1319	1130
Entry HV Adj Factor	0.978	0.978	0.980	0.980
Flow Entry, veh/h	47	186	484	531
Cap Entry, veh/h	678	900	1294	1107
V/C Ratio	0.069	0.206	0.374	0.480
Control Delay, s/veh	6.0	6.1	6.3	8.6
LOS	A	A	A	A
95th %tile Queue, veh	0	1	2	3

LOS Engineering, Inc.

PM Existing + Cumulative + Project
1: SR-76 & I-5 SB Ramp

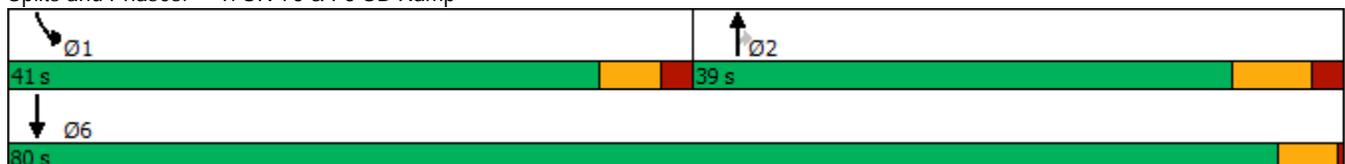
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	532	288	809	846
Future Volume (vph)	0	532	288	809	846
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	40.7	40.7	26.8	0.0
Actuated g/C Ratio	0.00	0.51	0.51	0.34	0.00
v/c Ratio	no cap	0.32	0.37	0.76	no cap
Control Delay		13.1	11.4	28.1	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	13.1	11.4	28.1	Error
LOS	F	B	B	C	F
Approach Delay	Err	12.5			Err
Approach LOS	F	B			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 45
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection LOS: F
 Intersection Capacity Utilization 44.0%
 ICU Level of Service A
 Analysis Period (min) 15

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

PM Existing + Cumulative + Project
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	686	532	288	809	846
Future Volume (veh/h)	0	686	532	288	809	846
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			578	313	879	920
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1209	539	1229	0
Arrive On Green			0.34	0.34	0.36	0.83
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			578	313	879	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			5.3	6.7	9.0	0.0
Cycle Q Clear(g_c), s			5.3	6.7	9.0	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1209	539	1229	0
V/C Ratio(X)			0.48	0.58	0.72	0.00
Avail Cap(c_a), veh/h			2784	1242	2968	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			10.7	11.1	11.4	0.0
Incr Delay (d2), s/veh			0.3	1.0	0.8	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			1.7	2.0	2.8	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			11.0	12.1	12.2	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			891			879
Approach Delay, s/veh			11.4			12.2
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	20.3	20.8				41.1
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	11.0	8.7				0.0
Green Ext Time (p_c), s	3.6	5.3				0.0

Intersection Summary

HCM 6th Ctrl Delay			11.8			
HCM 6th LOS			B			

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative + Project
2: SR-76 & I-5 NB Ramp

Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↘	↑↑	↘	↘↘	↑↑
Traffic Volume (vph)	290	1265	123	517	1292
Future Volume (vph)	290	1265	123	517	1292
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.58	1.41	0.11	1.80	0.91
Control Delay	290.1	219.0	2.7	396.6	31.2
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	290.1	219.0	2.7	396.6	31.2
LOS	F	F	A	F	C
Approach Delay	290.1	199.8			135.7
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 140
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.80
 Intersection Signal Delay: 196.3
 Intersection Capacity Utilization 133.3%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

PM Existing + Cumulative + Project
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	290	828	1265	123	517	1292
Future Volume (veh/h)	290	828	1265	123	517	1292
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	315	900	1375	134	562	1404
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1818	586	587	2893
Arrive On Green	0.00	0.00	0.51	0.51	0.17	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1375	134	562	1404
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	13.3	2.5	6.9	5.2
Cycle Q Clear(g_c), s	0.0	0.0	13.3	2.5	6.9	5.2
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1818	586	587	2893
V/C Ratio(X)	0.00	0.00	0.76	0.23	0.96	0.49
Avail Cap(c_a), veh/h	0	0	1818	586	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	8.4	9.3	17.7	1.2
Incr Delay (d2), s/veh	0.0	0.0	3.0	0.9	26.9	0.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	4.0	0.6	4.6	0.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	11.4	10.2	44.6	1.8
LnGrp LOS	A	A	B	B	D	A
Approach Vol, veh/h	0		1509			1966
Approach Delay, s/veh	0.0		11.3			14.1
Approach LOS			B			B
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	13.0	30.0			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	8.9	15.3			7.2	0.0
Green Ext Time (p_c), s	0.0	4.9			13.0	0.0

Intersection Summary

HCM 6th Ctrl Delay	12.8
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative + Project
3: Loretta St & SR-76

Timings

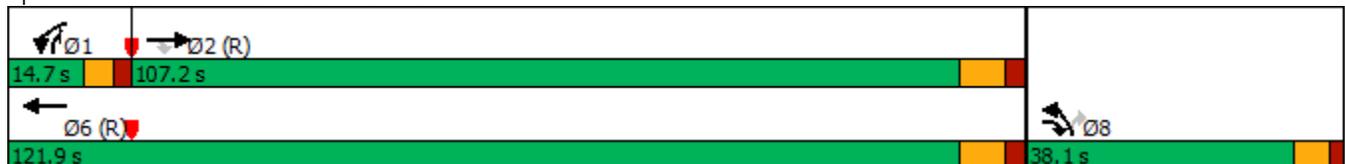


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	2237	73	32	1770	25	21
Future Volume (vph)	2237	73	32	1770	25	21
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	1.08	0.06	0.35	0.76	0.08	0.05
Control Delay	72.2	0.3	85.1	9.2	52.9	34.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	72.2	0.3	85.1	9.2	52.9	34.2
LOS	E	A	F	A	D	C
Approach Delay	69.9			10.5	44.3	
Approach LOS	E			B	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.08
 Intersection Signal Delay: 43.9
 Intersection LOS: D
 Intersection Capacity Utilization 81.9%
 ICU Level of Service D
 Analysis Period (min) 15

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

PM Existing + Cumulative + Project
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (veh/h)	2237	73	32	1770	25	21
Future Volume (veh/h)	2237	73	32	1770	25	21
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2432	79	35	1924	27	23
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2245	1319	79	2530	356	387
Arrive On Green	0.63	0.63	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2432	79	35	1924	27	23
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	101.1	1.4	3.1	54.4	2.0	1.8
Cycle Q Clear(g_c), s	101.1	1.4	3.1	54.4	2.0	1.8
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2245	1319	79	2530	356	387
V/C Ratio(X)	1.08	0.06	0.44	0.76	0.08	0.06
Avail Cap(c_a), veh/h	2245	1319	100	2530	356	387
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.70	0.70	1.00	1.00
Uniform Delay (d), s/veh	29.5	2.4	74.5	14.5	52.0	46.3
Incr Delay (d2), s/veh	45.9	0.1	2.7	1.6	0.4	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	55.5	1.2	1.5	21.3	0.9	0.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	75.3	2.5	77.2	16.0	52.4	46.6
LnGrp LOS	F	A	E	B	D	D
Approach Vol, veh/h	2511			1959	50	
Approach Delay, s/veh	73.1			17.1	49.8	
Approach LOS	E			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.8	109.1			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	5.1	103.1			56.4	4.0
Green Ext Time (p_c), s	0.0	0.0			29.7	0.1

Intersection Summary

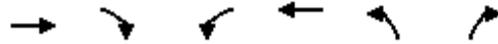
HCM 6th Ctrl Delay	48.6
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative + Project
4: Canyon Dr & SR-76

Timings

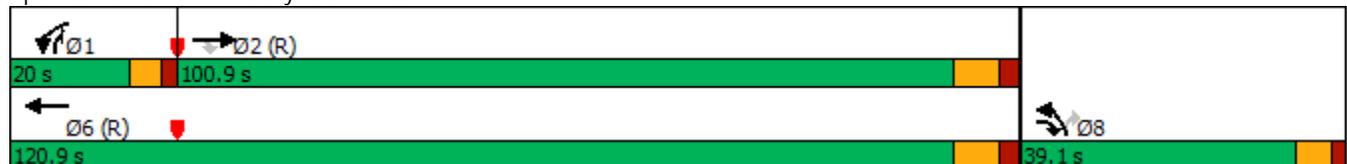


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙↙	↙
Traffic Volume (vph)	2195	48	161	1783	7	118
Future Volume (vph)	2195	48	161	1783	7	118
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.9	39.1	20.0	120.9	39.1	20.0
Total Split (%)	63.1%	24.4%	12.5%	75.6%	24.4%	12.5%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effect Green (s)	102.8	124.6	23.6	132.1	13.8	43.5
Actuated g/C Ratio	0.64	0.78	0.15	0.83	0.09	0.27
v/c Ratio	1.05	0.04	0.67	0.66	0.03	0.30
Control Delay	66.9	2.6	43.1	22.1	61.0	45.9
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	66.9	2.6	43.1	22.1	61.0	45.9
LOS	E	A	D	C	E	D
Approach Delay	65.5			23.8	46.8	
Approach LOS	E			C	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.05
 Intersection Signal Delay: 46.2
 Intersection LOS: D
 Intersection Capacity Utilization 93.6%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Existing + Cumulative + Project
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑↑	↑
Traffic Volume (veh/h)	2195	48	161	1783	7	118
Future Volume (veh/h)	2195	48	161	1783	7	118
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2386	52	175	1938	8	128
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2487	1247	159	2931	301	280
Arrive On Green	0.70	0.70	0.09	0.82	0.09	0.09
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	2386	52	175	1938	8	128
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	98.2	1.2	14.3	33.6	0.3	11.6
Cycle Q Clear(g_c), s	98.2	1.2	14.3	33.6	0.3	11.6
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2487	1247	159	2931	301	280
V/C Ratio(X)	0.96	0.04	1.10	0.66	0.03	0.46
Avail Cap(c_a), veh/h	2487	1247	159	2931	713	469
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	21.9	3.8	72.8	5.4	66.8	59.0
Incr Delay (d2), s/veh	1.4	0.0	54.0	0.1	0.0	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	38.5	0.6	9.0	10.5	0.2	4.8
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	23.4	3.8	126.8	5.5	66.9	60.2
LnGrp LOS	C	A	F	A	E	E
Approach Vol, veh/h	2438			2113	136	
Approach Delay, s/veh	23.0			15.6	60.6	
Approach LOS	C			B	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	20.0	120.0			140.0	20.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 14	92.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	16.3	100.2			35.6	13.6
Green Ext Time (p_c), s	0.0	0.0			34.2	0.4

Intersection Summary

HCM 6th Ctrl Delay	20.7
HCM 6th LOS	C

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative + Project
5: Benet Rd & SR-76

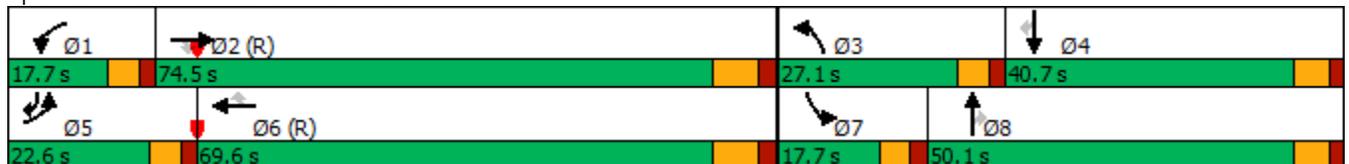
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	215	2072	90	43	1605	19	137	23	99	24	15	197
Future Volume (vph)	215	2072	90	43	1605	19	137	23	99	24	15	197
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	22.6	74.5	74.5	17.7	69.6	69.6	27.1	50.1	50.1	17.7	40.7	22.6
Total Split (%)	14.1%	46.6%	46.6%	11.1%	43.5%	43.5%	16.9%	31.3%	31.3%	11.1%	25.4%	14.1%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	32.1	92.4	92.4	12.0	68.8	68.8	17.9	28.7	28.7	12.0	15.7	53.9
Actuated g/C Ratio	0.20	0.58	0.58	0.08	0.43	0.43	0.11	0.18	0.18	0.08	0.10	0.34
v/c Ratio	0.66	1.10	0.10	0.36	1.15	0.03	0.76	0.07	0.29	0.20	0.09	0.35
Control Delay	48.9	96.8	7.1	66.6	109.2	0.1	91.7	53.9	9.2	73.4	62.0	20.4
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	48.9	96.8	7.1	66.6	109.2	0.1	91.7	53.9	9.2	73.4	62.0	20.4
LOS	D	F	A	E	F	A	F	D	A	E	E	C
Approach Delay		89.1			106.8			56.8			28.4	
Approach LOS		F			F			E			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 22.6 (14%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.15
 Intersection Signal Delay: 90.6
 Intersection LOS: F
 Intersection Capacity Utilization 98.0%
 ICU Level of Service F
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



PM Existing + Cumulative + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	215	2072	90	43	1605	19	137	23	99	24	15	197
Future Volume (veh/h)	215	2072	90	43	1605	19	137	23	99	24	15	197
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	234	2252	98	47	1745	21	149	25	108	26	16	214
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	1933	862	117	1791	799	171	336	285	92	252	381
Arrive On Green	0.11	0.54	0.54	0.07	0.50	0.50	0.10	0.18	0.18	0.05	0.13	0.13
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	234	2252	98	47	1745	21	149	25	108	26	16	214
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	87.0	4.8	4.1	76.6	1.1	13.2	1.8	9.6	2.2	1.2	19.0
Cycle Q Clear(g_c), s	16.9	87.0	4.8	4.1	76.6	1.1	13.2	1.8	9.6	2.2	1.2	19.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	1933	862	117	1791	799	171	336	285	92	252	381
V/C Ratio(X)	1.24	1.17	0.11	0.40	0.97	0.03	0.87	0.07	0.38	0.28	0.06	0.56
Avail Cap(c_a), veh/h	188	1933	862	134	1791	799	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.53	0.53	0.53	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	36.5	17.7	71.7	38.7	19.9	71.3	54.6	57.8	73.1	60.4	53.3
Incr Delay (d2), s/veh	113.9	74.9	0.0	1.2	10.6	0.0	21.4	0.1	0.8	1.7	0.1	1.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	13.7	56.9	1.8	1.9	35.5	0.4	7.1	0.9	4.0	1.1	0.6	7.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	185.4	111.4	17.8	72.9	49.2	20.0	92.8	54.7	58.6	74.7	60.5	54.6
LnGrp LOS	F	F	B	E	D	B	F	D	E	E	E	D
Approach Vol, veh/h		2584			1813			282			256	
Approach Delay, s/veh		114.5			49.5			76.3			57.0	
Approach LOS		F			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.2	95.0	21.1	27.7	22.6	88.6	13.9	34.8				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.1	89.0	15.2	21.0	18.9	78.6	4.2	11.6				
Green Ext Time (p_c), s	0.0	0.0	0.2	0.6	0.0	0.0	0.0	0.5				

Intersection Summary

HCM 6th Ctrl Delay	85.5
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Existing + Cumulative + Project
6: Fousat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 		
Traffic Volume (veh/h)	332	1821	164	139	1329	212	86	261	187	161	177	252
Future Volume (veh/h)	332	1821	164	139	1329	212	86	261	187	161	177	252
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	361	1979	178	151	1445	230	93	284	203	175	192	274
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	309	1888	842	259	1836	819	276	783	243	281	549	431
Arrive On Green	0.09	0.53	0.53	0.07	0.52	0.52	0.08	0.15	0.15	0.08	0.15	0.15
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	361	1979	178	151	1445	230	93	284	203	175	192	274
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	14.3	85.0	9.5	6.8	53.0	13.1	4.1	8.0	19.9	7.8	7.7	14.7
Cycle Q Clear(g_c), s	14.3	85.0	9.5	6.8	53.0	13.1	4.1	8.0	19.9	7.8	7.7	14.7
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	309	1888	842	259	1836	819	276	783	243	281	549	431
V/C Ratio(X)	1.17	1.05	0.21	0.58	0.79	0.28	0.34	0.36	0.84	0.62	0.35	0.64
Avail Cap(c_a), veh/h	309	1888	842	287	1836	819	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	72.8	37.5	19.8	71.6	31.5	21.9	69.6	60.7	65.8	71.1	60.4	63.4
Incr Delay (d2), s/veh	79.4	23.5	0.1	2.5	3.5	0.9	0.7	0.3	7.4	3.3	0.4	1.6
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	9.8	42.3	3.6	3.1	23.5	5.2	1.8	3.5	8.6	3.6	3.5	5.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	152.3	61.0	19.9	74.1	35.0	22.7	70.3	61.0	73.2	74.5	60.8	65.0
LnGrp LOS	F	F	B	E	C	C	E	E	E	E	E	E
Approach Vol, veh/h		2518			1826			580			641	
Approach Delay, s/veh		71.2			36.7			66.8			66.3	
Approach LOS		E			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	93.0	18.5	30.8	20.0	90.7	18.7	30.6				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	8.8	87.0	6.1	16.7	16.3	55.0	9.8	21.9				
Green Ext Time (p_c), s	0.2	0.0	0.1	2.4	0.0	3.2	0.2	2.6				
Intersection Summary												
HCM 6th Ctrl Delay			58.8									
HCM 6th LOS			E									
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Existing + Cumulative + Project
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.9					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	25	11	235	16	5	204
Future Vol, veh/h	25	11	235	16	5	204
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	27	12	255	17	5	222

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	363	264	0	0	272
Stage 1	264	-	-	-	-
Stage 2	99	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	636	774	-	-	1290
Stage 1	752	-	-	-	-
Stage 2	874	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	633	774	-	-	1290
Mov Cap-2 Maneuver	633	-	-	-	-
Stage 1	752	-	-	-	-
Stage 2	871	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.7	0	0.2
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	670	1290
HCM Lane V/C Ratio	-	-	0.058	0.004
HCM Control Delay (s)	-	-	10.7	7.8
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

PM Existing + Cumulative + Project
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	2.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	81	0	212	36	0	129
Future Vol, veh/h	81	0	212	36	0	129
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	0	230	39	0	140

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	390	250	0	0	269
Stage 1	250	-	-	-	-
Stage 2	140	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	614	789	-	-	1295
Stage 1	792	-	-	-	-
Stage 2	887	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	614	789	-	-	1295
Mov Cap-2 Maneuver	614	-	-	-	-
Stage 1	792	-	-	-	-
Stage 2	887	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.8	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	614	1295
HCM Lane V/C Ratio	-	-	0.143	-
HCM Control Delay (s)	-	-	11.8	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.5	0

LOS Engineering, Inc.

PM Existing + Cumulative + Project
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	11.0			
Intersection LOS	B			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	120	239	867	496
Demand Flow Rate, veh/h	122	243	885	506
Vehicles Circulating, veh/h	677	655	98	224
Vehicles Exiting, veh/h	53	328	701	674
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	7.3	9.6	13.3	8.5
Approach LOS	A	A	B	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	122	243	885	506
Cap Entry Lane, veh/h	692	707	1249	1098
Entry HV Adj Factor	0.982	0.983	0.980	0.980
Flow Entry, veh/h	120	239	867	496
Cap Entry, veh/h	679	695	1223	1076
V/C Ratio	0.176	0.343	0.709	0.461
Control Delay, s/veh	7.3	9.6	13.3	8.5
LOS	A	A	B	A
95th %tile Queue, veh	1	2	6	2

LOS Engineering, Inc.

Appendix L

Horizon Year Segment Volumes

OCEANSIDE GENERAL PLAN CIRCULATION ELEMENT UPDATE

City of Oceanside, California

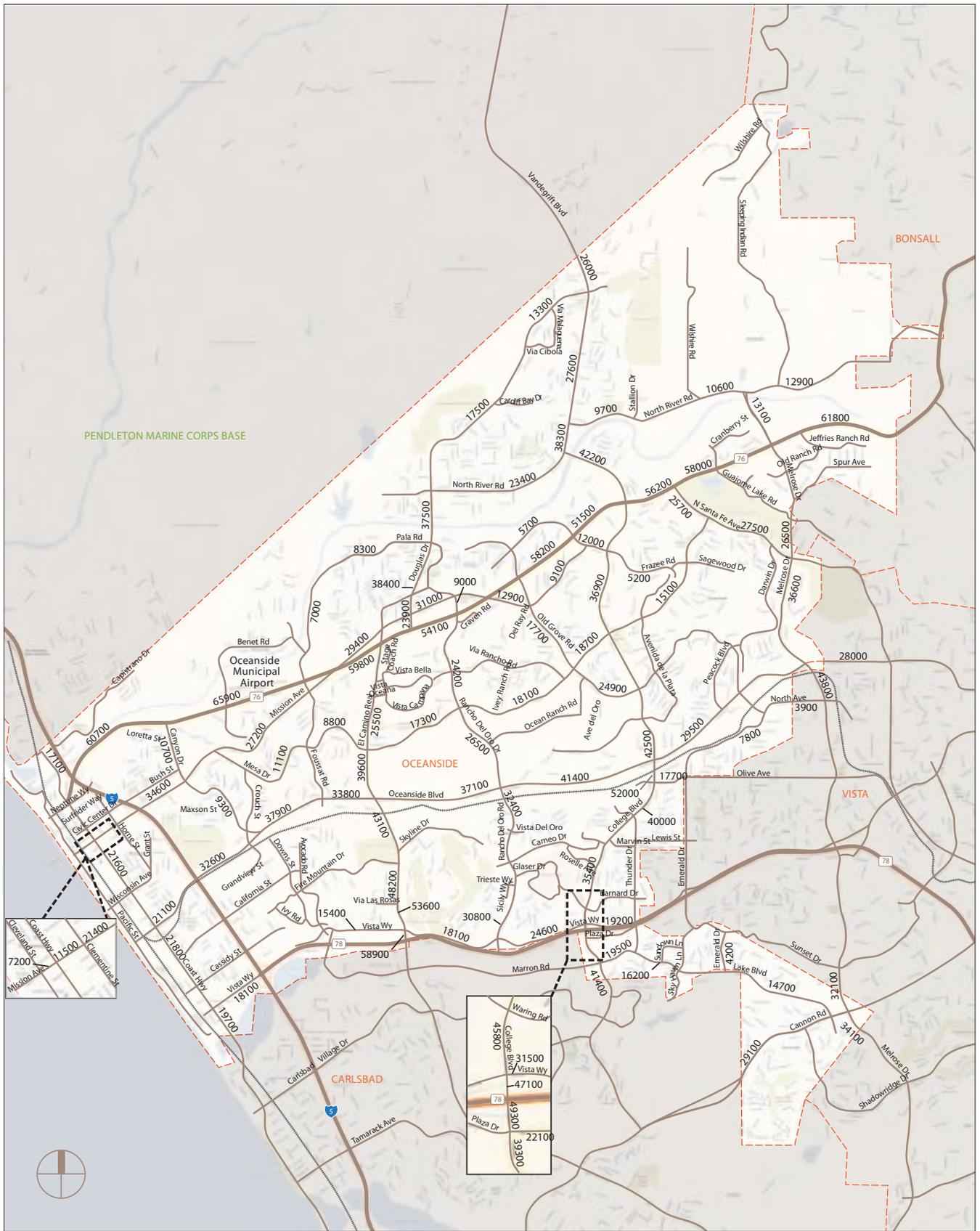
Prepared for
City of Oceanside
Transportation Engineering Division
300 North Coast Highway
Oceanside, CA 92054

Prepared by



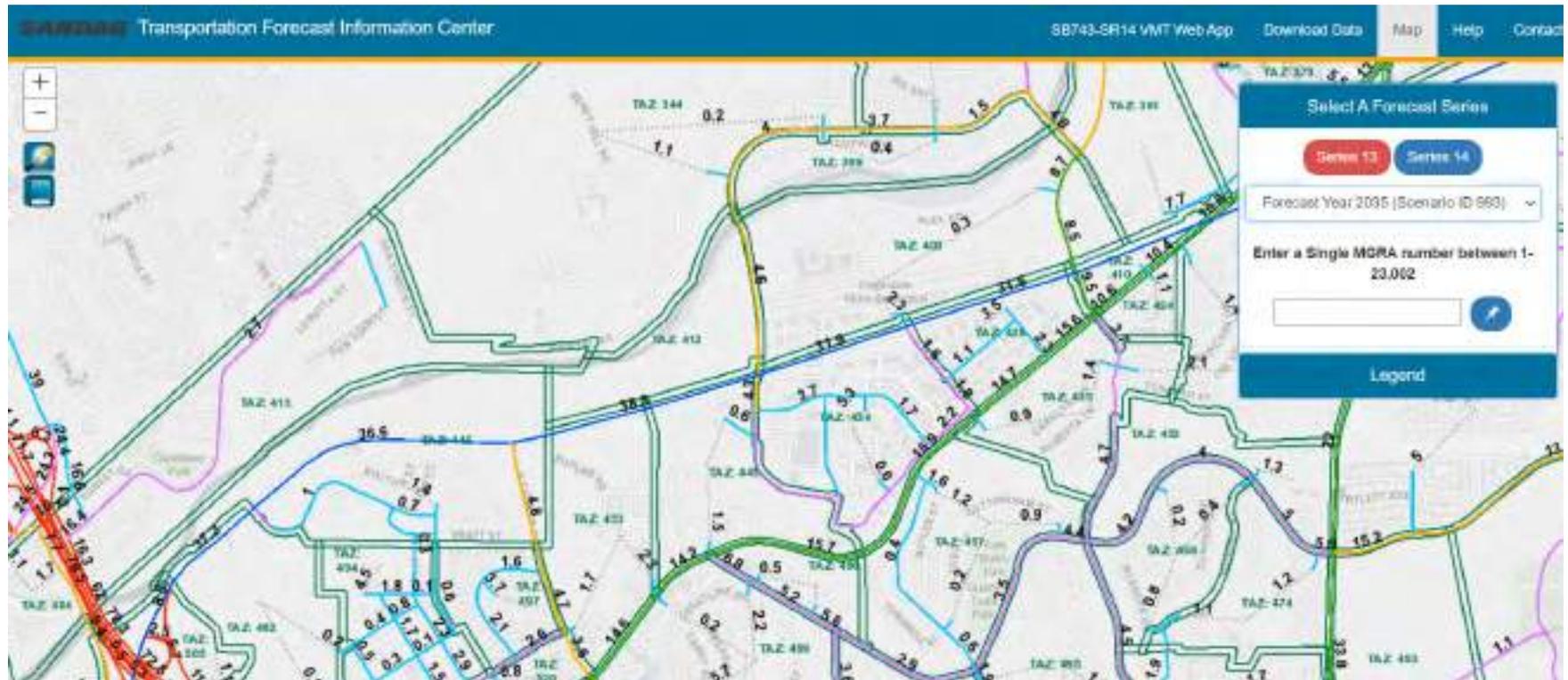
701 B Street, Suite 1810
San Diego, CA 92101

September 2012



2030 Master Transportation Roadway Plan Traffic Volumes

SANDAG Series 13 Year 2035 Volumes



Appendix M

Horizon Year 2030 Intersection LOS Worksheets

AM Horizon Year
1: SR-76 & I-5 SB Ramp

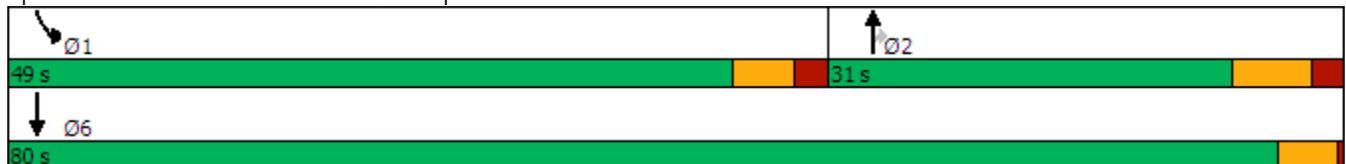
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	370	290	1420	590
Future Volume (vph)	0	370	290	1420	590
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		31.0	31.0	49.0	80.0
Total Split (%)		38.8%	38.8%	61.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	26.1	26.1	41.4	0.0
Actuated g/C Ratio	0.00	0.33	0.33	0.52	0.00
v/c Ratio	no cap	0.35	0.59	0.87	no cap
Control Delay		22.2	27.1	23.1	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	22.2	27.1	23.1	Error
LOS	F	C	C	C	F
Approach Delay	Err	24.3			Err
Approach LOS	F	C			F

Intersection Summary

Cycle Length: 80	
Actuated Cycle Length: 80	
Natural Cycle: 60	
Control Type: Actuated-Uncoordinated	
Maximum v/c Ratio: Err	
Intersection Signal Delay: Err	Intersection LOS: F
Intersection Capacity Utilization 44.1%	ICU Level of Service A
Analysis Period (min) 15	

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

AM Horizon Year
1: SR-76 & I-5 SB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↗↗	
Traffic Volume (veh/h)	0	440	370	290	1420	590
Future Volume (veh/h)	0	440	370	290	1420	590
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			402	315	1543	641
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			939	419	1827	0
Arrive On Green			0.26	0.26	0.53	0.89
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			402	315	1543	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			5.7	11.0	23.0	0.0
Cycle Q Clear(g_c), s			5.7	11.0	23.0	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			939	419	1827	0
V/C Ratio(X)			0.43	0.75	0.84	0.00
Avail Cap(c_a), veh/h			1424	635	2478	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			18.4	20.4	12.1	0.0
Incr Delay (d2), s/veh			0.3	2.7	2.1	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			2.2	4.0	7.5	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			18.7	23.2	14.2	0.0
LnGrp LOS			B	C	B	A
Approach Vol, veh/h			717			1543
Approach Delay, s/veh			20.7			14.2
Approach LOS			C			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	37.6	22.8				60.4
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	25.0	13.0				0.0
Green Ext Time (p_c), s	7.0	2.9				0.0

Intersection Summary

HCM 6th Ctrl Delay	16.3
HCM 6th LOS	B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
2: SR-76 & I-5 NB Ramp

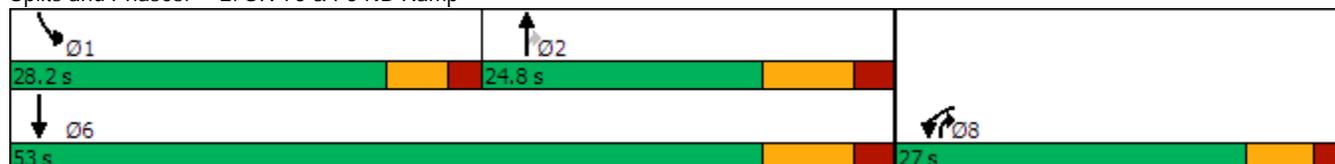
Timings

					
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations					
Traffic Volume (vph)	110	720	120	1180	1850
Future Volume (vph)	110	720	120	1180	1850
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	1.33	1.05	0.14	1.33	1.01
Control Delay	175.6	80.8	8.4	182.9	41.8
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	175.6	80.8	8.4	182.9	41.8
LOS	F	F	A	F	D
Approach Delay	175.6	70.5			96.8
Approach LOS	F	E			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 110
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.33
 Intersection Signal Delay: 107.5
 Intersection LOS: F
 Intersection Capacity Utilization 126.8%
 ICU Level of Service H
 Analysis Period (min) 15

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Horizon Year
2: SR-76 & I-5 NB Ramp



Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Volume (veh/h)	110	820	720	120	1180	1850
Future Volume (veh/h)	110	820	720	120	1180	1850
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1900	1870	1870	1870	1870
Adj Flow Rate, veh/h	120	891	783	130	1283	2011
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	0	2	2	2	2
Cap, veh/h	0	0	1181	345	1414	3017
Arrive On Green	0.00	0.00	0.33	0.33	0.41	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	783	130	1283	2011
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	10.0	3.7	18.5	10.4
Cycle Q Clear(g_c), s	0.0	0.0	10.0	3.7	18.5	10.4
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1181	345	1414	3017
V/C Ratio(X)	0.00	0.00	0.66	0.38	0.91	0.67
Avail Cap(c_a), veh/h	0	0	1181	345	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	15.1	17.7	14.7	1.4
Incr Delay (d2), s/veh	0.0	0.0	2.9	3.1	8.4	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	4.0	1.3	7.6	0.5
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	18.1	20.8	23.1	2.6
LnGrp LOS	A	A	B	C	C	A
Approach Vol, veh/h	0		913			3294
Approach Delay, s/veh	0.0		18.5			10.6
Approach LOS			B			B
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	27.4	25.6			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	20.5	12.0			12.4	0.0
Green Ext Time (p_c), s	1.2	2.4			22.6	0.0

Intersection Summary

HCM 6th Ctrl Delay		12.3	
HCM 6th LOS		B	

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
3: Loretta St & SR-76

Timings

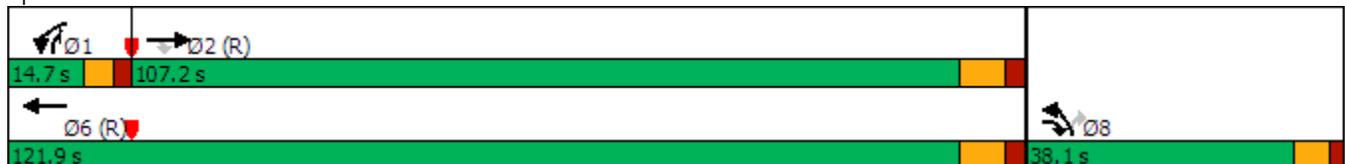


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙	↗
Traffic Volume (vph)	1480	30	20	2990	60	30
Future Volume (vph)	1480	30	20	2990	60	30
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.2	38.1	14.7	121.9	38.1	14.7
Total Split (%)	67.0%	23.8%	9.2%	76.2%	23.8%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	102.1	143.4	9.0	113.9	32.0	47.1
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.29
v/c Ratio	0.71	0.02	0.22	1.29	0.18	0.07
Control Delay	22.1	0.4	63.6	156.6	54.8	12.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	22.1	0.4	63.6	156.6	54.8	12.2
LOS	C	A	E	F	D	B
Approach Delay	21.7			156.0	40.5	
Approach LOS	C			F	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.29
 Intersection Signal Delay: 109.7
 Intersection LOS: F
 Intersection Capacity Utilization 102.7%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

AM Horizon Year
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↖	↗
Traffic Volume (veh/h)	1480	30	20	2990	60	30
Future Volume (veh/h)	1480	30	20	2990	60	30
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1609	33	22	3250	65	33
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2278	1333	63	2530	356	373
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1609	33	22	3250	65	33
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	47.5	0.5	1.9	113.9	4.8	2.6
Cycle Q Clear(g_c), s	47.5	0.5	1.9	113.9	4.8	2.6
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2278	1333	63	2530	356	373
V/C Ratio(X)	0.71	0.02	0.35	1.28	0.18	0.09
Avail Cap(c_a), veh/h	2278	1333	100	2530	356	373
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	18.8	2.1	75.4	23.0	53.1	47.8
Incr Delay (d2), s/veh	1.9	0.0	0.3	128.4	1.1	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	19.8	0.5	0.9	88.6	2.3	1.1
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	20.7	2.1	75.7	151.5	54.3	48.3
LnGrp LOS	C	A	E	F	D	D
Approach Vol, veh/h	1642			3272	98	
Approach Delay, s/veh	20.3			150.9	52.2	
Approach LOS	C			F	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	11.3	110.6			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.2			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.9	49.5			115.9	6.8
Green Ext Time (p_c), s	0.0	20.6			0.0	0.2

Intersection Summary

HCM 6th Ctrl Delay	106.2
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
4: Canyon Dr & SR-76

Timings

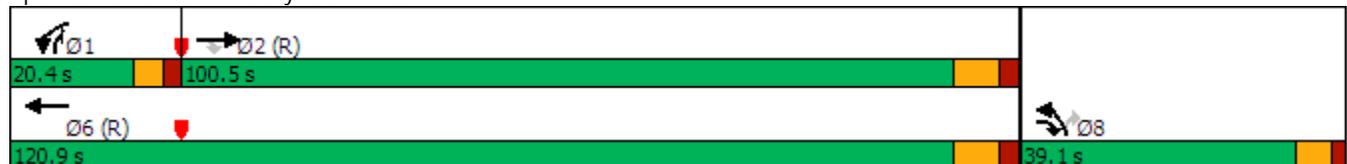


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↘↘	↙
Traffic Volume (vph)	1520	20	100	3010	30	100
Future Volume (vph)	1520	20	100	3010	30	100
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.5	39.1	20.4	120.9	39.1	20.4
Total Split (%)	62.8%	24.4%	12.8%	75.6%	24.4%	12.8%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effect Green (s)	112.3	134.1	14.1	132.1	13.8	34.0
Actuated g/C Ratio	0.70	0.84	0.09	0.83	0.09	0.21
v/c Ratio	0.67	0.02	0.70	1.12	0.11	0.31
Control Delay	30.4	2.3	58.6	84.1	64.4	42.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	30.4	2.3	58.6	84.1	64.4	42.0
LOS	C	A	E	F	E	D
Approach Delay	30.1			83.3	47.2	
Approach LOS	C			F	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20.4 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.12
 Intersection Signal Delay: 65.2
 Intersection LOS: E
 Intersection Capacity Utilization 102.5%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

AM Horizon Year
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↖↗	↗
Traffic Volume (veh/h)	1520	20	100	3010	30	100
Future Volume (veh/h)	1520	20	100	3010	30	100
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1652	22	109	3272	33	109
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2579	1274	130	2964	269	239
Arrive On Green	0.73	0.73	0.07	0.83	0.08	0.08
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1652	22	109	3272	33	109
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	38.1	0.4	9.7	133.5	1.4	10.0
Cycle Q Clear(g_c), s	38.1	0.4	9.7	133.5	1.4	10.0
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2579	1274	130	2964	269	239
V/C Ratio(X)	0.64	0.02	0.84	1.10	0.12	0.46
Avail Cap(c_a), veh/h	2579	1274	164	2964	713	442
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.65	0.65	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	11.2	3.1	73.3	13.3	68.7	62.0
Incr Delay (d2), s/veh	0.8	0.0	3.0	47.3	0.2	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	14.7	0.2	4.6	59.7	0.6	4.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	12.0	3.1	76.3	60.6	68.9	63.4
LnGrp LOS	B	A	E	F	E	E
Approach Vol, veh/h	1674			3381	142	
Approach Delay, s/veh	11.9			61.1	64.7	
Approach LOS	B			E	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	17.3	124.1			141.5	18.5
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	11.7	40.1			135.5	12.0
Green Ext Time (p_c), s	0.1	21.9			0.0	0.4

Intersection Summary

HCM 6th Ctrl Delay	45.3
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
5: Benet Rd & SR-76

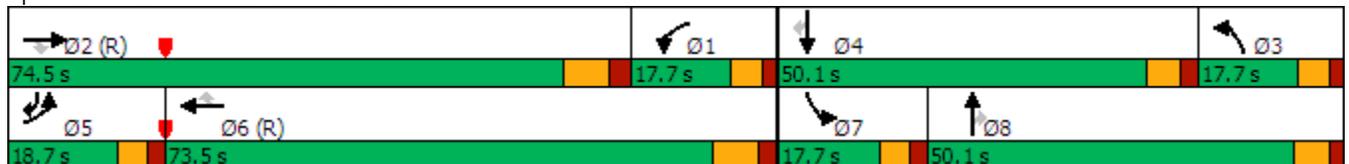
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	130	1370	170	40	2700	10	70	10	60	10	20	330
Future Volume (vph)	130	1370	170	40	2700	10	70	10	60	10	20	330
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	18.7	74.5	74.5	17.7	73.5	73.5	17.7	50.1	50.1	17.7	50.1	18.7
Total Split (%)	11.7%	46.6%	46.6%	11.1%	45.9%	45.9%	11.1%	31.3%	31.3%	11.1%	31.3%	11.7%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	19.0	96.0	96.0	12.0	85.5	85.5	19.1	28.7	28.7	12.0	11.0	30.4
Actuated g/C Ratio	0.12	0.60	0.60	0.08	0.53	0.53	0.12	0.18	0.18	0.08	0.07	0.19
v/c Ratio	0.67	0.70	0.18	0.33	1.55	0.01	0.36	0.03	0.15	0.08	0.17	0.89
Control Delay	63.8	28.4	6.9	71.2	276.8	0.0	66.6	52.9	0.8	70.6	73.7	54.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	63.8	28.4	6.9	71.2	276.8	0.0	66.6	52.9	0.8	70.6	73.7	54.2
LOS	E	C	A	E	F	A	E	D	A	E	E	D
Approach Delay		28.9			272.8			37.4			55.7	
Approach LOS		C			F			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.55
 Intersection Signal Delay: 167.4
 Intersection LOS: F
 Intersection Capacity Utilization 121.2%
 ICU Level of Service H
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Horizon Year
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	130	1370	170	40	2700	10	70	10	60	10	20	330
Future Volume (veh/h)	130	1370	170	40	2700	10	70	10	60	10	20	330
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	141	1489	185	43	2935	11	76	11	65	11	22	359
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	145	1477	659	263	1763	787	129	438	371	52	352	427
Arrive On Green	0.08	0.83	0.42	0.15	0.99	0.50	0.07	0.23	0.23	0.03	0.19	0.31
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	141	1489	185	43	2935	11	76	11	65	11	22	359
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	12.6	66.5	12.4	3.4	79.4	0.6	6.6	0.7	3.6	1.0	1.5	26.9
Cycle Q Clear(g_c), s	12.6	66.5	12.4	3.4	79.4	0.6	6.6	0.7	3.6	1.0	1.5	26.9
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	145	1477	659	263	1763	787	129	438	371	52	352	427
V/C Ratio(X)	0.97	1.01	0.28	0.16	1.66	0.01	0.59	0.03	0.18	0.21	0.06	0.84
Avail Cap(c_a), veh/h	145	1477	659	263	1763	787	134	514	436	134	514	565
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.72	0.72	0.72	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.3	13.5	30.9	59.6	0.6	20.4	71.9	47.2	23.5	75.9	53.3	27.4
Incr Delay (d2), s/veh	55.9	21.9	0.8	0.0	299.2	0.0	6.3	0.0	0.2	2.0	0.1	8.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	8.1	12.0	5.0	1.5	73.5	0.2	3.3	0.3	2.1	0.5	0.7	10.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	129.2	35.4	31.7	59.6	299.8	20.4	78.2	47.2	23.7	77.9	53.4	35.9
LnGrp LOS	F	F	C	E	F	C	E	D	C	E	D	D
Approach Vol, veh/h		1815			2989			152			392	
Approach Delay, s/veh		42.3			295.4			52.6			38.1	
Approach LOS		D			F			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	31.6	74.5	17.7	36.2	18.7	87.4	10.3	43.6				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 67	12.0	* 44	* 13	65.5	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.4	68.5	8.6	28.9	14.6	81.4	3.0	5.6				
Green Ext Time (p_c), s	0.0	0.0	0.0	1.2	0.0	0.0	0.0	0.2				

Intersection Summary

HCM 6th Ctrl Delay	183.7
HCM 6th LOS	F

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
6: Foussat Rd & SR-76

Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	40	1300	70	190	2510	90	160	70	110	200	110	60
Future Volume (vph)	40	1300	70	190	2510	90	160	70	110	200	110	60
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	48.0	48.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (s)	23.0	66.2	66.2	23.0	66.2	66.2	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (%)	14.4%	41.4%	41.4%	14.4%	41.4%	41.4%	11.7%	32.6%	32.6%	11.7%	32.6%	32.6%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.0	87.1	87.1	14.8	92.6	92.6	13.0	19.6	19.6	13.0	19.6	19.6
Actuated g/C Ratio	0.08	0.54	0.54	0.09	0.58	0.58	0.08	0.12	0.12	0.08	0.12	0.12
v/c Ratio	0.15	0.73	0.08	0.65	1.33	0.10	0.63	0.12	0.40	0.78	0.28	0.15
Control Delay	78.2	23.6	6.5	79.9	182.0	4.6	81.7	59.8	11.8	91.2	63.0	1.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	78.2	23.6	6.5	79.9	182.0	4.6	81.7	59.8	11.8	91.2	63.0	1.0
LOS	E	C	A	E	F	A	F	E	B	F	E	A
Approach Delay		24.3			169.3			54.6			68.2	
Approach LOS		C			F			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.33
 Intersection Signal Delay: 112.1
 Intersection LOS: F
 Intersection Capacity Utilization 107.0%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



AM Horizon Year
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	 
Traffic Volume (veh/h)	40	1300	70	190	2510	90	160	70	110	200	110	60
Future Volume (veh/h)	40	1300	70	190	2510	90	160	70	110	200	110	60
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	43	1413	76	207	2728	98	174	76	120	217	120	65
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	239	2104	938	259	2124	948	281	472	146	281	328	258
Arrive On Green	0.07	0.59	0.59	0.07	0.60	0.60	0.08	0.09	0.09	0.08	0.09	0.09
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	43	1413	76	207	2728	98	174	76	120	217	120	65
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.9	43.1	3.3	9.4	95.6	4.2	7.8	2.2	11.9	9.8	5.1	3.5
Cycle Q Clear(g_c), s	1.9	43.1	3.3	9.4	95.6	4.2	7.8	2.2	11.9	9.8	5.1	3.5
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	239	2104	938	259	2124	948	281	472	146	281	328	258
V/C Ratio(X)	0.18	0.67	0.08	0.80	1.28	0.10	0.62	0.16	0.82	0.77	0.37	0.25
Avail Cap(c_a), veh/h	374	2104	938	374	2124	948	281	1468	456	281	1022	802
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.68	0.68	0.68	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	70.2	22.1	14.0	72.8	32.2	13.8	71.1	66.9	71.3	72.1	68.2	67.5
Incr Delay (d2), s/veh	0.2	1.2	0.1	7.6	131.6	0.2	4.1	0.2	10.7	12.5	0.7	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	18.2	1.2	4.5	78.4	1.6	3.6	1.0	5.3	4.9	2.4	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	70.4	23.3	14.1	80.5	163.8	14.0	75.2	67.1	82.0	84.6	68.9	68.0
LnGrp LOS	E	C	B	F	F	B	E	E	F	F	E	E
Approach Vol, veh/h		1532			3033			370			402	
Approach Delay, s/veh		24.2			153.2			75.7			77.2	
Approach LOS		C			F			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	102.7	18.7	20.9	16.8	103.6	18.7	20.9				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 17	58.2	* 13	46.0	* 17	58.2	* 13	46.0				
Max Q Clear Time (g_c+I1), s	11.4	45.1	9.8	7.1	3.9	97.6	11.8	13.9				
Green Ext Time (p_c), s	0.3	8.4	0.2	1.0	0.1	0.0	0.1	0.9				

Intersection Summary

HCM 6th Ctrl Delay	105.1
HCM 6th LOS	F

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	1.3					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔		↔		↔↔↔	
Traffic Vol, veh/h	30	20	80	20	10	280
Future Vol, veh/h	30	20	80	20	10	280
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	33	22	87	22	11	304

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	242	98	0	0	109	0
Stage 1	98	-	-	-	-	-
Stage 2	144	-	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	736	957	-	-	1480	-
Stage 1	889	-	-	-	-	-
Stage 2	829	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	729	957	-	-	1480	-
Mov Cap-2 Maneuver	729	-	-	-	-	-
Stage 1	889	-	-	-	-	-
Stage 2	822	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	9.8	0	0.3
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	806	1480
HCM Lane V/C Ratio	-	-	0.067	0.007
HCM Control Delay (s)	-	-	9.8	7.5
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Horizon Year
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	W		T			T
Traffic Vol, veh/h	0	0	110	0	0	270
Future Vol, veh/h	0	0	110	0	0	270
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	120	0	0	293

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	413	120	0	0	120	0
Stage 1	120	-	-	-	-	-
Stage 2	293	-	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12	-
Critical Hdwy Stg 1	5.42	-	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218	-
Pot Cap-1 Maneuver	595	931	-	-	1468	-
Stage 1	905	-	-	-	-	-
Stage 2	757	-	-	-	-	-
Platoon blocked, %			-	-		-
Mov Cap-1 Maneuver	595	931	-	-	1468	-
Mov Cap-2 Maneuver	595	-	-	-	-	-
Stage 1	905	-	-	-	-	-
Stage 2	757	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	1468	-
HCM Lane V/C Ratio	-	-	-	-
HCM Control Delay (s)	-	-	0	0
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	0	0

LOS Engineering, Inc.

AM Horizon Year
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	8.2			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	38	196	495	647
Demand Flow Rate, veh/h	38	200	504	660
Vehicles Circulating, veh/h	788	410	55	172
Vehicles Exiting, veh/h	44	149	771	438
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	6.5	6.3	6.5	10.1
Approach LOS	A	A	A	B
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	38	200	504	660
Cap Entry Lane, veh/h	618	908	1305	1158
Entry HV Adj Factor	0.997	0.979	0.981	0.980
Flow Entry, veh/h	38	196	495	647
Cap Entry, veh/h	616	889	1280	1135
V/C Ratio	0.062	0.220	0.386	0.570
Control Delay, s/veh	6.5	6.3	6.5	10.1
LOS	A	A	A	B
95th %tile Queue, veh	0	1	2	4

LOS Engineering, Inc.

PM Horizon Year
1: SR-76 & I-5 SB Ramp

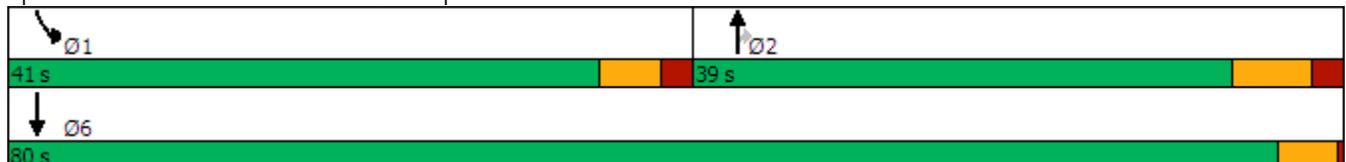
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	620	320	900	910
Future Volume (vph)	0	620	320	900	910
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	38.4	38.4	29.1	0.0
Actuated g/C Ratio	0.00	0.48	0.48	0.36	0.00
v/c Ratio	no cap	0.40	0.44	0.78	no cap
Control Delay		15.2	14.7	27.1	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	15.2	14.7	27.1	Error
LOS	F	B	B	C	F
Approach Delay	Err	15.1			Err
Approach LOS	F	B			F

Intersection Summary

Cycle Length: 80	
Actuated Cycle Length: 80	
Natural Cycle: 50	
Control Type: Actuated-Uncoordinated	
Maximum v/c Ratio: Err	
Intersection Signal Delay: Err	Intersection LOS: F
Intersection Capacity Utilization 46.0%	ICU Level of Service A
Analysis Period (min) 15	

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

PM Horizon Year
1: SR-76 & I-5 SB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	920	620	320	900	910
Future Volume (veh/h)	0	920	620	320	900	910
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			674	348	978	989
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1265	564	1298	0
Arrive On Green			0.36	0.36	0.38	0.85
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			674	348	978	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			7.0	8.4	11.5	0.0
Cycle Q Clear(g_c), s			7.0	8.4	11.5	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1265	564	1298	0
V/C Ratio(X)			0.53	0.62	0.75	0.00
Avail Cap(c_a), veh/h			2456	1095	2618	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			11.9	12.4	12.7	0.0
Incr Delay (d2), s/veh			0.3	1.1	0.9	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			2.3	2.6	3.7	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			12.3	13.5	13.6	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			1022			978
Approach Delay, s/veh			12.7			13.6
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	23.2	23.4				46.6
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	13.5	10.4				0.0
Green Ext Time (p_c), s	4.0	6.1				0.0
Intersection Summary						
HCM 6th Ctrl Delay			13.1			
HCM 6th LOS			B			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Horizon Year
2: SR-76 & I-5 NB Ramp

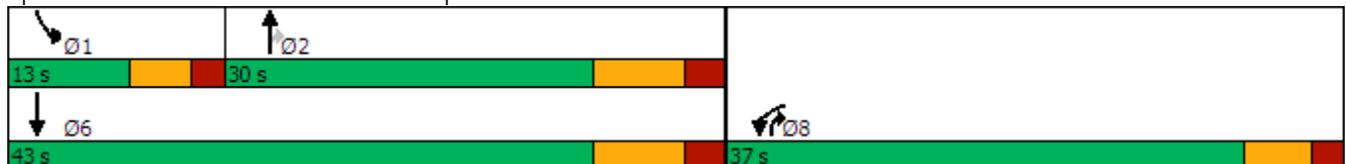
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	310	1410	140	530	1410
Future Volume (vph)	310	1410	140	530	1410
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effect Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.94	1.58	0.13	1.84	0.99
Control Delay	445.7	289.5	2.8	415.9	44.5
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	445.7	289.5	2.8	415.9	44.5
LOS	F	F	A	F	D
Approach Delay	445.7	263.7			145.9
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 150
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.94
 Intersection Signal Delay: 269.8
 Intersection Capacity Utilization 156.2%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



PM Horizon Year
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	310	1110	1410	140	530	1410
Future Volume (veh/h)	310	1110	1410	140	530	1410
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1900	1870	1870	1870	1870
Adj Flow Rate, veh/h	337	1207	1533	152	576	1533
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	0	2	2	2	2
Cap, veh/h	0	0	1818	586	587	2893
Arrive On Green	0.00	0.00	0.51	0.51	0.17	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1533	152	576	1533
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	15.9	2.9	7.1	6.1
Cycle Q Clear(g_c), s	0.0	0.0	15.9	2.9	7.1	6.1
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1818	586	587	2893
V/C Ratio(X)	0.00	0.00	0.84	0.26	0.98	0.53
Avail Cap(c_a), veh/h	0	0	1818	586	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	9.0	9.4	17.8	1.3
Incr Delay (d2), s/veh	0.0	0.0	5.0	1.1	32.4	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	5.2	0.7	5.1	0.3
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	14.0	10.5	50.2	2.0
LnGrp LOS	A	A	B	B	D	A
Approach Vol, veh/h	0		1685			2109
Approach Delay, s/veh	0.0		13.7			15.2
Approach LOS			B			B
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	13.0	30.0			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	9.1	17.9			8.1	0.0
Green Ext Time (p_c), s	0.0	3.3			14.3	0.0

Intersection Summary

HCM 6th Ctrl Delay		14.5
HCM 6th LOS		B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year
3: Loretta St & SR-76

Timings

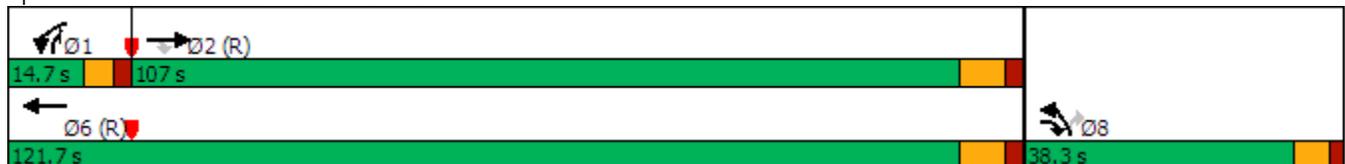


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↑
Traffic Volume (vph)	2510	100	30	1880	30	20
Future Volume (vph)	2510	100	30	1880	30	20
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.0	38.3	14.7	121.7	38.3	14.7
Total Split (%)	66.9%	23.9%	9.2%	76.1%	23.9%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	101.9	143.4	9.0	113.7	32.2	47.3
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.30
v/c Ratio	1.21	0.08	0.33	0.81	0.09	0.05
Control Delay	127.2	0.4	82.0	13.0	53.0	38.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	127.2	0.4	82.0	13.0	53.0	38.0
LOS	F	A	F	B	D	D
Approach Delay	122.4			14.1	47.0	
Approach LOS	F			B	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.21
 Intersection Signal Delay: 76.3
 Intersection LOS: E
 Intersection Capacity Utilization 89.5%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

PM Horizon Year
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵	↵
Traffic Volume (veh/h)	2510	100	30	1880	30	20
Future Volume (veh/h)	2510	100	30	1880	30	20
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2728	109	33	2043	33	22
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2245	1320	77	2525	358	388
Arrive On Green	0.63	0.63	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2728	109	33	2043	33	22
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	101.1	2.0	2.9	62.6	2.4	1.7
Cycle Q Clear(g_c), s	101.1	2.0	2.9	62.6	2.4	1.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2245	1320	77	2525	358	388
V/C Ratio(X)	1.22	0.08	0.43	0.81	0.09	0.06
Avail Cap(c_a), veh/h	2245	1320	100	2525	358	388
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.65	0.65	1.00	1.00
Uniform Delay (d), s/veh	29.5	2.4	74.6	15.8	52.0	46.3
Incr Delay (d2), s/veh	101.2	0.1	2.4	1.9	0.5	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	72.7	1.6	1.4	24.7	1.1	0.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	130.6	2.5	77.0	17.7	52.5	46.6
LnGrp LOS	F	A	E	B	D	D
Approach Vol, veh/h	2837			2076	55	
Approach Delay, s/veh	125.7			18.6	50.1	
Approach LOS	F			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.6	109.1			121.7	38.3
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.0			113.7	32.2
Max Q Clear Time (g_c+I1), s	4.9	103.1			64.6	4.4
Green Ext Time (p_c), s	0.0	0.0			30.2	0.1

Intersection Summary

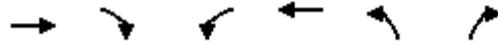
HCM 6th Ctrl Delay	80.1
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year
4: Canyon Dr & SR-76

Timings

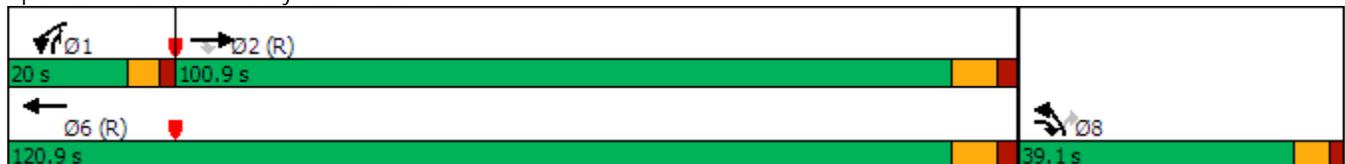


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↓	↑↑	↓	↓
Traffic Volume (vph)	2440	60	180	1890	10	120
Future Volume (vph)	2440	60	180	1890	10	120
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.9	39.1	20.0	120.9	39.1	20.0
Total Split (%)	63.1%	24.4%	12.5%	75.6%	24.4%	12.5%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effct Green (s)	98.8	120.6	27.6	132.1	13.8	47.5
Actuated g/C Ratio	0.62	0.75	0.17	0.83	0.09	0.30
v/c Ratio	1.21	0.05	0.64	0.70	0.04	0.28
Control Delay	137.1	3.2	39.1	29.6	61.8	44.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	137.1	3.2	39.1	29.6	61.8	44.3
LOS	F	A	D	C	E	D
Approach Delay	133.9			30.5	45.7	
Approach LOS	F			C	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.21
 Intersection Signal Delay: 85.9
 Intersection LOS: F
 Intersection Capacity Utilization 101.4%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Horizon Year
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↗	↖	↑↑	↗↖	↗
Traffic Volume (veh/h)	2440	60	180	1890	10	120
Future Volume (veh/h)	2440	60	180	1890	10	120
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2652	65	196	2054	11	130
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2482	1247	159	2927	305	282
Arrive On Green	0.70	0.70	0.09	0.82	0.09	0.09
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	2652	65	196	2054	11	130
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	111.8	1.5	14.3	38.7	0.5	11.8
Cycle Q Clear(g_c), s	111.8	1.5	14.3	38.7	0.5	11.8
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2482	1247	159	2927	305	282
V/C Ratio(X)	1.07	0.05	1.23	0.70	0.04	0.46
Avail Cap(c_a), veh/h	2482	1247	159	2927	713	469
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	24.1	3.8	72.8	5.9	66.7	58.9
Incr Delay (d2), s/veh	31.7	0.0	109.2	0.1	0.0	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	54.3	0.7	11.4	12.1	0.2	4.9
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	55.9	3.8	182.0	6.0	66.7	60.1
LnGrp LOS	F	A	F	A	E	E
Approach Vol, veh/h	2717			2250	141	
Approach Delay, s/veh	54.6			21.4	60.6	
Approach LOS	D			C	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	20.0	119.8			139.8	20.2
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 14	92.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	16.3	113.8			40.7	13.8
Green Ext Time (p_c), s	0.0	0.0			37.5	0.4

Intersection Summary

HCM 6th Ctrl Delay		40.1	
HCM 6th LOS		D	

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	240	2290	120	50	1750	20	170	30	120	20	10	160
Future Volume (veh/h)	240	2290	120	50	1750	20	170	30	120	20	10	160
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	261	2489	130	54	1902	22	185	33	130	22	11	174
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	1934	863	121	1801	804	206	339	287	83	210	345
Arrive On Green	0.11	0.54	0.54	0.07	0.51	0.51	0.12	0.18	0.18	0.05	0.11	0.11
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	261	2489	130	54	1902	22	185	33	130	22	11	174
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	87.1	6.5	4.7	81.1	1.1	16.4	2.4	11.7	1.9	0.8	15.4
Cycle Q Clear(g_c), s	16.9	87.1	6.5	4.7	81.1	1.1	16.4	2.4	11.7	1.9	0.8	15.4
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	1934	863	121	1801	804	206	339	287	83	210	345
V/C Ratio(X)	1.39	1.29	0.15	0.44	1.06	0.03	0.90	0.10	0.45	0.26	0.05	0.50
Avail Cap(c_a), veh/h	188	1934	863	134	1801	804	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.44	0.44	0.44	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	36.5	18.1	71.6	39.4	19.7	69.8	54.6	58.4	73.6	63.4	55.0
Incr Delay (d2), s/veh	177.3	129.4	0.0	1.1	31.7	0.0	29.9	0.1	1.1	1.7	0.1	1.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	17.0	71.6	2.5	2.2	42.7	0.4	9.2	1.1	4.8	0.9	0.4	6.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	248.8	165.8	18.1	72.8	71.1	19.8	99.7	54.7	59.5	75.3	63.5	56.1
LnGrp LOS	F	F	B	E	F	B	F	D	E	E	E	E
Approach Vol, veh/h		2880			1978			348			207	
Approach Delay, s/veh		166.7			70.6			80.4			58.5	
Approach LOS		F			E			F			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.6	95.1	24.2	24.1	22.6	89.1	13.2	35.1				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.7	89.1	18.4	17.4	18.9	83.1	3.9	13.7				
Green Ext Time (p_c), s	0.0	0.0	0.1	0.5	0.0	0.0	0.0	0.6				

Intersection Summary

HCM 6th Ctrl Delay	121.9
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year
6: Foussat Rd & SR-76

Timings

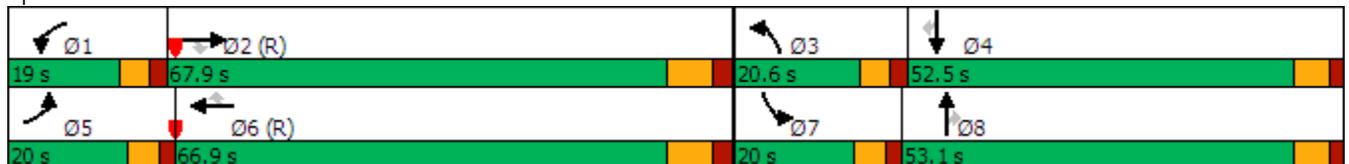
Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	40	2300	170	130	1650	140	100	170	180	70	90	30
Future Volume (vph)	40	2300	170	130	1650	140	100	170	180	70	90	30
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	20.0	67.9	67.9	19.0	66.9	66.9	20.6	53.1	53.1	20.0	52.5	52.5
Total Split (%)	12.5%	42.4%	42.4%	11.9%	41.8%	41.8%	12.9%	33.2%	33.2%	12.5%	32.8%	32.8%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.0	89.0	89.0	12.9	92.6	92.6	13.1	19.6	19.6	13.0	19.5	19.5
Actuated g/C Ratio	0.08	0.56	0.56	0.08	0.58	0.58	0.08	0.12	0.12	0.08	0.12	0.12
v/c Ratio	0.15	1.27	0.20	0.51	0.88	0.16	0.39	0.30	0.54	0.27	0.23	0.08
Control Delay	53.2	155.6	21.3	77.1	35.5	6.0	74.1	63.3	11.8	71.8	62.0	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	53.2	155.6	21.3	77.1	35.5	6.0	74.1	63.3	11.8	71.8	62.0	0.3
LOS	D	F	C	E	D	A	E	E	B	E	E	A
Approach Delay		144.9			36.1			45.1			55.8	
Approach LOS		F			D			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.27
 Intersection Signal Delay: 91.5
 Intersection Capacity Utilization 102.1%
 Analysis Period (min) 15

Intersection LOS: F
 ICU Level of Service G

Splits and Phases: 6: Foussat Rd & SR-76



PM Horizon Year
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 	 	 
Traffic Volume (veh/h)	40	2300	170	130	1650	140	100	170	180	70	90	30
Future Volume (veh/h)	40	2300	170	130	1650	140	100	170	180	70	90	30
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	43	2500	185	141	1793	152	109	185	196	76	98	33
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	239	1927	860	259	1947	869	279	740	230	271	508	398
Arrive On Green	0.07	0.54	0.54	0.07	0.55	0.55	0.08	0.14	0.14	0.08	0.14	0.14
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	43	2500	185	141	1793	152	109	185	196	76	98	33
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.9	86.8	9.7	6.3	73.7	7.7	4.8	5.1	19.3	3.3	3.9	1.6
Cycle Q Clear(g_c), s	1.9	86.8	9.7	6.3	73.7	7.7	4.8	5.1	19.3	3.3	3.9	1.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	239	1927	860	259	1947	869	279	740	230	271	508	398
V/C Ratio(X)	0.18	1.30	0.22	0.55	0.92	0.18	0.39	0.25	0.85	0.28	0.19	0.08
Avail Cap(c_a), veh/h	309	1927	860	287	1947	869	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	70.2	36.6	19.0	71.4	33.0	18.1	69.8	60.7	66.7	69.5	60.4	59.5
Incr Delay (d2), s/veh	0.0	134.1	0.1	1.8	8.6	0.4	0.9	0.2	8.7	0.6	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	72.7	3.7	2.9	33.5	3.0	2.2	2.3	8.4	1.5	1.8	0.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	70.2	170.7	19.0	73.2	41.6	18.5	70.7	60.9	75.4	70.0	60.6	59.6
LnGrp LOS	E	F	B	E	D	B	E	E	E	E	E	E
Approach Vol, veh/h		2728			2086			490			207	
Approach Delay, s/veh		158.8			42.1			68.9			63.9	
Approach LOS		F			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	94.8	18.6	29.0	16.8	95.7	18.3	29.3				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	8.3	88.8	6.8	5.9	3.9	75.7	5.3	21.3				
Green Ext Time (p_c), s	0.2	0.0	0.2	0.8	0.0	0.0	0.1	1.9				

Intersection Summary

HCM 6th Ctrl Delay	103.1
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year
7: Benet Rd & Airport Rd

Intersection						
Int Delay, s/veh	1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	Y		T			↑↑↑
Traffic Vol, veh/h	20	10	200	10	10	120
Future Vol, veh/h	20	10	200	10	10	120
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	11	217	11	11	130

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	297	223	0	0	228
Stage 1	223	-	-	-	-
Stage 2	74	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	689	816	-	-	1339
Stage 1	784	-	-	-	-
Stage 2	900	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	683	816	-	-	1339
Mov Cap-2 Maneuver	683	-	-	-	-
Stage 1	784	-	-	-	-
Stage 2	892	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.2	0	0.6
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	722	1339
HCM Lane V/C Ratio	-	-	0.045	0.008
HCM Control Delay (s)	-	-	10.2	7.7
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.1	0

LOS Engineering, Inc.

PM Horizon Year
 8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	TT		TT			TT
Traffic Vol, veh/h	0	0	220	0	0	130
Future Vol, veh/h	0	0	220	0	0	130
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	0	0	239	0	0	141

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	380	239	0	0	239
Stage 1	239	-	-	-	-
Stage 2	141	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	622	800	-	-	1328
Stage 1	801	-	-	-	-
Stage 2	886	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	622	800	-	-	1328
Mov Cap-2 Maneuver	622	-	-	-	-
Stage 1	801	-	-	-	-
Stage 2	886	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	0	0	0
HCM LOS	A		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	-	1328
HCM Lane V/C Ratio	-	-	-	-
HCM Control Delay (s)	-	-	0	0
HCM Lane LOS	-	-	A	A
HCM 95th %tile Q(veh)	-	-	-	0

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PM Horizon Year
9: Foussat Rd & Alex Rd

HCM 6th Roundabout

Intersection				
Intersection Delay, s/veh	9.6			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	70	245	777	402
Demand Flow Rate, veh/h	71	249	793	410
Vehicles Circulating, veh/h	582	565	111	232
Vehicles Exiting, veh/h	60	339	542	582
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	5.8	8.5	11.4	7.3
Approach LOS	A	A	B	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	71	249	793	410
Cap Entry Lane, veh/h	762	775	1232	1089
Entry HV Adj Factor	0.981	0.983	0.980	0.980
Flow Entry, veh/h	70	245	777	402
Cap Entry, veh/h	748	762	1207	1067
V/C Ratio	0.093	0.321	0.644	0.376
Control Delay, s/veh	5.8	8.5	11.4	7.3
LOS	A	A	B	A
95th %tile Queue, veh	0	1	5	2

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Appendix N

Horizon Year 2030 + Project Intersection LOS Worksheets

AM Horizon Year + Project
1: SR-76 & I-5 SB Ramp

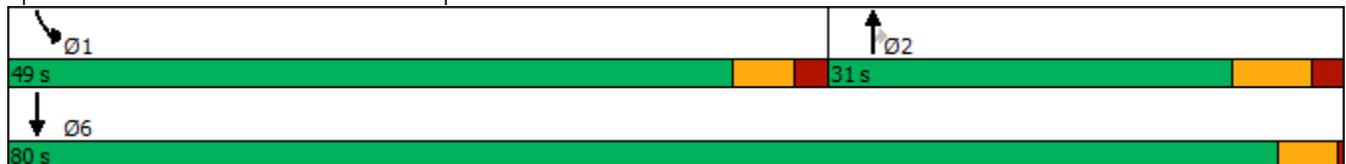
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	370	290	1432	590
Future Volume (vph)	0	370	290	1432	590
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		31.0	31.0	49.0	80.0
Total Split (%)		38.8%	38.8%	61.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	25.7	25.7	41.8	0.0
Actuated g/C Ratio	0.00	0.32	0.32	0.52	0.00
v/c Ratio	no cap	0.35	0.60	0.87	no cap
Control Delay		22.4	27.5	23.0	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	22.4	27.5	23.0	Error
LOS	F	C	C	C	F
Approach Delay	Err	24.6			Err
Approach LOS	F	C			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 60
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: Err
 Intersection Signal Delay: Err
 Intersection Capacity Utilization 44.1%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service A

Splits and Phases: 1: SR-76 & I-5 SB Ramp



AM Horizon Year + Project
1: SR-76 & I-5 SB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↖↖	
Traffic Volume (veh/h)	0	440	370	290	1432	590
Future Volume (veh/h)	0	440	370	290	1432	590
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			402	315	1557	641
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			936	418	1837	0
Arrive On Green			0.26	0.26	0.53	0.89
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			402	315	1557	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			5.7	11.1	23.4	0.0
Cycle Q Clear(g_c), s			5.7	11.1	23.4	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			936	418	1837	0
V/C Ratio(X)			0.43	0.75	0.85	0.00
Avail Cap(c_a), veh/h			1411	629	2455	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			18.6	20.6	12.2	0.0
Incr Delay (d2), s/veh			0.3	2.8	2.3	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			2.2	4.1	7.7	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			19.0	23.4	14.4	0.0
LnGrp LOS			B	C	B	A
Approach Vol, veh/h			717			1557
Approach Delay, s/veh			20.9			14.4
Approach LOS			C			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	38.1	22.9				61.0
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 43	24.2				* 76
Max Q Clear Time (g_c+I1), s	25.4	13.1				0.0
Green Ext Time (p_c), s	7.0	2.9				0.0
Intersection Summary						
HCM 6th Ctrl Delay			16.5			
HCM 6th LOS			B			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

AM Horizon Year + Project
2: SR-76 & I-5 NB Ramp

Timings

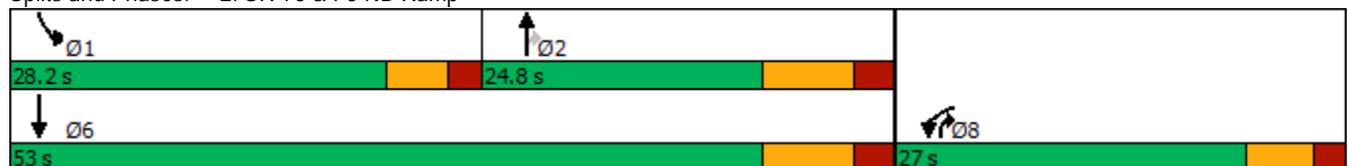
	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	110	756	120	1192	1862
Future Volume (vph)	110	756	120	1192	1862
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	27.0	24.8	27.0	28.2	53.0
Total Split (%)	33.8%	31.0%	33.8%	35.3%	66.3%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	20.9	16.8	45.7	22.5	45.0
Actuated g/C Ratio	0.26	0.21	0.57	0.28	0.56
v/c Ratio	1.33	1.11	0.14	1.34	1.02
Control Delay	175.6	98.0	8.4	188.7	43.7
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	175.6	98.0	8.4	188.7	43.7
LOS	F	F	A	F	D
Approach Delay	175.6	85.8			100.3
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 100
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.34
 Intersection Signal Delay: 112.1
 Intersection Capacity Utilization 128.1%
 Analysis Period (min) 15

Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



LOS Engineering, Inc.

AM Horizon Year + Project
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	110	820	756	120	1192	1862
Future Volume (veh/h)	110	820	756	120	1192	1862
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	120	891	822	130	1296	2024
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1174	341	1421	3017
Arrive On Green	0.00	0.00	0.33	0.33	0.41	0.85
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	822	130	1296	2024
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	10.7	3.7	18.7	10.6
Cycle Q Clear(g_c), s	0.0	0.0	10.7	3.7	18.7	10.6
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1174	341	1421	3017
V/C Ratio(X)	0.00	0.00	0.70	0.38	0.91	0.67
Avail Cap(c_a), veh/h	0	0	1174	341	1467	3017
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	15.5	17.8	14.7	1.4
Incr Delay (d2), s/veh	0.0	0.0	3.5	3.2	8.8	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	4.3	1.3	7.7	0.5
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	19.0	21.0	23.5	2.6
LnGrp LOS	A	A	B	C	C	A
Approach Vol, veh/h	0		952			3320
Approach Delay, s/veh	0.0		19.2			10.8
Approach LOS			B			B
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	27.5	25.5			53.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 23	16.8			45.0	20.9
Max Q Clear Time (g_c+I1), s	20.7	12.7			12.6	0.0
Green Ext Time (p_c), s	1.1	2.2			22.7	0.0

Intersection Summary

HCM 6th Ctrl Delay		12.6
HCM 6th LOS		B

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year + Project
3: Loretta St & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↑	↑↑	↑	↑
Traffic Volume (veh/h)	1552	30	20	3014	60	30
Future Volume (veh/h)	1552	30	20	3014	60	30
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1771	33	22	3276	65	33
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2278	1333	63	2530	356	373
Arrive On Green	0.64	0.64	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	1771	33	22	3276	65	33
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	57.0	0.5	1.9	113.9	4.8	2.6
Cycle Q Clear(g_c), s	57.0	0.5	1.9	113.9	4.8	2.6
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2278	1333	63	2530	356	373
V/C Ratio(X)	0.78	0.02	0.35	1.29	0.18	0.09
Avail Cap(c_a), veh/h	2278	1333	100	2530	356	373
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	20.5	2.1	75.4	23.0	53.1	47.8
Incr Delay (d2), s/veh	2.7	0.0	0.3	133.0	1.1	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	22.0	0.4	0.9	90.3	2.3	1.1
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	23.2	2.1	75.7	156.1	54.3	48.3
LnGrp LOS	C	A	E	F	D	D
Approach Vol, veh/h	1804			3298	98	
Approach Delay, s/veh	22.8			155.5	52.2	
Approach LOS	C			F	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	11.3	110.6			121.9	38.1
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.0			113.9	32.0
Max Q Clear Time (g_c+I1), s	3.9	59.0			115.9	6.8
Green Ext Time (p_c), s	0.0	18.0			0.0	0.2

Intersection Summary

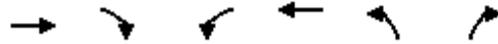
HCM 6th Ctrl Delay	107.6
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year + Project
4: Canyon Dr & SR-76

Timings

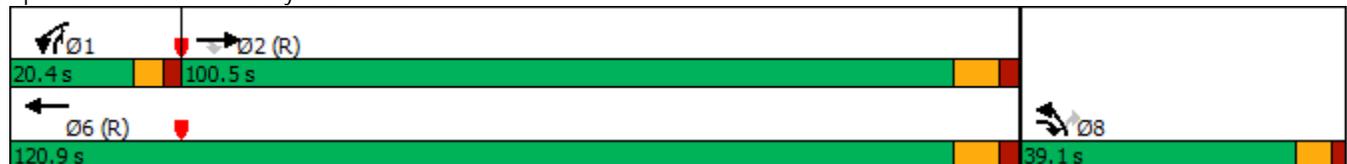


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↘↘	↙
Traffic Volume (vph)	1592	20	100	3034	30	100
Future Volume (vph)	1592	20	100	3034	30	100
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.5	39.1	20.4	120.9	39.1	20.4
Total Split (%)	62.8%	24.4%	12.8%	75.6%	24.4%	12.8%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effct Green (s)	112.3	134.1	14.1	132.1	13.8	34.0
Actuated g/C Ratio	0.70	0.84	0.09	0.83	0.09	0.21
v/c Ratio	0.70	0.02	0.70	1.13	0.11	0.31
Control Delay	32.0	2.7	54.7	94.8	64.4	44.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	32.0	2.7	54.7	94.8	64.4	44.1
LOS	C	A	D	F	E	D
Approach Delay	31.6			93.5	48.8	
Approach LOS	C			F	D	

Intersection Summary

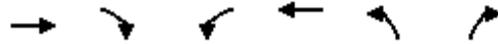
Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20.4 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.13
 Intersection Signal Delay: 71.9
 Intersection LOS: E
 Intersection Capacity Utilization 103.1%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

AM Horizon Year + Project
4: Canyon Dr & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵↵	↵
Traffic Volume (veh/h)	1592	20	100	3034	30	100
Future Volume (veh/h)	1592	20	100	3034	30	100
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	1730	22	109	3298	33	109
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2579	1274	130	2964	269	239
Arrive On Green	0.73	0.73	0.07	0.83	0.08	0.08
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	1730	22	109	3298	33	109
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	41.6	0.4	9.7	133.5	1.4	10.0
Cycle Q Clear(g_c), s	41.6	0.4	9.7	133.5	1.4	10.0
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2579	1274	130	2964	269	239
V/C Ratio(X)	0.67	0.02	0.84	1.11	0.12	0.46
Avail Cap(c_a), veh/h	2579	1274	164	2964	713	442
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.55	0.55	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	11.7	3.1	73.3	13.3	68.7	62.0
Incr Delay (d2), s/veh	0.8	0.0	3.0	51.2	0.2	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	15.9	0.2	4.6	61.3	0.6	4.2
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	12.5	3.1	76.3	64.5	68.9	63.4
LnGrp LOS	B	A	E	F	E	E
Approach Vol, veh/h	1752			3407	142	
Approach Delay, s/veh	12.4			64.8	64.7	
Approach LOS	B			E	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	17.3	124.1			141.5	18.5
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 15	92.5			112.9	33.0
Max Q Clear Time (g_c+I1), s	11.7	43.6			135.5	12.0
Green Ext Time (p_c), s	0.1	23.1			0.0	0.4

Intersection Summary

HCM 6th Ctrl Delay	47.5
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	202	1370	170	40	2700	12	70	16	60	11	22	354
Future Volume (veh/h)	202	1370	170	40	2700	12	70	16	60	11	22	354
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	220	1489	185	43	2935	13	76	17	65	12	24	385
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	145	1477	659	239	1716	766	129	459	389	55	377	448
Arrive On Green	0.08	0.83	0.42	0.13	0.97	0.48	0.07	0.25	0.25	0.03	0.20	0.34
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	220	1489	185	43	2935	13	76	17	65	12	24	385
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	13.0	66.5	12.4	3.4	77.3	0.7	6.6	1.1	3.6	1.1	1.7	29.0
Cycle Q Clear(g_c), s	13.0	66.5	12.4	3.4	77.3	0.7	6.6	1.1	3.6	1.1	1.7	29.0
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	145	1477	659	239	1716	766	129	459	389	55	377	448
V/C Ratio(X)	1.52	1.01	0.28	0.18	1.71	0.02	0.59	0.04	0.17	0.22	0.06	0.86
Avail Cap(c_a), veh/h	145	1477	659	239	1716	766	134	514	436	134	514	565
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	0.68	0.68	0.68	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.5	13.5	30.9	61.4	2.7	21.6	71.9	46.0	23.6	75.6	51.7	26.3
Incr Delay (d2), s/veh	256.6	21.3	0.7	0.0	319.7	0.0	6.3	0.0	0.2	1.9	0.1	10.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	16.3	11.9	5.0	1.6	77.4	0.3	3.3	0.5	2.1	0.5	0.8	10.9
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	330.1	34.8	31.7	61.5	322.4	21.6	78.2	46.0	23.8	77.6	51.8	36.9
LnGrp LOS	F	F	C	E	F	C	E	D	C	E	D	D
Approach Vol, veh/h		1894			2991			158			421	
Approach Delay, s/veh		68.8			317.4			52.4			38.9	
Approach LOS		E			F			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	29.5	74.5	17.7	38.3	18.7	85.3	10.7	45.4				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.0	* 67	12.0	* 44	* 13	65.5	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.4	68.5	8.6	31.0	15.0	79.3	3.1	5.6				
Green Ext Time (p_c), s	0.0	0.0	0.0	1.3	0.0	0.0	0.0	0.3				

Intersection Summary

HCM 6th Ctrl Delay	202.1
HCM 6th LOS	F

Notes

User approved pedestrian interval to be less than phase max green.
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year + Project
6: Foussat Rd & SR-76

Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	40	1301	70	190	2512	123	160	76	110	212	112	60
Future Volume (vph)	40	1301	70	190	2512	123	160	76	110	212	112	60
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	48.0	48.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (s)	18.7	71.5	71.5	17.7	70.5	70.5	18.7	52.1	52.1	18.7	52.1	52.1
Total Split (%)	11.7%	44.7%	44.7%	11.1%	44.1%	44.1%	11.7%	32.6%	32.6%	11.7%	32.6%	32.6%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effect Green (s)	13.0	87.9	87.9	14.0	92.6	92.6	13.0	19.6	19.6	13.0	19.6	19.6
Actuated g/C Ratio	0.08	0.55	0.55	0.09	0.58	0.58	0.08	0.12	0.12	0.08	0.12	0.12
v/c Ratio	0.15	0.73	0.08	0.69	1.33	0.14	0.63	0.13	0.40	0.83	0.28	0.15
Control Delay	82.1	19.3	4.9	82.7	182.4	7.4	81.7	60.1	11.8	95.7	63.1	1.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	82.1	19.3	4.9	82.7	182.4	7.4	81.7	60.1	11.8	95.7	63.1	1.0
LOS	F	B	A	F	F	A	F	E	B	F	E	A
Approach Delay		20.3			168.0			54.7			71.4	
Approach LOS		C			F			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.33
 Intersection Signal Delay: 110.7
 Intersection LOS: F
 Intersection Capacity Utilization 107.1%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



LOS Engineering, Inc.

AM Horizon Year + Project
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 		
Traffic Volume (veh/h)	40	1301	70	190	2512	123	160	76	110	212	112	60
Future Volume (veh/h)	40	1301	70	190	2512	123	160	76	110	212	112	60
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	43	1414	76	207	2730	134	174	83	120	230	122	65
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	239	2103	938	259	2123	947	281	473	147	281	329	259
Arrive On Green	0.07	0.59	0.59	0.07	0.60	0.60	0.08	0.09	0.09	0.08	0.09	0.09
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	43	1414	76	207	2730	134	174	83	120	230	122	65
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.9	43.2	3.3	9.4	95.6	5.9	7.8	2.4	11.9	10.5	5.2	3.5
Cycle Q Clear(g_c), s	1.9	43.2	3.3	9.4	95.6	5.9	7.8	2.4	11.9	10.5	5.2	3.5
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	239	2103	938	259	2123	947	281	473	147	281	329	259
V/C Ratio(X)	0.18	0.67	0.08	0.80	1.29	0.14	0.62	0.18	0.82	0.82	0.37	0.25
Avail Cap(c_a), veh/h	281	2103	938	259	2123	947	281	1468	456	281	1022	802
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.68	0.68	0.68	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	70.2	22.1	14.0	72.8	32.2	14.2	71.1	67.0	71.3	72.3	68.2	67.4
Incr Delay (d2), s/veh	0.2	1.2	0.1	16.0	132.3	0.3	4.1	0.2	10.5	17.2	0.7	0.5
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	18.3	1.2	4.8	78.6	2.3	3.6	1.1	5.3	5.3	2.4	1.3
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	70.4	23.3	14.1	88.8	164.5	14.5	75.2	67.1	81.8	89.5	68.9	67.9
LnGrp LOS	E	C	B	F	F	B	E	E	F	F	E	E
Approach Vol, veh/h		1533			3071			377				417
Approach Delay, s/veh		24.2			152.8			75.5				80.1
Approach LOS		C			F			E				F
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	102.7	18.7	20.9	16.8	103.6	18.7	20.9				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	63.5	* 13	46.0	* 13	62.5	* 13	46.0				
Max Q Clear Time (g_c+I1), s	11.4	45.2	9.8	7.2	3.9	97.6	12.5	13.9				
Green Ext Time (p_c), s	0.0	10.6	0.2	1.1	0.0	0.0	0.0	0.9				
Intersection Summary												
HCM 6th Ctrl Delay			105.3									
HCM 6th LOS			F									
Notes												
User approved pedestrian interval to be less than phase max green.												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

AM Horizon Year + Project
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	1.1					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔↔		↔		↔↔↔	
Traffic Vol, veh/h	30	20	160	20	10	308
Future Vol, veh/h	30	20	160	20	10	308
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	33	22	174	22	11	335

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	341	185	0	0	196
Stage 1	185	-	-	-	-
Stage 2	156	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13
Critical Hdwy Stg 1	5.43	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219
Pot Cap-1 Maneuver	653	857	-	-	1376
Stage 1	814	-	-	-	-
Stage 2	817	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	646	857	-	-	1376
Mov Cap-2 Maneuver	646	-	-	-	-
Stage 1	814	-	-	-	-
Stage 2	809	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.4	0	0.2
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	717	1376
HCM Lane V/C Ratio	-	-	0.076	0.008
HCM Control Delay (s)	-	-	10.4	7.6
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

AM Horizon Year + Project
8: Benet Rd & Eddy Jones Way

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.7					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↘↗		↑	↗		↘
Traffic Vol, veh/h	28	0	110	80	0	270
Future Vol, veh/h	28	0	110	80	0	270
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	50	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	30	0	120	87	0	293

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	413	120	0	0	207
Stage 1	120	-	-	-	-
Stage 2	293	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	595	931	-	-	1364
Stage 1	905	-	-	-	-
Stage 2	757	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	595	931	-	-	1364
Mov Cap-2 Maneuver	595	-	-	-	-
Stage 1	905	-	-	-	-
Stage 2	757	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.4	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	595	1364
HCM Lane V/C Ratio	-	-	0.051	-
HCM Control Delay (s)	-	-	11.4	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

Intersection				
Intersection Delay, s/veh	8.7			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	68	196	537	647
Demand Flow Rate, veh/h	69	200	547	660
Vehicles Circulating, veh/h	788	453	55	215
Vehicles Exiting, veh/h	87	149	802	438
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	7.2	6.6	6.9	11.0
Approach LOS	A	A	A	B
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	69	200	547	660
Cap Entry Lane, veh/h	618	869	1305	1108
Entry HV Adj Factor	0.984	0.979	0.981	0.980
Flow Entry, veh/h	68	196	537	647
Cap Entry, veh/h	608	851	1280	1086
V/C Ratio	0.112	0.230	0.419	0.596
Control Delay, s/veh	7.2	6.6	6.9	11.0
LOS	A	A	A	B
95th %tile Queue, veh	0	1	2	4

PM Horizon Year + Project
1: SR-76 & I-5 SB Ramp

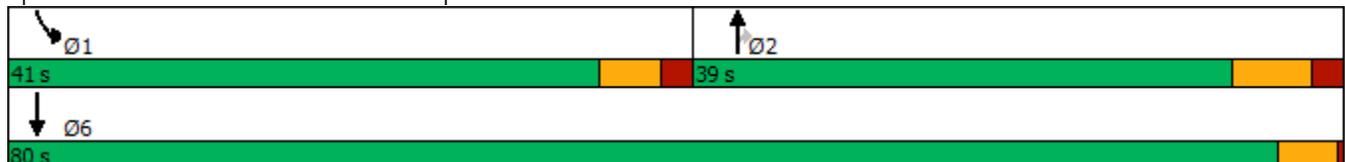
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations		↑↑	↗	↘↘	
Traffic Volume (vph)	0	620	320	936	910
Future Volume (vph)	0	620	320	936	910
Turn Type		NA	Perm	Prot	NA
Protected Phases		2		1	6
Permitted Phases			2		
Detector Phase		2	2	1	6
Switch Phase					
Minimum Initial (s)		5.0	5.0	5.0	4.0
Minimum Split (s)		22.8	22.8	10.7	20.0
Total Split (s)		39.0	39.0	41.0	80.0
Total Split (%)		48.8%	48.8%	51.3%	100.0%
Yellow Time (s)		4.8	4.8	3.7	3.5
All-Red Time (s)		2.0	2.0	2.0	0.5
Lost Time Adjust (s)		0.0	0.0	0.0	
Total Lost Time (s)		6.8	6.8	5.7	
Lead/Lag		Lag	Lag	Lead	
Lead-Lag Optimize?		Yes	Yes	Yes	
Recall Mode		Min	Min	None	Min
Act Effect Green (s)	0.0	37.4	37.4	30.1	0.0
Actuated g/C Ratio	0.00	0.47	0.47	0.38	0.00
v/c Ratio	no cap	0.41	0.45	0.79	no cap
Control Delay		15.8	15.7	26.7	
Queue Delay		0.0	0.0	0.0	
Total Delay	Error	15.8	15.7	26.7	Error
LOS	F	B	B	C	F
Approach Delay	Err	15.7			Err
Approach LOS	F	B			F

Intersection Summary

Cycle Length: 80	
Actuated Cycle Length: 80	
Natural Cycle: 55	
Control Type: Actuated-Uncoordinated	
Maximum v/c Ratio: Err	
Intersection Signal Delay: Err	Intersection LOS: F
Intersection Capacity Utilization 46.0%	ICU Level of Service A
Analysis Period (min) 15	

Splits and Phases: 1: SR-76 & I-5 SB Ramp



LOS Engineering, Inc.

PM Horizon Year + Project
1: SR-76 & I-5 SB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			↑↑	↗	↗↗	
Traffic Volume (veh/h)	0	920	620	320	936	910
Future Volume (veh/h)	0	920	620	320	936	910
Initial Q (Qb), veh			0	0	0	0
Ped-Bike Adj(A_pbT)				1.00	1.00	
Parking Bus, Adj			1.00	1.00	1.00	1.00
Work Zone On Approach			No			No
Adj Sat Flow, veh/h/ln			1870	1870	1870	1870
Adj Flow Rate, veh/h			674	348	1017	989
Peak Hour Factor			0.92	0.92	0.92	0.92
Percent Heavy Veh, %			2	2	2	2
Cap, veh/h			1252	558	1333	0
Arrive On Green			0.35	0.35	0.39	0.86
Sat Flow, veh/h			3647	1585	3456	0
Grp Volume(v), veh/h			674	348	1017	0
Grp Sat Flow(s),veh/h/ln			1777	1585	1728	0
Q Serve(g_s), s			7.2	8.7	12.2	0.0
Cycle Q Clear(g_c), s			7.2	8.7	12.2	0.0
Prop In Lane				1.00	1.00	
Lane Grp Cap(c), veh/h			1252	558	1333	0
V/C Ratio(X)			0.54	0.62	0.76	0.00
Avail Cap(c_a), veh/h			2396	1069	2555	0
HCM Platoon Ratio			1.00	1.00	1.00	1.00
Upstream Filter(I)			1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh			12.4	12.8	12.8	0.0
Incr Delay (d2), s/veh			0.4	1.1	0.9	0.0
Initial Q Delay(d3),s/veh			0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln			2.4	2.7	4.0	0.0
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh			12.7	14.0	13.7	0.0
LnGrp LOS			B	B	B	A
Approach Vol, veh/h			1022			1017
Approach Delay, s/veh			13.1			13.7
Approach LOS			B			B
Timer - Assigned Phs	1	2				6
Phs Duration (G+Y+Rc), s	24.1	23.6				47.7
Change Period (Y+Rc), s	* 5.7	6.8				* 6.8
Max Green Setting (Gmax), s	* 35	32.2				* 76
Max Q Clear Time (g_c+I1), s	14.2	10.7				0.0
Green Ext Time (p_c), s	4.2	6.1				0.0
Intersection Summary						
HCM 6th Ctrl Delay			13.4			
HCM 6th LOS			B			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Horizon Year + Project
2: SR-76 & I-5 NB Ramp

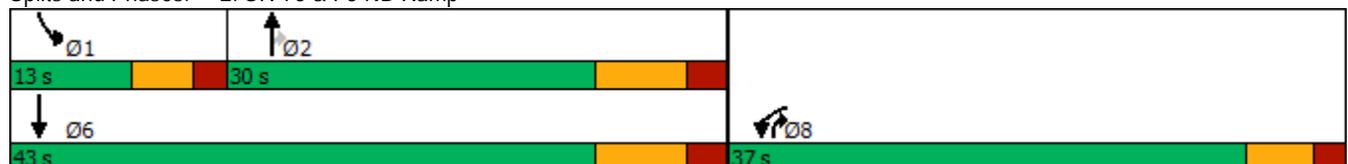
Timings

	↙	↑	↘	↙	↓
Lane Group	WBL	NBT	NBR	SBL	SBT
Lane Configurations	↙	↑↑	↘	↙↘	↑↑
Traffic Volume (vph)	310	1426	140	566	1446
Future Volume (vph)	310	1426	140	566	1446
Turn Type	Prot	NA	pm+ov	Prot	NA
Protected Phases	8	2	8	1	6
Permitted Phases			2		
Detector Phase	8	2	8	1	6
Switch Phase					
Minimum Initial (s)	5.0	5.0	5.0	5.0	5.0
Minimum Split (s)	22.1	24.0	22.1	10.7	24.0
Total Split (s)	37.0	30.0	37.0	13.0	43.0
Total Split (%)	46.3%	37.5%	46.3%	16.3%	53.8%
Yellow Time (s)	4.1	5.5	4.1	3.7	5.5
All-Red Time (s)	2.0	2.5	2.0	2.0	2.5
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	6.1	8.0	6.1	5.7	8.0
Lead/Lag		Lag		Lead	
Lead-Lag Optimize?		Yes		Yes	
Recall Mode	None	Max	None	None	Max
Act Effct Green (s)	30.9	22.0	60.9	7.3	35.0
Actuated g/C Ratio	0.39	0.28	0.76	0.09	0.44
v/c Ratio	1.94	1.59	0.13	1.96	1.02
Control Delay	445.7	297.2	2.8	470.0	50.8
Queue Delay	0.0	0.0	0.0	0.0	0.0
Total Delay	445.7	297.2	2.8	470.0	50.8
LOS	F	F	A	F	D
Approach Delay	445.7	270.9			168.7
Approach LOS	F	F			F

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Natural Cycle: 140
 Control Type: Actuated-Uncoordinated
 Maximum v/c Ratio: 1.96
 Intersection Signal Delay: 279.4
 Intersection Capacity Utilization 157.7%
 Analysis Period (min) 15
 Intersection LOS: F
 ICU Level of Service H

Splits and Phases: 2: SR-76 & I-5 NB Ramp



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PM Horizon Year + Project
2: SR-76 & I-5 NB Ramp

						
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations			 		 	 
Traffic Volume (veh/h)	310	1110	1426	140	566	1446
Future Volume (veh/h)	310	1110	1426	140	566	1446
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00	1.00		1.00	1.00	
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No		No			No
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	337	1207	1550	152	615	1572
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	0	0	1818	586	587	2893
Arrive On Green	0.00	0.00	0.51	0.51	0.17	0.81
Sat Flow, veh/h	0	0	3647	1585	3456	3647
Grp Volume(v), veh/h	0	0	1550	152	615	1572
Grp Sat Flow(s),veh/h/ln	0	0	1777	1585	1728	1777
Q Serve(g_s), s	0.0	0.0	16.2	2.9	7.3	6.3
Cycle Q Clear(g_c), s	0.0	0.0	16.2	2.9	7.3	6.3
Prop In Lane	0.00	0.00		1.00	1.00	
Lane Grp Cap(c), veh/h	0	0	1818	586	587	2893
V/C Ratio(X)	0.00	0.00	0.85	0.26	1.05	0.54
Avail Cap(c_a), veh/h	0	0	1818	586	587	2893
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.00	0.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	0.0	0.0	9.1	9.4	17.9	1.3
Incr Delay (d2), s/veh	0.0	0.0	5.3	1.1	50.4	0.7
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.0	0.0	5.4	0.7	6.6	0.3
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	0.0	0.0	14.4	10.5	68.3	2.1
LnGrp LOS	A	A	B	B	F	A
Approach Vol, veh/h	0		1702			2187
Approach Delay, s/veh	0.0		14.0			20.7
Approach LOS			B			C
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	13.0	30.0			43.0	0.0
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 7.3	22.0			35.0	30.9
Max Q Clear Time (g_c+I1), s	9.3	18.2			8.3	0.0
Green Ext Time (p_c), s	0.0	3.1			14.7	0.0
Intersection Summary						
HCM 6th Ctrl Delay			17.8			
HCM 6th LOS			B			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Horizon Year + Project
3: Loretta St & SR-76

Timings

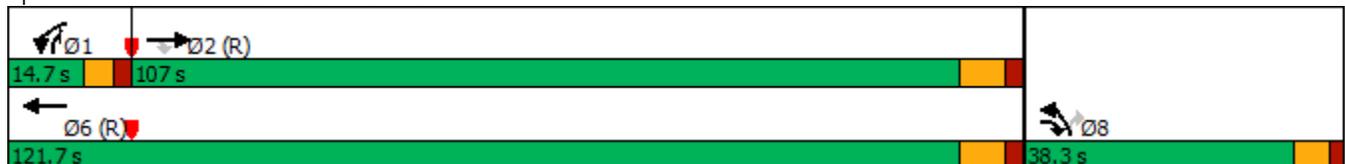


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙	↙
Traffic Volume (vph)	2543	100	30	1952	30	20
Future Volume (vph)	2543	100	30	1952	30	20
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	10.0	9.0	25.0	10.0	9.0
Minimum Split (s)	33.0	38.1	14.7	33.0	38.1	14.7
Total Split (s)	107.0	38.3	14.7	121.7	38.3	14.7
Total Split (%)	66.9%	23.9%	9.2%	76.1%	23.9%	9.2%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Max	None	C-Max	Max	None
Act Effct Green (s)	101.9	143.4	9.0	113.7	32.2	47.3
Actuated g/C Ratio	0.64	0.90	0.06	0.71	0.20	0.30
v/c Ratio	1.23	0.17	0.33	0.84	0.09	0.05
Control Delay	134.1	0.5	77.7	15.9	53.0	38.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	134.1	0.5	77.7	15.9	53.0	38.0
LOS	F	A	E	B	D	D
Approach Delay	123.5			16.8	47.0	
Approach LOS	F			B	D	

Intersection Summary

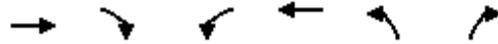
Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 14.7 (9%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.23
 Intersection Signal Delay: 78.6
 Intersection LOS: E
 Intersection Capacity Utilization 90.4%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 3: Loretta St & SR-76



LOS Engineering, Inc.

PM Horizon Year + Project
3: Loretta St & SR-76



Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵	↵
Traffic Volume (veh/h)	2543	100	30	1952	30	20
Future Volume (veh/h)	2543	100	30	1952	30	20
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2764	239	33	2122	33	22
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2245	1320	77	2525	358	388
Arrive On Green	0.63	0.63	0.04	0.71	0.20	0.20
Sat Flow, veh/h	3647	1585	1781	3647	1781	1585
Grp Volume(v), veh/h	2764	239	33	2122	33	22
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1781	1585
Q Serve(g_s), s	101.1	4.7	2.9	68.6	2.4	1.7
Cycle Q Clear(g_c), s	101.1	4.7	2.9	68.6	2.4	1.7
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2245	1320	77	2525	358	388
V/C Ratio(X)	1.23	0.18	0.43	0.84	0.09	0.06
Avail Cap(c_a), veh/h	2245	1320	100	2525	358	388
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	0.61	0.61	1.00	1.00
Uniform Delay (d), s/veh	29.5	2.6	74.6	16.6	52.0	46.3
Incr Delay (d2), s/veh	108.1	0.3	2.3	2.2	0.5	0.3
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	74.8	3.9	1.4	27.1	1.1	0.7
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	137.6	2.9	76.9	18.8	52.5	46.6
LnGrp LOS	F	A	E	B	D	D
Approach Vol, veh/h	3003			2155	55	
Approach Delay, s/veh	126.9			19.7	50.1	
Approach LOS	F			B	D	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	12.6	109.1			121.7	38.3
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 9	99.0			113.7	32.2
Max Q Clear Time (g_c+I1), s	4.9	103.1			70.6	4.4
Green Ext Time (p_c), s	0.0	0.0			29.3	0.1

Intersection Summary

HCM 6th Ctrl Delay	81.8
HCM 6th LOS	F

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year + Project
4: Canyon Dr & SR-76

Timings

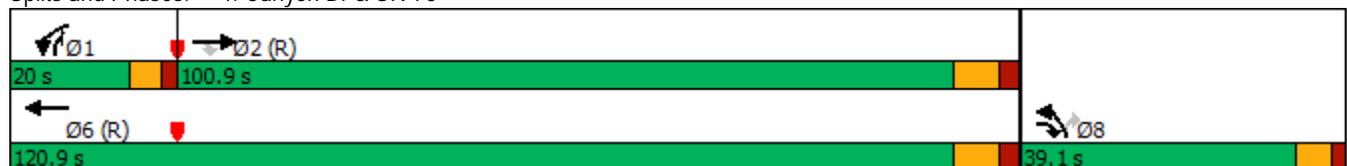


Lane Group	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↙	↑↑	↙↘	↙
Traffic Volume (vph)	2473	60	180	1962	10	120
Future Volume (vph)	2473	60	180	1962	10	120
Turn Type	NA	pm+ov	Prot	NA	Prot	pm+ov
Protected Phases	2	8	1	6	8	1
Permitted Phases		2				8
Detector Phase	2	8	1	6	8	1
Switch Phase						
Minimum Initial (s)	25.0	9.0	10.0	25.0	9.0	10.0
Minimum Split (s)	34.0	39.1	15.7	33.0	39.1	15.7
Total Split (s)	100.9	39.1	20.0	120.9	39.1	20.0
Total Split (%)	63.1%	24.4%	12.5%	75.6%	24.4%	12.5%
Yellow Time (s)	5.5	4.1	3.7	5.5	4.1	3.7
All-Red Time (s)	2.5	2.0	2.0	2.5	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	8.0	6.1	5.7	8.0	6.1	5.7
Lead/Lag	Lag		Lead			Lead
Lead-Lag Optimize?	Yes		Yes			Yes
Recall Mode	C-Max	Min	None	C-Max	Min	None
Act Effct Green (s)	98.8	120.6	27.6	132.1	13.8	47.5
Actuated g/C Ratio	0.62	0.75	0.17	0.83	0.09	0.30
v/c Ratio	1.23	0.05	0.64	0.73	0.04	0.28
Control Delay	144.1	3.2	40.2	30.9	61.8	44.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	144.1	3.2	40.2	30.9	61.8	44.3
LOS	F	A	D	C	E	D
Approach Delay	140.7			31.7	45.7	
Approach LOS	F			C	D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 21 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.23
 Intersection Signal Delay: 89.5
 Intersection LOS: F
 Intersection Capacity Utilization 102.3%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 4: Canyon Dr & SR-76



LOS Engineering, Inc.

PM Horizon Year + Project
4: Canyon Dr & SR-76

						
Movement	EBT	EBR	WBL	WBT	NBL	NBR
Lane Configurations	↑↑	↑	↵	↑↑	↵↵	↵
Traffic Volume (veh/h)	2473	60	180	1962	10	120
Future Volume (veh/h)	2473	60	180	1962	10	120
Initial Q (Qb), veh	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)		1.00	1.00		1.00	1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach	No			No	No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	2688	65	196	2133	11	130
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2
Cap, veh/h	2482	1247	159	2927	305	282
Arrive On Green	0.70	0.70	0.09	0.82	0.09	0.09
Sat Flow, veh/h	3647	1585	1781	3647	3456	1585
Grp Volume(v), veh/h	2688	65	196	2133	11	130
Grp Sat Flow(s),veh/h/ln	1777	1585	1781	1777	1728	1585
Q Serve(g_s), s	111.8	1.5	14.3	42.4	0.5	11.8
Cycle Q Clear(g_c), s	111.8	1.5	14.3	42.4	0.5	11.8
Prop In Lane		1.00	1.00		1.00	1.00
Lane Grp Cap(c), veh/h	2482	1247	159	2927	305	282
V/C Ratio(X)	1.08	0.05	1.23	0.73	0.04	0.46
Avail Cap(c_a), veh/h	2482	1247	159	2927	713	469
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.09	1.00	1.00
Uniform Delay (d), s/veh	24.1	3.8	72.8	6.2	66.7	58.9
Incr Delay (d2), s/veh	38.1	0.0	109.2	0.1	0.0	1.2
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	56.5	0.7	11.4	13.3	0.2	4.9
Unsig. Movement Delay, s/veh						
LnGrp Delay(d),s/veh	62.2	3.8	182.0	6.4	66.7	60.1
LnGrp LOS	F	A	F	A	E	E
Approach Vol, veh/h	2753			2329	141	
Approach Delay, s/veh	60.9			21.2	60.6	
Approach LOS	E			C	E	
Timer - Assigned Phs	1	2			6	8
Phs Duration (G+Y+Rc), s	20.0	119.8			139.8	20.2
Change Period (Y+Rc), s	* 5.7	8.0			8.0	6.1
Max Green Setting (Gmax), s	* 14	92.9			112.9	33.0
Max Q Clear Time (g_c+I1), s	16.3	113.8			44.4	13.8
Green Ext Time (p_c), s	0.0	0.0			39.2	0.4
Intersection Summary						
HCM 6th Ctrl Delay			43.2			
HCM 6th LOS			D			
Notes						
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.						

PM Horizon Year + Project
5: Benet Rd & SR-76

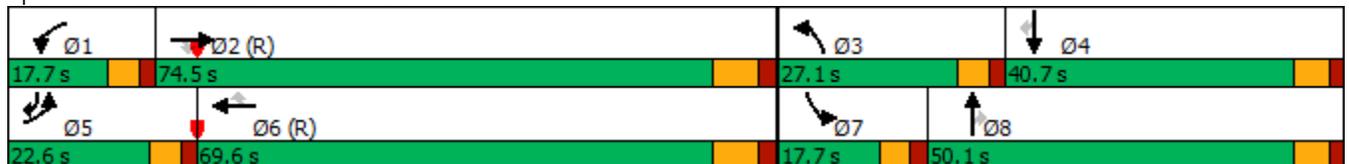
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	273	2290	120	50	1750	21	170	32	120	22	16	232
Future Volume (vph)	273	2290	120	50	1750	21	170	32	120	22	16	232
Turn Type	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2		1	6		3	8		7	4	5
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	5
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	33.0	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	22.6	74.5	74.5	17.7	69.6	69.6	27.1	50.1	50.1	17.7	40.7	22.6
Total Split (%)	14.1%	46.6%	46.6%	11.1%	43.5%	43.5%	16.9%	31.3%	31.3%	11.1%	25.4%	14.1%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	37.4	90.5	90.5	12.0	61.6	61.6	19.8	30.6	30.6	12.0	15.7	59.2
Actuated g/C Ratio	0.23	0.57	0.57	0.08	0.38	0.38	0.12	0.19	0.19	0.08	0.10	0.37
v/c Ratio	0.72	1.24	0.14	0.41	1.40	0.03	0.85	0.10	0.32	0.18	0.09	0.38
Control Delay	40.1	155.3	11.2	72.7	214.6	0.0	99.4	53.8	9.4	73.0	62.2	22.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	40.1	155.3	11.2	72.7	214.6	0.0	99.4	53.8	9.4	73.0	62.2	22.1
LOS	D	F	B	E	F	A	F	D	A	E	E	C
Approach Delay		137.1			208.2			61.4			28.6	
Approach LOS		F			F			E			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 22.6 (14%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.40
 Intersection Signal Delay: 152.0
 Intersection LOS: F
 Intersection Capacity Utilization 105.9%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

PM Horizon Year + Project
5: Benet Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	273	2290	120	50	1750	21	170	32	120	22	16	232
Future Volume (veh/h)	273	2290	120	50	1750	21	170	32	120	22	16	232
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	297	2489	130	54	1902	23	185	35	130	24	17	252
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	188	1778	793	121	1645	734	206	417	353	88	292	415
Arrive On Green	0.11	0.50	0.50	0.07	0.46	0.46	0.12	0.22	0.22	0.05	0.16	0.16
Sat Flow, veh/h	1781	3554	1585	1781	3554	1585	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	297	2489	130	54	1902	23	185	35	130	24	17	252
Grp Sat Flow(s),veh/h/ln	1781	1777	1585	1781	1777	1585	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	16.9	80.1	7.1	4.7	74.1	1.3	16.4	2.4	11.1	2.1	1.2	22.3
Cycle Q Clear(g_c), s	16.9	80.1	7.1	4.7	74.1	1.3	16.4	2.4	11.1	2.1	1.2	22.3
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	188	1778	793	121	1645	734	206	417	353	88	292	415
V/C Ratio(X)	1.58	1.40	0.16	0.44	1.16	0.03	0.90	0.08	0.37	0.27	0.06	0.61
Avail Cap(c_a), veh/h	188	1778	793	134	1645	734	238	514	436	134	404	510
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	0.44	0.44	0.44	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	71.6	40.0	21.8	71.6	43.0	23.4	69.8	49.2	52.6	73.3	57.5	51.8
Incr Delay (d2), s/veh	262.7	180.2	0.0	1.1	73.6	0.0	29.9	0.1	0.6	1.7	0.1	1.4
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	21.5	78.9	2.7	2.2	49.0	0.5	9.2	1.1	4.5	1.0	0.6	9.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	334.2	220.1	21.8	72.8	116.6	23.4	99.7	49.3	53.3	75.0	57.6	53.3
LnGrp LOS	F	F	C	E	F	C	F	D	D	E	E	D
Approach Vol, veh/h		2916			1979			350			293	
Approach Delay, s/veh		222.9			114.3			77.4			55.3	
Approach LOS		F			F			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.6	88.1	24.2	31.1	22.6	82.1	13.6	41.8				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 21	34.6	* 17	61.6	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.7	82.1	18.4	24.3	18.9	76.1	4.1	13.1				
Green Ext Time (p_c), s	0.0	0.0	0.1	0.7	0.0	0.0	0.0	0.6				

Intersection Summary

HCM 6th Ctrl Delay	166.0
HCM 6th LOS	F

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year + Project
6: Foussat Rd & SR-76

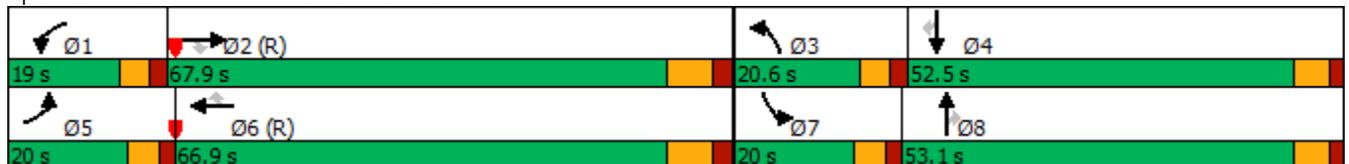
Timings

Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	40	2302	170	130	1651	155	100	173	180	105	96	30
Future Volume (vph)	40	2302	170	130	1651	155	100	173	180	105	96	30
Turn Type	Prot	NA	Perm									
Protected Phases	5	2		1	6		3	8		7	4	
Permitted Phases			2			6			8			4
Detector Phase	5	2	2	1	6	6	3	8	8	7	4	4
Switch Phase												
Minimum Initial (s)	13.0	25.0	25.0	12.0	25.0	25.0	13.0	12.0	12.0	13.0	13.0	13.0
Minimum Split (s)	18.7	33.0	33.0	17.7	48.0	48.0	18.7	52.1	52.1	18.7	20.0	20.0
Total Split (s)	20.0	67.9	67.9	19.0	66.9	66.9	20.6	53.1	53.1	20.0	52.5	52.5
Total Split (%)	12.5%	42.4%	42.4%	11.9%	41.8%	41.8%	12.9%	33.2%	33.2%	12.5%	32.8%	32.8%
Yellow Time (s)	3.7	5.5	5.5	3.7	5.5	5.5	3.7	4.1	4.1	3.7	4.1	4.1
All-Red Time (s)	2.0	2.5	2.5	2.0	2.5	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	8.0	5.7	8.0	8.0	5.7	6.1	6.1	5.7	6.1	6.1
Lead/Lag	Lead	Lag	Lag									
Lead-Lag Optimize?	Yes											
Recall Mode	None	C-Max	C-Max	None	C-Max	C-Max	None	Min	Min	None	Min	Min
Act Effct Green (s)	13.0	88.9	88.9	12.9	92.5	92.5	13.1	19.6	19.6	13.1	19.7	19.7
Actuated g/C Ratio	0.08	0.56	0.56	0.08	0.58	0.58	0.08	0.12	0.12	0.08	0.12	0.12
v/c Ratio	0.15	1.27	0.20	0.51	0.88	0.17	0.39	0.30	0.54	0.41	0.24	0.07
Control Delay	53.6	156.6	21.4	77.2	35.6	5.9	74.1	63.4	11.8	74.3	62.1	0.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	53.6	156.6	21.4	77.2	35.6	5.9	74.1	63.4	11.8	74.3	62.1	0.3
LOS	D	F	C	E	D	A	E	E	B	E	E	A
Approach Delay		145.8			36.0			45.2			59.5	
Approach LOS		F			D			D			E	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 20 (13%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.27
 Intersection Signal Delay: 91.6
 Intersection LOS: F
 Intersection Capacity Utilization 102.1%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 6: Foussat Rd & SR-76



LOS Engineering, Inc.

PM Horizon Year + Project
6: Foussat Rd & SR-76

HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	 	 		 	 		 	  		 		
Traffic Volume (veh/h)	40	2302	170	130	1651	155	100	173	180	105	96	30
Future Volume (veh/h)	40	2302	170	130	1651	155	100	173	180	105	96	30
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	43	2502	185	141	1795	168	109	188	196	114	104	33
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	239	1919	856	259	1939	865	279	741	230	279	516	405
Arrive On Green	0.07	0.54	0.54	0.07	0.55	0.55	0.08	0.15	0.15	0.08	0.15	0.15
Sat Flow, veh/h	3456	3554	1585	3456	3554	1585	3456	5106	1585	3456	3554	2790
Grp Volume(v), veh/h	43	2502	185	141	1795	168	109	188	196	114	104	33
Grp Sat Flow(s),veh/h/ln	1728	1777	1585	1728	1777	1585	1728	1702	1585	1728	1777	1395
Q Serve(g_s), s	1.9	86.4	9.7	6.3	74.2	8.6	4.8	5.2	19.3	5.0	4.1	1.6
Cycle Q Clear(g_c), s	1.9	86.4	9.7	6.3	74.2	8.6	4.8	5.2	19.3	5.0	4.1	1.6
Prop In Lane	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	239	1919	856	259	1939	865	279	741	230	279	516	405
V/C Ratio(X)	0.18	1.30	0.22	0.55	0.93	0.19	0.39	0.25	0.85	0.41	0.20	0.08
Avail Cap(c_a), veh/h	309	1919	856	287	1939	865	322	1500	466	309	1031	809
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	0.09	0.09	0.09	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	70.2	36.8	19.2	71.4	33.4	18.5	69.8	60.7	66.7	69.9	60.2	59.2
Incr Delay (d2), s/veh	0.0	137.1	0.1	1.8	9.1	0.5	0.9	0.2	8.6	1.0	0.2	0.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	0.8	73.2	3.7	2.9	33.9	3.4	2.2	2.3	8.4	2.3	1.9	0.6
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	70.2	173.9	19.2	73.2	42.5	19.0	70.7	60.9	75.4	70.9	60.4	59.2
LnGrp LOS	E	F	B	E	D	B	E	E	E	E	E	E
Approach Vol, veh/h		2730			2104			493			251	
Approach Delay, s/veh		161.8			42.7			68.8			65.0	
Approach LOS		F			D			E			E	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	17.7	94.4	18.6	29.3	16.8	95.3	18.6	29.3				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 13	59.9	* 15	46.4	* 14	58.9	* 14	47.0				
Max Q Clear Time (g_c+I1), s	8.3	88.4	6.8	6.1	3.9	76.2	7.0	21.3				
Green Ext Time (p_c), s	0.2	0.0	0.2	0.8	0.0	0.0	0.2	1.9				
Intersection Summary												
HCM 6th Ctrl Delay	104.3											
HCM 6th LOS	F											
Notes												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Horizon Year + Project
7: Benet Rd & Airport Rd

HCM 6th TWSC

Intersection						
Int Delay, s/veh	0.8					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations						
Traffic Vol, veh/h	20	10	236	10	10	201
Future Vol, veh/h	20	10	236	10	10	201
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	-	50	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	22	11	257	11	11	218

Major/Minor	Minor1	Major1	Major2			
Conflicting Flow All	372	263	0	0	268	0
Stage 1	263	-	-	-	-	-
Stage 2	109	-	-	-	-	-
Critical Hdwy	6.08	6.23	-	-	4.13	-
Critical Hdwy Stg 1	5.43	-	-	-	-	-
Critical Hdwy Stg 2	6.03	-	-	-	-	-
Follow-up Hdwy	3.669	3.319	-	-	2.219	-
Pot Cap-1 Maneuver	629	775	-	-	1294	-
Stage 1	752	-	-	-	-	-
Stage 2	864	-	-	-	-	-
Platoon blocked, %			-	-	-	-
Mov Cap-1 Maneuver	623	775	-	-	1294	-
Mov Cap-2 Maneuver	623	-	-	-	-	-
Stage 1	752	-	-	-	-	-
Stage 2	855	-	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	10.7	0	0.4
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	667	1294
HCM Lane V/C Ratio	-	-	0.049	0.008
HCM Control Delay (s)	-	-	10.7	7.8
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.2	0

LOS Engineering, Inc.

Intersection						
Int Delay, s/veh	2					
Movement	WBL	WBR	NBT	NBR	SBL	SBT
Lane Configurations	↔		↑	↑		↔
Traffic Vol, veh/h	81	0	220	36	0	130
Future Vol, veh/h	81	0	220	36	0	130
Conflicting Peds, #/hr	0	0	0	0	0	0
Sign Control	Stop	Stop	Free	Free	Free	Free
RT Channelized	-	None	-	None	-	None
Storage Length	0	-	-	50	-	-
Veh in Median Storage, #	0	-	0	-	-	0
Grade, %	0	-	0	-	-	0
Peak Hour Factor	92	92	92	92	92	92
Heavy Vehicles, %	2	2	2	2	2	2
Mvmt Flow	88	0	239	39	0	141

Major/Minor	Minor1	Major1	Major2		
Conflicting Flow All	380	239	0	0	278
Stage 1	239	-	-	-	-
Stage 2	141	-	-	-	-
Critical Hdwy	6.42	6.22	-	-	4.12
Critical Hdwy Stg 1	5.42	-	-	-	-
Critical Hdwy Stg 2	5.42	-	-	-	-
Follow-up Hdwy	3.518	3.318	-	-	2.218
Pot Cap-1 Maneuver	622	800	-	-	1285
Stage 1	801	-	-	-	-
Stage 2	886	-	-	-	-
Platoon blocked, %			-	-	-
Mov Cap-1 Maneuver	622	800	-	-	1285
Mov Cap-2 Maneuver	622	-	-	-	-
Stage 1	801	-	-	-	-
Stage 2	886	-	-	-	-

Approach	WB	NB	SB
HCM Control Delay, s	11.7	0	0
HCM LOS	B		

Minor Lane/Major Mvmt	NBT	NBRWBLn1	SBL	SBT
Capacity (veh/h)	-	-	622	1285
HCM Lane V/C Ratio	-	-	0.142	-
HCM Control Delay (s)	-	-	11.7	0
HCM Lane LOS	-	-	B	A
HCM 95th %tile Q(veh)	-	-	0.5	0

Intersection				
Intersection Delay, s/veh	9.8			
Intersection LOS	A			
Approach	EB	WB	NB	SB
Entry Lanes	1	1	1	1
Conflicting Circle Lanes	1	1	1	1
Adj Approach Flow, veh/h	157	245	797	402
Demand Flow Rate, veh/h	160	249	813	410
Vehicles Circulating, veh/h	582	585	111	252
Vehicles Exiting, veh/h	80	339	631	582
Ped Vol Crossing Leg, #/h	0	0	0	0
Ped Cap Adj	1.000	1.000	1.000	1.000
Approach Delay, s/veh	7.1	8.8	11.9	7.5
Approach LOS	A	A	B	A
Lane	Left	Left	Left	Left
Designated Moves	LTR	LTR	LTR	LTR
Assumed Moves	LTR	LTR	LTR	LTR
RT Channelized				
Lane Util	1.000	1.000	1.000	1.000
Follow-Up Headway, s	2.609	2.609	2.609	2.609
Critical Headway, s	4.976	4.976	4.976	4.976
Entry Flow, veh/h	160	249	813	410
Cap Entry Lane, veh/h	762	760	1232	1067
Entry HV Adj Factor	0.979	0.983	0.980	0.980
Flow Entry, veh/h	157	245	797	402
Cap Entry, veh/h	746	747	1208	1046
V/C Ratio	0.210	0.328	0.660	0.384
Control Delay, s/veh	7.1	8.8	11.9	7.5
LOS	A	A	B	A
95th %tile Queue, veh	1	1	5	2

Appendix O

SR-76/Benet Rd Intersection Improvement LOS Worksheets

AM Existing + Cumulative + Project
5: Benet Rd & SR-76

With Improvement
Timings

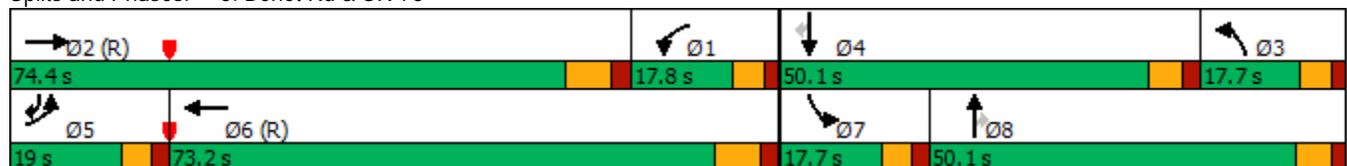


Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↕↕↕	↘	↕↕↕	↘	↕	↗	↘	↕	↗
Traffic Volume (vph)	178	1220	47	2340	52	10	48	14	14	273
Future Volume (vph)	178	1220	47	2340	52	10	48	14	14	273
Turn Type	Prot	NA	Prot	NA	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2	1	6	3	8		7	4	5
Permitted Phases							8			4
Detector Phase	5	2	1	6	3	8	8	7	4	5
Switch Phase										
Minimum Initial (s)	13.0	25.0	12.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	17.7	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	19.0	74.4	17.8	73.2	17.7	50.1	50.1	17.7	50.1	19.0
Total Split (%)	11.9%	46.5%	11.1%	45.8%	11.1%	31.3%	31.3%	11.1%	31.3%	11.9%
Yellow Time (s)	3.7	5.5	3.7	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.0	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	5.7	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes									
Recall Mode	None	C-Max	None	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	28.1	99.5	12.1	80.0	18.8	25.1	25.1	12.0	11.2	40.8
Actuated g/C Ratio	0.18	0.62	0.08	0.50	0.12	0.16	0.16	0.08	0.07	0.26
v/c Ratio	0.62	0.47	0.38	1.01	0.28	0.04	0.13	0.11	0.12	0.59
Control Delay	58.7	21.0	91.4	62.4	64.1	53.3	0.7	71.3	72.2	27.3
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	58.7	21.0	91.4	62.4	64.1	53.3	0.7	71.3	72.2	27.3
LOS	E	C	F	E	E	D	A	E	E	C
Approach Delay		25.3		62.9		35.6			31.4	
Approach LOS		C		E		D			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.01
 Intersection Signal Delay: 46.8
 Intersection LOS: D
 Intersection Capacity Utilization 88.6%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



AM Existing + Cumulative + Project
5: Benet Rd & SR-76

With Improvement
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	178	1220	138	47	2340	15	52	10	48	14	14	273
Future Volume (veh/h)	178	1220	138	47	2340	15	52	10	48	14	14	273
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	193	1326	150	51	2543	16	57	11	52	15	15	297
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	148	1931	218	325	2769	17	123	360	305	65	294	381
Arrive On Green	0.08	0.83	0.42	0.18	1.00	0.53	0.07	0.19	0.19	0.04	0.16	0.26
Sat Flow, veh/h	1781	4653	526	1781	5236	33	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	193	970	506	51	1652	907	57	11	52	15	15	297
Grp Sat Flow(s),veh/h/ln	1781	1702	1776	1781	1702	1864	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	13.3	18.0	26.0	3.9	0.0	7.3	4.9	0.8	2.9	1.3	1.1	22.1
Cycle Q Clear(g_c), s	13.3	18.0	26.0	3.9	0.0	7.3	4.9	0.8	2.9	1.3	1.1	22.1
Prop In Lane	1.00		0.30	1.00		0.02	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	148	1413	737	325	1800	986	123	360	305	65	294	381
V/C Ratio(X)	1.30	0.69	0.69	0.16	0.92	0.92	0.46	0.03	0.17	0.23	0.05	0.78
Avail Cap(c_a), veh/h	148	1413	737	325	1800	986	134	514	436	134	514	568
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.3	9.5	17.5	55.0	0.0	0.3	71.6	52.5	23.9	74.9	57.3	30.0
Incr Delay (d2), s/veh	176.9	2.7	5.2	0.2	9.0	14.8	2.7	0.0	0.3	1.8	0.1	4.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	13.4	4.1	9.0	1.8	2.2	4.3	2.4	0.4	1.8	0.6	0.5	8.1
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	250.2	12.2	22.7	55.3	9.0	15.2	74.3	52.5	24.1	76.7	57.4	34.1
LnGrp LOS	F	B	C	E	A	B	E	D	C	E	E	C
Approach Vol, veh/h		1669			2610			120			327	
Approach Delay, s/veh		42.9			12.0			50.6			37.1	
Approach LOS		D			B			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	37.2	74.4	17.1	31.2	19.0	92.6	11.5	36.9				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.1	* 66	12.0	* 44	* 13	65.2	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.9	28.0	6.9	24.1	15.3	9.3	3.3	4.9				
Green Ext Time (p_c), s	0.0	14.4	0.0	1.0	0.0	39.4	0.0	0.2				
Intersection Summary												
HCM 6th Ctrl Delay				25.7								
HCM 6th LOS				C								
Notes												
User approved pedestrian interval to be less than phase max green.												
* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.												

PM Existing + Cumulative + Project
5: Benet Rd & SR-76

With Improvement
Timings

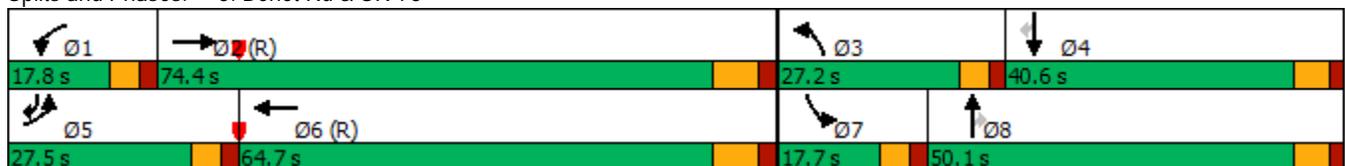


Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↙	↕↕↕	↙	↕↕↕	↙	↕	↗	↙	↕	↗
Traffic Volume (vph)	215	2072	43	1605	137	23	99	24	15	197
Future Volume (vph)	215	2072	43	1605	137	23	99	24	15	197
Turn Type	Prot	NA	Prot	NA	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2	1	6	3	8		7	4	5
Permitted Phases							8			4
Detector Phase	5	2	1	6	3	8	8	7	4	5
Switch Phase										
Minimum Initial (s)	13.0	25.0	12.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	17.7	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	27.5	74.4	17.8	64.7	27.2	50.1	50.1	17.7	40.6	27.5
Total Split (%)	17.2%	46.5%	11.1%	40.4%	17.0%	31.3%	31.3%	11.1%	25.4%	17.2%
Yellow Time (s)	3.7	5.5	3.7	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.0	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	5.7	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lag	Lead	Lag	Lead	Lag	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes									
Recall Mode	None	C-Max	None	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	26.8	92.4	12.0	74.1	17.9	28.7	28.7	12.0	15.7	48.6
Actuated g/C Ratio	0.17	0.58	0.08	0.46	0.11	0.18	0.18	0.08	0.10	0.30
v/c Ratio	0.79	0.80	0.36	0.75	0.75	0.07	0.27	0.20	0.09	0.38
Control Delay	56.9	45.4	62.5	44.3	91.6	53.9	3.2	73.4	62.1	21.0
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	56.9	45.4	62.5	44.3	91.6	53.9	3.2	73.4	62.1	21.0
LOS	E	D	E	D	F	D	A	E	E	C
Approach Delay		46.5		44.8		54.4			28.9	
Approach LOS		D		D		D			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 22.6 (14%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.80
 Intersection Signal Delay: 45.4
 Intersection LOS: D
 Intersection Capacity Utilization 82.8%
 ICU Level of Service E
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



PM Existing + Cumulative + Project
5: Benet Rd & SR-76

With Improvement
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	215	2072	90	43	1605	19	137	23	99	24	15	197
Future Volume (veh/h)	215	2072	90	43	1605	19	137	23	99	24	15	197
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	234	2252	98	47	1745	21	149	25	108	26	16	214
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	243	2750	119	117	2483	30	171	328	278	92	245	423
Arrive On Green	0.14	0.55	0.55	0.07	0.48	0.48	0.10	0.18	0.18	0.05	0.13	0.13
Sat Flow, veh/h	1781	5018	217	1781	5201	63	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	234	1524	826	47	1142	624	149	25	108	26	16	214
Grp Sat Flow(s),veh/h/ln	1781	1702	1831	1781	1702	1859	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	20.9	58.6	59.5	4.1	42.2	42.2	13.2	1.8	9.6	2.2	1.2	18.3
Cycle Q Clear(g_c), s	20.9	58.6	59.5	4.1	42.2	42.2	13.2	1.8	9.6	2.2	1.2	18.3
Prop In Lane	1.00		0.12	1.00		0.03	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	243	1865	1003	117	1625	888	171	328	278	92	245	423
V/C Ratio(X)	0.96	0.82	0.82	0.40	0.70	0.70	0.87	0.08	0.39	0.28	0.07	0.51
Avail Cap(c_a), veh/h	243	1865	1003	135	1625	888	239	514	436	134	403	558
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	68.7	29.6	29.8	71.7	32.9	32.9	71.3	55.1	58.4	73.1	60.9	49.7
Incr Delay (d2), s/veh	47.6	4.1	7.6	2.2	2.6	4.6	21.2	0.1	0.9	1.7	0.1	0.9
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	12.8	24.8	28.0	1.9	18.1	20.3	7.1	0.9	4.0	1.1	0.6	7.4
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	116.3	33.7	37.4	73.9	35.4	37.5	92.6	55.2	59.2	74.7	61.1	50.6
LnGrp LOS	F	C	D	E	D	D	F	E	E	E	E	D
Approach Vol, veh/h		2584			1813			282			256	
Approach Delay, s/veh		42.4			37.2			76.5			53.7	
Approach LOS		D			D			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.2	95.7	21.1	27.0	27.5	84.4	13.9	34.2				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.4	* 22	34.5	* 22	56.7	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.1	61.5	15.2	20.3	22.9	44.2	4.2	11.6				
Green Ext Time (p_c), s	0.0	4.6	0.2	0.6	0.0	9.0	0.0	0.5				

Intersection Summary

HCM 6th Ctrl Delay	43.0
HCM 6th LOS	D

Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

AM Horizon Year + Project
5: Benet Rd & SR-76

With Improvement
Timings

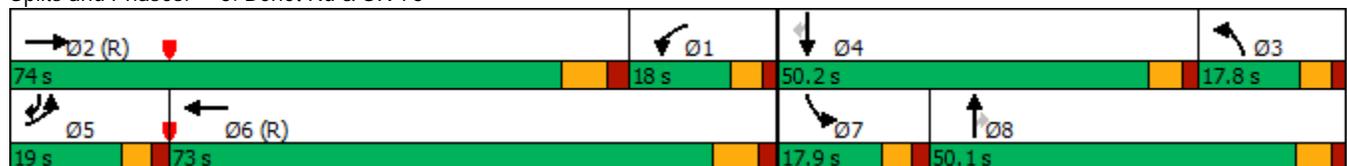


Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕	↖	↕	↖	↕	↗	↖	↕	↗
Traffic Volume (vph)	202	1370	40	2700	70	16	60	11	22	354
Future Volume (vph)	202	1370	40	2700	70	16	60	11	22	354
Turn Type	Prot	NA	Prot	NA	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2	1	6	3	8		7	4	5
Permitted Phases							8			4
Detector Phase	5	2	1	6	3	8	8	7	4	5
Switch Phase										
Minimum Initial (s)	13.0	25.0	12.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	17.7	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	19.0	74.0	18.0	73.0	17.8	50.1	50.1	17.9	50.2	19.0
Total Split (%)	11.9%	46.3%	11.3%	45.6%	11.1%	31.3%	31.3%	11.2%	31.4%	11.9%
Yellow Time (s)	3.7	5.5	3.7	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.0	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	5.7	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lead	Lag	Lag	Lag	Lag	Lag	Lead	Lead	Lead
Lead-Lag Optimize?	Yes									
Recall Mode	None	C-Max	None	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	32.2	95.7	12.2	72.2	19.1	28.7	28.7	12.0	11.0	43.6
Actuated g/C Ratio	0.20	0.60	0.08	0.45	0.12	0.18	0.18	0.08	0.07	0.27
v/c Ratio	0.62	0.56	0.32	1.29	0.36	0.05	0.15	0.09	0.19	0.73
Control Delay	56.9	22.6	75.1	166.2	66.6	53.6	0.8	70.7	74.0	36.1
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	56.9	22.6	75.1	166.2	66.6	53.6	0.8	70.7	74.0	36.1
LOS	E	C	E	F	E	D	A	E	E	D
Approach Delay		26.6		164.9		38.1			39.3	
Approach LOS		C		F		D			D	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 0 (0%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.29
 Intersection Signal Delay: 103.6
 Intersection LOS: F
 Intersection Capacity Utilization 100.5%
 ICU Level of Service G
 Analysis Period (min) 15

Splits and Phases: 5: Benet Rd & SR-76



LOS Engineering, Inc.

AM Horizon Year + Project
5: Benet Rd & SR-76

With Improvement
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 							
Traffic Volume (veh/h)	202	1370	170	40	2700	12	70	16	60	11	22	354
Future Volume (veh/h)	202	1370	170	40	2700	12	70	16	60	11	22	354
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	220	1489	185	43	2935	13	76	17	65	12	24	385
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	148	1898	236	245	2526	11	129	458	388	55	376	450
Arrive On Green	0.08	0.82	0.41	0.14	0.96	0.48	0.07	0.24	0.24	0.03	0.20	0.34
Sat Flow, veh/h	1781	4601	571	1781	5247	23	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	220	1101	573	43	1903	1045	76	17	65	12	24	385
Grp Sat Flow(s),veh/h/ln	1781	1702	1768	1781	1702	1866	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	13.3	25.7	35.0	3.4	77.0	77.0	6.6	1.1	3.6	1.1	1.7	28.9
Cycle Q Clear(g_c), s	13.3	25.7	35.0	3.4	77.0	77.0	6.6	1.1	3.6	1.1	1.7	28.9
Prop In Lane	1.00		0.32	1.00		0.01	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	148	1404	729	245	1639	899	129	458	388	55	376	450
V/C Ratio(X)	1.49	0.78	0.79	0.18	1.16	1.16	0.59	0.04	0.17	0.22	0.06	0.85
Avail Cap(c_a), veh/h	148	1404	729	245	1639	899	135	514	436	136	516	569
HCM Platoon Ratio	1.00	2.00	1.00	1.00	2.00	1.00	1.00	1.00	1.00	1.00	1.00	1.67
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	73.3	10.5	19.9	60.9	3.0	3.4	71.9	46.0	23.4	75.6	51.7	26.2
Incr Delay (d2), s/veh	251.0	4.5	8.3	0.3	79.6	85.7	6.1	0.0	0.2	1.9	0.1	10.1
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	16.4	5.1	12.8	1.6	19.3	23.0	3.3	0.5	2.1	0.5	0.8	10.8
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	324.3	14.9	28.2	61.3	82.5	89.2	78.0	46.1	23.6	77.6	51.8	36.3
LnGrp LOS	F	B	C	E	F	F	E	D	C	E	D	D
Approach Vol, veh/h		1894			2991			158			421	
Approach Delay, s/veh		54.9			84.5			52.2			38.3	
Approach LOS		D			F			D			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	30.0	74.0	17.7	38.3	19.0	85.0	10.7	45.3				
Change Period (Y+Rc), s	8.0	* 8	6.1	* 6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	12.3	* 66	12.1	* 44	* 13	65.0	* 12	44.0				
Max Q Clear Time (g_c+I1), s	5.4	37.0	8.6	30.9	15.3	79.0	3.1	5.6				
Green Ext Time (p_c), s	0.0	15.2	0.0	1.3	0.0	0.0	0.0	0.3				

Intersection Summary

HCM 6th Ctrl Delay	69.8
HCM 6th LOS	E

Notes

- User approved pedestrian interval to be less than phase max green.
- * HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

PM Horizon Year + Project
5: Benet Rd & SR-76

With Improvement
Timings

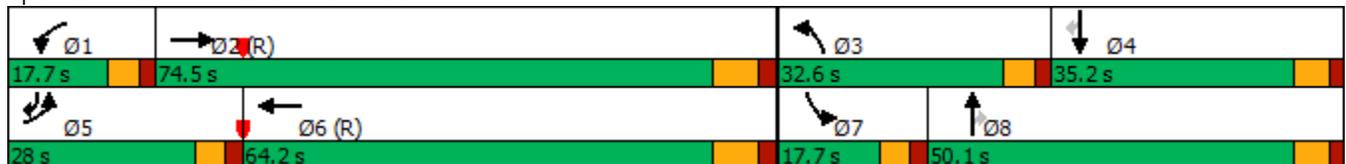


Lane Group	EBL	EBT	WBL	WBT	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕↕↕	↖	↕↕↕	↖	↕	↗	↖	↕	↗
Traffic Volume (vph)	273	2290	50	1750	170	32	120	22	16	232
Future Volume (vph)	273	2290	50	1750	170	32	120	22	16	232
Turn Type	Prot	NA	Prot	NA	Prot	NA	Perm	Prot	NA	pm+ov
Protected Phases	5	2	1	6	3	8		7	4	5
Permitted Phases							8			4
Detector Phase	5	2	1	6	3	8	8	7	4	5
Switch Phase										
Minimum Initial (s)	13.0	25.0	12.0	25.0	12.0	12.0	12.0	12.0	11.0	13.0
Minimum Split (s)	18.7	33.0	17.7	33.0	17.7	50.1	50.1	17.7	20.0	18.7
Total Split (s)	28.0	74.5	17.7	64.2	32.6	50.1	50.1	17.7	35.2	28.0
Total Split (%)	17.5%	46.6%	11.1%	40.1%	20.4%	31.3%	31.3%	11.1%	22.0%	17.5%
Yellow Time (s)	3.7	5.5	3.7	5.5	3.7	4.1	4.1	3.7	4.1	3.7
All-Red Time (s)	2.0	2.5	2.0	2.5	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	5.7	8.0	5.7	8.0	5.7	6.1	6.1	5.7	6.1	5.7
Lead/Lag	Lead	Lag	Lead	Lag	Lead	Lag	Lag	Lead	Lag	Lead
Lead-Lag Optimize?	Yes									
Recall Mode	None	C-Max	None	C-Max	None	Min	Min	None	Min	None
Act Effct Green (s)	38.1	90.0	12.0	60.4	21.4	31.1	31.1	12.0	14.6	58.8
Actuated g/C Ratio	0.24	0.56	0.08	0.38	0.13	0.19	0.19	0.08	0.09	0.37
v/c Ratio	0.71	0.92	0.41	1.00	0.78	0.10	0.30	0.18	0.10	0.39
Control Delay	41.7	48.4	68.5	62.0	88.7	52.8	5.7	73.0	64.4	22.5
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	41.7	48.4	68.5	62.0	88.7	52.8	5.7	73.0	64.4	22.5
LOS	D	D	E	E	F	D	A	E	E	C
Approach Delay		47.7		62.2		54.3			29.1	
Approach LOS		D		E		D			C	

Intersection Summary

Cycle Length: 160
 Actuated Cycle Length: 160
 Offset: 22.6 (14%), Referenced to phase 2:EBT and 6:WBT, Start of Green
 Natural Cycle: 150
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 1.00
 Intersection Signal Delay: 52.3
 Intersection Capacity Utilization 89.5%
 Analysis Period (min) 15
 Intersection LOS: D
 ICU Level of Service E

Splits and Phases: 5: Benet Rd & SR-76



PM Horizon Year + Project
5: Benet Rd & SR-76

With Improvement
HCM 6th Signalized Intersection Summary

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations		 			 							
Traffic Volume (veh/h)	273	2290	120	50	1750	21	170	32	120	22	16	232
Future Volume (veh/h)	273	2290	120	50	1750	21	170	32	120	22	16	232
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870	1870
Adj Flow Rate, veh/h	297	2489	130	54	1902	23	185	35	130	24	17	252
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	2	2	2	2	2	2
Cap, veh/h	248	2515	130	121	2261	27	208	406	344	88	280	458
Arrive On Green	0.14	0.51	0.51	0.07	0.43	0.43	0.12	0.22	0.22	0.05	0.15	0.15
Sat Flow, veh/h	1781	4971	257	1781	5200	63	1781	1870	1585	1781	1870	1585
Grp Volume(v), veh/h	297	1696	923	54	1245	680	185	35	130	24	17	252
Grp Sat Flow(s),veh/h/ln	1781	1702	1824	1781	1702	1859	1781	1870	1585	1781	1870	1585
Q Serve(g_s), s	22.3	78.5	81.0	4.7	52.1	52.2	16.4	2.4	11.2	2.1	1.2	21.5
Cycle Q Clear(g_c), s	22.3	78.5	81.0	4.7	52.1	52.2	16.4	2.4	11.2	2.1	1.2	21.5
Prop In Lane	1.00		0.14	1.00		0.03	1.00		1.00	1.00		1.00
Lane Grp Cap(c), veh/h	248	1722	923	121	1480	808	208	406	344	88	280	458
V/C Ratio(X)	1.20	0.98	1.00	0.44	0.84	0.84	0.89	0.09	0.38	0.27	0.06	0.55
Avail Cap(c_a), veh/h	248	1722	923	134	1480	808	299	514	436	134	340	509
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Uniform Delay (d), s/veh	68.8	38.9	39.5	71.6	40.3	40.3	69.6	49.9	53.4	73.3	58.4	48.1
Incr Delay (d2), s/veh	120.7	18.3	29.7	2.5	5.9	10.3	19.9	0.1	0.7	1.7	0.1	1.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	18.5	36.6	43.2	2.2	23.0	26.2	8.7	1.1	4.6	1.0	0.6	8.7
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	189.5	57.3	69.2	74.2	46.2	50.6	89.6	50.0	54.1	75.0	58.5	49.1
LnGrp LOS	F	E	F	E	D	D	F	D	D	E	E	D
Approach Vol, veh/h		2916			1979			350			293	
Approach Delay, s/veh		74.5			48.5			72.4			51.8	
Approach LOS		E			D			E			D	
Timer - Assigned Phs	1	2	3	4	5	6	7	8				
Phs Duration (G+Y+Rc), s	16.6	89.0	24.4	30.1	28.0	77.6	13.6	40.9				
Change Period (Y+Rc), s	* 5.7	8.0	* 5.7	6.1	* 5.7	8.0	* 5.7	6.1				
Max Green Setting (Gmax), s	* 12	66.5	* 27	29.1	* 22	56.2	* 12	44.0				
Max Q Clear Time (g_c+I1), s	6.7	83.0	18.4	23.5	24.3	54.2	4.1	13.2				
Green Ext Time (p_c), s	0.0	0.0	0.3	0.5	0.0	1.8	0.0	0.6				

Intersection Summary

HCM 6th Ctrl Delay	63.9
HCM 6th LOS	E

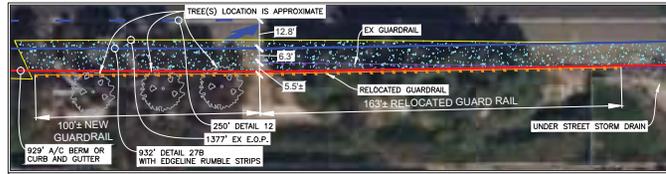
Notes

* HCM 6th computational engine requires equal clearance times for the phases crossing the barrier.

Appendix P

Conceptual Drawing of Proposed Improvement and Fair Share Calculations

CONCEPTUAL DESIGN BASED ON AERIAL SURVEY/TOPO DATA WAS NOT REFERENCED AND MAY ALTER FINAL DESIGN



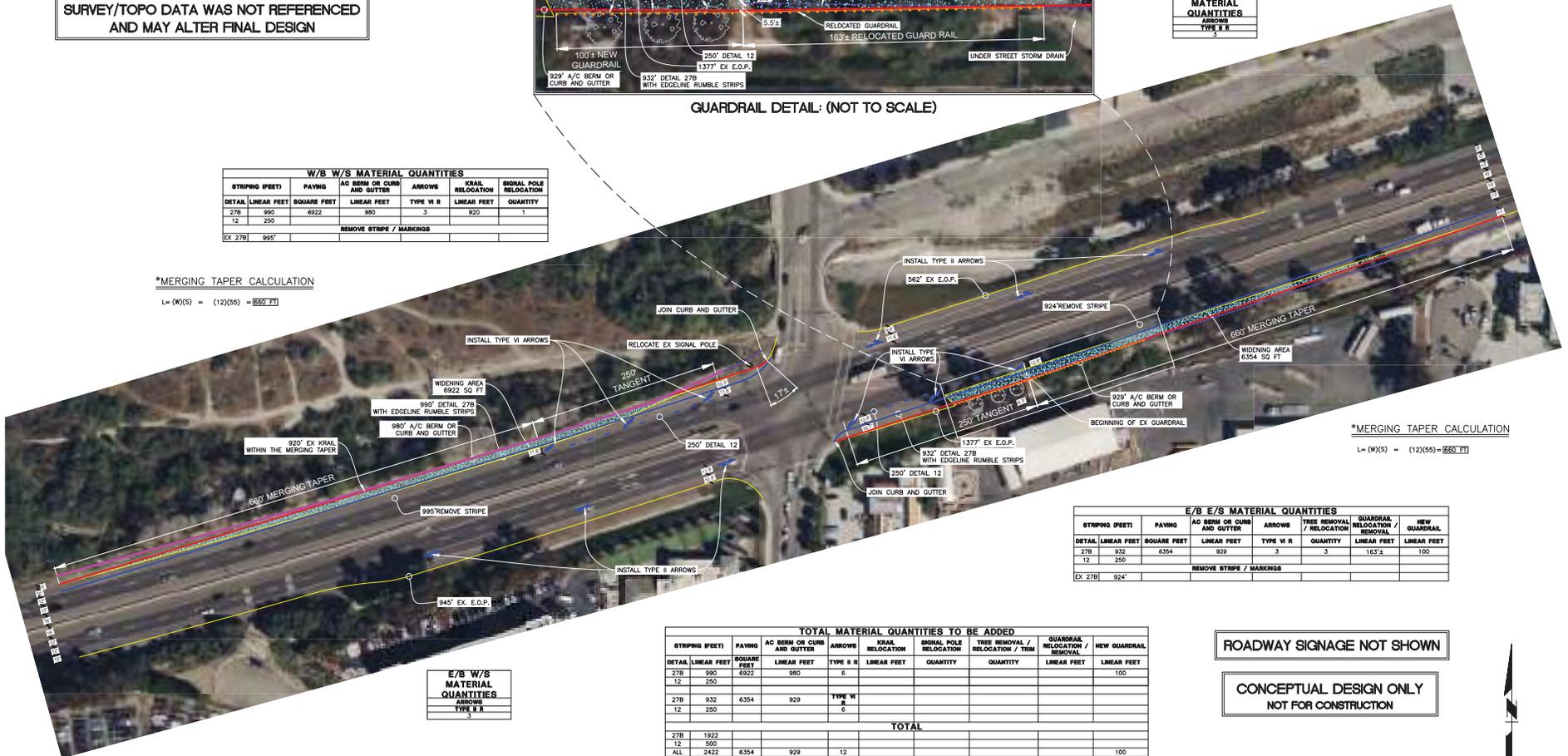
GUARDRAIL DETAIL: (NOT TO SCALE)

W/B E/S MATERIAL QUANTITIES	
ARROWS	TYPE II R
QUANTITY	3

W/B W/S MATERIAL QUANTITIES						
STRIPING FEET	PAVING	AC BERM OR CURB AND GUTTER	ARROWS	KRAIL RELOCATION	SIGNAL POLE RELOCATION	
DETAIL	LINEAR FEET	SQUARE FEET	LINEAR FEET	TYPE VI R	LINEAR FEET	QUANTITY
27B	990	6922	980	3	920	1
12	250					
REMOVE STRIPS / MARKINGS						
EX 27B	995'					

*MERGING TAPER CALCULATION

$L = (W/S) = (12)(95) = 660 \text{ FT}$



*MERGING TAPER CALCULATION

$L = (W/S) = (12)(95) = 660 \text{ FT}$

E/B E/S MATERIAL QUANTITIES						
STRIPING FEET	PAVING	AC BERM OR CURB AND GUTTER	ARROWS	TREE REMOVAL / RELOCATION	GUARDRAIL RELOCATION / REMOVAL	NEW GUARDRAIL
DETAIL	LINEAR FEET	SQUARE FEET	LINEAR FEET	TYPE VI R	QUANTITY	LINEAR FEET
27B	932	6354	929	3	163	100
12	250					
REMOVE STRIPS / MARKINGS						
EX 27B	924'					

TOTAL MATERIAL QUANTITIES TO BE ADDED								
STRIPING FEET	PAVING	AC BERM OR CURB AND GUTTER	ARROWS	KRAIL RELOCATION	SIGNAL POLE RELOCATION	TREE REMOVAL / TRIM	GUARDRAIL RELOCATION / REMOVAL	NEW GUARDRAIL
DETAIL	LINEAR FEET	SQUARE FEET	LINEAR FEET	TYPE II R	LINEAR FEET	QUANTITY	QUANTITY	LINEAR FEET
27B	990	6922	980	6				100
12	250							
TOTAL								
27B	1922							
12	500							100
ALL	2422	6354	929	12				

TOTAL MATERIAL QUANTITIES TO BE REMOVED / RELOCATED								
STRIPING FEET	PAVING	AC BERM OR CURB AND GUTTER	ARROWS	KRAIL RELOCATION	SIGNAL POLE RELOCATION	TREE REMOVAL / RELOCATION / TRIM	GUARDRAIL RELOCATION / REMOVAL	NEW GUARDRAIL
DETAIL	LINEAR FEET	SQUARE FEET	LINEAR FEET	TYPE II R	LINEAR FEET	QUANTITY	QUANTITY	LINEAR FEET
EX 27B	995				920	1	3	163
EX 27B	924							
TOTAL								
ALL	1919				920	1	3	163

TOTAL MATERIAL QUANTITIES TO BE ADDED AND REMOVED								
STRIPING FEET	PAVING	AC BERM OR CURB AND GUTTER	ARROWS	KRAIL RELOCATION	SIGNAL POLE RELOCATION	TREE REMOVAL / TRIM	GUARDRAIL RELOCATION / REMOVAL	NEW GUARDRAIL
DETAIL	LINEAR FEET	SQUARE FEET	LINEAR FEET	TYPE II R	LINEAR FEET	QUANTITY	QUANTITY	LINEAR FEET
TOTAL								
4341	6354		929	12	920	1	3	163

E/B W/S MATERIAL QUANTITIES	
ARROWS	TYPE II R
QUANTITY	3

ROADWAY SIGNAGE NOT SHOWN

CONCEPTUAL DESIGN ONLY
NOT FOR CONSTRUCTION



LEGEND

- EX E.O.P. OR BERM OR CURB AND GUTTER
- EX GUARDRAIL
- EX KRAIL
- PR GUARDRAIL
- PR E.O.P. OR AC BERM OR CURB AND GUTTER
- PR DETAIL 27B
- PR DETAIL 12
- ROAD WIDENING
- ▶ EX TREE
- ▶ TYPE VI ARROW THERMOPLASTIC
- ▶ TYPE II ARROW THERMOPLASTIC

C² Mobility & Technology In Transportation CONSULTING COLLECTIVE
WALTER B. MUSIAL PE, RSP
Historic Santa Fe Depot
1050 Kettner Blvd, Suite D-551
San Diego, CA 92101
658.270.6444
www.c2-mobility.com

CITY OF OCEANSIDE
DRAWING NO. CP-1
CONCEPTUAL DESIGN PLAN FOR COST ESTIMATION
ROUTE 76 AND BENET RD
SHEET 1 OF 1 SHEETS

Fair Share Calculations

5) SR-76/Benet Rd

A= 3677 Existing number of vehicles entering the intersection (AM)
B= 4920 Horizon Year without Project number of vehicles entering the intersection (AM)
C= 5027 Horizon Year with Project number of vehicles entering the intersection (AM)

AM Percent of Fair-Share $(C-B)/(C-A) = 7.9\%$

A= (3713) Existing number of vehicles entering the intersection (PM)
B= (4980) Horizon Year without Project number of vehicles entering the intersection (PM)
C= (5096) Horizon Year with Project number of vehicles entering the intersection (PM)

PM Percent of Fair-Share $(C-B)/(C-A) = 8.4\%$ <= Project Responsibility
8.5% <= **Rounded to 8.5%**

October 8, 2024

Mr. Rob Dmohowski
City of Oceanside
300 N. Coast Highway
Oceanside, California 92054

Re: Airport Land Use Commission Consistency Determination – Construction of Warehouse, Office, and Manufacturing Facility at 250 Eddie Jones Way, City of Oceanside

Dear Mr. Dmohowski:

As the Airport Land Use Commission (ALUC) for San Diego County, the San Diego County Regional Airport Authority (SDCRAA) acknowledges receipt of an application for a determination of consistency for the project described above. The area covered by this project lies within the Airport Influence Area (AIA) for the Oceanside Municipal Airport - Airport Land Use Compatibility Plan (ALUCP).

ALUC staff has reviewed your application and accompanying materials and has determined that it meets our requirements for completeness. In accordance with SDCRAA Policy 8.30 and applicable provisions of the State Aeronautics Act (Cal. Pub. Util. Code §21670-21679.5), ALUC staff will report to the ALUC that the proposed project is **conditionally consistent** with the Oceanside Municipal Airport ALUCP based upon the facts and findings summarized below:

- (1) The project involves an alternative design for a project previously issued a consistency determination on August 10, 2023, reducing the size of the warehouse and manufacturing facility, with ancillary office use, to four buildings totaling 497,882 square feet on a property of 31.57 acres.
- (2) The proposed project lies within the 60-65 and 65-70 decibel Community Noise Equivalent Level (dB CNEL) noise exposure contour. The ALUCP identifies manufacturing and warehouse uses located within the 60-65 and 65-70 dB CNEL noise contour as compatible with airport uses.
- (3) The maximum height of the proposed project structures will be approximately 73 feet above mean sea level (45 feet above ground level) and the maximum height of the proposed temporary construction cranes will be

approximately 147 feet above mean sea level (120 feet above ground level). The proposed project would be compatible with the ALUCP airspace protection surfaces, provided that the structures and temporary cranes are marked and lighted in accordance with a determination of no hazard to air navigation issued by the Federal Aviation Administration (FAA) and an aviation easement for airspace is recorded with the County Recorder. Therefore, as a condition of project approval, the structures and temporary cranes must be marked and lighted in accordance with FAA procedures and an aviation easement for airspace recorded with the County Recorder.

- (4) The proposed project is located within Safety Zones 1, 2, 3, 5, and 6. Safety Zones 1 and 3 wholly entail landscaping with no building area. The ALUCP identifies warehouse uses located within Safety Zone 2, 5, and 6 as compatible with airport uses.

The ALUCP identifies manufacturing uses located within Safety Zone 6 as compatible and within Safety Zone 2 as conditionally compatible with airport uses, provided that the project complies with a maximum intensity of 60 people per acre, a maximum lot coverage of 50 percent, and a maximum FAR of 0.41. At 300 square feet per person, the project proposes 18 people per acre, 3 percent lot coverage, and a FAR of 0.029 and therefore complies with the Safety Zone 2 limitations.

The ALUCP identifies manufacturing uses located within Safety Zone 5 as conditionally compatible with airport uses, provided that the project complies with a maximum intensity of 150 people per acre, a maximum lot coverage of 70 percent, and a maximum FAR of 1.03. At 300 square feet per person, the project proposes 123 people per acre, 10 percent lot coverage, and a FAR of 0.099 and therefore complies with the Safety Zone 5 limitations.

The ALUCP provides that ancillary uses of less than 10 percent of total floor area are not subject to compatibility standards. The office floor area totals 7 percent and is thus not subject to intensity limitations.

- (5) The proposed project is located within the overflight notification area but does not involve any new residential use subject to overflight notification requirements.

(6) Therefore, if the proposed project contains the above-required conditions, the proposed project would be consistent with the Oceanside Municipal Airport ALUCP.

(7) A determination of consistency is not a “project” as defined by the California Environmental Quality Act (CEQA), Cal. Pub. Res. Code §21065.

The information above will be reported to the ALUC to confirm this letter at its public meeting on November 7, 2024. The determination of consistency will be final as of that meeting, unless the ALUC finds cause to delay such action, in which case a determination will be rendered within 60 days of the date of this letter, to be confirmed by additional correspondence.

Any determination rendered by the ALUC is limited to the project plans and descriptions submitted with the application and is not transferable to any revision of this or any similar, future project involving a change in land use, in building or crane height, or in building area in excess of 10 percent (provided area increase does exceed ALUCP standards) of any prior ALUC determination. Any change or exceedance in these characteristics requires a new consistency determination prior to decision-making consideration by the local agency.

Please contact Sid Noyce at (619) 400-2419 or snoyce@san.org if you have any questions regarding this letter.

Yours truly,



Ralph Redman
Manager, Airport Planning

cc: Amy Gonzalez, SDCRAA General Counsel
Sjohnna Knack, SDCRAA Planning and Environmental Affairs



Application for Discretionary Permit

Development Services Department / Planning Division
 (760) 435-3520
 Oceanside Civic Center 300 North Coast Highway
 Oceanside, California 92054-2885

STAFF USE ONLY

ACCEPTED

BY

Please Print Or Type All Information

HEARING

PART I - APPLICANT INFORMATION

1. APPLICANT RPG Oceanside Eddy Jones Way Owner LLC		2. STATUS owner/applicant		GENERAL PLAN AMENDMENT	
3. ADDRESS 315 S. Coast Highway, Suite U-12 Encinitas, CA 92024		4. PHONE / FAX / E-mail 619-995-1107 adam@rafpg.com		MASTER/SP.PLAN	
5. APPLICANT'S REPRESENTATIVE (or person to be contacted for information during processing) The Lightfoot Planning Group, attn. Dan Niebaum				ZONE AMENDMENT	
6. ADDRESS 5900 Pasteur Ct. Suite 110 Carlsbad, CA 92008		7. PHONE / FAX / E-mail (760) 692-1924 phone dan@lightfootpg.com		TENTATIVE MAP	
				TENTATIVE PARCEL MAP	
				DEVELOPMENT PLAN	
				C.U.P.	
				VARIANCE	
				COASTAL PERMIT	

PART II - PROPERTY DESCRIPTION

8. LOCATION 250 Eddy Jones Way (between Benet Rd and Alex Rd terminus)			9. SIZE 31.79 ac		
10. GENERAL PLAN LI - Limited Industrial	11. ZONING IL - Limited Industrial	12. LAND USE Vacated Manufacturing Warehouse Facility	13. ASSESSOR'S PARCEL NUMBER 145-021-29, 30 & 32		
14. LATITUDE 33.13'		15. LONGITUDE 117.21'			

PART III - PROJECT DESCRIPTION

16. GENERAL PROJECT DESCRIPTION Warehouse / distribution facility with 526,280 sf of warehouse space and 40,000 sf of office areas. Site improvements include landscape amenity and biological buffers, vehicular circulation areas, with designated parking for autos and truck trailers.					
17. PROPOSED GENERAL PLAN no change	18. PROPOSED ZONING no change	19. PROPOSED LAND USE Warehouse/Shipping	20. NO. UNITS na	21. DENSITY na	
22. BUILDING SIZE 566,280 sf	23. PARKING SPACES 400 auto spaces 139 truck trailer stalls	24. % LANDSCAPE 20%	25. % LOT COVERAGE or FAR 39%		

PART IV - ATTACHMENTS

<input checked="" type="checkbox"/>	26. DESCRIPTION/JUSTIFICATION	<input checked="" type="checkbox"/>	27. LEGAL DESCRIPTION	<input checked="" type="checkbox"/>	28. TITLE REPORT
<input checked="" type="checkbox"/>	29. NOTIFICATION MAP & LABELS	<input checked="" type="checkbox"/>	30. ENVIRONMENTAL INFO FORM	<input checked="" type="checkbox"/>	31. PLOT PLANS
<input checked="" type="checkbox"/>	32. FLOOR PLANS AND ELEVATIONS		33. CERTIFICATION OF POSTING		34. OTHER (See attachment for required reports)

PART V - SIGNATURES

SIGNATURES FROM ALL OWNERS OF THE SUBJECT PROPERTY ARE NECESSARY BEFORE THE APPLICATION CAN BE ACCEPTED. IN THE CASE OF PARTNERSHIPS OR CORPORATIONS, THE GENERAL PARTNER OR CORPORATION OFFICER SO AUTHORIZED MAY SIGN. (ATTACH ADDITIONAL PAGES AS NECESSARY).

35. APPLICANT OR REPRESENTATIVE (Print): The Lightfoot Planning Group, Dan Niebaum	36. DATE 1/17/2022	37. OWNER (Print): RPG Oceanside Eddy Jones Way Owner LLC, Adam Robinson	38. DATE 1/4/2022
Sign: <i>Dan Niebaum</i>	Sign: <i>[Signature]</i>		

- I DECLARE UNDER PENALTY OF PERJURY THAT THE ABOVE INFORMATION IS TRUE AND CORRECT. FURTHER, I UNDERSTAND THAT SUBMITTING FALSE STATEMENTS OR INFORMATION IN THIS APPLICATION MAY CONSTITUTE FRAUD, PUNISHABLE IN CIVIL AND CRIMINAL PROCEEDINGS.

- I HAVE READ AND AGREE TO ABIDE BY THE CITY OF OCEANSIDE DEVELOPMENT SERVICES DEPARTMENT AND ECONOMIC AND COMMUNITY DEVELOPMENT DEPARTMENT POLICY NO. 2011-01/POLICY AND PROCEDURE FOR DEVELOPMENT DEPOSIT ACCOUNT ADMINISTRATION.

Warehouse, Manufacturing and Office Facility 250 Eddie Jones Way

Development Plan, Conditional Use Permit & Variance Description & Justification November 2024 The Multi-Building and Truck Bay Reduction Alternative

Introduction

This application proposes a Development Plan and Conditional Use Permit for a warehouse, manufacturing and office facility located on a 31.79-acre site at 250 Eddie Jones Way. The site is comprised of three separate parcels (APNs: 145-021-29, 30 & 32) that will be consolidated in conjunction with development of the property. The property was previously occupied by an approximate 172,300 industrial manufacturing facility and associated site improvements vacated in the summer of 2021. The project application consists of a Development Plan and Conditional Use Permit addressing development of the site for the proposed facility along with a Variance to allow small height increases for portions of a proposed flood wall designed to surround the property.

The project site is within the Airport Neighborhood Planning Area located immediately north of the Oceanside Municipal Airport (OMA) on the east side of Benet Road. The San Luis Rey River and recreational trail border the site on the north and vacant light industrial land is located to the east. The terminus of Alex Road also connects to the site at its northeast corner. The property is situated approximately 900 feet north of the Highway 76 corridor. The General Plan designation for the property is Light Industrial (LI) with the associated zoning category of Limited Industrial (IL).

The Development Plan application will address the complete redevelopment of the site area with the existing facility and site improvements to be demolished. The proposed warehouse, manufacturing and office facility is classified as a “*Wholesaling, Distribution, and Storage Facility*” use by the Oceanside Zoning Ordinance (OZO). Wholesaling, Distribution, and Storage Facilities over 50,000 square feet in floor area require approval of a Conditional Use Permit to be established in the IL zoning district pursuant to the OZO. Wholesaling, Distribution, and Storage Facilities with more than six (6) heavy trucks on the premises at one time are considered Trucking Terminals pursuant to the OZO. Trucking Terminals also require approval of a Conditional Use Permit to be established in the IL Zoning District.

Development Plan

In response to public comments received on the Draft EIR, the Multi-Building and Truck Bay Reduction Alternative has been included as part of the Final EIR. Under the Multi-Building and Truck Bay Reduction Alternative, the same project site and similar development footprint would be developed with industrial warehouse, manufacturing and office uses similar to the proposed project and consistent with the General Plan land use and zoning designation for the site. Within a building footprint of 491,582 SF, this alternative would develop four (4) separate buildings on-

site, instead of one building as proposed under the project. The total building square footage of this alternative would be 497,822 SF (inclusive of mezzanine areas), including 40,651 sf of office (ancillary) use, 334,275 sf of warehouse uses, and 122,896 sf of manufacturing uses. The total building area for building 1 would be 109,660 SF, the total building area for building 2 would be 132,600 SF, the total building area for building 3 would be 121,547 SF, and the total building area for building 4 would be 134,015 SF. This Multi-Building and Truck Bay Reduction Alternative would include 56 dock-high doors (truck bays for semi-truck use) and 590 parking stalls which include 22 ADA stalls and 90 EV stalls. This alternative design places the truck bays on the east/west sides of the buildings as opposed to facing the north and south sides of the site. All truck trailer parking around the perimeter of the site has been removed with this four-building alternative plan.

Similar to the proposed project, access to the project site would be maintained and improved as necessary, with existing access points from Alex Road at the northeast corner and Benet Road at the southwest corner. The Alex Road access would be limited to passenger vehicles. Heavy truck traffic would not use Alex Road and would be limited to the Benet Road access point. The Benet Road entry has also been redesigned to incorporate a dedicated right-turn lane into the project site to allow for queuing of truck traffic separate from the north-bound travel lane of Benet Road.

This project alternative has been designed to maintain a 100-foot buffer from the edge of the San Luis Rey River riparian habitat as designated in the City of Oceanside Subarea Plan (SAP). This buffer is located along the northern edge of the property. Although the San Luis Rey River Trail and embankment runs through the buffer area forming a hard boundary between this site and the river habitat areas, proposed project structures, parking and circulation areas have been designed and located to specifically avoid the biological and planning buffers. The buffer area is designated to be replanted with native coastal species.

This project alternative design also accounts for required building setbacks and airspace height limits established by the Oceanside Municipal Airport Land Use Compatibility Plan. The project alternative proposes four multi-tenant buildings centrally located on site in adherence to airport airspace constraints on height and location of buildings. The proposed buildings, parking and circulation areas are designed to avoid the Runway Protection Zone (RPZ) which extends across the southwest corner of the site. Details of the elevation design and airport runway setbacks are provided with the civil and architectural plans as submitted.

A summary of development standards and project information is provided in Table 1 to demonstrate compliance with the standards established for the site.

**Table 1 - Development Regulations Compliance Summary
IL - Limited Industrial Zone**

DEVELOPMENT STANDARD	Required/Allowed	Proposed	Notes
Minimum Area	20,000 sf / .45 ac	31.79 ac	Consolidates (3) parcels
Floor Area Ratio (FAR)	1.0 (max)	0.36	Based on 497,882 SF of building area
Setbacks:			
Front (west)	10 ft (min)	132 ft	From Benet Road (ROW)
Side (south)	0 ft	179 ft	From Airport boundary
Side (north)	0 ft	80 - 151 ft	From SLR Trail corridor
Rear (east)	0 ft	63 ft	From east property line
Building Height	80 feet (max)	24 - 45 feet	Meets airport 7:1 transitional surface
Site Landscaping	12% (min)	23%	Per OZO Sec. 1330
Tree Canopy Area	12% (min)	14% / Net 13%*	Per OZO Sec. 3049
Permeable Surface Area	22% (min)	25% / Net 26.6%*	Per OZO Sec. 3049

* Per OZO Section 3049, the project alternative is utilizing additional tree canopy area that exceeds minimum requirements to offset the Permeable Surface Area minimum requirement at a 1:1 ratio. This information is also noted on plan Landscape Plan Sheet L2.00 and Civil Plan Sheet 1.

Architecture

Similar to the proposed project, the buildings for this project alternative are designed in a modern light-industrial style incorporating concrete tilt-up panels with horizontal reveals, offset wall planes, significant window elements and façade details to create visual interest on all building elevations. The buildings feature a cohesive design while distinguishing office and warehouse components.

Complementary materials, finishes, and colors are coordinated across all building elevations. Neutral colors are featured with vertical and horizontal accent banding integrated with canopy elements to enhance and break up the wall expanses. Prominent elements incorporated into the design include the following:

- Painted tilt-up concrete panels with horizontal and vertical scoring
- Off-set and articulated horizontal concrete panels
- Color variations in panels to offset wall expanses
- Recessed concrete banding (accent color)
- Metal / concrete canopies (accent color)
- Large, tinted window elements with clear anodized aluminum mullions and horizontal structural elements
- Clerestory windows above service docks
- Concrete screen walls adjacent to loading bays

Façade design details are purposely incorporated to mitigate the visual appearance of building elements over 36' in height and greater than 200' in length. Color variations are proposed for portions of vertical panels located at upper wall areas near the roofline. These wall sections will feature a lighter 'off-white' panel color to complement and offset from the primary darker 'gray' background color featured on the building facades. Horizontal 'off-white' accent panel banding is also incorporated into these upper façade areas along with clerestory windows integrated with metal panel surrounds. Vertical undulations are also incorporated at the top of the parapet wall areas, adding reveals to provide façade interest. These coordinated design elements will serve to visually break the mass of the buildings as viewed from surrounding areas.

Landscaping

The conceptual landscape plan for this alternative, as with the proposed project, is designed to complement the project character, enhance community spaces and soften the overall site environment. Plant materials have been selected for their appropriateness to scale and suitability of use throughout the site. Tree and shrub plantings are designed to enhance key site and architectural elements and to screen the perimeter edges of the project area.

Landscaping along the Benet Road frontage and Alex Road connection will provide upgraded streetscapes and project site entries. Additional planting areas around the site perimeter and throughout on-site parking areas are designed to complement project architecture while exceeding tree canopy and impervious surface area requirements for the site. A variety of perimeter tree species are proposed including California Sycamore, Coast Live Oak, and Bay Laurel. Shade and accent tree species planned along perimeter and interior site areas include Desert Willow, Chilean Mesquite, California Laurel and Willow Acacia.

Landscape areas located along the northern site boundary are designed to buffer loading, parking and circulation activities on site. As with the proposed project, the quantity of tree plantings proposed along the north edge of the site has increased significantly with this alternative. A variety of perimeter, shade and accent trees (24" – 36" box) are planned along the entire north boundary to form a prominent screening feature for the site. Additional landscaped areas are designed near the north facades of each building to provide additional tree landscaping that soften and buffer views of the loading bays and associated activities. Existing tree and shrub vegetation growth within the San Luis Rey riverbed and along its northern bank also provides a natural landscape screen and serves to buffer views to the site from the residential neighborhood located to the north.

The project alternative maintains the 100-foot biological buffer as required from the edge of the San Luis Rey River riparian habitat. The buffer area is adjacent to the San Luis Rey River Trail embankment and is designated to be replanted with native coastal species.

A perimeter wall is also designed around the boundary of the entire site as a flood protection feature. This is planned as a solid decorative masonry block wall system that will complement the adjacent landscaping to serve as screening and buffer features around the perimeter of the site.

Access, Circulation and Parking

The project site is well served by the existing network of nearby roads. Primary access to the site is currently provided via Alex Road on the east with a secondary access point to Benet Road on the west. As with the proposed project, these access points will be improved to full commercial driveway standards and maintained with the proposed alternative. Tractor trailer truck ingress/egress will be designated for and limited to the Benet Road access drive. Benet Road connects directly to Highway 76 approximately 1,000 feet southwest of the site. Alex Road connects the site to Highway 76 via Foussat Street southeast of the site. Highway 76 provides a direct route to Interstate 5 located approximately 1.7 miles to the west.

The project alternative will link to the existing sidewalk system in the area and improve pedestrian connections to surrounding properties. The project alternative also proposes to construct a sidewalk along the project frontage on Benet Road from Eddie Jones Way north to the San Luis Rey River access path (a distance of approximately 600 feet).

Internal circulation through the development site will consist of a system of vehicular drives and pedestrian walkways providing access around each building and serving parking areas throughout the site. Drives surrounding the buildings are designed at a 35-foot minimum width to provide for required fire department access adjacent to the proposed 45-foot high structure.

Employee and visitor parking areas are distributed throughout the site to best meet the needs of the proposed office, warehouse and manufacturing uses. The project alternative four-building design includes a total of 56 dock-high doors (truck bays for semi-truck use) and 590 parking stalls, which include 22 ADA stalls and 90 EV stalls. This alternative design places the truck bays on the east/west sides of the buildings as opposed to facing the north and south sides of the site. All truck trailer parking around the perimeter of the site has been removed with this four-building alternative plan.

The project parking summary is provided in Table 2, as follows:

Table 2 – Project Parking Summary

TYPE OF USE	USE SQ. FTG.	PARKING REQUIRED		PARKING PROVIDED
		Rate by Use	Spaces per Use	
Warehouse	334,275 sf	1/1500 sf	223	
Manufacturing	122,896 sf	1/750 sf	164	
Office	40,651 sf	1/300 sf	136	
Total Auto Parking:			523	590
*Due to rounding, parking requirements calculated for each building may total up to 527 spaces combined.				

Site Engineering

The proposed project alternative will develop this site as a single parcel. Existing improvements will be demolished and the site will be redeveloped with the planned office warehouse distribution facility. The similar to the proposed project, this alternative development will generally maintain the existing grades and landform of the existing site. The San Luis Rey levee embankment and Benet Road right-of-way are elevated approximately 8 – 12 feet above the proposed site grades and building pad elevations maintaining a berm effect around the northern and western edges of the site. The preliminary grading design indicates that 60,000 cubic yards of raw cut and 40,000 cubic yards of raw fill are required for the site development, resulting in a net export amount of 20,000 cubic yards. This is necessary to allow for the proposed building pads, parking and circulation areas.

Although the site is adjacent to the San Luis Rey River levee, in consideration of the site's location within FEMA Zone A99, and the fact that raising the site above the Base Flood Elevation (BFE) is not feasible, the traditional process of filling the site and obtaining a LOMR-F is not the appropriate process. The project will be required to provide alternative floodplain mitigation measures to the satisfaction of both the City of Oceanside, as the floodplain manager, and FEMA. The incorporation of a flood wall system has been reviewed with City staff as an acceptable mitigation measure. As such and similar to the proposed project, this project alternative proposes to construct a flood wall around the perimeter of the site, wrapping the parking area, to flood-proof the property. The flood wall will be designed as a solid decorative masonry block wall system, to be constructed around the perimeter of the site's graded pad area. This wall will also serve to screen the on-site uses and parking areas of the facility. Through coordination with staff, an existing BFE of 34.0' is shown for the site and the flood wall design has been revised to provide a consistent top of wall elevation (TW) of 35.5' to account for a buffer assuming the performed hydraulic analysis results in an existing BFE higher than the 34.0' elevation considering impacts of the proposed flood wall system. Exterior facing wall elevations along the majority of the site perimeter will range in height from approximately 7.9' to 9.9' above the exterior grade. Interior facing wall elevations will extend up to approximately 9.5' in height. Exhibits of the proposed solid masonry block wall system are attached to this document.

Wet Utilities

Public water and sewer facilities were connected to the former building on-site and extend within the OMA area and Benet Road right-of-way. Similar to the proposed project, this project alternative will connect to the existing water and sewer utilities with on-site systems designed as required to fully serve the proposed development.

Storm Drain

Similar to the proposed project, the site design for this project alternative includes a new storm water conveyance system on-site consisting of ribbon gutters, curb and gutter, and a detention vault system. The vault system incorporates modular wetlands for treatment and a force main pump to convey storm water to the existing storm drain located in Benet Road.

Variance

Similar to the proposed project, the following variance to Section 3040 Fence and Wall Development Standards is proposed for this project alternative:

Allow portions of site boundary walls to exceed the noted height standard of 8' in non-front yard areas for industrially zoned property. Exterior facing wall elevations will range in height from approximately 7.9' to 9.9' above the exterior grade around the perimeter of the site. Small sections of the wall along the west and northwest boundary will range from 8.0' up to approximately 9.9'. Additionally, sections of the wall located along the east property line and small sections along the south property line will range from approximately 8.0' to 9.5' in height. Also, a small section of wall near the northeast corner of the site will range up to approximately 8.6' in height. Visibility of these wall sections is significantly minimized from the project exterior due to their specific locations on the project perimeter and from landscaping planned adjacent to the wall exterior.

Interior facing wall elevations will extend up to approximately 9.5' in height, although these walls sections would face interior areas of the site and would not typically be subject to the Section 3040 standards.

As previously described, these wall structures are designed as a flood wall system to provide alternative floodplain mitigation measures as raising the site above the Base Flood Elevation is not feasible. As such, the walls are an integral component of the site plan with the varying heights based on the topographic conditions surrounding the site. The flood wall system is designed to provide a consistent top of wall elevation (TW) of 35.5' as required to exceed the BFE of 34.0'. The site section exhibits on Sheets 8 & 9 of the Civil Plan Set present the various wall designs and topographic conditions around the perimeter of the site. Landscaping buffers consisting of tree and shrub plantings are incorporated along the exterior side of the walls on the north, south and west portions of the site. The wall along the eastern site boundary will be adjacent to vacant industrially zoned property.

Findings for Development Plan

1. That the site plan and physical design of the project as proposed is consistent with the purposes of the Zoning Ordinance.

The proposed project alternative is consistent with and in compliance with the Zoning Ordinance and appropriate for the site located in the Light Industrial zone.

2. That the Development Plan as proposed conforms to the General Plan of the city.

The General Plan designation for the property is Light Industrial (LI). The proposed project alternative development and industrial use is consistent with the General Plan.

3. That the area covered by the Development Plan can be adequately, reasonably and conveniently served by existing and planned public services, utilities and public facilities.

Public utilities and facilities are already located on this property and in the surrounding area serving the previous industrial development on site. They are adequate to serve the proposed project alternative development.

4. That the project as proposed is compatible with existing and potential development on adjoining properties or in the surrounding neighborhood.

The project is within an area designated for light industrial uses adjacent to the Oceanside Municipal Airport and near other light industrial properties. The proposed use is compatible with potential light industrial development and future airport uses.

5. That the site plan and physical design of the project is consistent with the policies contained within Section 1.24 and 1.25 of the Land Use Element of the General Plan, the Development Guidelines for Hillsides, and Section 3039 of this ordinance.

The property includes a graded industrial pad and does not have slopes that would be subject to the Hillside Ordinance.

Findings for Conditional Use Permits

1. That the proposed location of this use is in accord with the objectives of this ordinance and the purposes of the district in which the site is located.

The purpose of the Conditional Use Permit is for Wholesaling, Distribution, and Storage facilities over 50,000 square feet in floor area and for a Trucking Terminal (facilities with more than six (6) heavy trucks on the premises at one time). The site is located within a Light Industrial zone, and the proposed uses are consistent with that zone.

2. That the proposed location of the conditional use and the proposed conditions under which it would be operated or maintained will be consistent with the General Plan; will not be detrimental to the public health, safety or welfare of persons residing or working in or adjacent to the neighborhood of such use; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the city.

The General Plan designation for the property is Light Industrial (LI). The proposed industrial use is consistent with the General Plan, and will not be detrimental to the health, safety and welfare of people working in the area, nor to the City overall. The Oceanside Municipal Airport is located adjacent to the property on the south, while the San Luis Rey River provides a significant buffer to the nearest residential land uses located to the north.

3. That the proposed conditional use will comply with the provisions of this ordinance, including any specific condition required for the proposed conditional use in the district in which it would be located.

The proposed project alternative, including its requested CUPs, will comply with the CUP ordinance and any conditions placed on the project development.

Findings for Variance

The City of Oceanside Zoning Ordinance stipulates that three specific findings must be made before a Variance can be approved. This proposal meets those conditions as follows:

1. That because of special circumstances or conditions applicable to the development site – including size, shape, topography, location or surroundings – strict application of the requirements of this ordinance deprive such property of privileges enjoyed by other property in the vicinity and under identical zoning classification.

Although the site is adjacent to the San Luis Rey River levee, in consideration of the site's location within FEMA Zone A99, and the fact that raising the site above the Base Flood Elevation (BFE) is not feasible, the traditional process of filling the site and obtaining a LOMR-F is not the appropriate process. The proposed wall structures have been proposed in coordination with city staff as a flood wall system to provide alternative floodplain mitigation measures. As such, the walls are an integral component of the site plan with the varying heights based on the topographic conditions surrounding the site. The flood wall system is designed to provide a consistent top of wall elevation (TW) of 35.5' as required to exceed the BFE of 34.0'. As designed, the walls will provide necessary flood protection and allow for proper grading of the site.

2. That granting the application will not be detrimental or injurious to property or improvements in the vicinity of the development site, or to the public health, safety or general welfare.

Granting the application will not be detrimental to property in the vicinity of the development site because the walls have been designed specifically for the subject property by a licensed engineer. Landscaping has been carefully planned to provide adequate screening of the walls with various tree, shrub, and vines with the wall as an aesthetic backdrop. Visibility of these wall sections is significantly minimized from the project exterior due to their specific locations on the project perimeter. The granting of the variance will allow the safe development of the site and would not impact the health, safety or welfare of nearby residents.

3. That granting the application is consistent with the purposes of this ordinance and will not constitute a grant of special privilege inconsistent with limitations on other properties in the vicinity and in the same zoning district.

The proposed project alternative is consistent with the purposes of the Oceanside Zoning Ordinance and will complement the overall site surroundings and provide a viable light industrial development for the city. Visibility of the wall sections is significantly minimized from the project exterior. The variance will allow for the necessary flood protection for development on this property. The flood wall system would be a viable flood mitigation alternative for other industrially zoned properties in the area and located in the same flood zone.

Wall Exhibits: Solid Masonry Block Wall System



Wall Exhibits: Solid Masonry Block Wall System



EXHIBIT A LEGAL DESCRIPTION

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF OCEANSIDE, IN THE COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL 1 (APN: 145-021-32-00):

THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHEAST QUARTER, TOGETHER WITH A PORTION OF THE NORTH HALF OF THE SOUTHWEST QUARTER DESCRIBED AS PARCEL 1 IN A GRANT DEED RECORDED DECEMBER 28, 1995 AS [INSTRUMENT NO. 1995-0592152](#) OF OFFICIAL RECORDS, ALL WITHIN SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4A OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS [INSTRUMENT NO. 1999-0796385](#) OF OFFICIAL RECORDS, DESCRIBED AS A WHOLE AS FOLLOWS:

BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHEAST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF [SURVEY NO. 13494](#), FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 479.25 FEET; THENCE NORTH 09°26'35" WEST 906.49 FEET TO THE SOUTHERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS [INSTRUMENT NO. 1994-0604672](#) OF OFFICIAL RECORDS OF SAN DIEGO COUNTY, AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 941.77 FEET TO WHICH A RADIAL LINE BEARS NORTH 28°02'18" WEST; THENCE SOUTHWESTERLY 48.72 FEET ALONG SAID CURVE AND SOUTHERLY LINE THROUGH A CENTRAL ANGLE OF 02°57'51"; THENCE SOUTH 58°59'51" WEST 155.19 FEET; THENCE SOUTH 54°01'29" WEST 46.25 FEET TO THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 1703.94 FEET TO WHICH A RADIAL LINE BEARS NORTH 40°18'45" WEST; THENCE SOUTHWESTERLY 144.55 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 04°51'38" TO SAID WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 13; THENCE SOUTH 00°23'25" WEST 9.15 FEET ALONG SAID WEST LINE TO THE MOST NORTHERLY CORNER OF SAID PARCEL 1 DESCRIBED IN DOCUMENT RECORDED DECEMBER 28, 1995 AS [INSTRUMENT NO. 1995-0592152](#) OF OFFICIAL RECORDS, AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHEASTERLY HAVING A RADIUS OF 1235.00 FEET TO WHICH A RADIAL LINE BEARS NORTH 44°43'52" WEST; THENCE, LEAVING SAID WEST LINE AND FOLLOWING ALONG THE NORTHWESTERLY LINE OF SAID PARCEL 1, SOUTHWESTERLY 170.73 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 07°55'15" TO THE CENTERLINE OF THE BENET ROAD, 60 FEET WIDE, DESCRIBED AS PARCEL C IN RESOLUTION NO. R94-189 OF THE CITY COUNCIL OF THE CITY OF OCEANSIDE RECORDED MAY 08, 1995 AS [INSTRUMENT NO. 1995-0192658](#) OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE LEAVING SAID NORTHWESTERLY LINE OF PARCEL 1 AND FOLLOWING ALONG SAID CENTERLINE OF BENET ROAD, SOUTH 11°45'56" EAST 460.14 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE WESTERLY HAVING A RADIUS OF 200.00 FEET; THENCE SOUTHERLY 77.47 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 02°13'10"; THENCE SOUTH 00°23'25" WEST 79.75 FEET TO THE POINT OF BEGINNING.

PARCEL 2 (APN: 145-021-29-00):

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EXHIBIT A
(Continued)

THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4B OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS [INSTRUMENT NO. 1999-0796385](#) OF OFFICIAL RECORDS, DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHEAST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF [SURVEY NO. 13494](#), FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 479.25 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 80°33'25" EAST 594.02 FEET; THENCE NORTH 09°26'35" WEST 944.68 FEET TO THE SOUTHERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS [INSTRUMENT NO. 1994-0604672](#) OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE, FOLLOWING ALONG SAID SOUTHERLY LINE, SOUTH 85°34'12" WEST 169.60 FEET; THENCE SOUTH 78°57'38" WEST 151.05 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHERLY HAVING A RADIUS OF 941.77 FEET; THENCE WESTERLY 279.41 FEET ALONG SAID CURVE THROUGH A CENTRAL ANGLE OF 16°59'56" TO A POINT WHICH BEARS NORTH 09°26'35" WEST FROM THE TRUE POINT OF BEGINNING; THENCE, LEAVING SAID SOUTHERLY LINE, SOUTH 09°26'35" EAST 906.49 FEET TO THE TRUE POINT OF BEGINNING.

PARCEL 3 (APN: 145-021-30-00):

THAT PORTION OF THE SOUTH HALF OF THE NORTHEAST QUARTER AND THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, BEING PARCEL 4C OF CERTIFICATE OF COMPLIANCE RECORDED DECEMBER 07, 1999 AS [INSTRUMENT NO. 1999-0796385](#) OF OFFICIAL RECORDS, DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHEAST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF [SURVEY NO. 13494](#), FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 1073.27 FEET TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING NORTH 80°33'25" EAST 484.51 FEET; THENCE NORTH 09°26'35" WEST 883.65 FEET TO THE SOUTHERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH WAS RECORDED OCTOBER 14, 1994 AS [INSTRUMENT NO. 1994-0604672](#) OF OFFICIAL RECORDS OF SAN DIEGO COUNTY, AND THE BEGINNING OF A NON-TANGENT CURVE CONCAVE SOUTHERLY HAVING A RADIUS OF 824.19 FEET TO WHICH RADIAL LINE BEARS NORTH 07°43'15" EAST; THENCE WESTERLY 171.75 FEET ALONG SAID CURVE AND SOUTHERLY LINE THROUGH A CENTRAL ANGLE OF 11°56'24"; THENCE SOUTH 85°34'12" WEST 317.54 FEET TO A POINT WHICH BEARS NORTH 09°26'35" WEST FROM THE TRUE POINT

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EXHIBIT A
(Continued)

OF BEGINNING; THENCE, LEAVING SAID SOUTHERLY LINE, SOUTH 09°26'35" EAST 944.68 FEET TO THE TRUE POINT OF BEGINNING.

PARCEL 4:

EXCLUSIVE EASEMENTS RESERVED IN GRANT DEED RECORDED OCTOBER 14, 1999 AS [INSTRUMENT NO. 1999-0694332](#) OF OFFICIAL RECORDS, UPON AND SUBJECT TO ALL THE PROVISIONS CONTAINED THEREIN, OVER THE FOLLOWING DESCRIBED PROPERTY:

THAT PORTION OF THE NORTH HALF OF THE SOUTHEAST QUARTER OF SECTION 13, TOWNSHIP 11 SOUTH, RANGE 5 WEST, SAN BERNARDINO MERIDIAN, IN THE CITY OF OCEANSIDE, COUNTY OF SAN DIEGO, STATE OF CALIFORNIA, ACCORDING TO OFFICIAL PLAT THEREOF, DESCRIBED AS FOLLOWS:

BEGINNING AT A POINT IN THE WEST LINE OF SAID SOUTHEAST QUARTER DISTANT SOUTH 00°23'25" WEST 794.57 FEET FROM THE CENTER OF SAID SECTION 13 SHOWN ON RECORD OF [SURVEY NO. 13494](#), FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAN DIEGO COUNTY ON OCTOBER 31, 1991; THENCE NORTH 80°33'25" EAST 2384.40 FEET TO THE WESTERLY LINE OF PARCEL 1 DESCRIBED IN A FINAL ORDER OF CONDEMNATION ISSUED OUT OF THE SUPERIOR COURT OF THE STATE OF CALIFORNIA, IN AND FOR THE COUNTY OF SAN DIEGO, NORTH COUNTY BRANCH, AS CASE NO. N 51717, A CERTIFIED COPY OF WHICH RECORDED OCTOBER 14, 1994 AS [INSTRUMENT NO. 1994-0604672](#) OF OFFICIAL RECORDS OF SAN DIEGO COUNTY; THENCE SOUTH 02°37'51" WEST 53.18 FEET ALONG SAID WESTERLY LINE TO THE SOUTHERLY LINE OF THAT CERTAIN PARCEL OF LAND DESCRIBED IN A GRANT DEED RECORDED SEPTEMBER 01, 1966 AS [INSTRUMENT NO. 142881](#) OF OFFICIAL RECORDS; THENCE SOUTH 80°33'25" WEST 2382.29 FEET ALONG SAID SOUTHERLY LINE AND THE WESTERLY PROLONGATION THEREOF, AS DESCRIBED IN A DEED TO THE CITY OF OCEANSIDE RECORDED JUNE 04, 1962 AS [INSTRUMENT NO. 94349](#) OF OFFICIAL RECORDS, TO SAID WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 13; THENCE NORTH 00°23'25" EAST 52.78 FEET TO THE POINT OF BEGINNING.

This page is only a part of a 2016 ALTA® Commitment for Title Insurance issued by Chicago Title Insurance Company. This Commitment is not valid without the Notice; the Commitment to Issue Policy; the Commitment Conditions; Schedule A; Schedule B, Part I—Requirements; and Schedule B, Part II—Exceptions; and a counter-signature by the Company or its issuing agent that may be in electronic form.



Robert Dmohowski

From: TJ&Kim Treasure <treasurefam121@gmail.com>
Sent: Friday, February 7, 2025 5:45 PM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Rick Robinson; Eric Joyce; figueroa@oceansideca.org; Peter Weiss; City Manager
Subject: Opposition for Zoning Exception for Eddie Jones Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City of Oceanside Planning Commission and Whom It May Concern,

We are residents of Oceanside, living in the community near Benet Road adjacent to the San Luis Rey River. We are reaching out on behalf of our neighborhood to express our strong opposition to the proposed Eddie Jones Development.

Our community values the natural beauty of the San Luis Rey River, the bike path, and the historic Abbey. These features make our area a cherished destination for both residents and visitors. Allowing large industrial developments in this location would disrupt the character of our neighborhood and negatively impact the surrounding environment. We urge you to uphold the existing zoning regulations and deny any zoning exceptions for this project.

This development raises serious concerns regarding increased traffic, pollution, health risks, safety, and emergency service response times. After speaking with many of our neighbors, we have found unanimous opposition to this project moving forward.

We respectfully ask that future developments align with the needs and well-being of the community, preserving the charm and livability of our neighborhood.

Sincerely,

TJ and Kimberly Treasure

& Family – Summer & Lexi

Robert Dmohowski

From: Bobbie Alvarez <balvz1@gmail.com>
Sent: Saturday, February 8, 2025 10:08 AM
To: Robert Dmohowski; esanchez@oceansideca.or; rwrobinson@oceansideca.or; Peter Weiss; znavarr@oceansideca.or; ejoyce@oceansideca.or; jimmyfigueroa@oceansideca.or; citymanager@oceansideca.or
Subject: The Eddie Jones warehouse proposal

Warning: External Source

To whom it may concern, I am writing to let you know that I highly oppose this building proposal!!!! This will severely impact the already heavy traffic on the 76 and surrounding roads, also it will have a negative impact on exit routes in case of an emergency such as a fire or earthquake. I do not see the need for this in our community. There are much better places for this to be that are more rural and not in this highly populated area. It's bad enough that every open grass space is being used for building homes that are already affecting the traffic in this area and then to add in all the semi trucks and workers cars it's just bad all around, not to mention the environmental impact this will have. I OPPOSE THIS and I hope you will listen to the people and not be so greedy...

Sincerely
a concerned citizen.

Robert Dmohowski

From: memaeeee@aol.com
Sent: Saturday, February 8, 2025 1:24 PM
To: Robert Dmohowski; City Council; Zeb Navarro; City Clerk; Planning-Planning Commission; Esther Sanchez; rwobinson@oceasideca.org; Peter Weiss; Eric Joyce; Jimmy Figueroa
Subject: Eddie Jones Project

Warning: External Source

Mission Statement: "The City of Oceanside's mission is to enhance the quality of life through outstanding service to its diverse community"

How does the Eddie Jones huge warehouse/distribution center enhance the quality of life for its residents? It does not. It actually does the opposite by polluting our otherwise clean air we currently breathe. Adding employees cars and numerous trucks, forklifts beeping day and night, and other equipment used for this type of business to function, will fill our air with noise and pollutants. It is a known fact that semi trucks spew pollutants, and especially at risk are our children. "No significant impact?" Almost laughable. Adding hundreds of cars and trucks will not impact our streets, our air, our noise? Common sense does not figure in to this? How can you close your eyes to this? Is the developers heavy wallet enough to persuade you that no harm will come from this project to those of us who live in the area?

The developer has suggested they may add lights to the skate park and/or provide a picnic area off the bike trail. (the homeless would love that) I see this as the developer trying to 'put frosting on the cake', but the 'cake' is made of chopped liver, so it is still distasteful. No thanks. That does not address the concerns of thousands of residents. It is a weak gesture of trying to pacify those of us who oppose this horrific warehouse/distribution center, call it what you will....it does not fit in this community.

Limiting 6 trucks per parcel was set up for a reason. (vastly different from 19 trucks per parcel the developer wants) What has changed since this zoning was put in place? Nothing. 18 trucks would be the max this developer is allowed. He did his due diligence before investing in this property and was aware of the limitations. Its simple: NO to the CUP. The zoning was set in place for a reason. What reason have you for changing it? If you consider the residents of Oceanside, you will not allow this project to be built in that location.

**Thank you for your time. I am hoping you actually read my email.
Carol Broeland
Airport resident**

Robert Dmohowski

From: Judy Sanders <judysanders7@gmail.com>
Sent: Saturday, February 8, 2025 9:35 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Cc: OceansideSpeaksOut@gmail.com
Subject: Please vote NO

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Aloha from an Oceanside Airport long time voting resident,

I am writing to implore you to VOTE NO ON 2/10 on the Eddie Jones project in our beloved neighborhood.

This project will greatly diminish our daily quality of life, not just for me and my family, but for all of us that live in this neighborhood. And even those that travel daily on the 76 freeway. Please consider all of us in your decision on the 10th. Thank you very much.

Sincerely,
A very anxious resident.
Judy Sanders

They may forget what you said

But they will never forget how you made them feel

Smile & be kind... it cost you nothing

Judy Sanders
760-498-8008

Robert Dmohowski

From: Sherry Kaye <sherrykaye1@hotmail.com>
Sent: Saturday, February 8, 2025 2:16 PM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Cc: Zeb Navarro
Subject: Fwd: [#XN6098712] Message from California Brisas Homeowners Association - RE: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Please do NOT allow a zoning exception to the developer.

Sent from my iPhone

Begin forwarded message:

From: Avalon Management Group <wecare@avalonweb.com>
Date: February 3, 2025 at 10:40:48 AM PST
To: sherrykaye1@hotmail.com
Subject: [#XN6098712] Message from California Brisas Homeowners Association - RE: Eddie Jones Warehouse

WHAT'S HAPPENING:

The developer of the Eddie Jones Warehouse Project has proposed three different building configurations for the land between the airport runway and the river. These configuration designs include 57 - 114 Diesel Truck docks.

OUR GOAL:

The existing zoning for this land only allows a handful of Diesel Truck docks. As residents in the community of homes adjacent and upwind of this development, we request that the developer be limited to the existing zoning regulation.

WHAT CAN YOU DO:

The City of Oceanside's Planning Commission will be meeting on February 10th to decide the future of this piece of land.

Before Feb 10 —> Email the list of City officials (see below), requesting that they do NOT allow a zoning exception to the developer. (**Please be sure to copy the City Clerk on any email you send.)

On Feb 10 —> Attend City of Oceanside's Planning Commission's Meeting at 5:30pm. Location is City Council Chambers, City Hall North 2nd Floor. (**Please wear blue

colored shirt.)

Between Feb 11-19 —> If the City's Commission approves one of the developer's proposals, please SIGN the "Appeal Letter" that will be circulated to residents within 1500 feet of the Project, and due back to the City on Feb 20th. (**You will be notified if you are a resident within 1500 feet of the project.)

THANK YOU FOR YOUR ENGAGEMENT AT THIS CRITICAL STAGE OF THE PROJECT!

Here's the list of City officials to email ASAP.
(** Yes, just copy them all. They expect it...)

Oceanside City Principal Planner
Rob Dmohowski
(760) 435-3563
rdmohowski@oceansideca.org

Oceanside City Clerk
Zebb Navarro, Ed. D.
(760) 435-3010
znavarro@oceansideca.org
Oceanside Mayor
Esther Sanchez
(760) 435-3057
esanchez@oceansideca.org

Deputy Mayor- District 1
Eric Joyce
(760) 435-3032
ejoyce@oceansideca.org

Council member- District 2
Richard (Rick) Robinson
(760) 435-3033
rwrobinson@oceansideca.org

Council member- District 3
Jimmy Figueroa
(760) 435-3047
jfigueroa@oceansideca.org

Council member- District 4
Peter Weiss
(760) 435-3066
pweiss@oceansideca.org

Oceanside City Manager
Jonathan Borrego
(760) 435-3065
CityManager@oceansideca.org

Robert Dmohowski

From: ANDREA TARGHETTA <atarghetta@comcast.net>
Sent: Sunday, February 9, 2025 2:39 PM
To: Robert Dmohowski
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Members of the Oceanside City Council:

I live in the Oceanside Airport area and am very concerned with the Eddie Jones Project having a detrimental effect on our community. There are traffic concerns at the intersection of Benet Road and SR-76. With heavy trucks in our neighborhood, that intersection can easily back up. The fumes alone cause distress, but also in emergencies, it stops traffic from leaving the area promptly.

Please consider a smaller warehouse or better yet, consider another business on that property. Thank you for your attention.

Andrea Targhetta

atarghetta@comcast.net

505-280-8892

“It is only with the heart that one can see rightly; what is essential is invisible to the eye.”

— Antoine de Saint-Exupéry, [The Little Prince](#)

Robert Dmohowski

From: funjewels4u <funjewels4u@cox.net>
Sent: Sunday, February 9, 2025 9:22 PM
To: Esther Sanchez; Robert Dmohowski; Rick Robinson; Peter Weiss; Zeb Navarro; Eric Joyce; Ryan Keim; City Manager
Cc: OceansideSpeaksOut@gmail.com
Subject: NO to Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hi :-)

I'm emailing you all to let you know that I am against the Eddie Jones Project for numerous reasons. Here's some ..

1) First & foremost, the safety of the residents, which I am one, that if there were an emergency of some sort, like a fire, we would be trapped. With only having 2 ways out of our community we would be unable to leave with more cars & semi-trucks that this project will add and the already approved Wave Park. The exit from the Wave Park will have a turn around that will slow things down and Eddie Jones is only one lane road to leave. Please consider us, just look at what happened at the Palisades & Eaton fires, we have many homeless in our area !!

2) Our City's Vision, why on earth would you put a bunch of semi trucks so close to the Ocean & a Wave Park ?? It's going to cause issues, something of this size etc. should be put in the Industrial Park that was made for this not in an area that is next to a river, almost to the beach, and a high-end resort with a wave pool !!

3) The environment, we're adding more noise, diesel pollution, wear & tear on a tiny road besides our big ones, It's not going to be good for the air, which then will go into the river which is so close & a Wave Pool, not good for animals in the area and us humans that live here or will be vacationing close by .. Who would want to vacation/camp when you're hearing semi-truck noise ?? Common sense says this is not smart.

4) Traffic will be horrible, it already is, so why do this ?? We back up on Benet just to get out in the mornings, there are always accidents happening on the 76 here between Benet & Foussatt and turning into our tract from the 76 on Benet you can be lined up & not make the turn so adding more cars & semi-trucks from the workers on a one lane road just is insane, which brings me back to point #1 - Safety

Please do not let this project pass, don't sell Oceanside out just for tax revenue !!

Sincerely,
Mary Hanson
3281 Toopal Dr O'side
760-672-6659

Robert Dmohowski

From: Megan Suster <megansuster@gmail.com>
Sent: Sunday, February 9, 2025 4:10 PM
To: Robert Dmohowski; City Council
Cc: oceansidespeaksout@gmail.com
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Good afternoon,

My name is Megan Suster and I live on Carolyn Circle--my backyard is very close to the intersection of Fousat and Mission Avenue.

I am writing because I am extremely concerned about the Eddie Jones Warehouse Project and the impact it will have on our immediate neighborhoods, as well as the Oceanside community at large.

There is extensive research showing the detrimental health impacts of allowing warehouses so close to neighborhoods, particularly neighborhoods that are predominantly families of color, who have likely been in the homes for generations. Our home has been in the family since the early 1970s, and we have watched the valley of Oceanside thrive and grow to become a dynamic and integral part of the city. Our Latino family patriarch settled here after serving in the Marines for 25 years, and was proud to become a homeowner so close to Camp Pendleton. It saddens me that we are now having to fight for our health and safety in the literal house we've made home for over 40 years.

Recently, the [Milken Institute School of Public Health](#) published that "A first-of-a-kind study now shows that people living in communities located next to these large warehouses are exposed to 20% more of a traffic-related air pollutant that can lead to asthma and other life-threatening health conditions." Most relevant to our family and neighborhood, "Communities with large racial and ethnic minority populations are often located near warehouses and thus are inhaling more nitrogen dioxide and other pollutants. For example, this study found that the proportion of Hispanic and Asian people living close to the largest clusters of warehouses is about 250% higher than the average nationwide." This is extremely concerning given the racial diversity of our neighborhood, and how our community health will be deeply harmed for generations to come by the approval of this warehouse project.

In addition to our health, we are also concerned about the dramatically increased noise level and the dangerous uptick in traffic. Many people already treat the 76 as a major freeway, often going 15-20 miles over the speed limit. When the 76 is congested, Mission Avenue becomes the alternate route, making it dangerous for pedestrians and drivers alike. This project will only further exacerbate existing congestion and traffic issues in the area. A recent study from the [Journal of Transport Geography](#) explains "Living in proximity to warehouses bring other negative effects as well, including traffic congestion, traffic collisions, road wear-and-tear, light, and noise pollution." We are worried since we are so close to the intersection of Fousat and Mission, that we will be exposed to idling trucks 24 hours a day, increasing the noise and light pollution of our neighborhood.

These are only a few of the many concerns related to this project, and but two of several studies easily available online. I would like to see studies like these play a larger role in informing the decision making around this project. It is not only obvious, but scientifically proven, that warehouses like the Eddie Jones Project HARM neighborhoods and communities, destroying local infrastructure and dramatically decreasing the quality of life for residents.

I appreciate your time in reading this message, and thank you for your continued willingness to listen to community voices. I am proud to be from Oceanside, and BEG you to put your residents first, and reject this project.

Megan Suster
Resident of Carolyn Circle

Robert Dmohowski

From: Brandyne <aloha8scooby@yahoo.com>
Sent: Monday, February 10, 2025 4:30 PM
To: Robert Dmohowski; Planning-Planning Commission
Subject: Re: Notice of Planning Commission Hearing - Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Please do not approve the Conditional Use Permit for more than the six truck bays. There is a reason that rule was established and this project should not be entitled to more than the six bays. That would decrease quality of life for the residents, just to allow a developer to increase their profits.

On Monday, January 27, 2025 at 03:10:40 PM PST, Robert Dmohowski <rdmohowski@oceansideca.org> wrote:

NOTICE OF PLANNING COMMISSION PUBLIC HEARING, CITY OF OCEANSIDE

On Monday, February 10, 2025 at 6:00 p.m. at City Council Chambers, 300 North Coast Hwy., the Planning Commission will conduct a Public Hearing on the following application:

Consideration of a DEVELOPMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001), and VARIANCE (V22-00001) to allow the construction of a warehouse, manufacturing, and office facility on a 31.79-acre site located at 250 Eddie Jones Way (APNs: 145-021-29, 30, & 32). The project being considered represents the Multi-Building and Truck Bay Reduction Alternative (Section 8.4.4) provided in the Final Environmental Impact Report (FEIR). The project consists of four separate buildings ranging in size from 109,660 square feet to 134,015 square feet for a cumulative total of 497,822 square feet of building area. A total of 56 truck bays are divided among the four buildings and 593 parking stalls are provided onsite for employees and customers. Access to the site is provided from both Benet Road and Alex Road with heavy truck traffic limited to the entrance on Benet Road. The project site is currently vacant land that was previously occupied by a 172,300 square-foot manufacturing facility that was demolished in 2022. A Conditional Use Permit is required per Article 13 of the Zoning Ordinance to allow an industrial facility for distribution and storage use to exceed 50,000 square-feet and to allow truck terminals with more than six heavy trucks on the premises at one time. A variance is requested to allow a proposed flood wall to exceed the maximum allowable wall height of eight (8) feet. The project site is within the Airport Neighborhood Planning Area and has a General Plan land use designation of Light Industrial (LI) and a Zoning Designation of Limited Industrial (IL). – **EDDIE JONES WAREHOUSE, MANUFACTURING, AND OFFICE FACILITY PROJECT – APPLICANT: RPG OCEANSIDE EDDY JONES WAY OWNER LLC**

Environmental Determination:

In accordance with the California Environmental Quality Act (CEQA), an Environmental Impact Report (EIR) has been prepared for this project (SCH No: 2022070365). A Draft EIR was made available for public review between October 26, 2023 and December 29, 2023. The Final EIR, including responses to public comments, a mitigation monitoring and reporting program, and findings of fact are available on the City's website at:

<https://www.ci.oceanside.ca.us/government/development-services/planning/environmental-documents-ceqa>

You are being notified of this hearing because you are listed as either an owner of real property within 1,500 feet or an occupant residing within 100 feet of the exterior boundary of the site. This notice invites you to provide testimony at the forthcoming public hearing. You may also submit written comments on the application prior to the public hearing; these comments will be provided to the Planning Commission and entered into the public record.

For information on attending or providing comments, please visit the City's Website at <http://records.ci.oceanside.ca.us/gov/dev/planning/agendas.asp>

Project materials and environmental documentation are available for review at the following website:

<https://crw.cityofocceanside.com/etrakit3/Search/project.aspx> please utilize the project number listed above to identify the record for the subject project.

The meeting agenda and staff report will be available a minimum of 72 hours before the meeting on the City's Website <http://records.ci.oceanside.ca.us/gov/dev/planning/agendas.asp>. For more information, please contact Principal Planner Rob Dmohowski at (760) 435-3563 or via email at rdmohowski@oceansideca.org

Should you wish to challenge this project at some future time, you may be limited to raising only those issues you or someone else raised at the public hearing, or in written correspondence delivered to the Planning Commission at or prior to the public hearing.



Robert Dmohowski

From: CAROL BROELAND <memaeae@aol.com>
Sent: Monday, February 10, 2025 4:16 PM
To: Planning-Planning Commission
Subject: Eddie Jones

Warning: External Source

Based in RPGs numbers, this project would only generate \$72,103 annually. Why would you want this project in our neighborhood? There are SO many reasons to turn them away. The zoning would not allow'
Sent from my iPhone

LAW OFFICE OF DONALD B. MOONEY

417 Mace Boulevard, Suite J-334

Davis, CA 95618

530-304-2424

dbmooney@dcn.org

February 10, 2025

VIA EMAIL

planningcommission@oceansideca.org

rdmohowski@oceansideca.org

Planning Commission
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054

***Re: Final Environmental Impact Report for Eddie Jones Warehouse,
Manufacturing and Distribution Facility Project (SCH No. 2022070365)***

Dear Commissioners:

The following comments on the Final Environmental Impact Report (“FEIR”) for the Eddie Jones Warehouse, Manufacturing and Distribution Facility Project (SCH No. 2022070365) are submitted on behalf of the Eddie Jones Go Fund Me Trust. As an initial matter, the Eddie Jones Go Fund Me Trust objects to the FEIR and the Project as the FEIR fails to meet the requirements of the California Environmental Quality Act (“CEQA”) Public Resources Code, section 21000 *et seq.* Thus, the FEIR is legally inadequate.

First, based upon the attached February 6, 2025 Technical Memorandum from Environmental Permitting Services it is clear that the FEIR failed to provide an adequate response to the expert comments regarding short-term emission rates of NOx, PM10, and PM2.5. CEQA requires that the FEIR must describe the disposition of any “significant environmental issues” raised by commenters. (Pub. Resources Code, § 21091(d)(2)(B); CEQA Guidelines, §§ 15088(c), 15132(d), 15204(a).) In *City of Long Beach v. Los Angeles Unified School District* (2009) 176 Cal.App.4th 889, 904, the court discussed the importance of an adequate response to comments.

The requirement of a detailed written response to comments helps to ensure that the lead agency will fully consider the environmental consequences of a decision before it is made, that the decision is well informed and open to public scrutiny, and that public participation in the environmental review process is meaningful.

Thus, the FEIR’s failure to adequately address the comments regarding short-term emission rates means that the City has failed to fully consider and address the air quality impacts associated with the proposed Project.

The Environmental Permitting Services' Technical Memorandum also indicates that the FEIR fails to mitigate the significant impacts associated with the short-term emission rates of NOx, PM10 and PM2.5. One of the fundamental purposes and requirements of CEQA is to identify ways in which a project's significant environmental impacts can be mitigated. (Pub. Resources Code, § 21002.1(a), 21081(a)(1).) Thus, CEQA requires that an EIR discuss mitigation measures that can minimize the project's significant environmental effects. (Pub. Resources Code, §§ 21061, 21100(b)(3), CEQA Guidelines, §§ 15126.4.). In the present matter, the FEIR fails to adequately disclose, analyze and mitigated the potential significant impacts air quality associated with the short-term emissions. As such, the FEIR fails to meet CEQA's legal requirements.

As discussed above, and in the previous comments, the EIRR fails to meet CEQA's most basic requirements of informed decision making and informed public participation. (*See Sierra Club v. County of Fresno, supra*, 6 Cal.5th at 513.) Thus, the FEIR is legally inadequate.

Sincerely,

A handwritten signature in blue ink that reads "Donald B. Mooney". The signature is fluid and cursive, with the first letters of the first and last names being capitalized and prominent.

Donald B. Mooney
Attorney for Eddie Jones
Go Fund Me Trust

Attachments

cc: Client
Mr. Rob Dmohowski (rdmohowski@oceansideca.org)



TECHNICAL MEMORANDUM

To: Justin Floyd
Eddie Jones GoFundMe Trust
Oceanside, California

Date: February 6, 2025

From: Ray Kapahi *RK*
Tel: 916-687-8352
E-Mail: ray.kapahi@gmail.com

Subject: Review of Project and Cumulative Impacts to Air Quality and Public Health
In the Final Environmental Impact Report (FEIR) for the Proposed Eddie Jones
Warehouse, Oceanside, California

I have reviewed the FIER dated November 2024 for the Eddie Jones Warehouse project. The focus of my review was to determine if comments in my previous (December 20, 2023) Technical Memorandum (Memo) were addressed. Specifically, that Memo noted that reliance solely on daily emissions does not fully characterize the impacts to nearby homes and businesses and that air quality impacts would be significant.

The Memo noted that impacts (harm to the public) are based on the concentration of various regulated air pollutants and therefore, these need to be estimated. Recall that CEQA Appendix G Checklist specifically asks if sensitive receptors would be exposed to high concentrations of air pollutants.

The FIER completely ignored this issue. The Memo demonstrated that project level and cumulative impacts from NO_x emissions would exceed the current air quality standards. It was further demonstrated that cumulative impacts would exceed the air quality standards for PM₁₀ and PM_{2.5}. These standards appear in Table 4.2-1 of the FEIR.

There is no change in the short-term emission rates of NO_x, PM₁₀ and PM_{2.5} in the FEIR (Table 4.2-6). Therefore, violations of air quality standards noted in my Memo (shown on the next page) would persist.

Air Pollutant	Maximum Off-Site Pollutant Concentrations Construction Phase (ug/m3)		Applicable Air Quality Standard ug/m3
	Project Level	Cumulative	
Oxides of Nitrogen (NOx) 1-hour	300	3,267	188
Respirable Particulate Matter (PM10) 24-hour	21.9	260.1	50
Fine Particulate Matter (PM2.5) 24-hour	12.3	51.1	35

Exceedances of 1-Hour NOx and 24-hour PM10 and PM2.5 standards is especially troubling as the Oceanside region has been designated as being Non-Attainment for the 8-hour ozone and 24-hour PM10 and PM2.5 state standards. Emissions from the Eddie Jones project exacerbate the prevailing air quality and contribute to on-going exceedances of these standards..

Robert Dmohowski

From: direlandacct@gmail.com
Sent: Monday, February 10, 2025 10:21 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Requesting do NOT allow a zoning exception to the developer of the Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

All,

I'm a homeowner in the Wanis community off Benet and Airport. My home is the last to back up to the canyon and Airport Road. We are original owners and have lived here for approx. 21 yrs. As time has passed we've watched our community change. The homeless situation behind my home is scary. I stopped walking the route up to the abbey where I would walk weekly with my neighbors. It isn't safe. Our street lights off Benet aren't always on. I've heard they have been vandalized multiple times. The encampment behind my home is out of control. Drone footage my neighbor sent to us shows structures built by homeless in the canyon. I had to call the police last year when a creepy man was screaming at 4am in my backyard. We've had to deal with semi-trucks coming from the auto wrecking yard off Airport Road for years. At times they don't make a full stop at the stop sign on Benet and Airport. They use their large vehicles to muscle through as if they have the right of way on Benet Road. I hear their horns honking frequently as well. Our community will be destroyed by adding the Eddie Jones Warehouse with more Semis. I worry about the fire safety in our community. Last year there was a fire on the hill near the water tower which my home backs up to. Benet Road was shut down in both directions. How would we be able to evacuate our entire community if there was a fire? Benet which ends at Fousat is the only way to cross the San Luis Rey River to get out. It doesn't make sense to add such a high volume of traffic to a rather large residential community with few emergency routes out. I feel as though we've been forgotten with homeless taking over the canyon area and now a huge warehouse with multiple semi-trucks backing up our small streets.

Please do NOT allow a zoning exception to the developer of the Eddie Jones Warehouse.

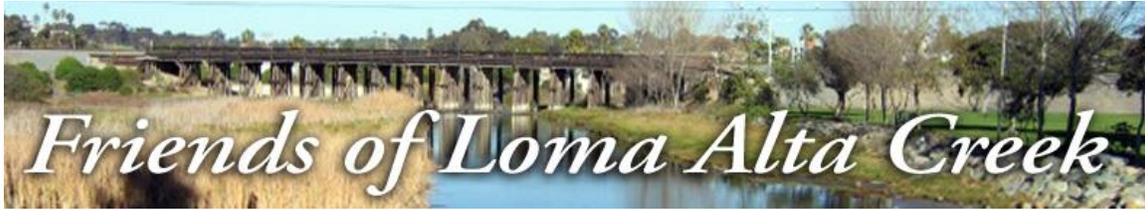
Thank you,
Dorothy

Dorothy Ireland-Rueda

Cell 760.419.9443

Fax 760.385.2030

direlandacct@gmail.com



550 Hoover St.
Oceanside CA 92054
deannie550@sbcglobal.net
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February 10, 2025

City of Oceanside
Sergio Madera, Planner
Planning Commission
300 N Coast Hwy
Oceanside CA 92054

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cc: City Clerk znavarro@oceansideca.org
council@oceansideca.org

Re: OPPOSE: Eddie Jones Project, Planning Commission hearing February 10, 2025\

To whom it may concern,

Thank you for offering us the opportunity to comment on this proposed project. Friends of Loma Alta Creek (Friends) want to ensure all development in the City is appropriate and will not overly impact the natural resources of the area, will significantly contribute to sustainability of the City and complies with existing zoning without imposing significant environmental impacts, including indirect impacts or Health & Safety concerns.

It may seem that the EIR is acceptable. It is not. There are too many discrepancies in the employee numbers, truck bay numbers and trip generation figures. It's clear this would be a 24/7 operation and that is NOT consistent with anything in Oceanside in that area.

You have the right to accept the EIR and DENY the project.

This would be based on **project inconsistency**. The code says the limit is 5-6 truck bays and only 50,000 square foot warehouse are allowed. That is the meaning of the ordinance and does not include this truck terminal type project. It's obvious that this project is also not consistent with the surrounding residential area or the airport area in general.

You may also deny the project based on **public opposition**. There seems to be quite a high number of opposition documents the city should have received. Please include ours.

You may also deny the project if the **fiscal benefit doesn't outweigh the impacts**. It's quite obvious that adding 499 employees and thousands commuter trips a day would have a huge negative impact in the area. That doesn't even include the likely 24/7 truck traffic and operations for such an

outrageously large facility. You should wonder why the proponent is not 'coming clean' to reveal this is a set up for an Amazon type facility. One was rejected in Oceanside already. You can do the same here. The merits of the project simply do not outweigh any potential benefit to the residents of Oceanside or anyone who uses Highway 76. This makes it an unsustainable project. Year after year of this much increased heavy traffic will takes its toll on our local roadway.

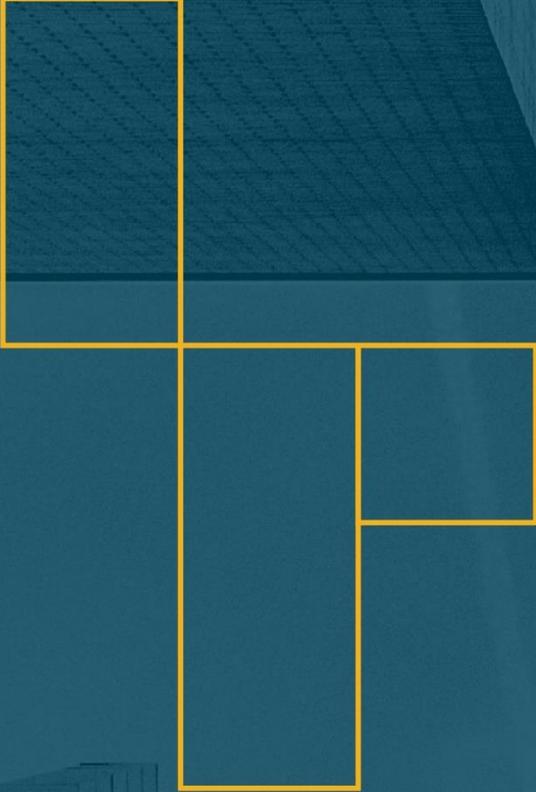
There is no justification for granting variances from the 5-6 bays allowed and the 50,000 square foot warehouse allowed. We ask that the proponent provide their fiscal analysis which apparently exists yet they failed to include in the application package. If the impact of hundreds of trucks and commuter cars a day most likely 24/7 can be offset by a measly \$80,000 a year we simply can't imagine. We already know the vast majority of this traffic will be from commuters who will not be spending significant dollars in Oceanside including buying homes here. Is the Project Sustainable and economically sensible? NO!

Thank you for accepting our comments and OPPOSE this project. It does not benefit the residents of Oceanside nor significantly enhance our tax coffers or investments in the City.

Friends of Loma Alta Creek



Nadine L. Scott, Attorney at Law
Friends of Loma Alta Creek



london moeder
advisors

Economic & Fiscal Impact Analysis for Eddie Jones Way Industrial Project

Prepared for:

RPG

May 2023

Prepared by:

Gary London, *Principal*

Nathan Moeder, *Principal*

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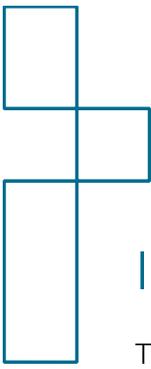
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Introduction

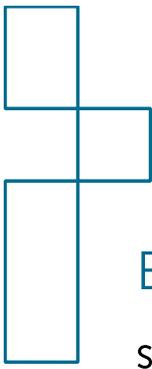
The purpose of this report is to detail the fiscal and economic impacts of a proposed industrial development in the City of Oceanside. The 566,905 square-foot building is currently being planned by RPG and is located at 250 Eddie Jones Way in Oceanside, CA, as depicted in the following map:



The proposed project is planned to include:

- 369,415 square feet of warehouse space
- 158,321 square feet of manufacturing space
- 39,170 square feet of office space
- 587 parking spaces

Research for this analysis was completed in May 2023. Conclusions and recommendations are strictly those of London Moeder Advisors. Users of this information should recognize that the assumptions and projections contained in this report *will* vary from the actual experience in the marketplace. Therefore, London Moeder Advisors is not responsible for the actions taken or any limitations, financial or otherwise of property owners, investors, developers, lenders, public agencies, operators or tenants.



Executive Summary

Summary of Impacts

London Moeder Advisors has evaluated the anticipated fiscal and economic impacts of the proposed development in the City of Oceanside. The planned 566,905 square-foot building is envisioned to contain a high-end industrial life science user. The following bullet points highlight the newly generated impacts by this user type:

Total Fiscal Impact

- ➔ Total gross property tax revenue of \$1.9 million annually.
- ➔ Total gross sales tax revenue of \$149,140 annually.

Fiscal Impact to City

- ➔ Total combined tax revenue of \$366,465 annually (\$3.7 million over a ten-year period). Currently the City receives an estimated \$46,046 from the property.¹
- ➔ Total annual net surplus for the City of approximately \$72,103 annually (\$721,032 over a ten-year period).
- ➔ A total of \$2.1 million in project development impact fees.

Construction Impact

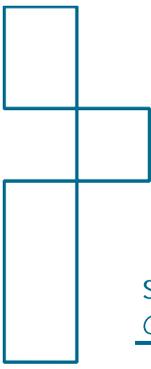
- ➔ 1,425 jobs during construction that generates \$192.7 million in wages.
- ➔ An additional 1,178 jobs in the region that are indirectly and induced by construction of the project.

Permanent Impact

- ➔ A total of 1,380 direct, indirect and induced permanent jobs.
- ➔ Total direct, indirect and induced labor income of \$128.8 million annually.
- ➔ Total annual economic output of \$365.1 million.

The following table details the fiscal and economic impacts of the development (“one-time” impacts) and the operation (annual recurring impacts) of the proposed project.

¹ Based on property taxes paid, of which the City receives 19.64%.



Summary of Fiscal and Economic Impacts
Oceanside - RPG Eddie Jones

Fiscal Impacts to the City (Annual Recurring)	Annual	10-Year	20-Year
Property Tax Revenue	\$327,156	\$3,271,562	\$6,543,125
Sales Tax - Warehouse Workers	\$8,861	\$88,607	\$177,214
Sales Tax - Manufacturing Workers	\$7,591	\$75,915	\$151,830
Sales Tax - Office Workers	\$8,856	\$88,565	\$177,130
Other Revenues	\$14,000	\$140,000	\$280,000
Total Revenue to City	\$366,465	\$3,664,649	\$7,329,298

Costs to City	(\$294,362)	(\$2,943,617)	(\$5,887,235)
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Annual Recurring Surplus	\$72,103	\$721,032	\$1,442,064
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Economic Impacts ("One-time")

Sales Tax Revenue from Development	\$150,000
Job Creation from Development	2,603
Total Development Fees	\$2,100,949

Economic Impacts (Annual Recurring)

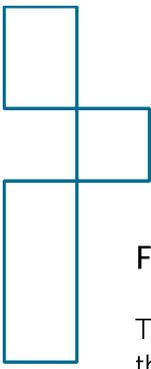
Job Creation from Operations	1,380
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IMPLAN Results - Regional Impacts

Construction Impacts ("One-time")

Employment	2,603
Labor Income	\$192,709,683
Total Value Added	\$260,258,801
Output	\$454,638,545

Permanent Impacts (Annual Recurring)	Annual	10-Year	20-Year
Employment	1,380		
Labor Income	\$128,797,205	\$1,287,972,051	\$2,575,944,103
Total Value Added	\$207,051,369	\$2,070,513,695	\$4,141,027,389
Output	\$365,093,315	\$3,650,933,152	\$7,301,866,304



Fiscal Impacts

The following bullet points highlight the incremental results of the analysis, which are detailed in this report:

- ➔ The project is estimated to generate \$1.7 million of new property tax revenue annually with \$327,156 being paid to the City of Oceanside annually.
- ➔ The total annual sales tax generated by the 499 employees at the Subject Project is estimated to be \$149,140. The City of Oceanside's net share is estimated to be \$25,309 annually.
- ➔ We have determined that the annual recurring revenues from the project's development are estimated to be \$366,465. The annual recurring expenses are estimated to be \$294,362. When we reconcile these, we estimate that the project will generate an annual recurring surplus of approximately \$72,103 to the City of Oceanside.
- ➔ In addition to the recurring revenue, the City of Oceanside will also receive an estimated \$2.1 million in development impact fees paid from the development.

Economic Impacts: Construction (One-time)

The following bullet points highlight the economic benefits which will be achieved through development of this project during its construction:

- ➔ The purchase of construction materials for the development of the project is estimated to generate \$618,750 in total sales tax revenue and \$150,000 in sales tax revenue for the City of Oceanside.
- ➔ Approximately 1,425 jobs will be created directly during construction, 1,178 jobs will be created either indirectly or induced to a total of 2,603 jobs.

The following table highlights the results of the IMPLAN model calculating the economic impacts from the development of the project ("one time" impacts):

Total Impact Summary *Construction Impacts*

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	1,425	\$107,290,575	\$109,667,980	\$200,174,984
Indirect Effect	579	\$48,429,147	\$83,029,573	\$145,622,907
Induced Effect	599	\$36,989,961	\$67,561,248	\$108,840,654
Total Effect	2,603	\$192,709,683	\$260,258,801	\$454,638,545

Source: London Moeder Advisors, IMPLAN

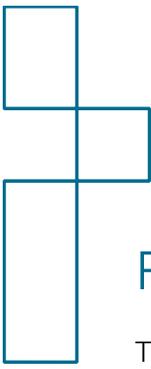
Economic Impacts: Recurring from Ongoing Operations

The following table highlights results of the IMPLAN model calculating the economic impacts from the annual operations of the project. These impacts are recurring once the project is completed and operations are stabilized:

Total Impact Summary *Permanent Impacts (Stabilized Operations)*

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	499	\$53,603,398	\$91,576,028	\$177,782,393
Indirect Effect	486	\$50,783,832	\$70,866,361	\$115,463,670
Induced Effect	395	\$24,409,975	\$44,608,981	\$71,847,252
Total Effect	1,380	\$128,797,205	\$207,051,369	\$365,093,315

Source: London Moeder Advisors, IMPLAN



Fiscal Impacts

This section details the fiscal impacts generated by taxes and fees associated with the project and is divided into the following four subsections:

- Revenues to the City of Oceanside
- Costs to the City of Oceanside
- Reconciliation of revenues and costs
- Project development fees

Revenues to the City of Oceanside

This section summarizes the various sources of revenue to the City of Oceanside stemming from the development of the project. The detailed tables on the revenues to the City are located in the Appendix (see [Fiscal Impact Tables](#)).

Property Tax

The development of the project will increase the property tax revenue collected by the City of Oceanside. The estimated development costs of the project are \$150 million dollars.

Based on the estimated construction costs of \$150 million and the property tax rate of 1.11051%, the annual property tax payment is estimated to be \$1.7 million. The portion of property tax revenue that is realized by the City is 19.64%. This results in an estimated \$327,156 of new property tax revenue that will be generated by the project annually, resulting in a 710% increase when compared with existing property taxes currently paid for the site.

Sales Tax & Other Revenue

To calculate sales tax revenue, we first determined the number of expected employees. Utilizing approximate industry standards of 185 square feet per office employee, 1,055 square feet per manufacturing employee and 2,110 square feet per warehousing employee, we estimate there will be a total of 499 employees working at the built project.

Sales Tax – Warehouse Employees

The warehouse portion of the project is expected to add a total of 175 new employees in the City of Oceanside. These employees will engage in daily spending within Oceanside. We have assumed that the warehouse employees will spend on average \$15 each working day in the year (241 total working days). This equates to a total of \$632,907 in retail expenditures. We assume that 70% of these expenditures are taxable. Therefore, the City's share of sales tax from warehouse employee expenditures is estimated to be \$8,861 annually.

Sales Tax – Manufacturing Employees

The manufacturing portion of the project is expected to add a total of 150 new employees in the City of Oceanside. These employees will engage in daily spending within Oceanside. We have assumed that the warehouse employees will spend on average \$15 each working day in the year (241 total working days). This equates to a total of \$542,250 in retail expenditures. We assume that 70% of these expenditures are taxable. Therefore, the City's share of sales tax from manufacturing employee expenditures is estimated to be \$7,591 annually.

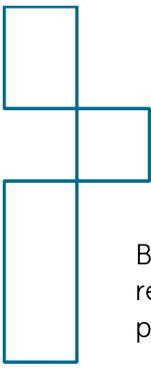
Sales Tax – Office Employees

The office portion of the project is expected to add a total of 175 new employees in the City of Oceanside. These employees will engage in daily spending within Oceanside. We have assumed that the office employees will spend on average \$15 each working day in the year (241 total working days). This equates to a total of \$632,606 in retail expenditures. We assume that 70% of these expenditures are taxable. Therefore, the City's share of sales tax from office employee expenditures is estimated to be \$8,856 annually.

Other Revenues

In addition to taxes, the City of Oceanside will receive revenue from franchise fees, licenses, permits, fines and forfeitures from the employees of the project.

There are individuals who live and work in the same city, therefore, the employee population must be adjusted to reach an "effective" population. We employed the industry standard methodology to estimate the effective population, which includes multiplying the jobs per population ratio of 0.49 (total employment divided by total population) by total employment and adding it to the resident population. The effective population (resident population as adjusted to employment population) for the project is estimated to be 246 persons.



By analyzing the 2022-2023 Adopted Budget for the City of Oceanside², we estimated the various revenues to the City on a dollar per capita basis and apply it to the effective population of the project. The result is an additional \$14,000 in annual revenue to the City of Oceanside.

Total City Revenues

In total, we estimate that the development of the project will generate an estimated \$366,465 in direct annual revenue to the City of Oceanside. The following tables summarize the various revenues to the City.

² City of Oceanside Adopted Budget Fiscal Year 2022-2023

Estimate of Annual Recurring Revenues
Oceanside - RPG Eddie Jones

I. Property Tax			
Project S.F.			566,905
Value PSF ¹			\$264.59
Estimated Value (Const. Costs + Land)			\$150,000,000
Property Tax Rate	1.11051%		\$1,665,765
City Portion (includes VLF) ³			19.64%
Total Property Tax to City			\$327,156
II. Sales Tax - Warehouse Workers			
Occupied S.F.			369,415
<u>S.F. Per Employee</u>			<u>2,110</u>
# Employees			175
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$632,907
Taxable Sales @ ⁴	70%		\$443,035
City Portion			2.00%
Warehouse Sales Tax			\$8,861
III. Sales Tax - Manufacturing Workers			
Occupied S.F.			158,321
<u>S.F. Per Employee</u>			<u>1,055</u>
# Employees			150
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$542,250
Taxable Sales @	70%		\$379,575
City Portion			2.00%
Manufacturing Sales Tax			\$7,591
IV. Sales Tax - Office Workers			
Occupied S.F.			32,374
<u>S.F. Per Employee</u>			<u>185</u>
# Employees			175
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$632,606
Taxable Sales @	70%		\$442,824
City Portion			2.00%
Office Sales Tax			\$8,856
Total Sales Tax			\$25,309

V. Other Revenues

Warehousing

Gross S.F.	369,415
Core Factor	0%
Net Rentable S.F.	369,415
S.F. Per Employee ⁵	2,110
# Employees	175

Manufacturing

Gross S.F.	158,321
Core Factor	0%
Net Rentable S.F.	158,321
S.F. Per Employee ⁶	1,055
# Employees	150

Office

Gross S.F.	39,170
Core Factor	13%
Net Rentable S.F.	34,078
Occupied S.F. (95% Occupancy)	32,374
S.F. Per Employee ⁷	185
# Employees	174

Total Employees	499
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<u>Jobs Per Population</u>	<u>0.49</u>
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Effective Population	246
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Adopted

FY 2022-23

	<u>Budget Amount</u>		<u>\$/Capita</u>	
Franchise Fees	\$4,256,319	Effective Population	\$19.48	\$4,798
Licenses & Permits	\$4,332,624	Effective Population	\$19.83	\$4,884
Fines and Forfeitures	\$3,842,598	Effective Population	\$17.59	\$4,332

Total Other Revenues				\$14,000
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Total Annual Revenues	\$366,465
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¹Based on RPG Eddy Jones Assumption

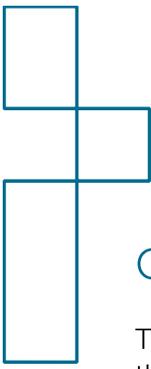
²City of Oceanside Treasurer Office

³London Moeder Assumptions

⁴Based on industry standards for warehouse space

⁵Based on industry standards for manufacturing space

⁶Based on industry standards for office space



Costs to the City of Oceanside

This section summarizes the various costs to the City of Oceanside which are likely to result from the development of the project, including but not limited to public safety and infrastructure.

To determine the City costs of the project we analyzed the City of Oceanside’s General Fund. We examined each expense category and determined if it applied to the resident population or effective population and calculated the amount accordingly. The following table summarizes our analysis of city-wide costs per the General Fund.

Summary of City of Oceanside Costs Oceanside - RPG Eddie Jones

<u>Demographic Profile</u>	<u>Total Population¹</u>	<u>Total Employment¹</u>	<u>Effective Pop Pop & Jobs</u>	
City of Oceanside	175,694	86,712		
<u>Jobs Per Population</u>		<u>0.49</u>		
City of Oceanside R.E.	175,694	42,796	218,490	
<u>General Fund Category</u>	<u>Budgeted Cost</u>			
General Government	\$36,611,000		11.8% of Core City Services Expenditures	
		<u>Service Population</u>	<u>Population Type</u>	<u>Amount Per Resident or R.E.</u>
Core City Services²				
Public Safety	\$97,186,000	Effective Population	218,490	\$444.81
Public Works	\$29,939,000	Effective Population	218,490	\$137.03
Community Development	\$36,491,000	Effective Population	218,490	\$167.01
Community/Cultural Services	\$17,325,000	Resident Population	175,694	\$98.61
Interest on Long-Term Debt	\$694,000	Effective Population	218,490	\$3.18
Water	\$65,951,000	Effective Population	218,490	\$301.85
Sewer	\$35,361,000	Effective Population	218,490	\$161.84
Waste Disposal	\$24,721,000	Effective Population	218,490	\$113.14
<u>Other</u>	<u>\$1,484,000</u>	<u>Effective Population</u>	<u>218,490</u>	<u>\$6.79</u>
Subtotal Core City Services	\$309,152,000			
Total	\$345,763,000			

¹City of Oceanside Annual Operating Budget

²City of Oceanside Annual Comprehensive Financial Report, p. 10



With the City expenses estimated on a per resident or effective population basis, we can apply these expenses to the estimated population of the project. The following table details the estimated annual recurring costs to the City of Oceanside from the development of the project. After a 20% reduction accounting for job transfer is applied, the amount of City costs is estimated to be \$294,362 annually.

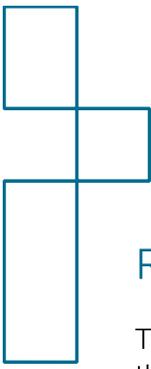
Estimate of Annual Recurring Costs

Oceanside - RPG Eddie Jones

<u>Demographic Profile</u>	<u>Total Population</u>	<u>Total Employment</u>	<u>Effective Pop Pop & Jobs</u>
Proposed Project	0	499	
<u>Jobs Per Population</u>		<u>0.49</u>	
Proposed Project R.E.	0	246	246

Worker Impact Population	246
Less: Reduction for Job Transfer within City of Oceanside 20%	(49)
Total New Jobs Onsite	197

	<u>Service Population</u>	<u>Population Type</u>	<u>Amount Per Resident or R.E.</u>	<u>Cost of Service</u>
Core City Services				
Public Safety	Effective Population	197	\$444.81	\$87,650
Public Works	Effective Population	197	\$137.03	\$27,001
Community Development	Effective Population	197	\$167.01	\$32,911
Community/Cultural Services	Resident Population	0	\$98.61	\$0
Interest on Long-Term Debt	Effective Population	197	\$3.18	\$626
Water	Effective Population	197	\$301.85	\$59,480
Sewer	Effective Population	197	\$161.84	\$31,891
Waste Disposal	Effective Population	197	\$113.14	\$22,295
Other	Effective Population	197	\$6.79	\$1,338
General Government		11.8% of Core City Services Expenditures		\$31,168
Total				\$294,362



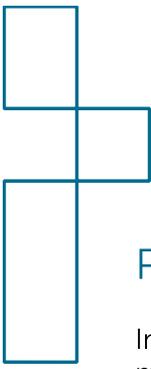
Reconciliation of City Revenues and Costs

The following section reconciles the revenues and costs to the City of Oceanside associated with the development of the project.

We have determined that the annual recurring revenues from development of the project are estimated to be \$366,465. The annual recurring expenses are estimated at \$294,362. Therefore, this project is anticipated to generate an annual recurring surplus of \$72,103 to the City of Oceanside, as detailed in the following table:

Estimate of Stabilized Fiscal Impact Oceanside - RPG Eddie Jones

<u>Annual Recurring Revenues</u>	<u>Totals</u>	<u>%</u>
Property Tax to City	\$327,156	89%
Sales Tax - Warehouse Workers	\$8,861	2.4%
Sales Tax - Manufacturing Workers	\$7,591	2.1%
Sales Tax - Office Workers	\$8,856	2.4%
Other Revenues	\$14,000	3.8%
Total Annual Revenues	\$366,465	100%
<u>Annual Recurring Expenses</u>	<u>Totals</u>	<u>%</u>
Public Safety	\$87,650	30%
Public Works	\$27,001	9%
Community Development	\$32,911	11%
Community/Cultural Services	\$0	0%
Interest on Long-Term Debt	\$626	0%
Water	\$59,480	20%
Sewer	\$31,891	11%
Waste Disposal	\$22,295	7.6%
Other	\$1,338	0.5%
General Government	\$31,168	10.6%
Total Annual Expenses	\$294,362	100%
Annual Recurring Surplus/(Deficit)	\$72,103	



Project Development Fees

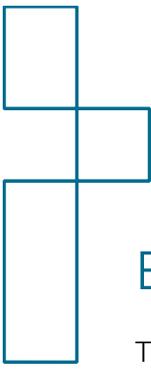
In addition to the recurring revenue, the City of Oceanside will receive an estimated total of \$2.1 million in development fees for the development of the project. These are one-time fees and are summarized in the following table. Although the project does not generate students, school fees will be assessed which benefit Oceanside Unified School District.

Project Development Fees

Oceanside - RPG Eddie Jones

Impact Fees	Totals	\$/S.F.
<i>Total Square Footage</i>	566,905	
Public Facility	\$511,348	\$0.90
School Fees ¹	\$374,157	\$0.66
Traffic Fees	\$816,343	\$1.44
Drainage Fee	\$399,101	\$0.70
Total Fees	\$2,100,949	\$3.71

¹The project itself does not generate students, but it pays school fees that benefit the school district.



Economic Impacts

The purpose of this section is to analyze both the direct and indirect economic impacts stemming from the development and operations of the project.

- ➔ **Direct Economic Activity**: This includes factors such as job creation, expenditures and business output.
- ➔ **Indirect Economic Activity**: Calculated by utilizing multipliers to translate direct expenditures into total economic activity.

Direct Economic Activity

The following section analyzes the direct economic activity stemming from the development and operation of the project.

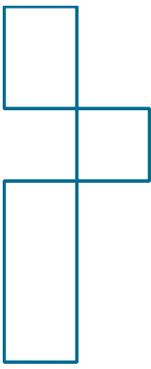
Constructions Sales Impacts

The development of the project is anticipated to cost \$150 million³. Based on industry averages, the total costs of materials for commercial development is estimated to be approximately 50% of the development costs. This provides an estimated materials cost of \$75 million. The remaining 50% is attributed to labor which is not taxable.

We have assumed that 10% of all construction materials used in this project will be purchased in the City of Oceanside. This is based on the cost and availability of materials in Oceanside relative to the surrounding area. This is a conservative estimate, as the actual percentage may be higher due to builder preferences and materials availability.

Multiplying the total materials cost of \$75 million by 10% produces the estimated dollar amount of materials purchased in the City of Oceanside, which is estimated to be \$7.5 million. This amount is assessed at the sales tax rate of 8.25%, with the city's portion being 2.0%. The purchase of construction materials for the development of the project is estimated to generate \$618,750 in total sales tax revenue and \$150,000 in sales tax revenue for the City of Oceanside, as detailed in the following table:

³ This figure excludes land value



Sales Tax Revenue Generated by Construction
Construction Materials

Oceanside - RPG Eddie Jones Development

Total Costs	\$150,000,000
Labor	\$75,000,000
% of Total	50%
Materials	\$75,000,000
% of Total	50%

% of Materials Purchased in Oceanside	10%
Materials Purchased in Oceanside	\$7,500,000

Tax Rate	8.25%
Total Sales Tax On Materials	\$618,750
Oceanside Tax Rate Component	2.00%
Sales Tax Revenue to Oceanside	\$150,000

Source: London Moeder Advisors

Job Creation

Local jobs will also be created from the development of the project. Job creation will come from construction as well as ongoing annual operations. Jobs are quoted on the basis of full-time annual equivalency, which represents the employment of one worker for one full year.

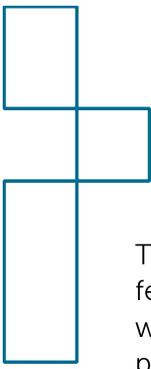
Construction job generation is estimated using a ratio of jobs per every \$1 million of construction costs. The ratio for the project is estimated to be 9.5.⁴ The \$150 million construction of the project is estimated to generate 1,425 new construction jobs during development:

Direct Construction Job Generation
Oceanside - RPG Eddie Jones Development

Construction Costs	\$150,000,000
Per Million Dollars	\$150.00
Jobs Generated Per Million Dollars	9.50
Total Jobs Generated (Full Time Equivalency)	1,425

Source: London Moeder Advisors

⁴ Based on a study prepared by Dr. Steven Fuller of George Mason University



To estimate the number of employees, we utilized industry standards of approximately 185 square feet per office employee, 1,055 square feet per manufacturing employee and 2,110 square feet per warehousing employee, we estimate there will be a total of 499 employees working at the built project. The following details the total employees by type:

Job Generation - Project Operations

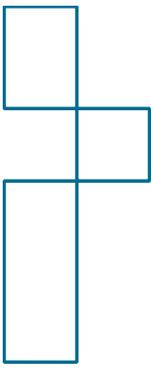
Full Time Annual Equivalency

Warehouse	
Occupied S.F.	369,415
<u>S.F. Per Employee</u>	<u>2,110</u>
# of Employees	175
Manufacturing	
Occupied S.F.	158,321
<u>S.F. Per Employee</u>	<u>1,055</u>
# of Employees	150
Office	
Occupied S.F.	32,374
<u>S.F. Per Employee</u>	<u>185</u>
# of Employees	174
Total	499

Source: London Moeder Advisors

In addition, jobs will be created through contract services, including such services as elevator maintenance, trash collection and window-washing, which are not reflected in this analysis.

In summary, the project is estimated to create 1,425 jobs during construction and 499 permanent jobs once completed and fully occupied. The following table summarizes total job estimates:



Job Generation
Full Time Annual Equivalency

Category	During Development	After Development
Construction		
Project Development	1,425	
Operations		
Warehouse		175
Manufacturing		150
Office		174
Total	1,425	499

Source: London Moeder Advisors

Indirect Economic Impact Analysis

This section analyzes the indirect economic impacts stemming from the development and operation of the project. The indirect and induced spending are calculated by utilizing multipliers to translate direct expenditures into total economic activity. The value of these multipliers is estimated using information about the City of Oceanside. The multipliers utilized in this analysis are based on the Regional Input-Output Multipliers RIMS II – Regional Accounts data from the U.S. Department of Commerce’s Bureau of Economic Analysis. The resulting input-output model provides a detailed accounting of overall levels of employment and spending on locally produced goods and services supported by the companies within Eddie Jones Way Industrial project and its employees.

Model Inputs

This section summarizes the various inputs utilized in this input-output model to estimate economic impacts.

Operating Expenses

The assumed tenant that would occupy the project is envisioned to be an analytical laboratory or instrument manufacturer. The following table details some of the companies in this sector as well as their annual expenses per employee. Based on the sector weighted average of \$207,560 per employee, the estimated annual operating expenses for the Subject Project are estimated to be \$103.6 million. The following table details these inputs for the economic model, while delineating the expenses by employee type.

Annual Operating Expenses (Based on 2022 Financials) *Analytical Laboratory / Instrument Manufacturing Companies*

<u>Company</u>	<u>NAICS #</u>	<u># of Employees</u>	<u>Expenses</u>	<u>Expenses/Employee</u>
Thermo Fisher	334516	130,000	\$8,993,000,000	\$69,177
Danaher Corporation	334519	80,000	\$22,783,000,000	\$284,788
Mettler-Toledo International	334516	16,500	\$2,793,000,000	\$169,273
Bio-rad	334516	8,250	\$2,320,000,000	\$281,212
Agilent	334515	18,000	\$5,230,000,000	\$290,556
Perkin Elmer. Inc.	334510	16,700	\$2,569,000,000	\$153,832
Dupont	325211	34,000	\$11,152,000,000	\$328,000
<u>Illumina</u>	334516	<u>7,800</u>	<u>\$8,763,000,000</u>	<u>\$1,123,462</u>
Total / Av. Wt.		311,250	\$64,603,000,000	\$207,560

Source: macro trends.net

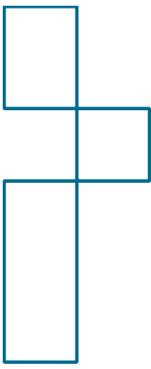
Annual Revenue

We then evaluated the annual gross business revenue that would be generated by an analytical laboratory or instrument manufacturer. Based on companies in this sector as an example, the annual revenue is estimated to be \$356,222 per employee. Applied to the Subject Project of 499 employees, this results in a total annual revenue estimated to be \$177.8 million. The following table details these inputs for the economic model, while delineating the revenue by employee type.

Annual Revenue (Based on 2022 Financials) Analytical Laboratory / Instrument Manufacturing Companies

<u>Company</u>	<u>NAICS #</u>	<u># of Employees</u>	<u>Revenue</u>	<u>Revenue/Employee</u>
Thermo Fisher	334516	130,000	\$44,920,000,000	\$345,538
Danaher Corporation	334519	80,000	\$31,471,000,000	\$393,388
Mettler-Toledo Internation.	334516	16,500	\$3,920,000,000	\$237,576
Bio-rad	334516	8,250	\$2,802,000,000	\$339,636
Agilent	334515	18,000	\$6,848,000,000	\$380,444
Perkin Elmer. Inc.	334510	16,700	\$3,312,000,000	\$198,323
Dupont	325211	34,000	\$13,017,000,000	\$382,853
<u>Illumina</u>	334516	<u>7,800</u>	<u>\$4,584,000,000</u>	<u>\$587,692</u>
Total / Av. Wt.		311,250	\$110,874,000,000	\$356,222

Source: macrotrends.net



Economic Impact Analysis Model Inputs

Construction Impacts

Construction Operations:

Number of Construction Employees 1,425

Cost Breakdown

Warehouse	369,415 SF	65%	\$97,745,081
Manufacturing	158,321 SF	28%	\$41,890,749
Office	39,170 SF	7%	\$10,364,170
Total	566,905 SF	100%	\$150,000,000

Economic Impact Analysis Model Inputs

Permanent Impacts (Stabilized Operations)

Building Operations

Stabilized Operating Expenses \$103,572,360

Warehouse

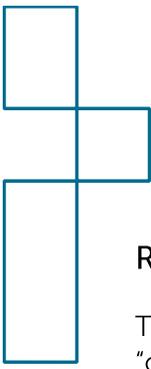
Occupied SF	369,415
SF per Employee	2,110
Total Employees	175
Revenue Per Employee	\$356,222
Annual Revenue	\$62,366,567

Manufacturing

Occupied SF	158,321
SF per Employee	1,055
Total Employees	150
Revenue Per Employee	\$356,222
Annual Revenue	\$53,433,253

Office

Occupied SF	32,374
SF per Employee	185
Total Employees	174
Revenue Per Employee	\$356,222
Annual Revenue	\$61,982,573



Results

The following table highlights the economic impacts of the development of the project. These are “one time” construction impacts that occur during the development phase of the project:

Total Impact Summary

Construction Impacts

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	1,425	\$107,290,575	\$109,667,980	\$200,174,984
Indirect Effect	579	\$48,429,147	\$83,029,573	\$145,622,907
Induced Effect	599	\$36,989,961	\$67,561,248	\$108,840,654
Total Effect	2,603	\$192,709,683	\$260,258,801	\$454,638,545

Source: London Moeder Advisors, IMPLAN

The following table highlights the recurring annual economic impacts based on the stabilized permanent operations of the Subject Project:

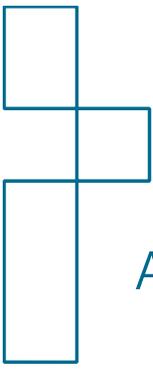
Total Impact Summary

Permanent Impacts (Stabilized Operations)

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	499	\$53,603,398	\$91,576,028	\$177,782,393
Indirect Effect	486	\$50,783,832	\$70,866,361	\$115,463,670
Induced Effect	395	\$24,409,975	\$44,608,981	\$71,847,252
Total Effect	1,380	\$128,797,205	\$207,051,369	\$365,093,315

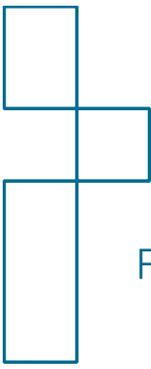
Source: London Moeder Advisors, IMPLAN

We have included tables in the [Appendix \(Indirect Economic Impact Tables\)](#) that demonstrate the additional results of the IMPLAN model.



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Appendix



Fiscal Impact Tables

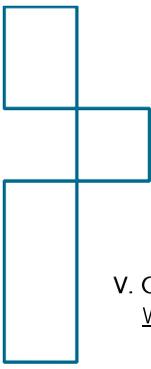
Project Development Fees
Oceanside - RPG Eddie Jones

Impact Fees	Totals	\$/S.F.
<i>Total Square Footage</i>	<i>566,905</i>	
Public Facility	\$511,348	\$0.90
School Fees ¹	\$374,157	\$0.66
Traffic Fees	\$816,343	\$1.44
Drainage Fee	\$399,101	\$0.70
Total Fees	\$2,100,949	\$3.71

¹The project itself does not generate students, but it pays school fees that benefit the school district.

Estimate of Annual Recurring Revenues
Oceanside - RPG Eddie Jones

I. Property Tax			
Project S.F.			566,905
Value PSF ¹			\$264.59
Estimated Value (Const. Costs + Land)			\$150,000,000
Property Tax Rate	1.11051%		\$1,665,765
City Portion (includes VLF) ³			19.64%
Total Property Tax to City			\$327,156
II. Sales Tax - Warehouse Workers			
Occupied S.F.			369,415
<u>S.F. Per Employee</u>			<u>2,110</u>
# Employees			175
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$632,907
Taxable Sales @ ⁴	70%		\$443,035
City Portion			2.00%
Warehouse Sales Tax			\$8,861
III. Sales Tax - Manufacturing Workers			
Occupied S.F.			158,321
<u>S.F. Per Employee</u>			<u>1,055</u>
# Employees			150
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$542,250
Taxable Sales @	70%		\$379,575
City Portion			2.00%
Manufacturing Sales Tax			\$7,591
IV. Sales Tax - Office Workers			
Occupied S.F.			32,374
<u>S.F. Per Employee</u>			<u>185</u>
# Employees			175
Expenditure per Worker per Day			\$15
Number of Working Days in Year			241
Total Annual Retail Expenditures by Office Workers			\$632,606
Taxable Sales @	70%		\$442,824
City Portion			2.00%
Office Sales Tax			\$8,856
Total Sales Tax			\$25,309



V. Other Revenues

<u>Warehousing</u>		
Gross S.F.		369,415
Core Factor		0%
Net Rentable S.F.		369,415
S.F. Per Employee ⁵		2,110
# Employees		175
<u>Manufacturing</u>		
Gross S.F.		158,321
Core Factor		0%
Net Rentable S.F.		158,321
S.F. Per Employee ⁶		1,055
# Employees		150
<u>Office</u>		
Gross S.F.		39,170
Core Factor		13%
Net Rentable S.F.		34,078
Occupied S.F. (95% Occupancy)		32,374
S.F. Per Employee ⁷		185
# Employees		174
Total Employees		499
<u>Jobs Per Population</u>		<u>0.49</u>
Effective Population		246

	Adopted			
	FY 2022-23			
	<u>Budget Amount</u>		<u>\$/Capita</u>	
Franchise Fees	\$4,256,319	Effective Population	\$19.48	\$4,798
Licenses & Permits	\$4,332,624	Effective Population	\$19.83	\$4,884
Fines and Forfeitures	\$3,842,598	Effective Population	\$17.59	\$4,332
Total Other Revenues				\$14,000

Total Annual Revenues	\$366,465
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¹Based on RPG Eddy Jones Assumption

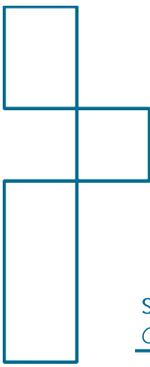
²City of Oceanside Treasurer Office

³London Moeder Assumptions

⁴Based on industry standards for warehouse space

⁵Based on industry standards for manufacturing space

⁶Based on industry standards for office space

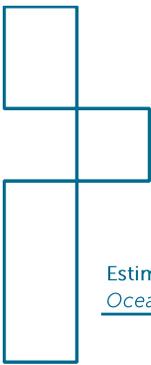


Summary of City of Oceanside Costs
Oceanside - RPG Eddie Jones

<u>Demographic Profile</u>	<u>Total Population¹</u>	<u>Total Employment¹</u>	<u>Effective Pop Pop & Jobs</u>	
City of Oceanside	175,694	86,712		
<u>Jobs Per Population</u>		<u>0.49</u>		
City of Oceanside R.E.	175,694	42,796	218,490	
<u>General Fund Category</u>	<u>Budgeted Cost</u>			
General Government	\$36,611,000		11.8% of Core City Services Expenditures	
		<u>Service Population</u>	<u>Population Type</u>	<u>Amount Per Resident or R.E.</u>
Core City Services²				
Public Safety	\$97,186,000	Effective Population	218,490	\$444.81
Public Works	\$29,939,000	Effective Population	218,490	\$137.03
Community Development	\$36,491,000	Effective Population	218,490	\$167.01
Community/Cultural Services	\$17,325,000	Resident Population	175,694	\$98.61
Interest on Long-Term Debt	\$694,000	Effective Population	218,490	\$3.18
Water	\$65,951,000	Effective Population	218,490	\$301.85
Sewer	\$35,361,000	Effective Population	218,490	\$161.84
Waste Disposal	\$24,721,000	Effective Population	218,490	\$113.14
<u>Other</u>	<u>\$1,484,000</u>	<u>Effective Population</u>	<u>218,490</u>	<u>\$6.79</u>
Subtotal Core City Services	\$309,152,000			
Total	\$345,763,000			

¹City of Oceanside Annual Operating Budget

²City of Oceanside Annual Comprehensive Financial Report, p. 10

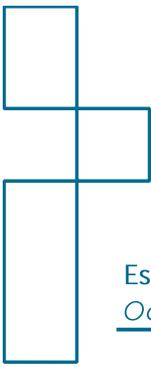


Estimate of Annual Recurring Costs
Oceanside - RPG Eddie Jones

<u>Demographic Profile</u>	<u>Total Population</u>	<u>Total Employment</u>	<u>Effective Pop Pop & Jobs</u>
Proposed Project	0	499	
<u>Jobs Per Population</u>		<u>0.49</u>	
Proposed Project R.E.	0	246	246

Worker Impact Population	246
Less: Reduction for Job Transfer within City of Oceanside 20%	(49)
Total New Jobs Onsite	197

	<u>Service Population</u>	<u>Population Type</u>	<u>Amount Per Resident or R.E.</u>	<u>Cost of Service</u>
Core City Services				
Public Safety	Effective Population	197	\$444.81	\$87,650
Public Works	Effective Population	197	\$137.03	\$27,001
Community Development	Effective Population	197	\$167.01	\$32,911
Community/Cultural Services	Resident Population	0	\$98.61	\$0
Interest on Long-Term Debt	Effective Population	197	\$3.18	\$626
Water	Effective Population	197	\$301.85	\$59,480
Sewer	Effective Population	197	\$161.84	\$31,891
Waste Disposal	Effective Population	197	\$113.14	\$22,295
Other	Effective Population	197	\$6.79	\$1,338
General Government		11.8% of Core City Services Expenditures		\$31,168
Total				\$294,362



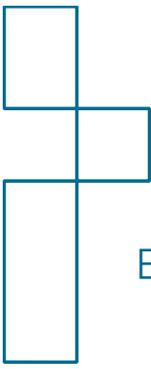
Estimate of Stabilized Fiscal Impact

Oceanside - RPG Eddie Jones

<u>Annual Recurring Revenues</u>	<u>Totals</u>	<u>%</u>
Property Tax to City	\$327,156	89%
Sales Tax - Warehouse Workers	\$8,861	2.4%
Sales Tax - Manufacturing Workers	\$7,591	2.1%
Sales Tax - Office Workers	\$8,856	2.4%
Other Revenues	\$14,000	3.8%
Total Annual Revenues	\$366,465	100%

<u>Annual Recurring Expenses</u>	<u>Totals</u>	<u>%</u>
Public Safety	\$87,650	30%
Public Works	\$27,001	9%
Community Development	\$32,911	11%
Community/Cultural Services	\$0	0%
Interest on Long-Term Debt	\$626	0%
Water	\$59,480	20%
Sewer	\$31,891	11%
Waste Disposal	\$22,295	7.6%
Other	\$1,338	0.5%
General Government	\$31,168	10.6%
Total Annual Expenses	\$294,362	100%

Annual Recurring Surplus/(Deficit)	\$72,103
---	-----------------



Economic Impact Tables

Sales Tax Revenue Generated by Construction *Construction Materials*

Oceanside - RPG Eddie Jones Development

Total Costs	\$150,000,000
Labor	\$75,000,000
% of Total	50%
Materials	\$75,000,000
% of Total	50%

% of Materials Purchased in Oceanside	10%
Materials Purchased in Oceanside	\$7,500,000

Tax Rate	8.25%
Total Sales Tax On Materials	\$618,750
Oceanside Tax Rate Component	2.00%
Sales Tax Revenue to Oceanside	\$150,000

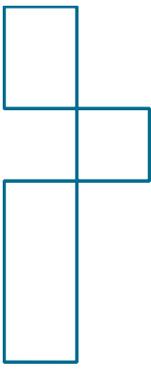
Source: London Moeder Advisors

Direct Construction Job Generation *Oceanside - RPG Eddie Jones Development*

Construction Costs	\$150,000,000
Per Million Dollars	\$150.00
Jobs Generated Per Million Dollars	9.50

Total Jobs Generated (Full Time Equivalency)	1,425
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Source: London Moeder Advisors



Job Generation - Project Operations

Full Time Annual Equivalency

Warehouse	
Occupied S.F.	369,415
<u>S.F. Per Employee</u>	<u>2,110</u>
# of Employees	175
Manufacturing	
Occupied S.F.	158,321
<u>S.F. Per Employee</u>	<u>1,055</u>
# of Employees	150
Office	
# of Units	32,374
<u>Units Per Employee</u>	<u>185</u>
# of Employees	174
Total	499

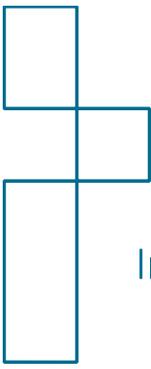
Source: London Moeder Advisors

Job Generation

Full Time Annual Equivalency

Category	During Development	After Development
Construction		
Project Development	1,425	
Operations		
Warehouse		175
Manufacturing		150
Office		174
Total	1,425	499

Source: London Moeder Advisors



Indirect Economic Impact Tables

Economic Impact Analysis Model Inputs

Construction Impacts

Construction Operations:

Number of Construction Employees 1,425

Cost Breakdown

Warehouse	369,415 SF	65%	\$97,745,081
Manufacturing	158,321 SF	28%	\$41,890,749
Office	39,170 SF	7%	\$10,364,170
Total	566,905 SF	100%	\$150,000,000

Economic Impact Analysis Model Inputs

Permanent Impacts (Stabilized Operations)

Building Operations

Stabilized Operating Expenses \$103,572,360

Warehouse

Occupied SF	369,415
SF per Employee	2,110
Total Employees	175
Revenue Per Employee	\$356,222
Annual Revenue	\$62,366,567

Manufacturing

Occupied SF	158,321
SF per Employee	1,055
Total Employees	150
Revenue Per Employee	\$356,222
Annual Revenue	\$53,433,253

Office

Occupied SF	32,374
SF per Employee	185
Total Employees	174
Revenue Per Employee	\$356,222
Annual Revenue	\$61,982,573

Total Impact Summary
Construction Impacts

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	1,425	\$107,290,575	\$109,667,980	\$200,174,984
Indirect Effect	579	\$48,429,147	\$83,029,573	\$145,622,907
Induced Effect	599	\$36,989,961	\$67,561,248	\$108,840,654
Total Effect	2,603	\$192,709,683	\$260,258,801	\$454,638,545

Source: London Moeder Advisors, IMPLAN

Top 10 Industries Impacted
Construction Impacts

Sector	Description	Employment	Labor Income	Value Added	Output
51	Construction of new manufacturing structures	1,425	\$90,945,642	\$109,667,980	\$200,174,984
396	Wholesale - Other durable goods merchant wholesalers	47	\$4,260,492	\$10,910,117	\$15,489,867
449	Owner-occupied dwellings	44	\$3,187,182	\$8,740,817	\$13,799,121
447	Other real estate	58	\$3,129,981	\$6,090,229	\$13,347,637
453	Commercial and industrial machinery and equipment rental and leasing	49	\$3,027,292	\$5,815,048	\$9,392,671
204	Ready-mix concrete manufacturing	33	\$2,562,580	\$5,456,656	\$7,853,869
417	Truck transportation	27	\$2,506,433	\$4,963,172	\$7,780,753
455	Legal services	30	\$2,107,248	\$4,566,215	\$7,663,480
695	Wholesale - Machinery, equipment, and supplies	25	\$1,613,155	\$3,525,388	\$7,459,886
472	Employment services	21	\$1,597,135	\$3,478,560	\$7,190,096

Source: IMPLAN

State and County Tax Impact
Construction Impacts

Description	Employee Compensation	Proprietor Income	Tax on Production and Imports	Households	Corporations
Social Insurance Tax- Employee Contribution	\$429,700				
Social Insurance Tax- Employer Contribution	\$502,363	\$0			
TOPI: Sales Tax			\$5,737,222		
TOPI: Property Tax			\$2,074,195		
TOPI: Motor Vehicle License			\$177,262		
TOPI: Severance Tax			\$13,649		
TOPI: Other Taxes			\$644,581		
TOPI: Special Assessments			\$2,667		
OPI: Corporate Profits Tax					\$1,234,989
Personal Tax: Income Tax				\$7,765,558	
Personal Tax: Motor Vehicle License				\$174,141	
Personal Tax: Property Taxes				\$37,203	
Personal Tax: Other Tax (Fish/Hunt)				\$26,999	
Total State and County Tax	\$932,063	\$0	\$8,649,576	\$8,003,901	\$1,234,989
Total State and County Tax	\$18,820,529				

Source: London Moeder Advisors, IMPLAN

Federal Tax Impact

Construction Impacts

Description	Employee Compensation	Proprietor Income	Tax on Production and Imports	Households	Corporations
Social Insurance Tax- Employee Contribution	\$9,536,291	\$1,138,528			
Social Insurance Tax- Employer Contribution	\$8,609,629				
TOPI: Excise Taxes			(\$1,661,244)		
TOPI: Custom Duty			(\$1,731,595)		
OPI: Corporate Profits Tax					\$1,936,732
Personal Tax: Income Tax				\$19,402,190	
Personal Tax: Estate and Gift Tax				\$0	
Total Federal Tax	\$18,145,920	\$1,138,528	(\$3,392,838)	\$19,402,190	\$1,936,732
Total Federal Tax	\$37,230,532				

Source: London Moeder Advisors, IMPLAN

Total Impact Summary
Permanent Impacts (Stabilized Operations)

Impact Type	Employment	Labor Income	Total Value Added	Output
Direct Effect	499	\$53,603,398	\$91,576,028	\$177,782,393
Indirect Effect	486	\$50,783,832	\$70,866,361	\$115,463,670
Induced Effect	395	\$24,409,975	\$44,608,981	\$71,847,252
Total Effect	1,380	\$128,797,205	\$207,051,369	\$365,093,315

Source: London Moeder Advisors, IMPLAN

Top 10 Industries Impacted
Permanent Impacts (Stabilized Operations)

Sector	Description	Employment	Labor Income	Value Added	Output
317	Analytical laboratory instrument manufacturing	499	\$53,330,766	\$91,586,581	\$177,802,881
469	Management of companies and enterprises	123	\$19,051,917	\$22,186,628	\$34,137,664
394	Wholesale - Household appliances and electrical and electronic goods	75	\$6,854,812	\$7,836,499	\$10,700,894
459	Custom computer programming services	28	\$2,080,388	\$7,165,776	\$9,920,525
449	Owner-occupied dwellings	18	\$2,060,129	\$5,634,038	\$9,063,276
447	Other real estate	20	\$1,797,043	\$3,758,727	\$5,896,043
455	Legal services	25	\$1,657,206	\$2,849,549	\$4,953,516
472	Employment services	16	\$1,227,088	\$2,690,233	\$4,128,112
393	Wholesale - Professional and commercial equipment and supplies	16	\$1,222,040	\$2,340,157	\$3,552,049
432	Cable and other subscription programming	13	\$1,088,190	\$2,304,384	\$2,934,380

Source: London Moeder Advisors, IMPLAN

State and County Tax Impact
Permanent Impacts (Stabilized Operations)

Description	Employee Compensation	Proprietor Income	Tax on Production and Imports	Households	Corporations
Social Insurance Tax- Employee Contribution	\$313,146				
Social Insurance Tax- Employer Contribution	\$366,100	\$0			
TOPI: Sales Tax			\$3,998,198		
TOPI: Property Tax			\$1,445,480		
TOPI: Motor Vehicle License			\$123,532		
TOPI: Severance Tax			\$9,512		
TOPI: Other Taxes			\$449,200		
TOPI: Special Assessments			\$1,859		
OPI: Corporate Profits Tax					\$1,547,617
Personal Tax: Income Tax				\$5,028,682	
Personal Tax: Motor Vehicle License				\$116,016	
Personal Tax: Property Taxes				\$24,486	
Personal Tax: Other Tax (Fish/Hunt)				\$17,341	
Total State and County Tax	\$679,246	\$0	\$6,027,781	\$5,186,525	\$1,547,617
Total State and County Tax	\$13,441,168				

Source: London Moeder Advisors, IMPLAN

Federal Tax Impact
Permanent Impacts (Stabilized Operations)

Description	Employee Compensation	Proprietor Income	Tax on Production and Imports	Households	Corporations
Social Insurance Tax- Employee Contribution	\$6,949,622	\$298,668			
Social Insurance Tax- Employer Contribution	\$6,274,313				
TOPI: Excise Taxes			(\$1,157,700)		
TOPI: Custom Duty			(\$1,206,727)		
OPI: Corporate Profits Tax					\$2,427,000
Personal Tax: Income Tax				\$12,437,723	
Personal Tax: Estate and Gift Tax				\$0	
Total Federal Tax	\$13,223,935	\$298,668	(\$2,364,426)	\$12,437,723	\$2,427,000
Total Federal Tax	\$26,022,900				

Source: London Moeder Advisors, IMPLAN

Corporate Profile

London Moeder Advisors

REPRESENTATIVE SERVICES

Market and Feasibility Studies	Development Services	Litigation Consulting
Financial Structuring	Fiscal Impact	Workout Projects
Asset Disposition	Strategic Planning	MAI Valuation
Government Processing	Capital Access	Economic Analysis

London Moeder Advisors (formerly The London Group) was formed in 1991 to provide real estate advisory services to a broad range of clientele. The firm principals, Gary London and Nathan Moeder, combine for over 60 years of experience. We have analyzed, packaged and achieved capital for a wide variety of real estate projects. Clients who are actively pursuing, developing and investing in projects have regularly sought our advice and financial analysis capabilities. Our experience ranges from large scale, master planned communities to urban redevelopment projects, spanning all land uses and development issues of all sizes and types. These engagements have been undertaken principally throughout North America and Mexico.

A snapshot of a few of the services we render for both the residential and commercial sectors:

- **Market Analysis** for mixed use, urban and suburban properties. Studies concentrate on market depth for specific products, detailed recommendations for product type, absorption and future competition. It also includes economic overviews and forecasts of the relevant communities.
- **Financial Feasibility Studies** for new projects of multiple types, including condominium, apartment, office, and master-planned communities. Studies incorporate debt and equity needs, sensitivity analyses, rates of return and land valuations.
- **Litigation support/expert witness services** for real estate and financial related issues, including economic damages/losses, valuations, historic market conditions and due diligence. We have extensive deposition, trial, mediation and arbitration experience.
- **Investment studies for firms acquiring or disposing of real estate.** Studies include valuation, repositioning projects and portfolios, economic/real estate forecasts and valuation of partnerships. Often, the commercial studies include the valuation of businesses.
- **Estate Planning services** including valuation of portfolios, development of strategies for disposition or repositioning portfolios, succession planning and advisory services for high net worth individuals. We have also been involved in numerous marriage dissolution assignments where real estate is involved.
- **Fiscal Impact, Job Generation and Economic Multiplier Effect Reports,** traditionally prepared for larger commercial projects and in support of Environmental Impact Reports. We have been retained by both developers and municipalities for these reports. The studies typically relate to the tax revenues and employment impacts of new projects.

The London Group also draws upon the experience of professional relationships in the development, legal services, financial placement fields as well as its own staff. Clients who are actively investigating and investing in apartment projects, retail centers, commercial projects, mixed use developments and large master plans have regularly sought our advice and financial analysis capabilities.

San Diego: 825 10th Ave | San Diego, CA 92101 | (619) 269-4010
Carlsbad: 5946 Priestly Dr. #201 | Carlsbad, CA 92008 | (619) 269-4012



Contact Information

This analysis was prepared by London Moeder Advisors commissioned by RPG.

Research for this project was completed in May 2023. Conclusions and recommendations are strictly those of London Moeder Advisors. Users of this information should recognize that assumptions and projections contained in this report *will* vary from the actual experience in the marketplace. Therefore, London Moeder Advisors is not responsible for the actions taken or any limitations, financial or otherwise, of property owners, investors, developers, lenders, public agencies, operators or tenants.

This assignment was completed by the staff of London Moeder Advisors. **Nathan Moeder**, Principal, served as project director. **Collin Wagstaff**, Analyst, conducted analysis and prepared exhibits in this report. **Gary London**, Senior Principal, provided strategic consultation, editing and recommendations. For further information or questions contact us at:

London Moeder Advisors

San Diego: 825 10th Ave | San Diego, CA 92101 | (619) 269-4010
Carlsbad: 5946 Priestly Dr. #201 | Carlsbad, CA 92008 | (619) 269-4012

www.londonmoeder.com

Robert Dmohowski

From: Jennifer Jacobs <jenn.jacobs@yahoo.com>
Sent: Monday, February 10, 2025 9:58 AM
To: Planning-Planning Commission
Subject: Eddie Jones project opposition letter - Kindly confirm receipt

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear City of Oceanside Planning Commission:

I am writing to express my opposition to the Eddies Jones warehouse project. I do not live close by but am deeply concerned by the potential impacts to Oceanside residents as well as sensitive wildlife. There must be more suitable locations for distribution centers with over 600 parking spaces and 57 truck bays than right next to our largest and most important wildlife corridor. It's truly baffling to see a project of this scale in this location with a parking lot right next to the San Luis Rey River trail.

As a wildlife biologist, I cannot emphasize enough how important the San Luis Rey River is to our local wildlife, as it serves as the largest continuous wildlife corridor in the area. Its importance was acknowledged in Oceanside's Subarea Plan, the City's commitment to the San Diego Multiple Habitat Conservation Program. The section of the San Luis Rey River near the project site provides critical breeding habitat for the endangered bird species Least Bell's Vireo. Figure 3 of the Biological Technical Report (BTR) within the EIR shows a number of documented occurrences of this species near or within the project boundary. However, the report contradicts this information by stating that 1) "Least Bell's Vireo is known to occur approximately 1.4 miles east of the project site," which is false and 2) "there is no suitable habitat to support this species within the project site," which we know is also false from the documented observations.

I urge the City to require protocol surveys for this endangered species, as they were surprisingly not conducted. This was also noted by the California Department of Fish and Wildlife (CDFW) in their public response to the draft EIR. I have worked with this species along the San Luis Rey River and know they often nest at the southern edge of the river's riparian habitat. Therefore, it is very likely that they use the northern edge of the project boundary, which is less than 100 feet away from the riparian habitat. I strongly encourage that a larger buffer zone be implement to protect this sensitive species.

Also this area lies within Oceanside's Wildlife Corridor Planning Zone as designated in Oceanside's Subarea Plan, which is meant to protect another federally threatened bird species, the Coastal California Gnatcatcher. United States Fish and Wildlife Service (USFWS) mentions this important detail in its public comment letter and further explains that this federally-protected species could utilize the project site for dispersal.

USFWS specifically mentions the following from Section 5.3.1.1 of the Subarea Plan (General Development Standards for the Wildlife Corridor Planning Zone), and recommends adherence to this policy:

“removal of native habitats shall be avoided to the maximum extent feasible, without precluding reasonable use of the property. New development on existing properties larger than 2 acres within this zone shall conserve at least 50 percent of the parcel as open space and may remove no more than 25 percent of the coastal sage scrub habitat.”

The Final EIR responded to this comment by stating that “This entire property was previously developed and therefore not subject to the General Development Standard.” The previous operation at this site ceased about five years ago, thereby allowing the land to revert to a more natural state with minimal disturbance, which increased its biological value. We should not consider every parcel as developed simply because it was developed once in the past because the habitat could now support our local sensitive species for foraging, nesting or dispersal. I therefore concur with USFWS that at least 50 percent of the parcel should be conserved as open space to adhere to the Subarea Plan.

I urge the City to please stick to its commitments to protect our sensitive wildlife per the Subarea Plan. Thank you for considering my recommendations.

Sincerely,
Jennifer Jacobs

Robert Dmohowski

From: Jennifer Jacobs <jenn.jacobs@yahoo.com>
Sent: Monday, February 10, 2025 10:22 AM
To: Robert Dmohowski
Subject: Eddie Jones project - Final EIR comment letter

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr. Dmohowski,

I am writing to once again express my opposition to the Eddies Jones warehouse project. I do not live close by but am deeply concerned by the potential impacts to Oceanside residents as well as sensitive wildlife. There must be more suitable locations for 24-hour distribution centers with over 600 parking spaces and 57 truck bays than right next to our largest and most important wildlife corridor. It's truly baffling to see a project of this scale in this location with a parking lot right next to the San Luis Rey River trail.

As a wildlife biologist, I cannot emphasize enough how important the San Luis Rey River is to our local wildlife, as it serves as the largest continuous wildlife corridor in the area. Its importance was acknowledged in Oceanside's Subarea Plan, the City's commitment to the San Diego Multiple Habitat Conservation Program. The section of the San Luis Rey River near the project site provides critical breeding habitat for the endangered bird species Least Bell's Vireo. Figure 3 of the Biological Technical Report (BTR) within the EIR shows a number of documented occurrences of this species near or within the project boundary. However, the report contradicts this information by stating that 1) "Least Bell's Vireo is known to occur approximately 1.4 miles east of the project site," which is false and 2) "there is no suitable habitat to support this species within the project site," which we know is also false from the documented observations.

I urge the City to require protocol surveys for this endangered species, as they were surprisingly not conducted. This was also noted by the California Department of Fish and Wildlife (CDFW) in their public response to the EIR. I have worked with this species along the San Luis Rey River and know they often nest at the southern edge of the river's riparian habitat. Therefore, it is very likely that they use the northern edge of the project boundary, which is less than 100 feet away from the riparian habitat. I strongly encourage that a larger buffer zone be implement to protect this sensitive species.

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I urge the City to please stick to its commitments to protect our sensitive wildlife per the Subarea Plan. Thank you for considering my recommendations.

Sincerely,
Jennifer Jacobs

Robert Dmohowski

From: Kent Williams <the.kent.williams@gmail.com>
Sent: Monday, February 10, 2025 10:28 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; clerk@oceansideca.org; Planning-Planning Commission; City Manager; City Clerk
Subject: I Oppose the Eddie Jones Project

Warning: External Source

Hi City Leaders,

I am a resident of Oceanside near where the Eddie Jones project is proposed. I oppose this development, and encourage you to NOT grant them a zoning exception. I'm concerned about the increased pollution and added traffic to an already busy area.

Thank you,

Kent Williams

Robert Dmohowski

From: Kim Whittemore <kimwhit@gmail.com>
Sent: Monday, February 10, 2025 2:44 PM
To: Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; Robert Dmohowski; Zeb Navarro
Cc: OceansideSpeaksOut@gmail.com; Mission Cove Seniors; sspear@chworks.org; rsingh@conam.com; John Carroll; philip.diehl@sduniontribune.com; tigest.layne@voiceofsandiego.org; Kyle Krahel-Frolander
Subject: Subject: Urgent Public Comment Opposing Eddie Jones Warehouse Project – Request for Inclusion in Public Record

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mayor Sanchez, City Council Members, Planning Commissioners, and Planning Staff,

I am writing to express my strong opposition to the proposed Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project. While I recognize that the city has met its minimum legal notification requirements, I urge you to consider the broader ethical, environmental, and procedural fairness concerns this project raises.

1. Exclusion of Impacted Communities from the Public Process

The City's Enhanced Notification Policy may extend beyond state law. However, it still failed to notify hundreds of residents within walking distance of the project site, specifically residents of **Mission Cove, La Misión, and Community Housing Works / Mission Cove Seniors**. These residents, many of whom are **low-income and/or low-income seniors**, will bear significant impact from increased truck traffic, air pollution, and noise, yet they (we) were excluded from formal outreach.

Unlike homeowners who chose to buy in an industrial area, these residents secured competitive low-income housing with no forewarning of an industrial expansion in their immediate surroundings. Their voices deserve to be heard before this project is approved.

2. Failure to Conduct Meaningful Community Outreach

The City's own policies require a **Community Outreach Plan** before a project can be scheduled for a hearing. There is no indication that the applicant made any effort to engage senior and low-income residents in the surrounding area.

I respectfully request that the Planning Commission **require evidence of proactive outreach** to these communities, and if none exists, **delay a decision until proper engagement occurs**.

3. Disproportionate Environmental Impact on Vulnerable Residents

The Final Environmental Impact Report (FEIR) highlights significant concerns related to **air quality, noise, and traffic**, which will have a disproportionate effect on vulnerable populations, particularly seniors and low-income residents. The failure to include these impacted voices in the public process raises serious environmental justice issues.

Formal Request:

I urge the Planning Commission to:

- **Delay approval of the project** until proper outreach to impacted communities has been conducted.
- **Consider the environmental justice implications** for vulnerable residents who were not given an opportunity to participate meaningfully in the process.
- **Ensure this letter is entered into the official public record** for tonight's meeting.

I appreciate your time and consideration. Oceanside residents, especially seniors, low-income individuals, and those without the ability to relocate, deserve fair representation in decisions that directly affect their quality of life.

Sincerely,

Kim

Kim Whittemore
3229 Mission Cove Way, Unit 204
Oceanside, CA

[Kim Whittemore](#)
781-258-1116

Robert Dmohowski

From: Anna Kasperowicz Kasperowicz <akasperowicz@yahoo.com>
Sent: Sunday, February 9, 2025 1:36 PM
To: Anna Kasperowicz Kasperowicz; Zeb Navarro; Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Oceanside Planning Commission,

I am writing to express my concerns regarding the proposed project in our neighborhood. As a homeowner, I am deeply troubled by the potential impact this development could have on my family's quality of life, as well as on the broader community.

As a mother of two children, my daily routine involves taking them to school and extracurricular activities. One of my biggest challenges is traffic. My two-mile drive to and from school can, at times, take up to 25 minutes due to delays on Highway 76 and Mission Road. At peak times, with trucks added to the mix, especially in the morning and after school, this situation will only worsen for all of Oceanside. My home is within 1,500 feet of this proposed warehouse, and it is also located next to a stop sign on Benet Road. My children's bedrooms face a street where trucks frequently pass by, accelerating to speeds of 45 miles per hour. I find it difficult to understand how this will not negatively affect the quality of life for my family and others in the area.

The city has indicated that trucks will be allowed on Benet Road, as well as the 500 employees who will drive through the residential area of Benet. This statement seems contradictory, and I am concerned about how the Planning Commission plans to prevent these trucks from creating further disruption and safety concerns in our neighborhood.

Additionally, the construction noise from a development of this size—three football fields within 1,500 feet of my home—will be unreasonable, and the pollution from diesel emissions will have health impacts on families living in the community, including children at the nearby skatepark. Studies have shown that air pollution contributes to asthma and other respiratory issues in children. Has the city conducted an independent study to assess the potential impacts on air quality based on this upcoming project, as well as the current state of traffic in 2025? Moreover, the developer has not addressed the comments regarding air pollution sent by the air quality expert in the environmental impact report.

The project's traffic report uses outdated data from 2021, which is no longer reliable. With the rapid growth in the area and multiple large projects underway, pollution and traffic will undoubtedly be much higher than anticipated. The claim that the project will only increase traffic by 1% seems vastly inaccurate given the new developments that were not accounted for in the report.

Lastly, I have concerns about fire safety and evacuation. As we saw during the Malibu fires last month, families were forced to flee on a two-lane road with limited access. Fire trucks could not get to the area until bulldozers cleared the cars. Benet Road, with only two exits and a narrow two-lane structure, raises significant safety concerns in the event of an emergency. I do not feel confident that the current structure of streets and city resources would be adequate if this project is approved.

I believe this project offers no substantial benefit to the community at this location. Instead, it will likely result in decreased property values, increased safety hazards, and a deterioration in the health and well-being of residents. This would be a step backward for our neighborhood.

I strongly urge you to update the traffic and environmental data with an independent assessment and health expert to ensure that the impacts of this project are fully understood. This will provide a more accurate picture of the risks involved and allow for a more informed decision. I cannot support this proposed project, or the conditional use permit requested.

Thank you for your attention to this matter. I hope you will consider the long-term consequences of this project on the residents of our community. I look forward to your response and to any steps you will take to address these concerns.

Sincerely,

The Floyd Family

Anna Kasperowicz -Floyd, Justin Floyd, Lukas Floyd, Natalia Floyd
496 Big Sky Drive
Oceanside, Ca 92058

Elzbieta Kasperowicz
698 Rivertree Drive

Robert Dmohowski

From: M Bash <prtymrty@hotmail.com>
Sent: Monday, February 10, 2025 11:14 AM
To: Eric Joyce; Robert Dmohowski; Zeb Navarro; Esther Sanchez; Jimmy Figueroa; Peter Weiss
Cc: City Manager
Subject: Eddie Jones Vote No

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

My elected officials,

I will be unable to attend this evening due to a death in the family.

I moved to hills of Oceanside for fresh air, and the quiet life. I opposed the Eddie Jones Project. It is too big and will disrupt my quality and everyone near me quality of life.

Noise and Air pollution, not to mention the extra time spent on roadways due to traffic.

Keep our community clean and calm. Please Vote no.

Sincerely,

Marty Basham

Sent from my T-Mobile 5G Device
Get [Outlook for Android](#)

Dear Planning Commissioners:

I strongly urge you to deny the Conditional Use Permit (CUP) application for the proposed project. This massive industrial facility, with its sprawling 50,000 square feet and trucking terminal, is simply incompatible with the neighborhood. It will contribute significantly to the worsening traffic situation at an already failing intersection, one that is critical to the safety and accessibility of the community. This additional congestion threatens not only daily commuters but also emergency ingress and egress, endangering the lives of residents who rely on these routes for urgent access.

While the Light Industrial zone was designed to accommodate industrial uses, the sheer scale of this operation is an overwhelming disruption to the area. It will bring noise pollution, traffic gridlock, and air quality degradation—disrupting the peaceful character of the neighborhood and burdening the small businesses that currently thrive near the airport. The project's impact extends beyond just the intersection—it is a direct threat to the quality of life for surrounding residents and the long-term sustainability of the community.

The proposed construction of a dedicated right-hand turn from Benet Road does nothing to address the core issue: the already inadequate capacity of the SR-76/Benet Road intersection. The influx of heavy trucks will only exacerbate the traffic backups, especially during peak hours, further worsening the flow on SR-76 and creating a hazardous bottleneck that will affect residents and travelers alike.

This project will have long-lasting repercussions for the people who live here. As a homeowner of the neighborhood directly north to the project, I can personally attest to how profoundly this facility would affect my family and the community at large. The city is not required to approve any CUP, and this proposal should be scrutinized with the community's best interests in mind. The consequences of approving such a large-scale industrial facility could be disastrous for the environment, local infrastructure, and businesses that depend on a functional, non-industrial atmosphere.

There was a vegetation fire in May of 2022 in our neighborhood where evacuations were voluntary. Our family chose to evacuate and traffic getting out of the neighborhood was gridlocked and we left right away. I cannot imagine if the fire were larger in scale or evacuations were mandatory. While I fully support the public safety services of the city, if a larger scale evacuation would be needed, this project would impact the area in some way. It is unrealistic to state that this project would have no impact. The project site is designated as a Very High Fire Hazard Severity Zone (Section 1.2.2.1 of the FEIR) and would impact the surrounding area.

This is not the right project for this neighborhood. It is out of scale, out of place, and out of touch with the needs and aspirations of the people who live here. I respectfully ask that you reject this application and choose the path that best serves the long-term well-being of our community.

Sincerely,
Melissa Flores
622 Wala Dr.
Oceanside, CA 92058

Robert Dmohowski

From: cantonmichaela@gmail.com
Sent: Monday, February 10, 2025 12:16 PM
To: Planning-Planning Commission; Eric Joyce; Esther Sanchez
Subject: Eddie Warehouse project

Warning: External Source

I am opposed of the Eddie warehouse development due to the extremely increase in size and lack of understanding by the planning department of the negativity impact to the area and specifically to the human population living near the site. It appears that the council and planning do not have any ideas what the warehouse with a 24 h operation will be. The noises, the smog, traffic and so on will be detrimental to human body(lack of sleep, cancers, etc) Secondly, the future of the resort , will be economically affected by it. Where are the competent people, working for the city, in truly scrutinize the project?!? We are 3 miles from the ocean and this is what Oceanside wants ?

Regards,
Micaela Canton

Robert Dmohowski

From: Penny Russell <pr-omha@marketec.com>
Sent: Monday, February 10, 2025 3:11 PM
To: City Council
Cc: Robert Dmohowski
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City Council and City Planner,

Please reconsider moving ahead on the Eddie Jones Project. A warehouse of this magnitude will have a negative effect on the surrounding communities in several ways:

1. The wear and tear on our local roads will be exacerbated.
2. In the event of an emergency, large semi-traffic will impact the local residents' ability to evacuate quickly.
3. When the westerly winds pick up the fumes of the diesel fuel will blow back into our surrounding communities; especially the senior mobilehome communities to the east of this potential project.
4. With the addition of the WavePark being built in the near future, the bottle neck this warehouse traffic will create on the already-crowded 76 is unthinkable.
5. The potential noise pollution in the closest surrounding residential areas will be intolerable.

There is a reason you see this type of capacity warehousing off the 15 freeway, out in the middle of nowhere. Please consider "people first over profit."

Thank you,
Penny Russell
818-399-2661

February 10, 2025

Chairperson Thomas Morrissey
Oceanside Planning Commission
300 North Coast Highway
Oceanside, CA 92054
PlanningCommission@oceansideca.org

Robert Dmohowski, Principal Planner
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054
rdmohowski@oceansideca.org

CC: Mayor Esther Sanchez, esanchez@oceansideca.org
Deputy Mayor Eric Joyce, ejoyce@oceansideca.org
District 2 Councilmember Richard Robinson, rwrobinson@oceansideca.org
District 3 Councilmember Jimmy Figueroa, jfigueroa@oceansideca.org
District 4 Councilmember Peter Weiss, pweiss@oceansideca.org
Oceanside City Manager Jonathan Borrego, CityManager@oceansideca.org
Oceanside City Clerk Dr. Zebb Navarro, Ed. D., znavarro@oceansideca.org
Oceanside Speaks Out, OceansideSpeaksOut@gmail.com

RE: Eddie Jones Warehouse Manufacturing and Distribution Facility Project
Applicant: RPG Oceanside Eddy Jones Way Owner LLC
SCH Number: 2022070365
Application: "Consideration of a DEVELOPMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001), and VARIANCE (V22-00001) to allow the construction of a warehouse, manufacturing, and office facility on a 31.79-acre site located at 250 Eddie Jones Way (APNs: 145-021-29, 30, & 32)."

Dear Chairperson Morrissey and Members of the Oceanside Planning Commission:

Teamsters Local Union No. 542 represents over 6,000 warehouse, logistics, delivery and related workers in San Diego County, including in Oceanside. As both community residents and workers in the industry, Teamsters want businesses to be both good employers and good neighbors.

The Planning Commission is considering the "Multi-Building and Truck Bay Reduction Alternative" (ie., the "4 Building Option") provided in the Final Environmental Impact Report (FEIR) for the Eddie Jones Project." Even though this option was designed to address some community concerns, we do not feel that it is in the best interests of Oceanside. We therefore urge the Planning Commission to reject the project, including certification of the Final Environmental Impact Report (FEIR), the proposed Conditional Use Permit (CUP) and any other necessary project approvals.

This Could Be An Amazon Project:

One reason Local 542 has participated in the deliberations over this project is the possibility that it could be an Amazon facility. We believe that Amazon facilities are bad for both workers and the communities they are located in. Jobs at such facilities are known for subpar wages,¹ and high injury and turnover rates.² Amazon has also faced multiple allegations of violating federal labor laws. These facilities also generate particularly acute impacts on traffic, parking and the environment.

¹ Bloomberg. [Amazon Has Turned a Middle-Class Warehouse Job into a McCareer](#). December 2020.

² "The Injury Machine: How Amazon's Production System Hurts Workers," Strategic Organizing Center, April 2022.

The developer – RPG – states that “no tenants have been identified for the project. However, the project is not consistent with Amazon’s distribution center specifications. Further, it has been reported recently that Amazon is scaling back on its warehouse locations. These two factors make it highly unlikely this space would be leased to Amazon.”

We continue to believe that this project could house one or more Amazon facilities, despite the developer’s arguments:

- The developer says the Eddie Jones project “is not consistent with Amazon’s distribution center specifications,” but Amazon has many types of facilities with various “specifications”:
 - These include sortation centers, fulfillment centers, airhubs, and cross-dock facilities.
 - Amazon is also evolving new structures. In 2023 and 2024, Amazon rolled out dozens of new “sub-same-day” (SSD) warehouses across North America.
 - These various facility types have a wide variety of “specifications”: they vary in size from less than 100,000 square feet to more than 5 million square feet. Amazon has created multi-tenant and multi-story projects. It leases some facilities and owns others. Some are increasingly using robotics and AI.
- It is true that Amazon has not been expanding as quickly as they did in 2020 and 2021, but they continue to build and lease warehouses. The City of Brea – about 60 miles away from Oceanside – just released the Draft EIR for a 150,000 square foot Amazon warehouse. A 3-million square foot warehouse is under construction in Niagara, NY and a developer is currently seeking approvals for a 4-million square foot warehouse in Waterbury, CT.

Moreover, Amazon tried – and failed – to build a 142,746 square foot last-mile delivery station in Oceanside in 2021. Amazon last mile facilities are typically between 100,000 to 150,000 square feet, and the “4 Building Option” under consideration will have “four separate buildings ranging in size from 109,660 to 134,015 square feet.” This obviously raises the possibility that this project could house up to 4 such last-mile facilities.

In development projects in other cities, Amazon has instructed partner developers to conceal Amazon’s involvement or interest. Given this, the developer should divulge if they have had any contact with Amazon related to the Eddie Jones project.

Inadequate Focus and Traffic Analysis in the EIR:

Local 542 submitted comments on the Draft EIR that were not adequately addressed in the Final EIR. We want to reiterate some of those points:

The focus of the EIR was too narrow and inadequate:

- The EIR “focuses entirely too narrowly on the Site itself, despite the fact that it is situated so closely to at least one existing residential area, a developing residential area..., a river, and several wildlife and nature preserves.”
- “The project setting description, or descriptions, throughout the DEIR are inadequate because they do not sufficiently incorporate any description of sensitive and protected nature reserves near the Site – and, importantly, along the identified trucking routes for the Project.”

The traffic impact analysis was also flawed:

- “The assumptions incorporated into the EIR’s traffic analysis does not adequately capture the type of facilities actually permitted for the Project... While an EIR does not need to study every possibility, reasonably foreseeable uses must be studied... Typically an EIR preparer handles this requirement by considering the most-intense use or mix of uses being entitled. However in this instance, the preparer studied *an* intense use, but by failing to consider a different character of use, did not study foreseeable impacts.” The analysis assumed a “high cube warehouse” which are typically over 200,000 square feet, even though other uses are permitted, including a last-mile distribution center, which entail different VMT profiles.
- The proposed mitigation measure for the transportation impact is inadequate because: 1) it fails to consider the possibility that the project could be used for last mile deliveries, and 2) because it assumes a “Voluntary Employer Commute Program” that is completely speculative, with no evidence that workers would participate.

The Eddie Jones Project Is Harmful to the Local Community:

Finally, we do not think this is the right project for Oceanside. We agree with the points raised by Oceanside Speaks Out:³

- This project will create severe traffic congestion as well as air, noise and light pollution – harming the community's safety, health, and well-being.
- This is especially concerning given the project's proximity to the Airport Community, OceanKamp, Alex Road Skate Park, the Whelan Lake Bird Sanctuary and the San Luis Rey River.
- There are inadequate road/traffic improvements planned, even though the project will use the only 2 roads in and out of the community.

For all the foregoing reasons, we strongly urge the Oceanside Planning Commission to reject the Eddie Jones project, to deny certification of the Final Environmental Impact Report (FEIR) and to reject any associated approvals, including the Conditional Use Permit (CUP). Thank you.

Sincerely,



Salvador Abrica
Political Coordinator
Teamsters Local 542
(619) 582-0542
sabrica@teamsters542.org

³ [OjzG8SvjRLuWen_8Uq_OKe0Wi8y9TPaiHQHM8xLwZxc2UjgpdJDTRTDa9PtxWbLpA42KcYkF2u1AodMp0](#)

January 27, 2025

Dear Members of City of Oceanside Planning Commission:

I would like to state the following objections to the FEIR of the Eddie Jones Warehouse, Manufacturing and Distribution Facility Project:

1. Inadequate noise level testing for the 4th version plan proposed. No significant noise reduction for the plan has been shown to reduce noise disturbances even with the proposal to reconfigure the building placements to surround noise sources. Please show significant measurements.
2. Inadequate testing for noise levels in both in the DEIR, and FEIR.. Testing is absent appropriate measurements and controls to show: combinations of sources at intervals through the entire workday during routine operations – from low use to peak operations - over the course of a year - as well as effects shown from cumulative sound measurements taken from appropriate distances.
3. The absence of noise testing including Prince of Peace Abbey church grounds, open to the public, from the foothills of the property to the plateau and surrounds. Prince of Peace Abbey offers visitors opportunities for quiet enjoyment, and peaceful reflection.
4. The exclusion of mention of Prince of Peace Abbey in both DEIR and FEIR with respect to all CEQA stipulations. The Prince of Peace's listing under in the City of Oceanside General Plan in Aesthetics – for its scenic vista, and ranking with other locations as meeting Aesthetic standards is omitted. Visual Resources in the City of Oceanside are listed as scenic vistas viewable for miles around. The Prince of Peace Abbey also qualifies as a viewpoint onto the scenic watershed of the San Luis River, and surrounding coastal plateaus and valleys viewable to Camp Pendleton, its environs also enjoyed as a scenic resource listed in the Oceanside General Plan – Aesthetics.

How has the Eddie Jones Project shown it will not disturb the lifeway and peace of both human and wildlife within hearing and sight of its operation? Eddie Jones Project has not shown that Airport Community households, Wanis Preserve, and its local area residents, as well as worshippers and visitors and the monastic community all within earshot of the proposed complex will not be exposed to disturbing sounds as will multi-species so exposed by the excess of decibels.

How does Eddie Jones Project noise testing show how noise will expand upward and outward from the site?

How has the Eddie Jones Project FEIR shown protection of Infant ears from noise disturbance? Families working from home? Protection of birdsong from mating disturbance and vulnerability to predators? Sleeping families? Worshippers who come to pray in quiet, who join visitors from all over the world to enjoy the peace of the Prince of Peace Abbey?

Lastly, how can any projections posited by Eddie Jones Project be considered as relevant being that a tenant is not yet established? Without tenant, how can the Project 'idea' be shown to have been properly tested? How can fire protection be assured – evacuation measures, etc?

Please address these concerns and those of a large segment of Oceanside.

I await your response,

Windy Bravo Dolan , Oblate

Prince of Peace Abbey

windybravo@yahoo.com

Robert Dmohowski

From: Eliza Terhan <IrishSeaLass@outlook.com>
Sent: Friday, February 7, 2025 9:32 AM
To: Robert Dmohowski
Subject: OPPOSED to Current EDDIE JONES Project in conjunction with build-out on Airport Rd

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

REGARDING THE “EDDIE JONES” PROPOSED PROJECT/S

When all the surrounding neighborhoods were approved by the City of Oceanside, and then built out across and up the hill from TE Connectivity, this parcel contained a large, somewhat unobtrusive building. In part, this was due to the large green area surrounding the building, including picnic areas and large trees.

This land use was ACCORDING TO ZONING, and the large park-like grounds served as a nice transition from Hwy 76, to across the river and into the neighborhoods.

The current proposal requires a ZONE VARIANCE, and proposes to cover the vast majority of the land with concrete buildings, truck bays, and parking. That a project of this size is so close to the river, with the ocean just a few miles downstream, is a very poor idea, ecologically. (The trees have already been removed – it is clear that environment and aesthetics play no part in this proposed development.)

The number of truck bays and related heavy traffic is unsafe for all of the people in the neighborhoods that must travel between this development and the main highway. I am strongly opposed to this current plan!

I also learned that there will be even more traffic and activity than we were made aware of at all the informational meetings. The City should have been up front about the 4-story hotel and 4 office buildings being shoe-horned into small parcels of land on the south side of Airport Road.

Currently, it is already a challenge for cars to turn left onto Benet from Airport Rd, as they attempt to access Hwy 76. All this additional traffic will beg for a traffic light, which - so close to the Benet/Hwy 76 intersection - will be a traffic back-up nightmare.

To complicate this traffic snarl and safety situation with a project the size of the current Eddie Jones proposal is simply unacceptable.

In earnest,

Jayne Moynihan

3351 Toopal Drive

(Within the 1500 ft of this proposed massive build-out.)

Sent from [Outlook](#)

Response to the recently released Final Environmental Impact Report

(RAF PACIFICA GROUP)

WildFire Issues with the Fourth Iteration of the Project

Prepared by Michael Tenhover (February 7, 2025)

The following memo describes concerns with the Fourth Iteration of the “eddie jones warehouse project” (the “Project”). The Wildfire Evacuation Study (WES) section is seriously flawed with its identification of where a wildfire will start, what areas should be of concern, available evacuation times for the community, and the amount of time it will take for a Wildlife to threaten the nearby community

Table of Contents

- I. Major Issue with the FEIR Fourth Iteration - related to Wildfire
- II. FireWise/Cal Fire identification of Areas of Concern- Fire Safety
- III. Recommendations- Studies that must be completed before the Project can be considered for approval

Appendix I- Fire Safety Issues- Planning Commission August 2023

I. Major Issue with the FEIR- Fourth Iteration- related to Wildfire

In the FEIR, the Wildfire Evacuation Plan has used a number of questionable and unreasonable assumptions. Based on these, the plan states that in the event of a Wildfire, the Wanis View community will have 2 hours before they are threatened by the fire. They also state that with the Project and the issues with evacuation routes, a worse case for the time to evacuate is 1 hour and 32 minutes. **None of these assumptions/results seemed to be accurate.**

Specific Problems:

1. The Study has assumed a start location for the Fire, shown below:

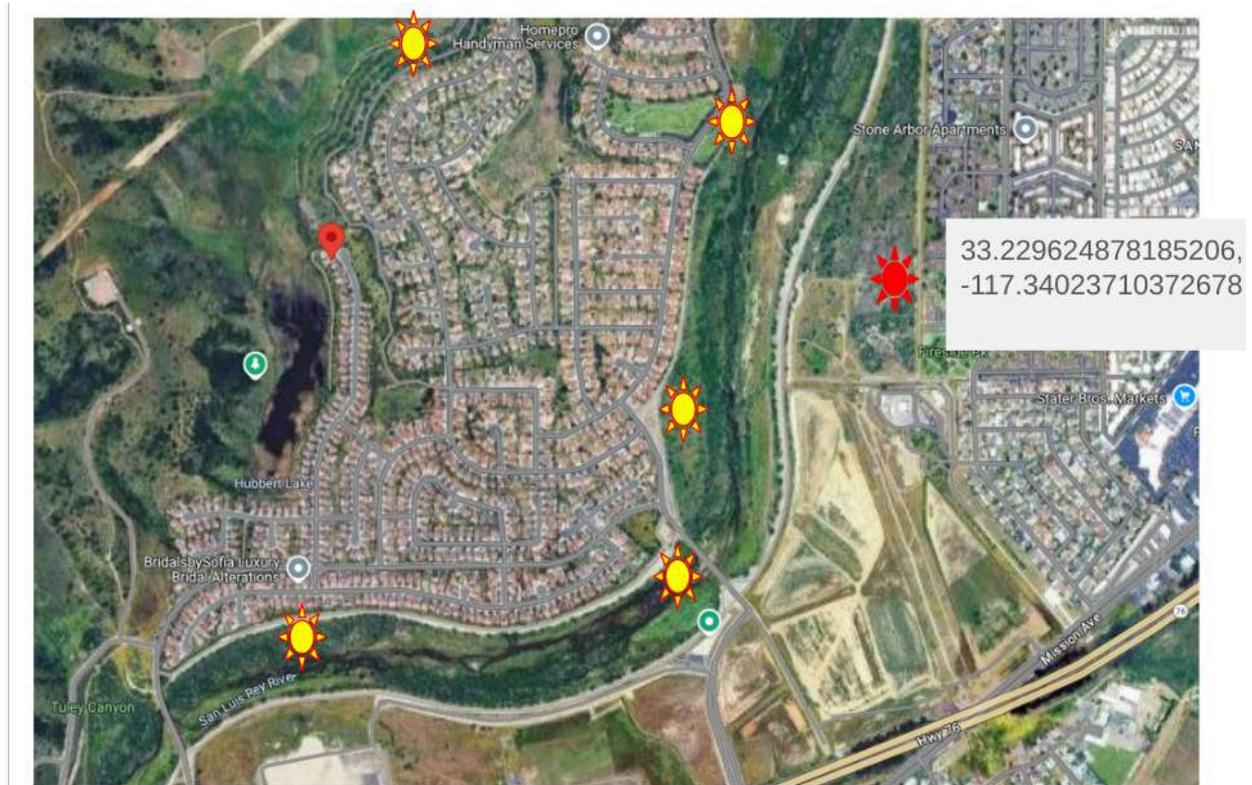


Figure 1.

The fire is assumed to start at the RED symbol on the Right/Middle of the figure above (GPS coordinates shown). This is in an area with low levels of fuel as well as low levels of fuel to the East and South and as far away from the area as possible. This seems to be a “best case” for both slow fire propagation as well as distance from the Wanis View Homes and the Project.

It is not surprising that the study found this fire to move slowly and provide time for evacuation, but it is not at all realistic.

2. Studies need to include both other start points and variations in wind direction.

Also shown in the above figure are Yellow symbols that are equally (if not even more likely for a wildfire to start). The WES needs to use the same methodology to evaluate the fire behavior for these locations. They also need to include variations in wind direction– not just East, but NorthEast, and even NorthWest.

Wildfires can start from natural sources but also can be the result of open fires (homeless camps cooking, heating), off-road vehicles, and arsonists.

3. Fuel Loads in the area are highly variable

The area in question can have huge variations in the fuel loading depending on the amount of seasonal rain, brush abatement measures, and summer heat. This input also needs to be included in the fire models.

4. The appropriate goal of a WES.

The goal of this exercise should be to find the minimum time for a fire to reach the homes, not the maximum time a fire can burn in the area before being a threat.

It is easy to see that a Wildfire starting on any of the Yellow symbols will be an almost immediate threat to the community.

5. Evacuation Times for the Community.

We are lucky in Oceanside to have excellent leaders in both our Fire and Police Departments. However, even these capable groups will find it difficult to evacuate this complex neighborhood in the time that will be available. Rather than the two hours claimed in the WES, there may be as little as 30 minutes to perform this essential task, depending on where the wildfire will start and wind direction. The added difficulty might be the day of the week as well as time of day for the evacuation. **With such a tight timeline, the impact of the Project and associated heavy trucks will have on hindering evacuation will be very significant.**

II. FireWise/CAI Fire identification of areas of concern.

Volunteers in the Wanis View area formed a Firewise program in 2023. In part of the certification process they worked with Cal Fire to identify areas of concern for Fire Safety. This exercise and results were presented to the Oceanside City Council in August 2023 (Appendix I). Shown below is the result of this evaluation:

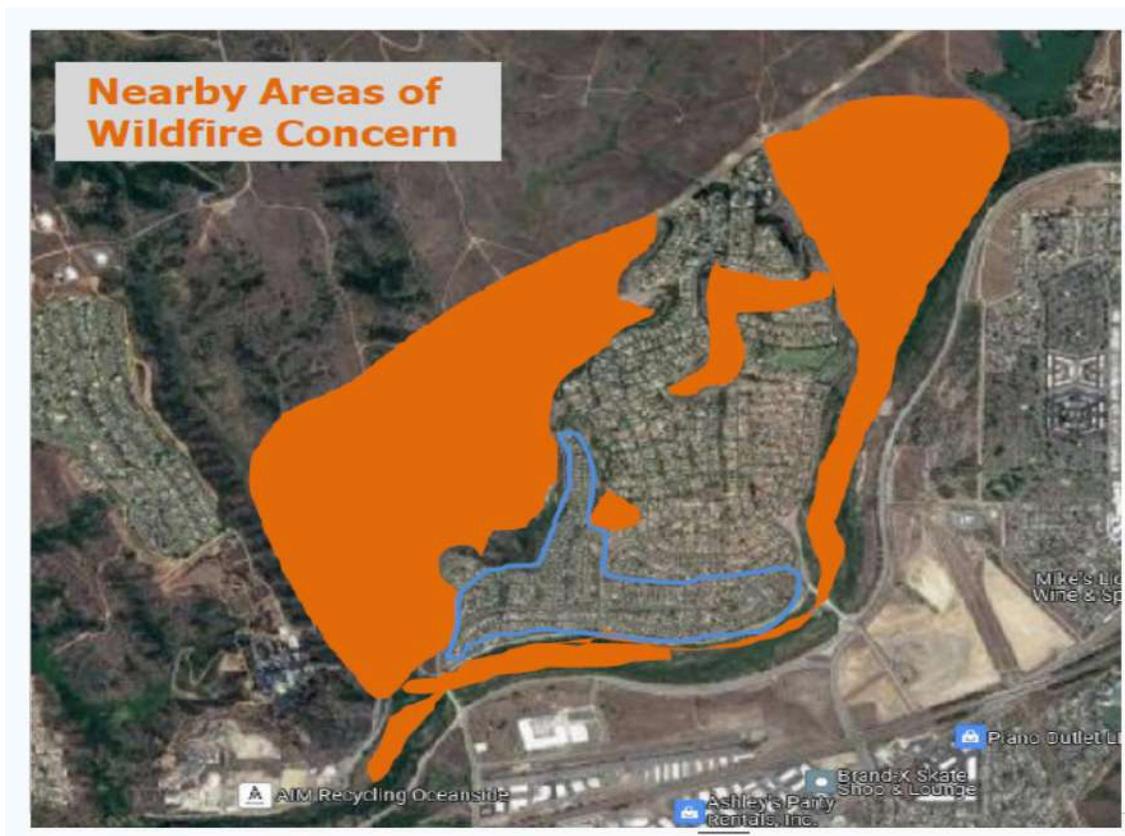


Figure 2

The Orange areas are places based on fuel, terrain, and wind direction that were designated “Areas of Concern”. This information is not considered in the WES

and paints a very different picture of where wildfire could start, where abundant fuel is sometimes present, and the proximity to the residents.

III. Recommendations- Studies that must be completed before the Project can be considered for approval.

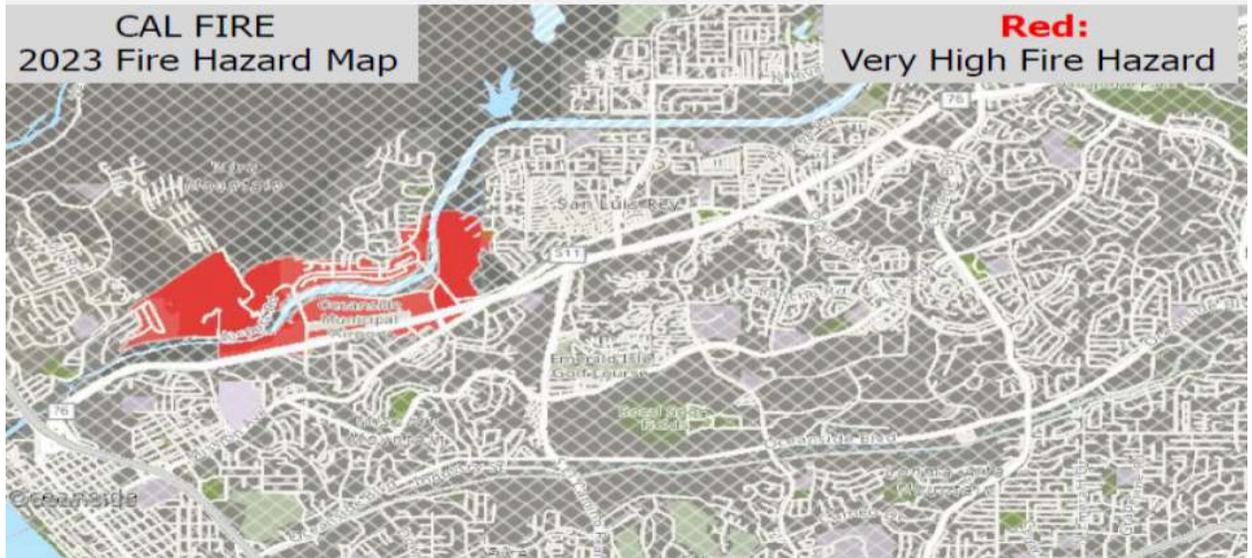
They must include:

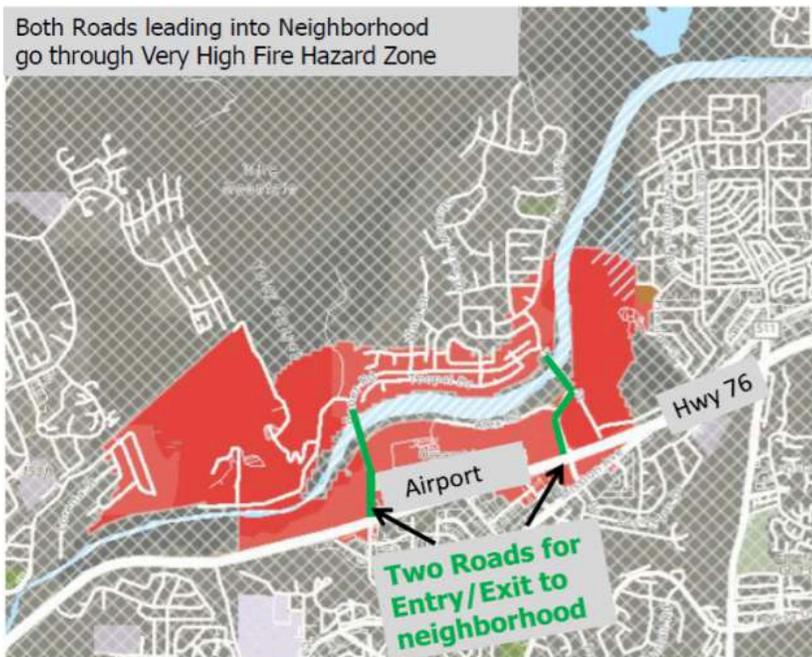
1. As described above we need studies that provide information on the “most dangerous” scenarios, not just one favorable for the Project.
2. These should include at a minimum, the locations shown in Figure 1 - Yellow symbols.
3. The studies must also look at variations in wind direction and fuel loads

Appendix I. Presentation on Fire Safety Issues Oceanside City Council (August 2023)



FIREWISE PROGRAM– Only Oceanside Area in CAL FIRE’s Very High Fire Hazard Zone

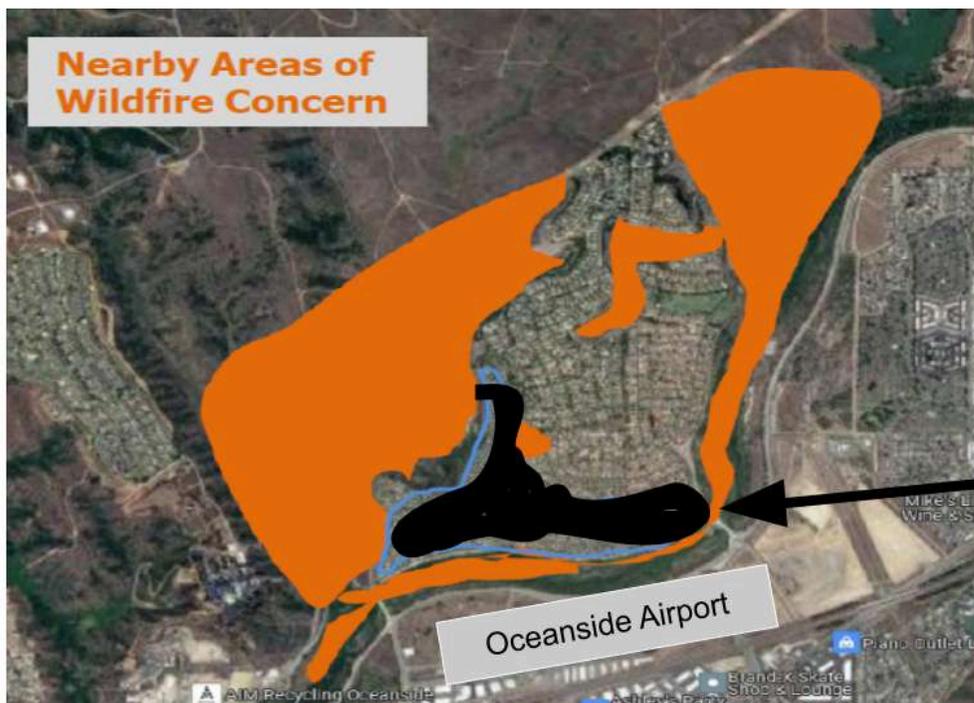




Evacuation and
Emergency Vehicle

We only have two
roads, both of
which are in the
Fire Hazard Zone

5000 residents



Our
Program
Area

Comments in Opposition to the Eddie Jones Warehouse Development

By Dolores L. Laag, Long-standing Oceanside Resident

Dolores L. Laag, 611 Rainwood Ct, Oceanside, CA 92058

To whom it may concern: I respectfully submit the following comments in opposition to the Eddie Jones Warehouse Development Project and request that the permit for the project be denied for the reasons stated in the text below. In addition I request that my comments be fully read by those responsible for either affirming or denying permitting of the project. This is so they may understand how I arrived at my opinion instead of just “checking the box” that I stand in opposition. I feel that my comments, albeit less technical, less clinical and less hypothetical represent a more practical and down to earth understanding of the actual conditions in the community in question.

In 1987 a new residential area was opened within the City of Oceanside just North of the San Luis Rey River. My Parents, Manuel and Betty Carrasco relocated there from Riverside. My son and I spent most of our weekends there visiting the grandparents and we developed many long term relationships with others living in the community. When my parents became ill I began to spend most of my time there taking care of their needs. This turned into a full time occupation and after my Mother passed recently I have taken over their residence and continue to live there. This property lies approximately 1,900 feet from the nearest project boundary.

The only ingress and egress to the community are two single lane roads, Benet and Foussat. Each provides a signal lane in each direction over the San Luis Rey River. The community is land-locked by the river on the South and Camp Pendleton to the North. In an emergency there are only 2 lanes of traffic to evacuate the entire community on. There is no exit whatsoever through the military reservation due to both legal and terrain constraints.

In the Environmental Impact Report submitted for The Eddie Jones Warehouse Development Project they claim parking for almost 600 vehicles and over 150 trucks/trailers distributed around the property. This is the equivalent number of vehicles that would be parked at a public shopping mall complex. I can not think of any examples of shopping malls in the State of California where a city allowed single lane access point. Here the only access will be on Benet or Foussat which are shared by the residents of my community. They list streets and highways nearby but if the vehicles can't negotiate segments of Benet or Foussat and their traffic control signals at the 76 first they won't be able to make it to any other street or highway. Everyone who lives in the Oceanside Community knows that the I5 to the West is stalled in either direction for more than half of each day and that each signal on the 76 is usually backed up 50 to 100 cars per cycle. Under normal everyday conditions the project is going to seriously impede the flow of traffic in the entire area. This is an undeniable fact and the residents of my community will be asked to shoulder a majority of the burden.

The creators of the EIR also report that “this evacuation study acknowledges that the project does not expose people or structures, either directly or indirectly, to a significant risk of loss, injury or death involving wildland fires”. I can’t begin to tell you how astoundingly absurd that statement is.

I was present during the fire storms of 2003 and my Mother, a profound Parkinson’s Patient, and I were under an evacuation watch for over a week. The Cedar fire burned from the East County to the ocean with behavior identical to that of the recent fires in the Los Angeles Palisades and Alta Dena areas. The EIR doesn’t even consider such a possibility for a fire storm once the project is completed and in full operation. As of this writing over 16,000 structures burned in the recent Los Angeles area fires killing almost 30 people. I hope I’m wrong but it is very likely that it could again happen here within my lifetime.

Based upon what everyone of us saw in Los Angeles the residents of my community will be trapped in just such a firestorm (¹). The cars and trucks being evacuated from the Eddie Jones Warehouse will have Benet clogged just like the canyon roads in LA were when Cal Fire had to take such extraordinary action as to use bulldozers to remove stalled and abandoned vehicles just to allow fire fighting equipment access into the communities where it was needed. In such an event Foussat will likely be closed as it is on the side where the fire storm will approach from. If the project is allowed to proceed it is my opinion that an identical disaster is waiting to happen.

The whole valley is a micro climate. One can see it clearly when the air is calm. Absent an ocean breeze or Santa Ana Wind the air is trapped between the bluffs and ridges on the Northern and Southern sides. In the Los Angeles Harbors, not located in such a valley, the particulate pollution from Trains, Trucks and Boats is so severe that many diverse plans are being implemented to mitigate it. These micro particles are believed to cause breathing and developmental problems in children and respiratory problems in adults with sensitivities to them. The trapped and stagnant air quality will drop significantly with the massive source of new airborne particulates being generated by the warehouse operation.

Finally, this whole valley was a wildlife corridor starting at the ocean and moving up the San Luis Rey Riverbed. Because of the clearing of the land on the south side of the river embankment the wildlife is now confined to the riverbed itself. The diminishment of small prey for the apex animals to feed on has forced them up into the residential areas to find easily available domestic animals. Many residents have lost pets to coyotes that freely roam the streets at night. I personally have lost a pet to a mountain lion that was ensconced in my back yard. If this project is to be built there is no possible way of this situation ever improving. The City of Oceanside has been very clear that the coyotes are part of the region and they are not going to manage them.

I think that the quality of life in Oceanside would be better served to convert this land and the other vacant parcels nearby to into parkland. It is part of a natural floodplain anyhow and at some point in time there is a real chance that a major flood will overrun the area. I think most environmentally conscious people would agree that returning the land to a riparian wetland would be a much wiser choice.

There is nothing positive or redeeming about this project. It will diminish the lifestyle of all residents and wild life in the area. It will restrict the current traffic flows in the area, specifically on Benet and Foussat Streets, but also on the 76 and I5 it feeds to/from. It is for these reasons, and others not stated here, that I am opposed to The Eddie Jones Warehouse Project and would request denial of their permit application.

Thank you for taking the time to read my response.

Signed: Dolores L. Laag

A handwritten signature in black ink, appearing to read "Dolores L. Laag". The signature is written in a cursive style with a large initial "D".

Longtime Resident, Oceanside, CA

- (1) Please watch this video as it eerily describes the exact scenario being set into motion if this project is approved: <https://youtu.be/9YSU-M0m1Jk?si=0HaCxZtCVDaf5kPM>

Appendix A, Recipients

Oceanside City Principal Planner

Rob Dmohowski

(760) 435-3563

rdmohowski@oceansideca.org

Oceanside City Clerk

Zebb Navarro, Ed. D.

(760) 435-3010

znavarro@oceansideca.org

Oceanside Mayor

Esther Sanchez

(760) 435-3057

esanchez@oceansideca.org

Deputy Mayor- District 1

Eric Joyce

(760) 435-3032

ejoyce@oceansideca.org

Council member- District 2

Richard (Rick) Robinson

(760) 435-3033

rwrobinson@oceansideca.org

Council member- District 3

Jimmy Figueroa

(760) 435-3047

jfigueroa@oceansideca.org

Council member- District 4

Peter Weiss

(760) 435-3066

pweiss@oceansideca.org

Oceanside City Manager

Jonathan Borrego

(760) 435-3065

CityManager@oceansideca.org

Robert Dmohowski

From: Jamie Hesseling <jhesseling@icloud.com>
Sent: Friday, February 7, 2025 11:29 AM
To: Planning-Planning Commission
Subject: Eddie Jones

Warning: External Source

Will do my best to vote out anyone who votes this in to know it will lower our property values, congest our daily commute, pollute our environment and lower our communities quality of life. For what? What are you getting out of this? We are starting to see the tip of the iceberg of corruption politicians are open to at the expense of the people. Careful we are all watching and the people want politicians for the people NOT for corruption or corporations! Vote for your constituents! Not your pockets! One of many frustrated voters, Jamie Sent from my iPhone

Robert Dmohowski

From: Janis <janisjones@me.com>
Sent: Friday, February 7, 2025 12:00 PM
To: Planning-Planning Commission
Subject: Item #4. Vote no on "DEVELOPMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001), and VARIANCE (V22-00001)"

Warning: External Source

Dear Planning Commissioners,

I urge you to stop the Eddy Jones Warehouse project, which does not belong in the proposed location. It is much too close to the San Luis Rey River, our beautiful Oceanside beaches, and the Pacific Ocean. Additionally, it threatens people who recreate at the Prince Memorial Skatepark and those who enjoy the San Luis Rey River Trail. These are vital, cherished community spaces that should be protected.

The proposed project is also too close to several residential neighborhoods, including Wanis View Estates, La Mission Village apartments, the homes near Buddy Todd Park on E. Barnwell and Turnbull Streets, and those by Pablo Tac Elementary School among others. In fact, resulting air pollution has the potential to impact the entire Oceanside community because coastal breezes and Santa Ana winds will carry project particulates into areas further away than expected. Allowing the Eddy Jones Warehouse to be built could even be perceived as an environmental justice issue due to its location in an area surrounded by lower-income neighborhoods. It's hard to imagine that a project of similar scope would be approved so close to a waterway, the coastline, and homes in the affluent beach towns to the south of us.

Air pollution, noise pollution, light pollution, and yes, brake and tire pollution will all affect our community in ways we cannot yet predict. Toxic tire pollution is particularly concerning to me because it is one of the leading contributors to microplastic pollution. Commercial trucks and delivery vehicles generate more tire wear than passenger cars due to their size and weight, thus contributing significantly to this alarming form of pollution. (See link below.)

We must protect Oceanside residents and the natural beauty of our coastline from destructive developments like this. If the smaller 142,000 sq. ft. Amazon facility in the Ivey Ranch area can be halted, then the massive 497,822-square-foot Eddy Jones project in an equally if not more sensitive area can also be stopped. I urge you to follow Irvine's lead in reevaluating where such large-scale warehouses should be placed now and in the future (see link below).

Mega-warehouses definitely do not belong near recreational areas, precious natural resources, or any place where people live, play, and work. Please act in the best interest of Oceanside residents and the environment.

For more information on the harmful effects of tire pollution, please see this article from Yale Environment 360: <https://e360.yale.edu/features/tire-pollution-toxic-chemicals>

For information on what Irvine is doing to stop mega warehouses, please see this article from Voice of Orange County: <https://voiceofoc.org/2025/01/irvine-rethinking-how-large-warehouses-and-residences-mix/>

Sincerely,
Janis Jones

Robert Dmohowski

From: AMY LOCKWOOD <aklmoose@cox.net>
Sent: Tuesday, February 4, 2025 10:18 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim; Peter Weiss; City Manager; Oceansidespeaksout@gmail.com
Subject: Opposition to the Eddie Jones Warehouse project

Warning: External Source

To: Oceanside City Officials

Re: the Eddie Jones Warehouse Project

February 4, 2025

Good morning,

We live in the California Brisas subdivision just north of the proposed Eddie Jones Warehouse project, adjacent to the Oceanside Municipal airport and have serious concerns about the project.

This project will increase noise and air pollution in our neighborhood and increase daily traffic on Benet, Fousat and 76, the later impacting not only our community but all our Oceanside neighbors.

Our neighborhood has needed to be evacuated in the past, with limited warning, due to wildfires. With only two egress roads, combining this project with the Wave park project currently in progress, the risk to our families (and interference with fire/police efforts) is dangerous and irresponsible.

While we understand the area is zoned for industrial use, the proposed size is far too large and would have an outsized impact on the local community and Oceanside at large.

Please do not allow a zoning exception to the developer of this project.

Thank you for your consideration,

Amy Lockwood

Lee Masters

3215 Ricewood Drive

Robert Dmohowski

From: Dee Keck <4dees@cox.net>
Sent: Thursday, February 6, 2025 11:02 PM
To: Robert Dmohowski
Subject: Letita Ivory Eddie Jones Project (Not in favor)

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

On Thu, Feb 6, 2025 at 2:28 PM LETITA IVORY <letitav@cox.net> wrote:

Being a 40 year resident of Oceanside I am in opposition of this endeavor. Our infrastructure is bursting at the seams with all the lawless building of additional homes and apartments which we voted against and will result in chaos for the city. You may not want to hear it but I guarantee you will see it with more homelessness and traffic congestion with all in/outlets in the city. Additional tax revenue is nothing but greed in which someone's pockets is being lined. I don't care what's Newsome is dictating. Look at Los Angeles as the example. We need to stop the expansion. Our city is not in favor of this project! NO on Eddie Jones!

[Yahoo Mail - Email Simplified](#)



Virus-free. www.avg.com



February 7, 2025

City of Oceanside Planning Commission
Sent via Email

Subject: Comments on Eddy Jones Warehouse Final EIR and Project

Honorable Chair and Commissioners

The Eddy Jones Warehouse Project is one you might be inclined to approve. It is an industrial use on industrial zoned land. It sat vacant for several years—providing no jobs. According to the analysis done for the Economic Development Element of our General Plan, our greatest job producing need is for even more land to be zoned industrial.

But this project has failed to address critical impacts it will have on our community. This is a project that requires a Conditional Use Permit (CUP) because it exceeds the limits of 50,000 square feet and no more than 6 heavy duty trucks at one time. This requirement for a CUP gives you broad discretion to add Conditions of Approval and ensure that the remaining concerns with this project are fully addressed.

The following are the key issues we ask you to address in the CUP:

- **Edge effects of this development on the adjacent sensitive habitat along the San Luis Rey River**

Attached are standard conditions of approval used by Oceanside for many years after the MHCP was adopted in 2003 and then got buried. Not 100% of them apply to every project. But they were all considered and incorporated as appropriate for development next to sensitive habitat. The responses to comments verify that many of these have been ignored. Feral cats are estimated to kill over 2 billion birds a year in this country. The sensitive river habitat supports endangered Least Bell's Vireo and California Coastal Gnatcatcher, among others. We submit that throughout this region one can walk behind industrial or commercial buildings and find that someone is leaving out food and water for feral cats. It is not sufficient to just say the project won't do that—it needs to be conditioned to do so. Also, the concern about lighting is not just building lighting that is required to be low sodium and directed to not spillover to sensitive habitat. The headlights from these trucks extend beyond the limits of the project and can disrupt many nocturnal animals. There is a reason for every one of these conditions—and all need to be considered.

- **GHG and Air Quality impacts from all of this truck traffic has not been adequately addressed**

5020 Nighthawk Way – Oceanside, CA 92056

www.preservecalavera.org

Nonprofit 501(c)3 ID#33-0955504

The FEIR responses to comments on page RTC-97 states “Consistent with the City’s formally adopted VMT standards the DEIR uses SANDAG’s Employee VMT by census tract...” The drivers of the trucks accessing this facility are not employees, and consequently they are not included in the analysis of VMT and the associated GHG emissions. It is a step in the right direction that the project added the recommended environmental justice mitigation measures. The Inland Empire has had extensive experience with large warehouses like this. Included are attachments that discuss some of the litigation, settlement, analysis and recommended mitigation measures that have affected that area. Our local Air Pollution Control District has a study underway to develop local standards for these large warehouses. But the requirement for a CUP allows you to incorporate the recommendations from others who have studied this issue.

- Impact on safe emergency evacuation

We appreciate that the developer of this project prepared the requested Fire Evacuation Time Study. But we disagree with the conclusion that adding 10 minutes to a worst-case scenario that already results in a 59 minute evacuation time is a “less than significant impact.” Furthermore, there are numerous recommendations in the study that need to be incorporated into project conditions. These include “The Wildfire Evacuation Study will require regular adjustment and continuous coordination by the Owner(s) and or Property Manager and fire/law enforcement agencies during each stage of the construction process.” It also says that in order to evacuate more people(as will be required with this project) that emergency managers must “ 1. Provide more lead time to evacuate and 2. Control traffic levels during evacuations so fewer vehicles are trying to exit at the same time.” It further notes the need for “diligent public education and emergency personnel training and familiarity,” and the need to require notification of the presence of special needs populations to the Fire Safety Coordinator. All of the recommendations of this study need to be formally incorporated into the project conditions of approval. **Please note that the project Conditions of Approval in Res 2025-PO4 do not reference this report or incorporate any of its recommendations.**

- Limit of “56 trucks” is not sufficient, or accurate

The project description for this new alternative specifically says it will include bays for “56 heavy trucks plus 45 grade level loading docks for small trucks.” All trucks will contribute to the traffic, GHG and air quality impacts. But of course heavy duty trucks will have the greatest impact. This needs to correctly limit the project to “ 56 **heavy** trucks and no more than a total of 101 truck loading docks”.

- TDM Plan has not been provided

Other city projects include the draft TDM Plan that is required by City Ordinance and to comply with the CAP. That makes it possible to determine if it appears to be sufficient to address the additional trips that will be generated, and to provide an opportunity for public comment. This project has not included even the draft TDM plan. Failure to provide this document leaves

compliance with the CAP unknown and leaves this a this a potentially significant impact that has not been addressed.

Thank you for considering our comments.

Sincerely,
Diane Nygaard
On behalf of Preserve Calavera

Attachments:

- A. MHCP/C SAP Edge Effect Conditions
- B. Warehouse Mitigation Measures

Link Attachments:

- C. [Warehouse Litigation 7-21-2021 v2](#)
- D. [State of Emergency Public Health Request](#)
- E. [Industrial Warehousing Report - Revised 2018](#)

Attachment A: MHCP/C SAP Edge Effect Conditions

City of Oceanside - Standard Project Conditions

SELECT AS APPROPRIATE - FOR PROJECTS LOCATED ADJACENT TO RESERVE AREAS OR WITH OTHERWISE SENSITIVE HABITAT ISSUES

1. A qualified biologist shall be retained by the applicant to review the final grading plans, access routes and staging areas, monitor all aspects of construction, educate contractors about the biological sensitivities associated with the area and ensure compliance with mitigation measures.
2. The qualified biologist shall conduct a training session for all project personnel prior to any grading/construction activities. At a minimum the training shall include a description of the target species of concern, its habitats, the general provisions of the Endangered Species Act (Act) and the MHCP, the need to adhere to the provision of the Act and the MHCP, the penalties associated with violating the provisions of the Act, the general measures that are being implemented to conserve the target species of concern as they relate to the project, any provisions for wildlife movement, and the access routes to and project site boundaries within which the project activities must be accomplished.
3. A water pollution and erosion control plan shall be developed that describes sediment and hazardous materials control, dewatering or diversion structures, fueling and equipment management practices and other factors as deemed necessary. Erosion control measures shall be monitored on a regularly scheduled basis, particularly during time or rainfall. Corrective measures shall be implemented in the event erosion control strategies are inadequate. Sediment/erosion control measures shall be continued at the project site until such time as the revegetation efforts are successful at soil stabilization.
4. The footprint of habitat disturbance shall be minimized to the maximum extent feasible. Access to sites shall be via pre-existing access routes to the greatest extent possible.
5. The upstream and downstream limits of project disturbance plus lateral limits of disturbance on either side of the stream shall be clearly defined and marked in the field and reviewed by the biologist prior to initiation of work.
6. Placement of equipment and personnel within environmentally sensitive habitat areas stream channels or on sand and gravel bars, banks and adjacent upland habitats used by target species of concern shall be avoided. Activities that can not be conducted without

placing equipment or personnel in sensitive habitats shall be timed to avoid the breeding season of the target species of concern.

7. When stream flows must be diverted, the diversions shall be conducted using sandbags or other methods requiring minimal instream impacts. Silt fencing or other sediment trapping materials shall be installed at the downstream end of the construction activity to minimize the transport of sediments off-site. Settling ponds where sediment is collected shall be cleaned out in a manner that prevents the sediment from re-entering the stream. Care shall be exercised when removing silt fences, as feasible, to prevent debris or sediment from returning to the stream.
8. Equipment storage, fueling and staging areas shall be located to minimize risks of direct drainage into riparian areas or other environmentally sensitive habitats. These designated areas shall be located in such a manner as to prevent runoff from entering sensitive habitats. All necessary precautions shall be taken to prevent the release of cement or other toxic substances into surface waters. All project related spills of hazardous materials shall be reported to appropriate entities including but not limited to the City of Oceanside, FWS, and CDFG, SWQCB and shall be cleaned up immediately and contaminated soils removed to approved disposal areas.
9. Erodible fill material shall not be deposited into water courses. Brush, loose soils, or other similar debris material shall not be stockpiled within the stream channel or on its banks.
10. Stockpiling of materials and other aspects of construction staging shall be limited to disturbed areas without native vegetation, areas to be impacted by project development or in non sensitive habitats.
11. "No-fueling zones" shall be established within a minimum of 10 meters (33 feet) from all drainages and fire sensitive areas.
12. Scheduling of construction activities shall minimize potential impacts to biological resources. Construction adjacent to drainages shall occur during periods of minimum flow (i.e. summer through first rain of fall) to avoid excessive sedimentation and erosion and to avoid impacts to drainage dependent species. Construction near riparian

areas or other sensitive habitats shall be scheduled to avoid the breeding season (March through September) and potential impacts to breeding bird species.

13. Construction activities during the breeding season (dates tbd depending upon species of concern- some start in Feb March through September) shall be limited to those that will not produce significant noise impacts (i.e. noise levels greater than 60 dBLEq –decibels equivalent sound level) at the edge of the habitat of concern.
14. Conduct preconstruction surveys at potential impact areas between mid-May and mid-June.
15. Human and pet access to preserve areas shall be limited to designated trails by use of natural vegetation, topography, signs and limited fencing.
16. Artificial lighting adjacent to the preserve area shall be eliminated except where essential for roadway, facility use and safety and security purposes. Where use of artificial lighting is necessary it shall be limited to low-pressure sodium sources. Use of low voltage outdoor or trail lighting, spotlights or bug lights is prohibited. All light sources shall be shielded so that lighting is focused downward to restrict any light spillover onto sensitive habitat.
17. The HOA shall establish an education program for homeowners regarding responsible pet ownership. The program shall encourage a) keeping pets indoors, especially at night; b)having pets neutered or spayed to reduce unwanted reproduction and long-range wandering; c)belling of cats to reduce their effectiveness as predators; d) prohibiting release of unwanted pets into the wild; e) keeping dogs on leashes when walking them on trails in preserve areas.
18. The HOA shall establish a feral animal removal program.
19. The qualified biologist shall monitor construction activities throughout the duration of the project to ensure that all practicable measures are being employed to avoid incidental disturbance of habitat and any target species of concern outside the project footprint. Construction monitoring reports shall be completed and provided to the City of Oceanside, FWS and CDFG summarizing how the project is in compliance with applicable conditions. The project biologist shall be empowered to halt work activity if

- necessary and to confer with staff from the City of Oceanside, FWS and CDFG to ensure the proper implementation of species and habitat protection measures.
20. The removal of native vegetation shall be avoided and minimized to the maximum extent practicable. Temporary impacts shall be returned to pre-existing contours and revegetated with appropriate native species. All revegetation plans shall be prepared and implemented consistent with Appendix C (Revegetation Guidelines of the Final MHCP Plan – Volume II) and shall require written concurrence of the FWS and CDFG.
 21. To avoid attracting predators of the target species of concern, the project site shall be kept clean of debris as possible. All food related trash items shall be enclosed in sealed containers and regularly removed from the site. Pets of project personnel shall not be allowed on site where they may come in contact with any listed species.
 22. Construction employees shall strictly limit their activities, vehicles, equipment, and construction materials to the proposed footprint and designated staging areas and routes of travel. The construction area(s) shall be the minimal area necessary to complete the project and shall be specified in the construction plans. Construction limits shall be fenced with orange snow screen. Exclusion fencing shall be maintained until the completion of all construction activities. All employees shall be instructed that their activities are restricted to the construction areas.
 23. Any habitat destroyed that is not in the identified project footprint shall be disclosed immediately to the City of Oceanside, FWS and CDFG and shall be compensated at a minimum ratio of 5:1.
 24. If dead or injured listed species are located, initial notification must be made within three working days, in writing to the Service's Division of Law Enforcement in Torrance California and by telephone and in writing to the applicable jurisdiction, Carlsbad Field Office of the FWS, and CDFG.
 25. The City of Oceanside shall have the right to access and inspect any sites of approved projects including any restoration/enhancement area for compliance with project conditions and BMPs. The FWS and CDFG may accompany the City representatives on this inspection.

26. Any planting stock to be brought onto the site for landscaping or ecological restoration shall be first inspected by a qualified pest inspector to ensure it is free of pest species that could invade natural areas, including but not limited to Argentine ants, fire ants, and other insect pests. Any planting stock found to be infested with such pests shall not be allowed on the project site or within 300 ft of natural habitats. The stock shall be quarantined, treated or disposed of according to best management principles by qualified experts in a manner that precludes invasions into natural habitats.
27. New utility lines or towers or modification of existing utility lines or towers shall implement designs that preclude or minimize harm to wildlife due to collisions or electrocution. Information on such designs is available at www.migratorybirds.fws.gov/issues/towers.
28. Use bridges, instead of culverts for all major riparian crossings and regional wildlife movement corridors. The site of the riparian crossing and its importance as a wildlife corridor should dictate the design. (Where appropriate based on site specific survey results) Wildlife undercrossings shall be designed and implemented (for new roads or road improvement projects that could disrupt wildlife movements or result in increased road kill). Such undercrossings, along with any necessary wildlife fencing or other facilities, shall be designed based on best available information to maximize use of the undercrossing by species of concern. Undercrossing design shall strive to maximize the openness index (widthXheight/length), minimize traffic noise within the crossing, use skylight openings within the underpass to allow for vegetative cover within the underpass, use appropriate fencing to funnel wildlife into the crossing rather than across the road surface, and screen the undercrossing opening and access path with natural vegetation. Undercrossing design shall be subject to review and approval by the City of Oceanside, FWS and CDFG prior to issuance of grading permits.
29. All mitigation sites shall be conserved through fee title acquisition or conservation easement, and proof of recordation shall be provided to the City of Oceanside prior to land disturbance.
30. Use of retaining walls shall be minimized. Development on the site shall be configured to existing topography to minimize grading and landform alteration.

31. Existing natural drainages and watersheds shall be maintained. The project shall restore or minimize changes to natural hydrological processes.

32. Detention basins shall incorporate earthen berms to allow growth of natural vegetation.

Note : There are additional specific conditions in areas of equestrian use

CONDITIONS SPECIFIC TO BUENA VISTA LAGOON

33. The Buena Vista Lagoon boundary (and/or wetland area) shall be delineated and criteria used to identify any wetlands existing on site shall be those of Section 30121 of the Coastal Act. Mapping of wetlands, conditions to protect sensitive resources and siting of development shall be done in consultation and subject to approval of DFG.

34. Landscaping on the site shall be utilized as a visual buffer and shall be compatible with the surrounding native vegetation and preserved open space through installation of native, non-invasive, drought tolerant plant species.

35. Approved landscaping shall be installed immediately upon completion of construction and maintained by the property owner in good growing condition for the life of the development.

36. Landscaping screening of structures with specimen trees and fire-retardant vegetation of substantial height, shall be required to screen and soften the view of structures from _____ (Interstate 5/ Buena Vista Lagoon etc.) and public vista points.

37. A HOA shall implement a landscape management plan that includes herbicide/pesticide management and removal of invasive species..

38. On-site sensitive biological resources areas inclusive of the 100-ft buffer area shall be left in their natural state (or restored with native drought tolerant vegetation) and used only for those passive activities allowed as a condition of permit approval. The permissible passive activities and any other conditions of the permit shall be incorporated into a covenant of easement that shall be recorded against title to the property. The USFWS and DFG shall be named beneficiaries to any covenant of easement recorded pursuant to this condition.

39. The use of chemical pesticides for mosquito control is prohibited (rely on biological agents).

40. Access to buffer and sensitive habitat areas is prohibited during the breeding season (see species specific guidelines for breeding season dates) except for emergency access.

Attachment B: Warehouse Mitigation Measures

Warehouse Mitigation Measures¹

- 1) Trucks certified to meet or exceed ARB's 0.02 g/bhp-h optional low-NOx emissions standard (i.e., near-zero or zero emission).
- 2) Tier 4 or cleaner construction equipment.
- 3) Solar photovoltaic panels on site sufficient to supply all electric energy demands for the office space, air conditioning and dark shell lighting of the project.
- 4) Solar ready roofs.
- 5) Three minute limit on all diesel idling.
- 6) Roundabouts at major intersections.
- 7) Air Quality Complaints. Prior to the start of grading, developer must post legible, durable, weather-proof signs, of a size to be easily readable from the street, at all construction entrances, which state in English and Spanish (i) that diesel trucks servicing the Project site shall not idle for more than 3 minutes; and (ii) the name and telephone numbers of an authorized individual such as the Project Superintendent to be contacted to resolve dust and air quality complaints, and a phone number to the local air district to report violations. The signs must remain posted on the property until construction is complete. All legitimate dust complaints must be resolved within 24 hours of receipt.
- 8) A minimum of 250-foot building setbacks from adjacent properties, and a larger buffer from residential and other sensitive receptor facilities based upon site-specific analyses.
- 9) Maximize use of native plants in landscaped areas.
- 10) Maximize use of drought-tolerant landscape materials.
- 11) Maximize harvesting of rainwater and project drainage.
- 12) Design streets to capture runoff to irrigate medians and parkways (zero curb design).
- 13) Provide on-street truck parking turnouts.
- 14) Exceed Title 24 by at least 15%.
- 15) Accommodate alternate forms of transportation including, public transportation (bus), charging stations for electric cars, carpooling, and bicycles.
- 16) Install a sufficient number of electric vehicle charging stations to accommodate 30% of the projected number of employee vehicles. Electric charging units shall meet or exceed Level 2 Electric Vehicle Service Equipment standards.
- 17) Provide preferential parking locations for ZEVs and carpool/vanpool vehicles.
- 18) Zero-emission fork lifts and yard goats, or near-zero emission CNG using RNG if electric powered equipment is not readily available.
- 19) Electric plug-in capacity for all trucks equipped with transportation refrigeration units (TRUs), and TRUs shall be limited to diesel run-time of 15 minutes.
- 20) Promote the riding of bicycles, through the provision of bike racks / storage, showers and changing rooms.
- 21) Reduce 'heat-island' effect by incorporating lighter paving materials where possible and light roofing materials on all structures.

¹ Based on document prepared by Richard Drury of Lozeau Drury, LLP. Edited and revised by Joe Lyou, Coalition for Clean Air.

- 22) Employ adequate shielding features to ensure zero light spill off-site.
- 23) Minimize water use in restrooms. Use zero or ultra-low flow urinals, dual flush toilets, and EPA certified WaterSense high efficiency fixtures.
- 24) Employ a recycling program.
- 25) Divert construction waste from landfills.
- 26) Incorporate recycled materials where feasible.
- 27) Incorporate low-emitting adhesives, paints, coatings, and flooring systems.
- 28) Make the best use of day-light into the interior spaces.
- 29) If project changes use of agricultural land, create an agricultural easement for comparable agricultural land (production) in California.
- 30) All LEED-certified buildings.
- 31) Use non-reflective solar panels.
- 32) All sites to be gated and manned 24/7 to monitor/regulate truck access.
- 33) Build or arrange for a renewable LNG/CNG fueling station(s) as appropriate to support low-NOx trucks.
- 34) Construct sound walls and utilize rubberized asphalt.
- 35) Use non-diesel emergency backup generators.
- 36) Provide funding for work force development & education.
- 37) Create a first source hiring program that encourages and assists local residents in securing facility-related jobs.
- 38) Provide funding to local air districts for air quality improvement projects.
- 39) Construct active transportation paths and nature trails to the benefit of the community.
- 40) Locate truck check-in points sufficiently interior to the project to preclude queuing of trucks onto public streets.
- 41) Provide rest areas with free Wi-Fi and restrooms for truck drivers.

Robert Dmohowski

From: Valerie Bennett <valbennett@sbcglobal.net>
Sent: Friday, February 7, 2025 8:08 AM
To: Robert Dmohowski
Cc: Zeb Navarro; David Bennett
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City Principal Planner Dmohowski,

I am writing regarding the February 10th zoning decision for the piece of land adjacent to the airport, the Eddie Jones Warehouse Project.

We are asking you to please adhere to the existing zoning regulation and not allow an exception to the developer. This area is adjacent to a limited access neighborhood, river preservation and a skate park. The location is not suitable for the size of industrial complex the developer is requesting.

Thank you,

Valerie and Bennett
3299 Toopal Drive
Oceanside, CA

Robert Dmohowski

From: dave keck <dgkeck@cox.net>
Sent: Wednesday, February 5, 2025 4:51 PM
To: Robert Dmohowski
Cc: City Council; Planning-Planning Commission
Subject: CC&Rs for Eddie Jones CUPs

Warning: External Source

5 February 2025

From: Dave Keck

Subject: CC&Rs for Eddie Jones CUPs

To: Rob Dmohowski, Principal Planner Eddie Jones Project

I would like to make my opposition to the Eddie Jones Project very clear. I believe this project is seriously harmful in many ways for the entire Oceanside community. I have voiced many of those concerns at Planning Commission and to the City Council meetings numerous times.

Should this project be approved by the Planning Commission I am confident that it will be appealed to the City Council for final resolution.

I was informed that the 3 parcels, making up the project site, are going to be joined together into one piece of property. In doing so the numbers of large trucks are limited to 6 at any one time on the property in accordance with the current codes and therefore requires a Conditional Use Permit (CUP).

Any CUP authorizing more the 6 heavy trucks on the site is a clear violation of the intent of the Oceanside Zoning Ordinance, section 4105 A 2., which reads as follows;

"A. For Use Permits.

1. That the proposed location of the use is in accord with the objectives of this ordinance and the purposes of the district in which the site is located.
2. That the proposed location of the conditional use and the proposed conditions under which it would be operated or maintained will be consistent with the General Plan;
will not be detrimental to the public health, safety or welfare of persons residing or working in or adjacent to the neighborhood of such use; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the city. "

I am unsure who develops the actual wording for the associated required CUP and when exactly that happens. I trust that the recommended CUP will be a part of the Staff Report that is published, as part of with the agenda for 10 February for public review, prior to the actual meeting.

As a minimum, a set of CC&Rs should be established for all the CUPs that are recommended prior to approval. Any CUP should establish a set of restrictions and conditions to counter the negative impacts on our community.

1. Restrict the hours of operations. This is an effort to reduce the impact of light, noise and vibrations during the hours of darkness caused by both the tractor-trailer arrivals and departures and the smaller transport vehicles coming and going including forklifts.
2. Require all delivery trucks use dedicated routes, specifically restricting all trucks to utilize Benet Rd and not specifically north of the project as conditions for delivery and pick up. We need more than just road signs!
3. Have the owner and/or tenants file a facility management plan that shall be reviewed by the public prior to operations and be negotiated in a transparent public fashion.

Included in that management plan **shall** be procedures that allow, in case of fire or other emergency situations emergency vehicles unfettered access to allow for the speedy evacuation of **all** the residents north of the project.

4. The requirement that all of the Airport community, including Prince of the Peace Abby, be formally notified via registered correspondence should any future modifications to the CUP be considered.

Respectfully Submitted,

Dave Keck
3200 Canyon View Drive
Oceanside, CA 92058



Robert Dmohowski

From: Michelle <m22michaels@gmail.com>
Sent: Thursday, February 6, 2025 1:32 PM
To: Robert Dmohowski; City Council
Subject: No Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

I reside in Oceanside and have recently been made aware of the Eddy Jones project. This needs to be stopped. There are already enough accidents, traffic, planes landing or crashing on the 76. I am asking you as city council reps to stop this project. The traffic due to the amount of homes being built in the valley, we do not need additional trucks on the road. Tractor trailers also cause more damage to our roads, creating potholes, causing blown tires, taxes to rise as well as insurance rates.

Keep Oceanside safe, strong and growing in the right direction. By right direction I mean adding parks, dog parks, pickleball courts, keeping homeless off the streets. There are so many other things that can be done to make this unique city a place for outdoor entertainment.

Thank you,
Michelle Michaels
9493226255

Sent from my iPhone

Robert Dmohowski

From: juliet park <julietkpark@gmail.com>
Sent: Wednesday, February 5, 2025 10:17 PM
To: Robert Dmohowski
Cc: Rick Robinson; Eric Joyce; Jimmy Figueroa; Peter Weiss; City Manager; Esther Sanchez
Subject: Request of Limited Zoning for Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Sir or Madam,

I am writing this letter regarding 'Eddie Jones Warehouse Project' around our neighborhood.

The existing zoning for this land only allows a handful of handful of Diesel Truck docks, NOT 57~114 Truck docks as proposed configuration designs .

As residents in our the community of homes adjacent and upwind of this development, we request that the developer be limited to the existing zoning regulation.

Thank you in advance,
Juliet Park

Sent from my iPad



T 510.836.4200
F 510.836.4205

1939 Harrison Street, Ste. 150
Oakland, CA 94612

www.lozeaudrury.com
kylah@lozeaudrury.com

Via Email

February 6, 2025

Tom Morrissey, Chair
John Malik, Vice Chair
Michael Ogden, Commissioner
Kevin Dodds, Commissioner
Louisa Balma, Commissioner
Tom Rosales, Commissioner
Dennis A. Anthony, Commissioner
Oceanside Planning Commission
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92054
planningcommission@oceansideca.org

Rob Dmohowski, Principal Planner
City of Oceanside
Planning Division
300 North Coast Highway
Oceanside, CA, 92054
rdmohowski@oceansideca.org

Dr. Zeb Navarro, City Clerk
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92054
cityclerk@oceansideca.org

**Re: Comment on Final Environmental Impact Report, Eddie Jones
Warehouse (D22-00001, CUP22-00001, V22-00001; SCH NO.
2022070365) February 10, 2025 Planning Commission Agenda Item 4**

Dear Chair Morrissey, Vice Chair Malik, Honorable Commissioners, Mr. Dmohowski, and Dr. Navarro:

This comment is submitted on behalf of Supporters Alliance for Environmental Responsibility (“SAFER”) regarding the Final Environmental Impact Report (“FEIR”) prepared for the Eddie Jones Warehouse (D22-00001, CUP22-00001, V22-00001; SCH NO. 2022070365), which proposes the construction of a 566,905 square foot warehouse, manufacturing, and distribution facility, located at 250 Eddie Jones Way, in the City of Oceanside (“Project”).

SAFER is concerned that the FEIR fails as an informational document and fails to impose all feasible mitigation measures to reduce the Project’s impacts. SAFER requests that the Planning Department decline to approve the project and certify the EIR and instead address these shortcomings in a revised draft environmental impact report.

February 6, 2025

Comment on Final Environmental Impact Report, Eddie Jones Warehouse (D22-00001, CUP22-00001, V22-00001; SCH NO. 2022070365) February 10, 2025 Planning Commission Agenda Item 4

Page 2 of 2

SAFER reserves the right to supplement these comments during the administrative process. *Galante Vineyards v. Monterey Peninsula Water Management Dist.*, 60 Cal. App. 4th 1109, 1121 (1997).

Sincerely,

A handwritten signature in cursive script, appearing to read "Kylah Staley".

Kylah Staley
Lozeau Drury LLP

Robert Dmohowski

From: Sonja Carlin <sonjacarlin@gmail.com>
Sent: Thursday, February 6, 2025 11:26 AM
To: Planning-Planning Commission
Subject: High Fire danger area - Eddie Jones project

Warning: External Source

No Eddie Jones Project

>

> Dear Planning Commission - If you approve this project you are subjecting the nearby residents to a DEATH TRAP. We have one way in and one way out. I live in a culdesac about 1/4 of a mile from the proposed site. I had firemen in my back way when the hillside was burning up 18 months ago. Cars were unable to evacuate the streets due to a traffic jam caused by an evacuation area. Now picture 57 semi trucks trying to depart at the same time. People will die. Please do not allow this project to move forward. Please do NOT industrialize a residential area.

>

> Additionally there is one left hand turn lane off the 76 going in and out coming form HWY. I can see this backed up for a mile when you add semi trucks on top of regular residential traffic. It wasn't built for major business traffic. Please use some common sense. Our lives depend on it.

>

> Regards

>

> Sonja Carlin

> Sonjacarlin@gmail.com

> (949) 697-6300

> 883 Wala Dr

> Oceanside

Robert Dmohowski

From: ginny spencer <ginspencer@hotmail.com>
Sent: Thursday, February 6, 2025 5:53 AM
To: Robert Dmohowski; City Council
Cc: OceansideSpeaksOut@gmail.com
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City Council Members,

I am writing this letter to express my deep concern for the Eddie Jones Project. I have been a member of the Oceanside airport community for 9 years and have grown to love the area and my neighborhood.

Given the recently devastating Palisades fire and the impending roll out of the Surf camp, my primary concern is for the **safety** of the people in the surrounding Oceanside airport community. The immense difficulty that the Palisades residents experienced while escaping their neighborhood from the fire really affected me. I could not help but think that my neighborhood would be faced with the same predicament should or when a fire breaks out in our area. I have attended a few informational meetings offered by the Eddie Jones Project and, to date, I not yet heard of a plan that would sufficiently address this concern. This worries me.

I am also concerned about the impact of the Eddie Jones project on the **traffic**. I travel to work daily on the 76 and already experience plenty of traffic. I question the accuracy of the traffic study that was done for the Eddie Jones Project since the Surf Camp has not yet been completed. With both the Surf Camp and the Eddie Jones Project in action, I suspect that traffic will become intolerable. I have not heard of any plans that would sufficiently address this from the Eddie Jones Project or Caltrans. Placing such an inconvenience on the thousands of people who need to use this highway daily just for a warehouse that could be located elsewhere seems ridiculous and, frankly, irresponsible.

Lastly, the impact of the Eddie Jones project on the **air quality** of my neighborhood is a concern. California's Climate Action Plan has a statutory goal of reducing anthropogenic emissions by at least 85% by 2045. I am assuming that current emission values will serve as the baseline for determining whether the City of Oceanside meets this target in 2045. I strongly suspect that **the City of Oceanside will not be able to meet this goal** if it allows structures like the Eddie Jones warehouse to be built.

Thank you for your consideration.

Regards,

Virginia Spencer

3403 Northwood Drive, Oceanside CA 92058

Robert Dmohowski

From: Wendy Morris <beachwendy@gmail.com>
Sent: Thursday, February 6, 2025 4:36 PM
To: Planning-Planning Commission
Subject: Eddie Jones warehouse

Warning: External Source

Dear Planning Commissioners of Oceanside,

We see many significant problems that the Eddie Jones Warehouse project will bring to the residential neighborhood. We are concerned with the size and impact of the project. Of particular concern is the impact it will have on Benet Road traffic.

The project will increase the amount of traffic going from Hwy 76 into the large residential area to the north. This will greatly impact the local residents since Benet Rd. is the closest street access into the large area of homes from the west, and it connects to Foussat Road, which is the only other entrance to this area of homes.

The proposed truck traffic will be a problem for all residents every time they leave their home and every time they return to home. A separate traffic lane should be required for traffic using Benet Rd. to access and to depart the Eddie Jones Warehouse. In the case of an emergency evacuation these extra lanes are vital. Also, dual left hand turn lanes should be created on Hwy 76 for eastbound traffic turning north onto Benet Rd. Adequate left turn car storage does not presently exist. An exclusive right hand turn lane bypassing the traffic signal should be created for southbound Benet Rd. traffic turning west to enter Hwy 76 due to the presence of large trucks.

Besides the impact of congestion from traffic, it will greatly negatively increase the impact from noise and air quality. If nighttime operations are allowed, it will cause tremendous light pollution.

These negative impacts will be ongoing problems that will not go away.

The proposed project is not suitable at this location with its large residential neighborhood to the north, due to the large number of impacts.

Thank you for considering our concerns. We hope the project is denied.

Thank you,

William and Wendy Morris

Robert Dmohowski

From: Jeri Tharp <jerraytt16@gmail.com>
Sent: Thursday, February 6, 2025 8:39 AM
To: Robert Dmohowski
Subject: Oppose Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Please consider listening to tax payers and do not let this proposed project move forward. With only two ways to get into our community, the number of trucks allowed will impact emergency services, increase noise, air pollution and traffic into the 76.

We are requesting that you look and listen to the residents rancher than a business opportunity.

Sincerely,

Jeri & Ray Tharp
550 Fern Ridge Ct
Oceanside 92058
Sent from my iPhone
Sent from my iPhone

Robert Dmohowski

From: Keri Cleeremans <keri@elliptigo.com>
Sent: Thursday, February 6, 2025 4:04 PM
To: Robert Dmohowski; City Council
Subject: No on Eddy Jones

Warning: External Source

Hello City of Oceanside Planning and Council,

I've already written a few times, but it was noted recently that emails in rejection of this project from those of us who do not live specifically near Benet are not being as seriously considered. This is VERY unnerving because any tax payer's vote should count when deciding change of quality of life to the residents. Quality of Life you may roll your eyes at-but- I already sit on the 76 for up to 30 minutes from Rancho del Oro to the 5 M-F in either direction, now you want to add even more traffic and once again, NOT improving the roadway and infrastructure. It's been reported that the Eddy Jones project will donate money but that money does not go towards expansion of the 76 or traffic management. How is that helpful?

PLEASE reconsider this project. You are already adding a wave park, boutique hotel, condos, shops and MORE parking/traffic to the 76 at Foussat. Now you want to add giant big rigs.

Seriously. PLEASE consider us residents who have to commute on this road every day. Just take the 76 when a traffic light goes out-that will give you a good idea of how much impact a project like this and others are going to cause.

The City has yet to provide it's residents any sort of updates concerning approved developer's projects on how they are going to ASSIST residents with the number one concern of safety and traffic. In 20 years of living here, only the 5 has changed. That's not even from the City! Not a single main artery roadway within the City has been improved to impact the amount of residents coming and going. Ok, there have been traffic calming improvements and new traffic circles.....but that isn't helping reduce traffic. Take Mission Ave towards Academy on a weekday from 3pm-6pm and see how long you sit at the traffic light because it's a single lane in every direction. Because, you approved a new senior center and a new housing development! So triple the amount of cars on a single lane road.

Please, please, please think about how this impacts more than just the people that live around Benet.

Thanks,
Keri Cleeremans
Camino Del Flor 92057

Robert Dmohowski

From: Michael Kosec <mikekosec@gmail.com>
Sent: Wednesday, February 5, 2025 3:58 PM
To: Planning-Planning Commission
Subject: Eddie Jones Project- Opposition
Attachments: Planning Commission.docx

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

From: Michael Kosec
565 Fern Ridge Ct.
Oceanside
CA 92058

Dear Planning Commission Members,
Thank you for your service to our city.

It is my understanding that your goal is to responsibly manage our development in accordance with City Standards.

I understand that the Eddie Jones Project is zoned for Light Industrial use. I'm imploring you to reject the developers' requests for CUPs that violate these standards.

Attached are my point by point responses to the Draft EIR that I provided to Rob D.

The developers final EIR included a few pages of responses- to my and my fellow residents' critically serious concerns- regarding the environmental, health, traffic, and wildfire emergency evacuation threats posed by this Mega Warehouse development.

The developers ignored these concerns and basically said "Nothing to see here, move along.". They continue to insist- without any real world traffic study- that this 24/7 proposal would not affect our neighborhood and city.

You, as Commission members, drive the same 76 as I do.

It is already dangerously backed up from the 5 to Rancho Del Oro daily- Monday through Friday- starting at 2:30 PM.

Imagine adding an unmentioned number of tractor trailer trucks- each the length of 4 SUVs- exiting and entering the 76-24 hours a day / 7 days a week.

The developers' studies are totally skewed and speculative; without knowing the exact number of trucks, the identities of the tenants, the hours of operation, without any emergency evacuation plan developed with the city and impacted communities, their EIR is fluff and bluster...

As Adam Robinson told our community opposition group (paraphrase): "Sorry, you bought houses bordered by a Light Industrial Zone, I (RPG) can build what I want here, as long as the City of Oceanside allows.."

So, I'm asking you to say "NO" to Mr. Robinson- No, this project will not be granted CUPs for the number of trucks and for additional height.

Say NO to an untold number of tractor trailers, 24/7 air, light and noise pollution.

Say NO to the placement of this MEGA Warehouse bordering an environmentally sensitive area, a bike path used by thousand of people.

Say NO out of concern for the safety of the numerous unhoused individuals live along the bike path- as you know- and walk in the middle of Benet Road at all times of the day and night to cross 76.

Say NO to tractor trailer trucks "queuing" (Developers' words in Final EIR) on narrow Benet Road.

It's just a ridiculous, dangerous location- period. You, as Oceanside residents, already know that the Oceanside Project will add 700 homes= 1400 additional vehicles to 76 daily- plus the hundreds / thousands of daily visitors to the Wave Park, shops, restaurants etc. - all adding to the existing gridlock on 76.

You, as Oceanside residents, already know that there are weekly accidents already at the intersection of Foushat and 76 and that someone was killed crossing 76 near Benet last year. Imagine adding tractor trailers and 500 employee cars to the mix...

You, as Oceanside residents, already know that both sides of 76 around the proposed Eddie Jones Mega Warehouse are canyon tinderboxes.

You already know that there was a major wildfire near the Prince of Peace Abbey in 2022, causing the Airport Community to be evacuated. You already know that was a total chaotic mess ; now you know about this year's LA fires. The 2022 fire occurred before Oceanside- imagine thousands of cars and people trying to exit with the 1200 Airport homes' occupants- while fire engines try to enter and resolve an emergency.

The economic impact to the City will be a whopping 72K annually. That doesn't sound like a favorable deal / trade off for the City. It's a pittance.

Please consider everything I've written above and look over my responses to the attached Draft EIR, which is pretty much the same as the Final EIR. The Developers have no ties to Oceanside and can sell the property upon rental and ask for further CUPs to expand the number of truck bays at any time.

Please, please consider all this.

I (We) am / aren't opposed to Light Industrial use, but urge you to reject this proposal and wait for one that meets Oceanside's requirements without CUPs.

Thank you for reading and for hopefully making the right decision for Oceanside. It's not the Eddie Jones Project.

Respectfully,
Michael Kosec

Mr. Rob Dmohowski
Principal Planner
City of Oceanside
Oceanside, California

Dear Mr. Dmohowski,
Below please find:

December 29, 2023 Addendum-

Attached to 12/25/23 Original Responses.

Thank you.

Michael Kosec
565 Fern Ridge Ct
Oceanside CA

DEIR Section: Traffic

*Additional Response:

The Left Turn Lane from 76 E onto Benet Road, which is used by residents of the Airport Community to return home- is approximately 500 ft. long and cannot accommodate both semi-trucks and passenger vehicles- particularly at rush hours.

Approximately 12-13 passenger vehicles can fit into this left turn lane currently, which is currently usually filled to capacity at evening rush hour- before any consideration of 60+ foot long semi-trucks.

Proposed remediation is speculative and hypothetical and not under the control of the developer nor the City of Oceanside.

Measuring my sub-compact car with a semi-truck resulted in at least four of my cars fitting into one semi-truck.

Placing semi-trucks into the equation will result in passenger vehicles being backed up into the middle through lane on 76- placing both turning and through vehicles at severe risk of life and death collisions.

Proposed remediation will be years- if ever- in development, approval, and construction stages by CALTRANS.

Proposal needs to be rejected by Planning Commission as being critically flawed and dangerous.

DEIR Section: Bicycle and Pedestrian Safety

Response:

The current bike lanes are inadequate to accommodate both bicycle and semi-truck traffic. Bicyclists- particularly those wearing earbuds- are at high risk of collisions with semi-trucks entering and exiting the proposed Eddie Jones facility.

Semi-trucks will be making wide turns into and out off the facility- endangering bicyclists AND Pedestrians on Benet Road.

Regarding Pedestrians- Although the DEIR proposal includes fixing missing sidewalk- the fact remains that adults and children- walking and with young kids on bikes- will need to pass / cross in front of huge semi-trucks entering and exiting the proposed facility- placing them at severe risk.

Notably- These are adults and children living in a residential neighborhood setting- not an industrial park development and should not be placed at risk of collision from semi-trucks. The proposed facility has no plans for flashing lights or signage to warn Pedestrians and Cyclists they are at risk of collisions with semi-trucks.

Last Points-

Unhoused individuals living in the adjacent riverbed routinely walk on the sides and in the middle of Benet Road as they pass the proposed facility.

Frequently these individuals are not paying any attention to passing cars- let alone semi-trucks- as they navigate Benet to cross 76 for food and services from Brother Bennos.

These individuals are likewise at severe risk of injury / collision with the constant flow of semi-trucks entering and exiting the proposed facility.

The final Pedestrian Point- The DEIR's lack of any kind of visual, audio, or tactile sidewalk crossing markings- no traffic lights etc.- places Oceanside citizens with blindness and visual impairments in grave danger- as there are no warnings that they are entering a crosswalk where semi-trucks are entering and exiting.

This is a fundamental disability rights issue as well as a potential life and death issue for individuals with blindness and visual impairments.

December 25, 2023

Dear Mr. Dmhowski,

Below please find serious and consequential issues and deficiencies I have identified with references to corresponding sections of the Eddie Jones Project DEIR.

Michael S. Kosec
565 Fern Ridge Ct.
Oceanside
CA 92058

I will quote the section of the DEIR and then list my responses / objections.

1.

DEIR- Executive Summary Statement:

“The project’s development plan application addresses the complete redevelopment of the project site with the existing facility and site improvements to be demolished. The proposed warehouse and distribution facility is classified as a Wholesaling, Distribution, and Storage Facility use by the Oceanside Zoning Ordinance (OZO). Wholesaling, distribution, and storage facilities over 50,000 square feet in floor area require approval of a Conditional Use Permit to be established in the IL zoning district, pursuant to the OZO. Wholesaling, distribution, and storage facilities with more than six heavy trucks on the premises at one time are considered trucking terminals pursuant to the OZO. Trucking terminals also require approval of a Conditional Use Permit to be established in the IL zoning district.”

Response:

As defined by Oceanside Planning Commission:

IL Limited Industrial District. To provide areas appropriate for a wide range of (1) moderate to low-intensity industrial uses capable of being located adjacent to residential areas with minimal buffering and attenuation measures and (2) commercial services and light manufacturing.

The IL zones in Oceanside are ill-suited for- even with Conditional Use permits- large-scale warehousing and trucking terminal activities.

See below- source Community and Environmental Defense Services information sheet:

Warehouses Are Not A Light Industrial Use

According to the [USLegal.com Light Industry Law and Legal Definition webpage](#):

“Light industries cause relatively little pollution when compared to heavy industries. As light industry facilities have less environmental impact than those associated with heavy industry, zoning laws permit light industry near residential areas. It is a criterion for zoning classification.”

The Complete Real Estate Encyclopedia contains a similar definition for Light Industrial:

“Light industry usually consists of nonpolluting users with moderate energy demands engaged in assembling products, sewing, baking, or cleaning.”

In *Putting Atlanta Back To Work: Integrating Light Industry Mixed-Use Into Urban Development*, the following distinction is made between light and heavy industry:

“Generally, to locate in a light industrial zone, a business must not produce any loud noises, vibration, noxious fumes, or other hazardous byproducts – beyond the property line. In heavy industrial districts, generally a business must not produce these negative effects beyond the boundaries of the entire district.”

Given that:

Air pollution emitted from warehouse diesel truck traffic can harm the health of those living well beyond a warehouse property line,

Warehouse noise has disturbed nearby residents

Warehouses should not be allowed in light industrial zones based on the definitions and cautions above.

Even within the City’s more-intensive IG and IP zones, the above-referenced activities are highly conditional on Planning Commission approval.

Therefore, I believe the Planning Commission should not grant the RPG Conditional Use Permit request.

2. DEIR- Appendix J
Draft VMT Analysis:

Response:

This analysis and the entire DEIR completely fails to even consider the fire / emergency / natural disaster planning and evacuation needs of the impacted Airport community- should this project be approved and realized.

As the state of California Department of Transportation wrote to Mr. Dmohowski, Principal Planner, in its 8/17/22 letter:

“The TIS (Traffic Impact Study) should also evaluate fire/emergency evacuation for the adjacent housing community to the north that routinely uses Benet Road and Foussat Road as access to SR-76.”.

No such Evacuation Evaluation is included anywhere within the submitted DEIR.

It is entirely too late to submit one now- the DEIR is completed and the review period will be over on December 29, 2023.

To borrow a favorite catchphrase used by the developer during a Scoping meeting you don't get "another bite at the apple".

In regard to the safety of the Airport community-as evidenced by the Wala fire of 2022: A prompt, effective evacuation plan is literally a life and death issue. Trucks can be replaced; people cannot.

Should this development go forward without a Plan accepted by the City of Oceanside and distributed to the Airport Community residents- one that prioritizes residents' safety over Eddie Jones' profits,

the lives of these affected Oceanside residents and those of the substantial number of unhoused community members- who live in the adjacent riverbed- are all at grave risk from a fire or natural disaster- even with an Evacuation Plan. In this case-a massive development without an Emergency Evacuation Plan, the threat to human life is potentially catastrophic.

3. PCE- Comparing Passenger Cars to Trucks-

Appendix J- VMT Analysis

PCE used by developers- in traffic- dramatically underestimates the impact of semi-trucks on the 76.

For purposes of the DEIR, the developers use a formula: One semi-truck = 2 Passenger Cars. (PCE)

This is a fundamentally flawed formula when discussing traffic and congestion, which are constants on the section of the 76 between Foussat and Benet.

Approval of this DEIR and this project would gridlock the area and make it impossible to travel through at peak hours- morning and evening, despite the developers' claims in this section.

See below as reference:

<https://www.fhwa.dot.gov/reports/tswstudy/Vol3-Chapter9.pdf>:

"Trucks are larger and, more importantly, accelerate more slowly than passenger cars, and thus have a greater effect on traffic flow than passenger cars. On level terrain and in uncongested conditions conventional trucks may be equivalent to about two passenger cars in terms of their impact on traffic flow. In hilly or mountainous terrain and in congested traffic their effect on traffic flow often is much greater and they may be equivalent to 15 or more passenger cars."

4. VMT Section- Traffic Mitigation

Developers claim Voluntary Employee Carpool program will reduce 76 congestion.

This argument is hypothetical and unsupported by any evidence- especially since the tenants are unknown.

There are and will be no guarantees or requirements that will be implemented by tenants or the property owners to enforce this hypothetical.

Ownership of the property can change at any time- taking the program- even if implemented- with them.

5. VMT Section- Traffic Mitigation- Also addresses some Pollution issues-

Developers put forward several proposals for mitigation- including a designated truck turn lane from Benet onto 76.

Plan approval and highway construction are not under their control; these are under the authority of CALTRANS.

Any such construction would take years; the proposals are only ideas- there are many other options not suggested by the developers.

In any case it would take years to approve and construct these mitigations- if ever approved.

Meanwhile, the developers plan to start construction and then filling the buildings with tenants and trucks immediately upon completion- leading to years of further congestion- the opposite of remediation.

6. VMT Traffic Mitigation-

Developers propose to pay the City of Oceanside 8.5% of the unknown cost of the hypothetical mitigation approaches for traffic management and signs.

See Response on Issue 5. Highway construction is under the authority of CALTRANS.

Additional Response:

Developers exacerbate the traffic problems, offer the City of Oceanside a flat sum and absolve themselves of any further liabilities to mitigate and address the very serious issues they've caused.

7.

Executive Summary-Environmental Issues & Remediation

Developers state that they are working within CEQA guidelines, working with CA Department Toxic Substances Control within a voluntary agreement to remediate the hazardous waste materials in the soil and are monitoring the air quality as part of their agreement.

Response- It is noted that remediation efforts are ongoing and remain unfinished.

Site inspections are scheduled for 2024 and a final inspection in 2025- long after the buildings will have already been constructed.

The air quality monitoring results have not been posted by the developers or the state of California.

The potential for further environmental harm remains uncertain- especially as the developers disturb the contaminants in the soil. This is of especially serious public concern as the site is above the City of Oceanside's aquifer.

To date- The CA Department of Toxic Substances Control has held no Community meetings- as required- to inform the public about this project and its consequences for the health and safety of Oceanside residents- nor have the developers.

Meetings were supposed to have been held during the 45-day DEIR review period.

Robert Dmohowski

From: Arlen Roper <arlenroper@gmail.com>
Sent: Saturday, January 11, 2025 1:50 PM
Subject: Eddie Jones Warehouse

Warning: External Source

Hello City and Community Staff,
I hope you are all doing well.

I recently learned about the proposed Eddie Jones Warehouse on the 76, by the airport.

As an Oceanside resident I travel on the 76 in this area several days a week, and the 76 in general on most days.

The 76 is already a clogged mess of traffic on all but a few hours mid day.

The area of this proposed warehouse is already particularly busy with all the people coming from the 5 fwy.

Then when you hit Rancho Del Oro, for some reason that light seems to give priority to Rancho Del Oro, and not the 76. It is frequently backed up and you sit through multiple traffic light cycles to get on down the road...

Imagine adding even half a dozen trucks leaving the warehouse into this cycle. One truck is equal to about 4 cars and so add another light cycle to your trip to get where you are going...

TRucks also wear out pavement and so both city streets, and the already rough pavement of the 76 in that area will get quickly worse.

I live near the Fed-Ex warehouse facility and many mornings I have trouble getting down Oceanside Blvd or College because it is clogged up with Fed-Ex trucks. It was a mistake for the city to allow this facility in the middle of residential areas.

It would be another (probably bigger) mistake for the city to allow this huge warehouse to be developed on the 76.

Please place another check mark in the column of residents that oppose your approval of this project.

Thank you for your time, and have a great week!

Best regards,

arlen roper

residential building design - drafting - art

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Tuesday, January 14, 2025 8:45 PM
To: Robert Dmohowski
Cc: City Clerk; City Manager; City Council
Subject: Re: please do not approve the CUP for the revised Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hi Rob -

I thought I would resurface this email after seeing your email that the final EIR is out for review. Again, I want to reiterate that the revised proposal for the Eddie Jones Project is still outrageous and not acceptable. The rejected Amazon project was still a fraction of the revised proposal for Eddie Jones.

I am asking that the planning commission rejects the application for a Conditional Use Permit that is necessary for this project to move forward.

Thank you in advance for your consideration.

Gretchen

On Tue, Dec 3, 2024 at 5:49 PM Gretchen Gary <gretchengary@gmail.com> wrote:

Hi Rob -

I'm writing to you again to ask you to reject the application for a Conditional Use Permit (CUP) for the Eddie Jones project. Please don't be deceived by the developer's "strike through" marketing tactic to cut the number of mac truck bays in half to 57 truck bays. It was ridiculous to propose 114 truck bays in the first place... I'm sure the developer did that on purpose so they could later present a 50% markdown and promote it as a sign that the developer is making accommodations to comply with community concerns. They put an absurd proposal out at first only to make a 57 truck bay proposal look good by comparison.

I'm writing to remind everyone that this is still WAY TOO MANY for a semi-industrial property that is currently only zoned for 3 truck bays.

As a reminder, the Amazon Distribution Center that was proposed for the Ocean Park Industrial Park only proposed to have 15 truck bays and it was rejected unanimously via an appeal with the City Council after the Planning Commission approved the proposal.

I was at the City Council meeting when members of the St Cloud community and other surrounding communities came out in force to voice their opposition to what 15 truck bays would mean for their quality of living in residences nearby. That was for 15 truck bays. The revised Eddie Jones Project is proposing 4X that amount! It's still outrageous considering how many homes there are in the direct vicinity, most with young children in residence.

My property directly faces the project and is within 1500 feet. My master bedroom windows face the project and provide a birds eye view of the entire property. The light, air and noise pollution of 57 mac trucks is unbearable to

imagine. I implore you and the Planning Commission to reject the Conditional Use Permit that would enable the proposal.

Please consider these concerns. Most of my neighbors own their homes. As local tax paying citizens, we hope our city planners will consider our concerns as they envision the future for coastal Oceanside. I'm sure those who use the bike trail, skate park and future residents/visitors of the wave park would agree that a mega-industrial complex only 2 miles from the ocean is not a good fit for the future vision of our community.

Kind regards,
Gretchen Gary
471 Tishmal Ct.

Robert Dmohowski

From: Jeannie Dung pham <dp999_98@yahoo.com>
Sent: Tuesday, January 14, 2025 1:57 PM
To: Robert Dmohowski
Cc: Dr. Michael Tenhover; Dennis Stacy; RonSteffen; Carol Steffen; Donald pham
Subject: Re: Notice of Final Environmental Impact Report - Eddie Jones Warehouse Project

Warning: External Source

Hi Robert,

Thank you for sending me the website of the Eddi Jones project.

I don't understand all the documents about the Eddi Jones project. This is Oceanside City's duty.

I would like to know how you process them, but you have to care for residents in the area.

But when I read the document that you sent in section

8.4.4

"

8.4.4.3 Relation to Project Objectives The Multi-Building and Truck Bay Reduction Alternative was prepared in response to comments received about the proposed project. This project alternative would meet most of the project objectives while lessening potentially significant impacts of the project The alternative's four-building site plan would total a building area of 497,822 square feet (inclusive of mezzanine areas). The total alternative project footprint area would be 491,582 square feet. This alternative would be approximately 69,083 square feet (approximately 1.58 acres) smaller than the proposed project. This alternative would reduce the number of truck bays from 114, as analyzed in the Draft EIR, to 56. This alternative design would place the truck bays on the east/west sides of the buildings and would greatly reduce the number of bays visible from the existing homes to the north. The decrease in total building area in comparison to the proposed project would potentially reduce

some impacts to biological and cultural resources; however, this reduction would not be substantial, and mitigation would still be required, as the overall disturbance area of this alternative would remain similar to the proposed project. This alternative would meet all proposed project objectives, with the exception of objective 3 (maximize the allowable use of an existing industrial zoned site that is compatible with the adjacent light industrial zoned sites and Oceanside Municipal Airport). While this alternative would develop industrial uses consistent with the existing land use and zoning designation for the site, it would not maximize the allowable development on site to the extent feasible. This alternative would similarly maintain a 100-foot buffer from the edge of the San Luis Rey River riparian habitat along the project boundary's northern edge, as designated in the City of Oceanside Subarea Plan. Additionally, this alternative would incorporate required building setbacks and airspace height limits established by the OMALUCP. Although the Multi-Building and Truck Bay Reduction Alternative would meet all but one of the project objectives, it would not substantially reduce any potentially significant impacts identified under the proposed project to a less than significant level without mitigation. 8.5 Environmentally Superior Alternative"

As mentioned statement above didn't include our request concerning safety in our resident's environment, in which there are about 114 trucks driving in and out every day, especially peak time.

We just want to know how you solve the problem, when our Benet road is too small to handle big trucks in/out daily.

Further more our community has a lot of children ride bikes in/out of our area to go to school in Oceanside High School, Middle High, and Elementary School. How can the

big truck can see us on Benet Road? when small children ride bikes below?
In addition, it will create heavy traffic from HW76 when you turn left at Benet Road.
Pollution and noise will add on. Do you care for us?
Whenever the City can solve all the above problems then you can process the project otherwise please stop them in your power.
Thank you in advance for understanding and waiting for your reply.

Again, Thank you.

Sincerely.

Jeannie Dua Pham.

On Monday, January 13, 2025 at 05:29:51 PM PST, Robert Dmohowski <rdmohowski@oceansideca.org> wrote:

Dear interested party:

The City of Oceanside, acting as Lead Agency pursuant to the California Environmental Quality Act, has prepared a Final Environmental Impact Report (FEIR) evaluating the potential environmental effects associated with the proposed **Eddie Jones Warehouse, Manufacturing and Distribution Facility Project** ([SCH No: 2022070365](#)). A Draft EIR (DEIR) was made available for public review and comment between October 26, 2023 and December 29, 2023.

WHERE TO ACCESS THE FEIR: The FEIR, including comments letters that were submitted regarding the DEIR along with response to comments, is available to view at:

City of Oceanside CEQA Website

[Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project EIR | Oceanside, CA](#)

City of Oceanside Development Services Department

300 North Coast Highway

Oceanside, California 92054

City of Oceanside Library

Main Branch – 330 North Coast Highway

Mission Branch – 3861-B Mission Avenue

PROJECT LOCATION: A 31.79-acre site located at 250 Eddie Jones Way (APNs: 145-021-29, 30, & 32). The site is located directly north of the Oceanside Municipal Airport on the east side of Benet Road.

In response to public comments received on the DEIR, a Multi-Building and Truck Bay Reduction Alternative has been included in the FEIR (Section 8.4.4), which is a variation on the project and Multi-Building Alternative previously evaluated in the DEIR. The City, as Lead Agency, recommends consideration of the Multi-Building and Truck Bay Reduction Alternative which is considered the preferred alternative that meets most of the objectives of the project while lessening potentially significant impacts of the project. Plans for the Multi-Building and Truck Bay Reduction Alternative are also available to view on the City's website using project number **D22-00001**:

City of Oceanside eTRAKiT Project Search

<https://crw.cityofoceaside.com/etrakit3/Search/project.aspx>

PUBLIC HEARING: The project is tentatively scheduled to be considered by the Planning Commission on **February 10, 2025**. The Planning Commission will consider certification of the FEIR at the public hearing. Notification of the public hearing and agenda information will be provided in accordance with state law and the City's public notification requirements. Additional information on the Planning Commission is available at [Planning Commission | Oceanside, CA](#) .

Robert Dmohowski

Subject: FW: Notice of Final Environmental Impact Report - Eddie Jones Warehouse Project

From: Ron Steffen <22287mulholland@gmail.com>

Sent: Tuesday, January 14, 2025 2:52 PM

To: Jeannie Dung pham <dp999_98@yahoo.com>

Cc: Robert Dmohowski <RDmohowski@oceansideca.org>; Dr. Michael Tenhover <tenhover@protonmail.com>; Dennis Stacy <dennis.stacy@camoves.com>; Carol Steffen <cs22620@gmail.com>; Donald pham <dphamk25@yahoo.com>

Subject: Re: Notice of Final Environmental Impact Report - Eddie Jones Warehouse Project

Warning: External Source

Basically one there are two things that don't make sense for the City of Oceanside and the airport:

- 1-The described industrial wearhouse project does not fit so close to Oceanside airport. The height of the proposed structures and the square footage are conducive to an industrial park vs immediately next to an airport; and,
- 2 The type of employment that will be generated by the proposed structure vs a low profile executive office park will affect the employment opportunities. The proposed warehouse park will offer lower income employment, generally lower that the income required to buy in "Wanis View".

Alternatively an airport adjacent executive office park would easily be absorbed by the many medical and pharmaceutical companies which are in fact expanding in N.San Diego county. These companies offer higher paying employment. In addition, they could use the airport. Think in terms of Santa Monica airport and other regional executive airports and the positive effect they have on the adjacent areas.

Ronald Steffen
C: 818 425 8780

Robert Dmohowski

From: Gene O'Neal <hp11325@yahoo.com>
Sent: Thursday, January 16, 2025 3:18 PM
To: Robert Dmohowski; Esther Sanchez; Zeb Navarro; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Eddie Jones Warehouse, Manufacturing, and Distribution Facility

Warning: External Source

As a resident of Wanis View Estates, Oceanside California, I would like my City Council Members to be aware. This issue will be upon all of us and before my city officials shortly and we hope our City Council will be a good steward of the people they represent. Information is ongoing on our website, OceansideSpeaksOut2.org.

The following is a copy of an email between myself and some of my neighbors.

Regarding the Eddie Jones project, I wanted to share my perspective. During our meeting with Arlene, (January 6, 2025) a representative of RPG (Developer) when a "monetary" offer was introduced, it signaled to me that RGP is genuinely concerned about the strength of our Airport Community movement, Oceanside Speaks Out. (OSO) Large entities often use financial incentives as a strategic tool to create division, relying on individual self-interest to weaken opposition. In this case, it appears to be an attempt to fracture the unity of the Oceanside Speaks Out (OSO) movement.

The dedication and effort that many individuals—especially volunteers—have invested in challenging this large-scale development have given OSO significant power and influence. My concern is that the introduction of "monetary considerations" may tempt some to prioritize personal gain over the broader interests of the Airport Community. It is crucial that we remain unified, as our collective strength is what allows OSO to effectively advocate for the entire community.

Wanis View Estates stands to be most affected by this development, yet its affiliation with OSO provides it with considerable leverage. I believe RGP recognizes the real possibility that OSO could derail their current proposal, which is why maintaining our solidarity is essential. Only through OSO's collective influence can we work toward an outcome that truly benefits all stakeholders.

For these reasons, I firmly believe it is in our best interest to continue supporting OSO's efforts and to stand united in this cause.

Best,

Gene O'Neal

Robert Dmohowski

From: Cindi Endsley <cindiendsley@gmail.com>
Sent: Friday, January 17, 2025 8:55 AM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; City Manager; Jimmy Figueroa; Eric Joyce
Cc: Zeb Navarro
Subject: Strong Opposition to Proposed Distribution Center Development Near Residential and Coastal Areas-Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Cindi Endsley
3177 Toopal Dr.
Oceanside, CA 92058
January 17, 2025

City Planning Department and City Council
300 N Coast Hwy
Oceanside CA 92058

Subject: Opposition to Proposed Distribution Center Development-Eddie Jones Project

Dear City Planning Department and City Council Member,

I am writing to express my strong opposition to the proposed development of a distribution center behind my residence. While I recognize the importance of economic growth, the location and scale of this project are deeply concerning for our community, environment, and public infrastructure.

This proposed development is just three miles from the ocean, beach, pier, and harbor—areas that define the identity of Oceanside as a vibrant coastal destination. Cities that Oceanside models itself after, such as Huntington Beach, New Port, Santa Monica, and others, do not place large industrial facilities near their beaches or tourist hubs. Allowing this development would undermine the city's efforts to create a cohesive identity centered around tourism, recreation, and community appeal.

This area is already home to several existing and incoming developments focused on tourism and recreation, which align with the city's character and attract visitors. These include:

- **An existing skatepark**, which results in heavy bicycle and pedestrian traffic from minors. The proposed development would create a significant safety hazard for these young residents due to the increase in semi-truck traffic.
- **A skydiving facility**, which draws adventure-seekers to the area.

- **New recreational developments**, including a wave park, hotel, and other facilities, which will already contribute to increased traffic.

These tourism- and recreation-focused developments share a common theme that enhances the community's appeal. In contrast, a large-scale industrial distribution center is incompatible with this vision and poses significant risks.

The specific concerns include:

1. **Traffic Congestion and Safety Risks:**

The proposed development would bring an influx of semi-truck traffic to an already overburdened highway, exacerbating congestion, worsening delays for commuters, and increasing the risk of accidents. The traffic impact would extend beyond the immediate neighborhood, affecting residents and businesses throughout the city. The addition of heavy industrial traffic near the skatepark, bike path and residential areas puts children, pedestrians, and cyclists in particular danger. Furthermore, there are two entrances to the nearby housing development, one to be primarily occupied by this center, residents face restricted access and delays during emergencies.

2. **Environmental Impact:**

The site's proximity to a nature preserve and the riverbed makes it unsuitable for industrial activity. Truck emissions, noise, and potential pollution from runoff would harm the local ecosystem, threatening wildlife and the natural beauty of the area.

3. **Quality of Life for Residents:**

Noise pollution from truck bays, diminished air quality, and increased light pollution would disrupt not only the adjacent housing development but also the surrounding areas. These issues degrade the quality of life for residents and diminish Oceanside's appeal as a coastal destination.

4. **Economic and Property Impacts:**

Placing a large industrial facility in such close proximity to residential neighborhoods and tourism-focused developments will likely result in declining property values citywide. It could also deter future investments in recreational and tourism infrastructure, which are essential to Oceanside's economic growth.

5. **Incompatibility with Community Development and Oceanside's Vision:**

Oceanside has worked hard to position itself as a premier coastal destination. Approving a distribution center near the heart of the city undermines these efforts and sets a concerning precedent for future development. The broader community would bear the burden of this decision for years to come.

I urge the city to prioritize sustainable development that complements the community's existing identity and supports its long-term goals. Alternative locations for this particular project should be considered, preferably in areas with minimal impact on residential neighborhoods and natural habitats.

Thank you for considering my concerns. I hope the city will act in the best interests of its residents by rejecting this proposal. Please feel free to contact me for further discussion.

Sincerely,

Cindi Endsley

(949) 338-4831

[LinkedIn](#)

Robert Dmohowski

From: James Walker <jameswalk77@gmail.com>
Sent: Friday, January 17, 2025 4:19 PM
To: Robert Dmohowski
Cc: OceansideSpeaksOut@gmail.com
Subject: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

We live on Shadow Tree Drive and only have two routes to our home/neighborhood. This proposed warehouse/truck stop will make the already limited access far worse than it already is. Given the current situation in LA with the wildfires, this will make our already limited evacuation routes perilous. Please do not recommend approval of this venture.

James Walker
365 Shadow Tree Drive
Oceanside, CA 92058
Sent from my iPhone

Robert Dmohowski

From: Nathalie Vazquez
Sent: Tuesday, January 21, 2025 7:38 AM
To: Robert Dmohowski
Subject: FW: NO on Eddie Jones Project

FYI

-----Original Message-----

From: Amber Sweet <ambersweet80@gmail.com>
Sent: Saturday, January 18, 2025 8:47 AM
To: Zeb Navarro <znavarro@oceansideca.org>; Planning Web <planningstaff@oceansideca.org>; City Council <council@oceansideca.org>; City Manager <citymanager@oceansideca.org>; OceansidespeaksOut2@org
Subject: NO on Eddie Jones Project

Warning: External Source

Hi, I am asking that you please deny the CUP application and Eddie Jones Project. My family and other Neighbors matter this is not okay! Not only will the congestion and danger of only having two ways out of our community by 2 bridges. In Emergencies to get out that alone should be a NO. However more concerning is the fact that air pollution from diesel trucks and the causes it can give to my family and fellow neighbors. I pray that you all make the right choice as if you were in our position (wanis view)....caring for your family and community. Air pollution from diesel trucks can cause early childhood asthma, cognitive decline, risk of stroke and heart, disease, and much more.

Thank you for thinking of others and wanting what is best for others, The Smith Family

Sent from my iPhone

Robert Dmohowski

From: CAROL BROELAND <memaeeee@aol.com>
Sent: Saturday, January 18, 2025 4:14 PM
To: clerk@oceansideca.org
Cc: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; Eric Joyce; Zeb Navarro; citymanger@oceansideca.org; City Manager; City Clerk; Planning Web
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Please, do not allow the Eddie Jones project to proceed at the location of Benet/76. This project will impact our community tremendously. Even just the traffic issue, when combined with Oceankamp, will cause “planned gridlock”! Light and noise pollution would negatively affect the hundreds of homes near the project, not to mention the Prince of Peace Abbey, which would no longer effectively hold peaceful retreats or quiet prayer services.

I live in the airport community and I have experienced a local fire and the need to evacuate. Adding this enormous project, along with all employees who would be employed at the EJ facility, could easily trap me and my family in our neighborhood. One way in, one way out of our neighborhood area would surely cause gridlock and deaths, just as we have seen in the current LA fire. Reading the EIR, which shows the study says it is not a reason for concern....is almost laughable. Several hundred cars (or more) from Oceankamp, added to our regular neighborhood traffic, added to the new building going happening on Airport and Benet.....and the hundreds of cars from our established community, does it make any sense to you that our residents would not be impacted? The “study” is far from reality.

I could go on, because there are dozens of reasons to deny this project and deny the CUP, and there are no good reasons to give an okay!

How would this project benefit our wonderful city of Oceanside? Would it draw tourists? Would locals benefit in any way? Is the money behind this developer speaking louder than common sense? Is it louder than the voices of the residents who pay taxes and vote to put you in office? Is it louder than the Mission Statement for our city government ?

If you have actually taken time to read this, thank you. This is part of your job. I appreciate the opportunity to voice my opinion as a resident of the Airport community. The quality of my life is in your hands.

Carol and Walt Broeland

From Carol's iPad

Robert Dmohowski

From: Elizabeth Peifer <epeifer53@yahoo.com>
Sent: Saturday, January 18, 2025 12:29 PM
To: Planning-Planning Commission
Subject: EJP Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To the City Council,
I am in favor of the EJP project.

Robert Dmohowski

From: Kathy Stark <kathy.stark1@gmail.com>
Sent: Saturday, January 18, 2025 3:29 PM
To: Robert Dmohowski
Subject: Eddie Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Sent from my iPad I am writing to you to ask that you not approve Eddie Jones. I live in the Airport Community and with all the vegetation in the San Luis Rey, brush on either side of us and this huge warehouse at one end and The Wave Park at the other, we will be sitting ducks in the event of a fire. The noise, the pollution and the traffic!!!! The 76 is already SO BAD and neither of those businesses are in place yet. The tracker trailers will ruin the roads in this area. Too many negatives to this proposal. Please, please vote it down. There must be something better we can do there. Thank-you,
Kathy Stark
3323 Morning View Dr. Oceanside

Robert Dmohowski

From: Margot Bowlby <mbowlby59@gmail.com>
Sent: Saturday, January 18, 2025 4:37 PM
To: Planning Web; Robert Dmohowski; Zeb Navarro; City Council; City Clerk; City Manager
Subject: EDDIE JONES proposal

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

I am a 14 year resident of Oceanside. During this time I have increasing concerns about congestion especially on the 76 west of College. Without developers increasing funds that will directly be used to increase/enhance infrastructure and reduce congestion, I would strongly oppose this project.

Margot Bowlby

Robert Dmohowski

From: Elena Baker <elenathebrit@gmail.com>
Sent: Sunday, January 19, 2025 7:15 AM
To: Robert Dmohowski; Planning-Planning Commission; City Manager; City Clerk
Cc: oceansidespeaksout@gmail.com
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

This project must not go forward in the Airport community.

I live on Shadow Tree Dr and the future hazards that can/will happen to our small residential area could/would be disastrous.

We only have one road in and one road out - If a fire like that in LA were to happen here - we would die and it would be not only the fault of the council but also corporate greed.

This venture must be STOPPED!

from Elena Baker

Robert Dmohowski

From: Miranda Palmer <miranda@zynnyme.com>
Sent: Thursday, January 16, 2025 10:20 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Planning Commission Members,

I am writing to express my strong opposition to the proposed Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project located at 250 Eddie Jones Way. As a homeowner residing within one mile of the project site and a business owner in San Diego County, I have significant concerns regarding the adverse impacts this development may have on our community.

Project Scale and Community Character

The proposed development encompasses a 31.79-acre site and includes a 566,905-square-foot warehouse and distribution facility.

Eddie Jones Warehouse Opposition

Such a massive structure is incongruent with the existing character of our neighborhood. The sheer size of the facility threatens to erode the unique charm and sense of community that Oceanside residents cherish.

Environmental and Health Concerns

The project plans feature 114 semi-truck loading terminals and 60 semi-truck parking spots, which will introduce a substantial increase in diesel truck traffic.

Eddie Jones Warehouse Opposition

This surge in heavy-duty vehicle activity is expected to elevate levels of nitrogen dioxide and other pollutants, posing significant health risks to nearby residents. A recent nationwide study linked truck traffic from warehouses to a nearly 20% increase in nitrogen dioxide pollution in downwind neighborhoods within five miles.

Eddie Jones Warehouse Opposition

Given the prevailing wind patterns, our community is at heightened risk of exposure to these harmful emissions. The project is also right along the beloved San Luis Rey River Trail and means increased pollution for people that bike commute and enjoy leisure activities on that 11 mile trail.

Traffic and Infrastructure Strain

The addition of 114 semi-truck loading terminals and 60 semi-truck parking spots, along with over 590 auto parking spots, will exacerbate traffic congestion on local roads, particularly along State Route 76 and Benet Road.

Eddie Jones Warehouse Opposition

The increased traffic not only threatens the safety and convenience of residents but also places undue strain on existing infrastructure, potentially leading to increased wear and maintenance costs.

Economic Impact on Local Businesses

While the project may promise economic benefits, large-scale developments often attract outside corporations, potentially sidelining local entrepreneurs. The influx of non-local businesses can drive up operational costs, making it challenging for community-rooted enterprises to thrive. As a local business owner, I am deeply invested in the economic health of Oceanside and worry that the project's benefits will not outweigh the long-term costs to the community.

Lack of Comprehensive Community Input

Despite the project's significant implications, many residents feel that their concerns have not been adequately addressed. Effective urban planning necessitates transparent processes with robust opportunities for community engagement. The perceived lack of meaningful dialogue undermines the trust and collaboration essential to developing projects that genuinely serve the interests of all stakeholders.

In light of these concerns, I urge the Planning Commission to reconsider the approval of the Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project in its current form. It is imperative that we prioritize the health, safety, and well-being of Oceanside residents and preserve the character of our community for future generations.

Thank you for your attention to this critical matter.

Sincerely,

Miranda Palmer, LMFT (she/her/hers)
720 Wala Drive Oceanside, CA

Co-Founder & Consultant, zynnyme
miranda@zynnyme.com
www.zynnyme.com

Robert Dmohowski

From: Carol McConnell <cmccConnell4444@gmail.com>
Sent: Monday, January 20, 2025 2:33 PM
To: Robert Dmohowski
Cc: Zeb Navarro
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello Rob-
Happy New Year! I hope you're doing well.

As a resident in the community of homes adjacent and upwind of the proposed Eddie Jones Warehouse development, we request that the developer be limited to the existing zoning regulation, which only allows 5-6 Truck Terminals per parcel x 3 = 15-18 Truck Terminals maximum.

This is a significantly lower number of Truck Terminals compared to the developer's three options outlined in their Final EIR, ranging from 57 to 114 Diesel Truck Terminals.

Second item...

I spoke with you approx. 1.5 years ago about what topics/concerns should be addressed in our Appeal Letter (assuming the Planning Commission approves the project in February.) You said our Appeal Letter should respond to specific decisions made by the Commission.

How quickly after the Commission's decision will the specifics be published? We clearly want to respond as efficiently as possible, given the limited 10 day period.

Much appreciated,
Carol McConnell

Robert Dmohowski

From: Karen McCallick <ksmccallick@gmail.com>
Sent: Tuesday, January 21, 2025 5:36 PM
To: Robert Dmohowski
Subject: Proposed Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr. Dmohowski:

As a very concerned neighbor living in the Airport neighborhood, I once again strongly request that the planning commission vote **against** this project.

My primary concerns include:

- Compounding unsafe conditions for vehicles, pedestrians & bicyclists.
- Increasing traffic back-ups on an already very busy thoroughfare.
- Very unhealthy diesel fume pollution impacting humans and animals.
- Noise & light pollution of warehouse activity & semi trucks located **too close** to a neighborhood.

The entrance to the neighborhood via Benet Road is already a dangerous intersection. There have been multiple fatalities when people camping in the roadside bushes of the SLR river have been struck while unsafely darting across Benet Road during all hours of the night and day or attempting to unsafely run across the 76 against traffic. There have been some very horrible accidents in the area of this intersection.

Not only would the increased traffic generated by such a massive warehouse be detrimental to anyone traveling east or west on the very busy 76 corridor, but the safety of pedestrians, bicyclists, and the many nearby residents would be greatly impacted by its presence.

Please do not allow this project to be built at this location.

Sincerely,

Karen McCallick
3425 Spindrift Court
Oceanside, CA 92058

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Wednesday, January 22, 2025 2:40 PM
To: Planning-Planning Commission; Robert Dmohowski; City Clerk; City Manager; Zeb Navarro; City Council
Subject: research to support denial of Eddie Jones CUP and proposal

Follow Up Flag: Follow up
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Hello Rob and members of the planning commission,

I have been doing more research on the impacts of mega-distribution centers on surrounding communities and wanted to share the insights as I know you are considering the Conditional Use Permit (CUP) application and proposal for the Eddie Jones Project. I hope the following data points and reports will help support a recommendation to deny the CUP and proposal for the Eddie Jones Project.

A recent study [Prime Locations, Hidden Costs: Measuring the Impact of Amazon Distribution Centers on Housing Prices](#) published in October 2024, revealed “a discernible negative impact of almost 10% on house prices within 0.5 mile of new Amazon distribution facilities.” I would argue the impact on my home value would be even greater as I am less than 800 feet from this project, directly across the river with views of the project from my home office, master bedroom, master bathroom and upstairs balcony. In fact, my entire backyard where my children spend most of their time faces the length of the project, which yields an abundance of exposure to operations there.

I knew the land near the airport was zoned as light-commercial when I purchased, and moved to this home when the previous buildings were in operation. I would never have expected when I purchased this home that there would be a proposal for a mega industrial facility proposed on that land. I would argue that since the City approved the residential development of several thousand homes off Benet Road, this is no longer an industrial area as it once was back in the 1960s when the Eddie Jones land was first zoned as light-industrial. The presence of the Prince Skate Park further establishes this area as more of a family-oriented recreational space than an industrial park now. In addition to my children and others who live in the Wanis View neighborhood, there are many more who are at risk of health impacts from expected air pollution from 57+ semi trucks within several hundred feet, risks that include childhood asthma, heart disease, cognitive decline and risk of stroke according to [a report from the Environmental Defense Fund](#) published in 2023. Hundreds of children gather at the Prince Skate Park every day and engage in strenuous outdoor recreational activities. Both ozone and fine particulate matter, formed from chemicals in diesel exhaust, are known to be harmful to human health, [according to the EPA](#). The Eddie Jones project would bring concentrated levels of these chemicals to the immediate area where these children are present day and night. It would also bring dangerous levels of these chemicals to my backyard. I live only 0.15 mile from the project as stated in the FEIR – that's less than 800 feet!

Lastly, my concerns about noise pollution from the engines and backup alarms of 57 semi trucks, assisting forklifts and machinery possibly operating 24 hours a day so close to my bedroom literally keeps me up at night already. Per the [American Health Association](#), “Chronic noise, even at low levels, can cause annoyance, sleep disruption, and stress that contribute to cardiovascular disease, cerebrovascular disease, metabolic disturbances, exacerbation of psychological disorders, and premature mortality. Noise interferes with cognition and learning, contributes to behavior problems, and reduces achievement and productivity.” The valley of the San Luis Rey river amplifies noise in the quiet hours between sunset and sunrise, as evidenced in a video I sent to Rob Dmohowski of a single truck backup alarm at the adjacent airport (on the other side of the Eddie Jones land) when they were doing repairs on the runway in October 2022.

I reviewed the final EIR and saw that my name listed as someone who submitted feedback on the draft EIR, however I didn't see any of my concerns addressed. I would consider my above concerns on the health of my family and surrounding community to be a significant risk. I am not impressed with the developer's tactic of first presenting an outrageous design for 114 truck bays and then saying that they addressed the community's concerns with a 50% reduction to 57 truck bays. That was not a negotiation, that was psychological manipulation to make it look like they were making changes to accommodate the community when in fact, 57 truck bays is still 4X the number of truck bays that was proposed in the Amazon project that was unanimously rejected by the City Council. Given the shift towards residential and recreational land uses in this area, I would argue that any development should be forced to comply with the existing zoning, which is already dated and suboptimal for an area where homes are built so close by.

In closing, I am asking the Planning Commission to deny this CUP application and proposal. It is not in the best interest of the community and only selfishly benefits the developers and their financial pursuits.

Thank you for your consideration.

Gretchen Gary

471 Tishmal Ct

Robert Dmohowski

From: Thomas Le Vine <tomdlevine@gmail.com>
Sent: Wednesday, January 22, 2025 9:50 AM
To: Robert Dmohowski; City Council; Esther Sanchez; Ryan Keim; Rick Robinson; Eric Joyce; Peter Weiss
Subject: Re: Warehouse --- Eddie Jones Project --- Oceanside Neighborhood

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Good morning. Our family is firmly opposed to this project, and will not support any elected official who votes for this. Instead, we recommend placing the Eddie Jones warehouse / truck bays in an established commercial area (fill-in construction) between Highway 76 and Mission, with multiple roads and better ingress and egress. This way, Oceanside can promote new jobs and still maintain good planning and safety practices. I'm surprised by the burgeoning social media opposition.

We've experienced an evacuation due to a nearby hillside fire, and there are few entry and exit options as it is.

Homeless individuals, walking and riding bikes, are likely to die on Foussat and Benet, if this is approved. The pedestrian activity is less problematic now (because it is residential). I recommend you take the time to drive through the neighborhood on Foussat and Benet after dark. Please also consider the potential human toll from traffic, new traffic hazards, fumes and air pollution in a box canyon. Trucks are twice as likely to be involved in accidents, compared to other vehicles.

This project is six times the size of something requiring a variance. Would you put this next to your home? Quiet residences immediately adjacent to a 114-bay warehouse, and trucks operating 24 hours a day? This is unacceptable. What kind of city do you want Oceanside to become? I imagine an approval would create a lot of liability for the city in the future.

Hopefully, the voices of Oceanside citizens will rule the day. Thank you for the opportunity to comment.

Tom Le Vine
760-415-6055
TomDLeVine@gmail.com

Robert Dmohowski

From: douglas smith <smithdp5121@gmail.com>
Sent: Thursday, January 23, 2025 9:37 AM
To: Planning-Planning Commission
Cc: COHS; District Elections; Dee; Robert Dmohowski; Marilyn Smith
Subject: Project Number D22-00001 - Warehouse, Manufacturing and Office Facility 250 Eddie Jones Way
Attachments: 2023 October Figure 81_MultiBuildingAlternative.pdf
Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Oceanside Planning Commission

I am addressing you as a resident of Oceanside and, in particular, adjacent to the above project.

I understand that you are planning on having a vote on this project 10 February, 2025, utilizing the developer's proposal and the review by the planning staff. I would like to point out some problems contained within the document entitled "Development Plan, Conditional Use Permit & Variance, Description & Justification", dated November 2024

The document contains two figures, namely "Figure 8-1 Multi-Building Alternative" and "Figure 8-2 Reduced Building Footprint Alternative". I have extracted the figures from the above document and attached them.

As one who is impacted by this development, I have attended meetings, sent correspondence to planning staff and spoken at City Council meetings regarding my personal concerns that are shared by other Oceanside residents, specifically Oceanside Speaks Out (OSO).

The proposed solution has some variations from the Conditional Use Permit (CUP) that accompanied the original proposal of 109 bays.

According to the legend on the document:

Building 1	
Docks	39
Grade Level	6
Building 2	
Docks	33
Grade Level	6
Building 3	
Docks	28
Grade Level	4
Yielding	
Docks	72
Grade Level	16

Not counted in the calculation are 16 proposed docks, for a total of 130 docks of all types creating traffic, noise and pollution in excess of the CUP.

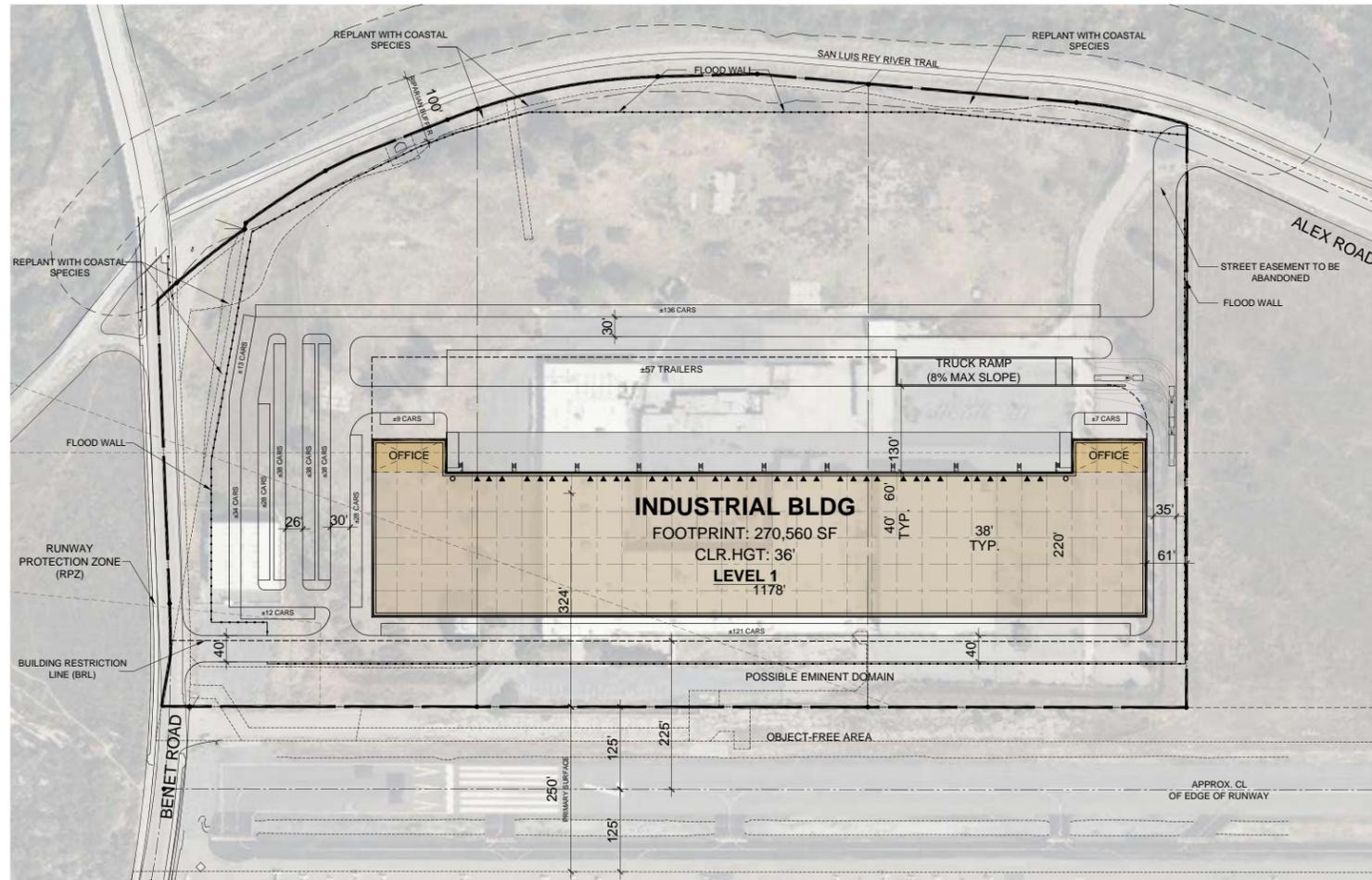
The second figure falls within the CUP but all are facing the residential community. Since trucks are all equipped with warning beepers and those are activated when driving backwards toward the building and alerting alerting potential victims, the projected sound will bounce off the concrete tilt-up building walls and project over the north wall to the residents.

We, the residents, have seen several site plan proposals, including "Option 4" for a required public hearing. It is not known if planning staff was presented with this option but it is not included. There were some accommodations in that plan for the residents but for some reason it was found wanting.

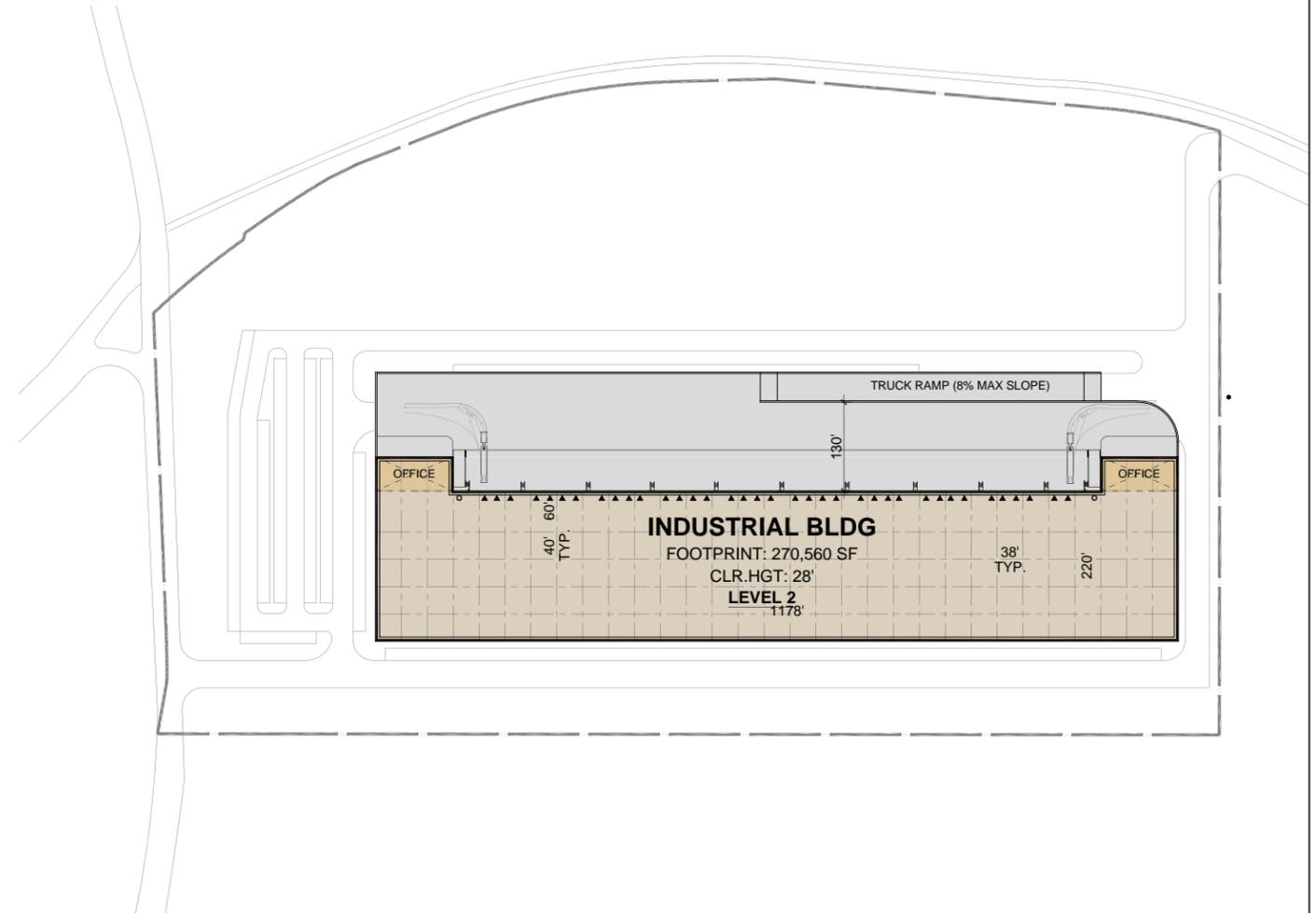
I am speaking for myself but you have heard from others cited and their concerns are mine.

Very Respectfully,

Douglas P. Smith
3215 Canyon View Drive
Oceanside, CA
92058



Conceptual Site Plan - Level 1



Conceptual Site Plan - Level 2

Robert Dmohowski

From: Merry Johnson <merrymacmurphy@gmail.com>
Sent: Thursday, January 23, 2025 6:21 AM
To: Robert Dmohowski
Subject: STOP the Eddie Jones project.

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

This project will be detrimental to Oceanside and its residents. Traffic and safety concerns are one of the most important issues. This will not benefit Oceanside residents in any way and will reduce the quality of life and reduce property values. This is going to degrade beautiful Oceanside. There is not one good reason to allow this project!

Please listen to the people who will be impacted by this project in a negative way. We do not want this and you should not approve it.

If you do not listen to us, and go head with this. Then you need to make the developer widen Mission road and do other things to prepare for all the increase in traffic. Strings need to be attached at least. I am so sick of developers taking over and city planners not protecting our lifestyle in Oceanside.

Do the right thing and do not allow the Eddie Jones project.

Merry Johnson
3755 Vista Campana N. Unit 8
Oceanside CA 92057

Robert Dmohowski

From: patti briscoe <smufish1@yahoo.com>
Sent: Thursday, January 23, 2025 11:36 AM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; Zeb Navarro; Eric Joyce; Ryan Keim; City Manager; OceansideSpeaksOut@gmail.com
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To all concerned, (or those who should be)

As a resident of Marlado Highlands for just under two years I have come to love Oceanside and the "Island" North of 76. I refer to it as an island as there are only 2 means (bridges) to enter and exit the neighborhoods over the river. We have an entrance at Foussat Road and another at Benet Road. There is currently no other way to access or exit our neighborhoods. Both egress points face south and would be choked off by the Eddie Jones Project on one side and the Surf Kamp on the other. There are no East, West or North options to enter or exit the "island"

As the wildfires rage and ravage in Los Angeles, and as close as Bonsal and Fallbrook, I am concerned with the safety of our "island", the residents, and homes in our community. If or when a fire were to break out, what would happen after the Surf Kamp project is complete at one of our access points and the Eddie Jones project at the other?

Any traffic study that has been previously done should be rendered invalid as the Surf Kamp complex has barely started. What will happen to our access points with an additional 700 homes, a hotel, retail and other entertainment features? That alone is a large complex for an "island" entrance! The Eddie Jones project would not only be an enormous complex at the other "island" limited access point but does nothing to serve the neighborhood or surrounding areas; except to add noise, air pollution and disruption to natural habitats in the area. There would be additional traffic not only on the "island" but the 76 and the ramps to and from Hwy 5. Being so close to the Camp Pendelton entrance both North and South traffic is already snarled at the 76 and 5 exits.

Does Oceanside want to be known as another coastal industrial area or as a Northern gateway to San Diego County? Placing a Warehouse at the entrance to our NW community is a colossal mistake to the future vision of the city of Oceanside, The city has come so far...please don't go back! Wouldn't this project be better suited closer to the boarder control North on HWY 5? Has there been any research done to mitigate what should be flowing through this trucking warehouse? Who will be checking the content of what passes through the warehouse?

I'm not sure how this project received clearance to be purchased or has even proceeded to this point given California's focus on Climate? Just today I received a letter in the mail regarding the Climate Action Plan and goals from CEA (Clean Energy Alliance). The Eddie Jones project seems to be totally misaligned with the climate plan. Being located so close to HWY 5 and the ocean as well as natural habitats, a bike path, skate park, airport, river and soon to be built Surf Kamp.

Once this project is approved there is no turning back! Oceanside continues to grow, is this really the best use of land within 3 miles of our beautiful Pacific Ocean? The Eddie Jones project makes no sense for anyone but greedy corporations. **No one living in Oceanside or the immediate area will be enhanced or enriched by this truck warehouse project. What is in it for the community, climate, or nature?**

thank you for your time and consideration

If this were Survivor reality show, I would vote to kick Eddie Jones off the "island"!

A concerned neighbor

Patti Briscoe

Robert Dmohowski

From: Stacy Jackson <stacyojackson@gmail.com>
Sent: Thursday, January 23, 2025 10:27 AM
To: Robert Dmohowski; Zeb Navarro; City Council; City Clerk; City Manager; Planning-Planning Commission
Subject: Opposition to the Eddie Jones Project in Oceanside

Follow Up Flag: Follow up
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Warning: External Source

I am writing to express my opposition to the Eddie Jones Project in Oceanside, California. My family lives in the housing area by the airport/skatepark that will be most impacted by this project. I opposed it for a variety of reasons:

1. It is outside the scope of what the land was originally used for in terms of warehouse size
2. The size of this warehouse, the sheer magnitude of the truck traffic and the 24/7 hours of operation will be a great disruption and disservice to the residents of the surrounding area. Trucks will clog the roadway and we only have two avenues of ingress/egress in our neighborhood. If there is a fire, it will be a challenge to evacuate everyone via these exit points and trucks will further clog access to 76. The traffic on 76 is already a nightmare and truck congestion will only make this work. I've seen trucks lined up by other warehouses and they just sit on the roadway - in the median - and this will be disruptive.
3. I also have environmental concerns regarding air quality due to the increased traffic and 24/7 operations schedule.
4. If a similar warehouse was not allowed in the Oceana industrial park, why should it be allowed near my house? If the council voted that down, because a council member was a NIMBY - why should it be ok for me and my neighbors.
5. Our neighborhood is already impacted by the housing, retail, wavepark at Foussat Road. This project is completely out of character with what is forming at these locations. We have a nice little tourist area growing and this will be an eyesore.

I will be at the upcoming meeting to voice my concerns in person. Additionally, my family of 4 will vote accordingly in any upcoming elections if this project goes through.

Kind regards,

Stacy Jackson

Robert Dmohowski

From: Steve DuBois <stevedubois1@aol.com>
Sent: Thursday, January 23, 2025 3:20 PM
To: Planning-Planning Commission
Subject: warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Please say no the proposal for a massive warehouse next to the O-Side airport We have way to much traffic on the 76 now with much more to come with North River Farms and other in work projects that will rely on the 76 for travel This massive warehouse will only add the the mess we already have

Stop digging us into a deeper hole than we are already in A concerned voter in Oceanside Steve Sent from my iPhone

Robert Dmohowski

From: Dr. Michael Tenhover <tenhover@protonmail.com>
Sent: Friday, January 24, 2025 12:00 PM
To: Robert Dmohowski
Subject: Comment on Final Environmental Impact Report - Eddie Jones Warehouse Project
Attachments: Response to Warehouse EIR Jan 2025 Tenhover .pdf

Warning: External Source

Hi Rob,

It was a pleasure meeting you at the last City Council meeting. It is always nice to connect an email contact with a face.

Please find enclosed my comments on the RAF PACIFICA GROUP's EIR

Eddie Jones Warehouse, Manufacturing and Distribution Facility Project ([SCH No: 2022070365](#)).

Thanks for your help in this matter,

Regards,
Mike Tenhover
873 Wala Drive
760 231-6396

Sent with [Proton Mail](#) secure email.

Response to the recently released Environmental Impact Report

(RAF PACIFICA GROUP)

Noise Issues with the Fourth Iteration of the Project

Prepared by Michael Tenhover (January 24, 2025)

*The following memo describes concerns with the Fourth Iteration of the “eddie jones warehouse project” (the “Project”). The new building configuration has improved the noise impact for some parts of the area. However, **the critical Wildlife region** in the adjacent San Luis Rey River and the **Wanis View homes** continue to have unacceptably high levels of noise from warehouse operations.*

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- I. Major Issue with the EIR Fourth Iteration - related to Noise
- II. Noise levels for Fourth Iteration and other building layouts
- III. Recommendations- Issues to be resolved
- IV. Alternate Layouts for Project: Good and Better

- V. Appendix 1:
Calculation Methodology for Fourth Iteration of Project

- VI. Appendix 2:
Calculation of Noise Levels for original Project design [1] (Dec 2024).

- VII. Appendix 3:
References

I. Major Issue with the EIR- Fourth Iteration- related to noise

- a. Unacceptable levels of noise from warehouse operations are still present in the River and Wanis View Homes areas.

II. Noise levels for Fourth Iteration and other building layouts.

The Noise levels for the Fourth Iteration have been calculated (see Appendix 1). This includes the locations: River, Wanis View Homes, Wanis View Preserve, and Prince of Peace Abbey (see photo/diagram below)



Figure 1. Photo of Project Site taken from the Wanis View Wildlife Preserve (.33.225231155953736, -117.3580229412305) The Fourth Iteration buildings (in brown) are shown with the proposed North-South Orientation.

Figure 2 shows the results of the calculations of noise levels for the original Project design [1], the Fourth Iteration (with 4 buildings going North to South) and a modified Fourth Iteration (with the 4 buildings going from East to West). In general, the transition from the Original Design to the Fourth Iteration has improved the Noise situation.

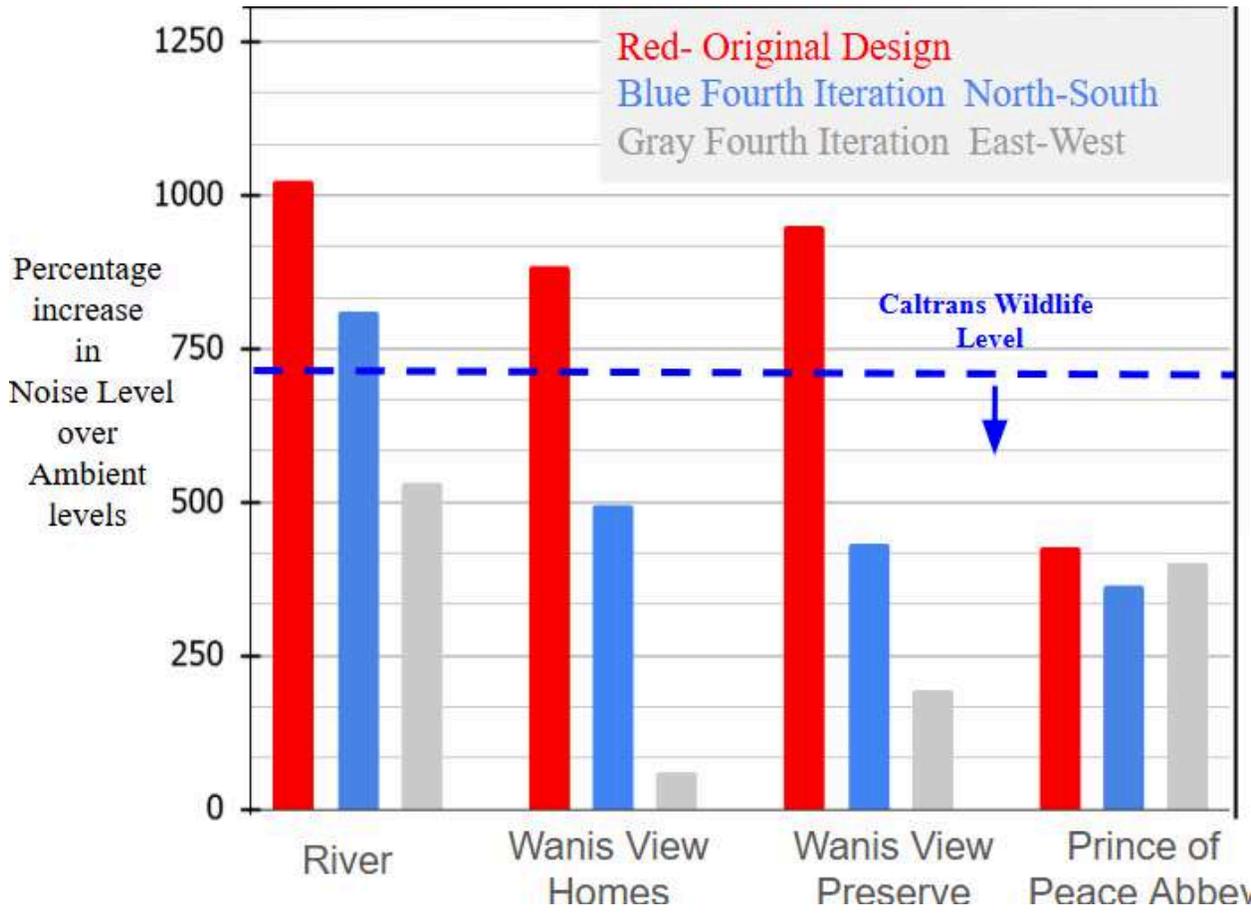


Figure 2. Noise Levels due to warehouse operations at the locations noted for various building layouts. Red is the original Project design. Blue is the Fourth Iteration design with the 4 buildings having a North-South orientation, and Gray is a modified Fourth Iteration with an East-West orientation for the 4 buildings.

In the case of River, the **red** and **blue** bars represent the area to the North of the Project, while the **gray** bar represents the area to the West of the Project.

Comments:

1. The Fourth Iteration continues to have unacceptable noise levels in the River area. (**blue bar**) This area is a critical component of the local wildlife biodiversity and wildlife corridor (see previous memo Dec 4, 2023 [1]). These noise levels exceed Caltrans guidelines for wildlife areas.
2. The noise levels for the River can be improved by adopting a modified Fourth Iteration with buildings going East to West. (**gray bar**) With this modification, the relevant section of the River is now West of the site. The distance is slightly larger in this case (from warehouse operations to River) and because of the road there, the ambient noise level is higher. Levels are still high but within Caltrans guidance.
3. The proposed Fourth Iteration has a high level of noise for the Wanis View Homes (**blue bar**).
4. The noise level for the Wanis View homes can be improved by changing the building layout to an East-West orientation (**gray bar**) when combined with other features (see section III. Recommendations, item 2 below)
5. Other layouts are possible for the Project that will have improved- lower noise levels than the North-South Fourth Iteration and the original design. (see section IV.)
6. The Fourth Iteration with either a North-South or East-West orientation has reduced the noise level for the Wanis View Preserve.
7. The noise levels for the Prince of Peace Abbey have not improved with the Fourth Iteration introduction.

III. Recommendations– Issues to be resolved.

1. **To resolve the Noise issue in the River area** a number of changes will be required. Changing the building layout to an East-West orientation improves the situation. In addition to this, other desirable measures to further lower the noise levels should be considered: These would include: 1) Strict restrictions on the number of trucks on-site at any specific time- a number

determined by Noise level calculations. 2) No truck travel along the North section of the Project- i.e. parallel to the River. 3) Restrictions on truck idling and forklift operations near the West ends of the channels . 4) No truck bays on the West section.

2. **To resolve the noise issue for the Wanis View homes.** The predicted, “worse case” noise values for the Fourth Iteration are not high enough to constitute a human health hazard. However they are high enough to impact the quality of life for the residents there. The Project should consider a design in which the buildings have an East-West orientation as long as no trucks move parallel to the River area in the extreme North section of the property. Truck movement should be restricted to the areas between the buildings and the areas adjacent to the airport.

IV. Alternate Layouts for Fourth Iteration to address Noise Concerns: East West Layout

Good:



Better



**Appendix I. Modification of Noise Calculations for "Fourth Iteration"
Parallel buildings with trucks, forklifts, etc. moving between them.**

The Fourth Iteration introduces trucks, forklifts, etc. stationed and moving between four buildings (channels), three channels in total. This is substantially different from the original Project design and thus warrants a new look at expected noise levels. In this exercise, it is also assumed that fewer trucks will be moving, idling or other substantial noise generating activities along the North section of the Project than was in the Original design. If this is not the case, the calculations will need to be redone as this will lead to a considerable increase in Noise for both the River and the Wanis View homes. Basic information used in the present memo is from reference 1.

The new problem with the Fourth Iteration to be addressed considers acoustic sources between buildings, which is approximated as a channel with vertical sides of finite height, parallel non-absorbing walls ($\alpha \ll 1$) and a once reflecting ground surface. This is similar to the problem of sound propagation in a tunnel or waveguide, in which reflections from the surrounding surfaces play an important role. [2-9] The effect of multiple reflections is modeled by the insertion of an infinite series of image sources for the acoustic source (rays). Each of the three channels of the Project (between the four separate buildings) are treated separately and added together with the usual methods.

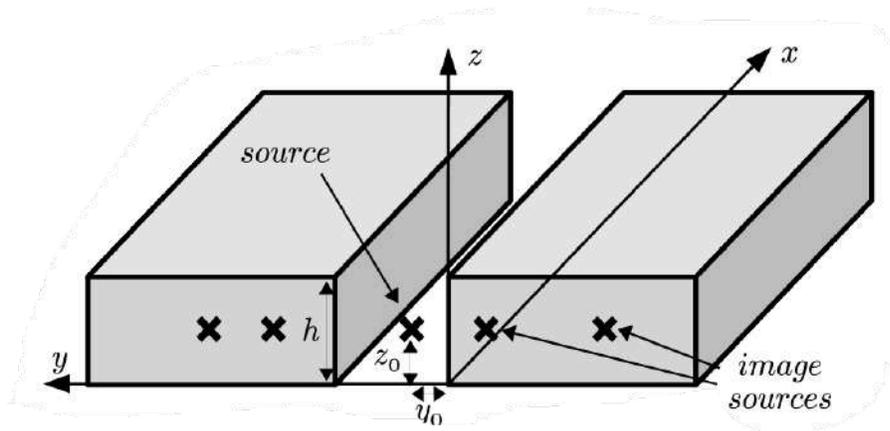
According to the ray model, the acoustic power flow (P) across a channel cross-section at distance x from the source is obtained by the incoherent sum of the free space power flows from each of the image sources.

When x is much greater than h (building height) and with absorption at the walls ($\alpha > 0$) this can be approximated by:

$$P \sim \frac{2}{\pi} \int_0^{\frac{\pi}{2}} \frac{(1 - \alpha)^{x \tan \theta}}{\sqrt{1 + \left(\frac{x}{h \cos \theta}\right)^2}} d\theta.$$

The power flow (for constant building height- h , $\alpha \ll 1$ and small values of z_0) which can be approximated as:

$$P = \frac{2}{\pi} \int_0^{\frac{\pi}{2}} \frac{1}{\sqrt{1 + \left(\frac{x}{h \cos \theta}\right)^2}} d\theta$$



The various quantities are defined in the adjacent diagram

$$\begin{aligned} x &= \cos \varphi \cos \theta, \\ y - y_0 &= \cos \varphi \sin \theta, \\ z &= \sin \varphi, \end{aligned}$$

Outside of the channels, the standard wave equation is used.

$$\nabla^2 p - \frac{1}{c^2} \frac{\partial^2 p}{\partial t^2} = 0$$

In which p is the acoustic pressure, c is the propagation speed, t is time.

The future owners/operators of the warehouse are unknown (at least not disclosed at the time of this note). The expected and future level of activity is also unknown. Still, a worst case for noise can be described.

Three figures of merit are relevant:

1. Number of active truck bays
2. The location of the bays in the Project
3. Time to unload/load the trucks.

With advances in automation, innovations in processing, the unload/load times are expected to drop over time and the level of activity of the warehouse could increase. The worst case would therefore be a quasi-continuous movement of trucks/forklifts/backup alerts accelerating, moving, idling, loading/unloading and moving between the four buildings of the Fourth Iteration. The results of this analysis is shown in Figure 2 above.

Appendix II. Noise level calculations for original Project design memo:

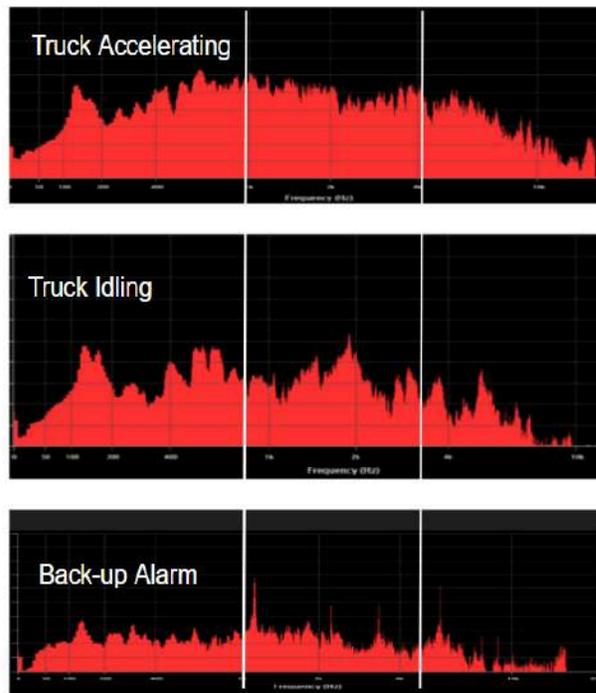
Response to the recently released Draft Environmental Impact Report (RAF PACIFICA GROUP) Devastating Impact of a Proposed Warehouse Project on Essential Wildlife Corridors Prepared by Wanis View Wildlife Preserve Volunteers (submitted Dec 4, 2023) [1]

Relevant Section on Noise Calculations:

Inputs: Literature data, discussions with an industry expert, visits to nearby warehouse operations. Background sound levels were measured at the various locations, both at daytime and nighttime.

Truck Warehouses are very noisy places, some of the noise levels (at 50 feet) are:

Source	Sounds Level (dBa) at 50 feet
Truck Idle	80-96
Truck accelerating	90
Engine Noise	78
Exhaust Noise	88
Low speed truck movement	88
Compressors/exhaust fans/HVAC	95
Trailer Refrigeration Units	96-104
Back-up alarms	80



Frequency Spectrum of sounds produced in a diesel truck warehouse

The White bars show the part of the Spectrum that is most important for Birds and other wildlife

dBa values are related more to human hearing (frequency 2000 to 6000 Hz)

These three dominant sounds at a warehouse all produce considerable energy in the frequency range of importance for wildlife (1000-4000 Hz, the region inside the white vertical bars in the above figure). The back-up alarms are actually designed to produce sound at 1200 Hz making them highly problematic for wildlife. Note the low frequency contributions in the truck idling and truck accelerating charts.

A noise signal component can be characterized with four quantities: amplitude, frequency content, time, and phase. It can be mathematically described as

$$x(t) = \sum a_i \cos(\omega_i t + \alpha_i) + \sum \sum b_{ij}(t) u(t - t_j) \times \cos(\omega_{ij} t + \alpha_{ij})$$

where a_i and $b_{ij}(t)$ denote the amplitude of a signal component, ω_i and ω_{ij} represent the frequency, $u(t)$ is the step function, t_j is the instant at which an event occurs, and a_i

and α_{ij} are phases of signal components.

The subject calculations are three dimensional in nature, but ignore some acoustic effects related to the terrain and topography of the nearby hills and canyons. The

nature of the ground in nearby areas would tend to reflect and perhaps focus some of the acoustic energy. The current calculations consider only noise from these sources: trucks accelerating, trucks idling, HVAC, back-up alarms. Other noise sources will depend on the specifics of the warehouse operations. For example, significant additional noise will be present if refrigerated operations are in place.

The usual correction factor for vegetation is applied to the final results. This does not completely exist now, but it is assumed the warehouse developer could add this to help reduce noise levels. The existing vegetation along the Wanis Nature trail is included. The ground-level (road surface) of the warehouse would be some 8-10 meters above sea level. The San Luis Rey river in that region is 4-5 meters above sea level. The Wanis nature trail is 14 meters above sea level. The Wanis View Preserve ranges from 8 to 60 meters above sea level.

Two Cases Considered:

1. Worst Case— assumptions and rationale.

The wildlife preserves and corridors are meant to function forever. The Wanis View Preserve agreement terms are “in perpetuity”. We cannot predict the future owners/operators of the warehouse and thus the level of activity. Still, a worst case for noise can be described. Two figures of merit are relevant. One is the possible numbers of truck bays and the other is the time to unload/load the trucks. With advances in automation, innovations in processing, the unload/load times are expected to drop over time and the level of activity of the warehouse could increase. The worst case would therefore be a quasi-continuous movement of trucks accelerating, moving, idling, loading/unloading, pausing as they move to enter/exit the warehouse.

2. Typical Case– assumptions and rationale.

A typical case is considered in which there are on average 2 trucks pulling in/out of the facility, 4 trucks idling, HVAC, and 4 back-up alarms in use.

Calculation Details: The starting point for the calculations is solving the standard wave equation:

$$\nabla^2 p - \frac{1}{c^2} \frac{\partial^2 p}{\partial t^2} = 0$$

In which p is the acoustic pressure, c is the propagation speed, t is time.

In evaluating the **Worst Case**, the continuous movement of trucks gives a cylindrical symmetry to the acoustic source (line sources parallel to the river basin). The form of p (r,t) in this case is best expressed by the H₀ and H₁ Hankel functions.

Location	Distance (feet)	Calculated Noise Level Increase
River	340	1025%
Wanis Nature Trail	1179	944%
Wanis View Preserve	1790	954%

Typical Case. This is a case in which a number of effectively point sound sources are distributed in space. These sources will be uncorrelated. Each of these point sources emit spherically symmetric acoustic waves with a boundary condition (reflection) on the hard ground surface as well as the building itself.

Location	Distance (feet)	Calculated Noise Level Increase
River	340	1012%
Wanis Nature Trail	1179	398%
Wanis View Preserve	1790	0%

Confirmation of Methodology used in this analysis.

The reliability of the current calculation methods was checked by comparing them to measurements over the area in question. Hwy 76 was used as a noise source (cylindrically symmetric source) located south of the warehouse. The measurements were conducted north of the warehouse site, so this includes all the terrain in question. The sound intensity was measured at various distances from Hwy 76 during early morning rush hour. (This corresponds to 76 dBA at 50 feet). Measurements at various locations were done in the absence of aircraft, wind and bird noise). Background noise levels were measured at early morning hours with minimal traffic on Hwy 76. At Rush Hour:

2400 feet from Hwy 76	Increase in Noise Level
Measurement	288%
Calculation	264%

Measurement-Calculated values are within 8%.

Appendix III. References

1. Memo prepared by Wanis View Preserve Volunteers in response to DEIR, submitted Dec 4, 2024.
2. Francis M. Wiener; Charles I. Malme; Creighton M. Gogos Sound Propagation in Urban Areas J. Acoust. Soc. Am. 37, 738–747 (1965)
3. J. Kang. Urban sound environment. Taylor and Francis, 2006
4. R. H. Lyon. Role of multiple reflections and reverberation in urban noise propagation. Journal of Applied Physics 111, 114906 (2012)
5. Miguel Molero, Simon Felix, Vincent Pagneux, and Olivier Richoux.. Sound propagation in periodic urban areas Soc. Am., 55(3):493–503, March 1974.

6. M. Hornikxa, M. Dohmena, K. Conenb, T. van Hooffc, The wind effect on sound propagation over urban areas: Predictions for generic urban sections Building and Environment Volume 144, 15 October 2018
7. M R Ismail The propagation of noise through the urban fabric Proc. |.O.A. Vol 22 Pan 2 (2000)
8. Wolfgang Probst Prediction of Sound radiated from Tunnel Openings March 2010 Noise Control Engineering Journal 58(2)
9. David Peter Hewett Sound Propagation in an Urban Environment thesis Term 2010

Robert Dmohowski

From: John Bucalo <racetrackspecialties@gmail.com>
Sent: Friday, January 24, 2025 6:35 AM
To: Planning-Planning Commission; City Clerk; Zeb Navarro; City Council; City Manager; Robert Dmohowski
Subject: Stop Eddie Jones CUP and proposal

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To the Planning Commission, Council members and all involved.

This is John Bucalo, I am an owner and resident of 471 Tishmal Court in beautiful Oceanside. My home is less than 800 ft from the proposed Eddie Jones project and sits along the San Luis River.

I am also a Licensed grading contractor and have worked building many projects like Eddie Jones and therefore have first hand knowledge of these kinds of projects and what they bring to the table.

1) If its 57 truck bays then its 57 back up alarms on the trucks and 57 alarms on the forklifts unloading the 40ft trailers. There are approximately 40 pallets, many double stacked - that's alot of trips that echo sound, Both Cal O.S.H.A. and the company's insurance companies will not let them remove the alarms or lower the sound by law.

2) Up to 60 trucks can and will be waiting on deck backing into spots (alarms). Many folks live in the trucks while on the road and run generators. This affects our air and sound quality and brings light pollution which has negative effects on human health and nearby wildlife.

All the trucks will also have a negative impact on our nearby traffic on both Benet Rd and Hwy 76. Not to mention trucks not familiar with the site will come through Foussat Rd and be diverted through the neighbourhood .

3) The builder states they don't know if the facility will be a 24 hr run facility or not. Most likely it will, and we already suffer from back up alarms from the nearby rental yard that echo through the night and early am.

When the airport serviced the runway recently it was during the night and the back up alarms were so loud I couldn't sleep . Imagine what it would be like with all the trucks of a massive warehouse / distribution center.

4) Oceanside is beautiful, is this a good project for this land? The Ocean Kamp project brings excitement and value to the valley .The valley has always been seen as a Ghetto and only the City Planning Commission can ensure it doesn't stay that way. Please don't approve projects like this one that would further degrade the quality of life here. Eddie Jones brings no positive impact to the community. We are 2 miles from the ocean. Let's build something that actually compliments the great city of Oceanside and its people.

Thanks for listening,
John Bucalo
760 908 7337

Robert Dmohowski

From: Linda Middleton <lindaincarlsbad@yahoo.com>
Sent: Friday, January 24, 2025 3:26 PM
To: Robert Dmohowski
Cc: Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; City Council
Subject: Eddie jones project concerns

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

I am writing to share my concerns about the Eddie Jone Warehouse project. I read the EIR report and see that they have decreased the number of truck bays - however the same concerns that have been raised in the past remain. For myself and my family, I am concerned about the size of this project in an area that will impact a large neighborhood of family homes. The project which could still run 24 hours a day with hundreds of large trucks driving into and out of our neighborhood will impact noise levels, pollution and most importantly a safe entrance and exit from the 76 highway into our neighborhood.

The 2 lanes in and out of Benet Road to the 76 highway do not allow for both automobile and large truck traffic. The developer mentioned an extra right turn lane - this is not enough to keep us from being backed up with only one way in and out. In the case of a fire (which we had less than 2 years ago) this could backlog traffic and cause people to be unable to leave the area. It could also affect emergency vehicle access. Before anything is built on that site, Benet Road needs to be widened to 4 lanes at least through to Alex road.

When I bought my home here 20 years ago there was a manufacturing plant on the property - It contained one story buildings and a large green space. Automobile traffic (employees) entered from Foussat so we saw little impact - maybe some traffic at the employees' start and stop work times. I understand the owner wants to develop the property - but the planning commission should NOT allow a zoning exception to the developer.

There has to be a better project for this site that meets current zoning. Do not allow a large developer to dictate what should happen. Oceanside continues to grow but it can be done in a way that continues a quality way of life for those of us who call it home.

Respectfully,

Les and Linda Middleton
601 Tukmal Dr
Oceanside, 92058

Robert Dmohowski

From: Madison Lang <madisonlng@yahoo.com>
Sent: Friday, January 24, 2025 10:59 PM
To: Robert Dmohowski
Cc: oceansidespeaksout@gmail.com
Subject: The Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Planning Commission **URGENT – FIRE DISASTER IMMINENT**

Dear Planning Commission Members,

This is a desperate plea. Time is running out. The Eddie Jones Project, in its current form, is a death trap.

Imagine this: You're on Highway 76, heading west from North Santa Fe Road towards the 5 freeway. A fire has started near the riverbed and has already moved to the other side of Foussat. You can't see the flames yet, but the smoke is choking the air, thick and black.

You pass the housing communities, the shopping center, the old church... and then you see it. A wall of fire, consuming everything in its path. Cars are jammed together, a sea of panicked faces. The traffic lights are out, fire trucks struggle to navigate the chaos.

To your left, Ocean Kamp is a nightmare. Hundreds of cars are trapped, desperate to escape the Santa Ana driven inferno that's closing in from both sides. The line of vehicles seems endless, a terrifying testament to the scale of this disaster.

To your right, it's even worse. A line of massive trucks, stretching all the way back to Benet, are stuck, their flammable cargo turning them into potential bombs. The radio blares a horrifying message: trucks from the Eddie Jones warehouse are adding to the gridlock, clogging the 5 freeway in both directions.

Panic grips you. Escape routes are swallowed by flames. Trapped between fire and a potential explosion, you're helpless. The San Luis Rey riverbed is now ablaze, a terrifying echo of the Paradise Fire.

This isn't a hypothetical scenario, Tom. This is what we face if we don't act. The Eddie Jones Project, with its massive influx of trucks and traffic, is a disaster waiting to happen. We need to address this NOW, before it's too late.

Please, we need your help. Lives depend on it.

Sincerely,

Ms. Madison Moorehead-Lang

760-458-9034 [Sent from Yahoo Mail for iPhone](#)

Robert Dmohowski

From: Mary Weseloh <mareejada@live.com>
Sent: Friday, January 24, 2025 12:11 AM
To: clerk@oceansideca.org; Robert Dmohowski; Planning-Planning Commission; City Manager
Cc: City Clerk; oceansidespeaksout@gmail.com
Subject: Eddie Jones Warehouse, Manufacturing, and Distribution Facility.

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City of Oceanside Representatives,

Our community is reaching out for help and consideration- please listen.

We do appreciate the desire for Oceanside to seek adding more revenue generating businesses to the city- the Eddie Jones project is just slated in the wrong area.

As a long-term resident of the Airport community, I would like to express my opposition to the proposed project of the Eddie Jones Warehouse, Manufacturing, and Distribution Facility. This is a project that will not only cause unbearable traffic, noise, and pollution to a now family centric neighborhood, it also shows ZERO benefit to the people living in this area.

We have already seen the semi trucks using Benet Rd as a training route for new drivers of massive transport trucks.

None of the residents here signed up for this kind of thing when purchasing homes to raise families in a peaceful place. This neighborhood now has property values north of \$1 million attracting more families with many children who should have the ability to ride bikes, walk, and play amid careful drivers rather than dodge "semis in training". This addition at the entrance of our neighborhood would seriously impede the attraction of buyers and renters with families to the area aside from drastically lowering current residents quality of life (and potentially our property values). We already have an airport with multiple unsavory incidents including the "Go Jump" crashes and the "Cocaine plane" as well as skyrocketing homelessness right at our front doors.

There are many families and taxpayers in this community who may be driven out of this area with the addition of this "choice" of business to plant right at the front of our tranquil neighborhood. There are many fully industrial zoned areas of Oceanside with vacancies where this would make sense- why here?

I wouldn't wish this in any another residential community and certainly don't want it here. Please consider how this would impact a large community of people who contribute a lot to this city- both culturally and FINANCIALLY... we all know that the type of people who live in Oceanside want to support their city and have pride in their homes. This will deter people from wanting to be here as part of this hometown community.

The ball is in your court- it's time to make the right decision.

Thank you for your time and consideration.

Mary Weseloh

525 Toota Court

Oceanside, Ca 92058

Robert Dmohowski

From: Hechavarria, Ray <ray.hechavarria@hp.com>
Sent: Friday, January 24, 2025 3:24 PM
To: clerk@oceansideca.org; Robert Dmohowski; Planning-Planning Commission; City Manager; City Clerk; Zeb Navarro; City Council
Cc: ray_hech@yahoo.com; oceansidespeaksout@gmail.com
Subject: Eddie Jones Warehouse Concerns!

Importance: High

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

I am writing this email to ask the planning commission to DENY the proposed project of the Eddie Jones Warehouse on Benet Rd. in Oceanside. As a 21-year resident of this community, I am concerned about the safety and traffic impacts of this project.

We have steadily seen the traffic increases on Hwy 76 through the years and the addition of this warehouse will take those traffic issues to another level.

Even more importantly, with the recent fires in Southern California, having a warehouse of this size so close to the residential community could severely impact evacuation routes in case of an emergency. Having grown up in the Altadena community in Los Angeles and personally seeing the direct impact that the recent large fires have had on friends and family, I am at least comforted by knowing that those closest to me were able to evacuate the area to get to safety. I am very concerned that we may not be as lucky in this community in case of a large fire emergency with the addition of traffic and impediments that a large warehouse like this would add in case of an emergency.

I respectfully request that you recommend that this project is NOT APPROVED when it comes up for vote.

Thank you,
Ray Hechavarria
Resident – Airport Community

Robert Dmohowski

From: Lisa Urbach <lurbach3@gmail.com>
Sent: Saturday, January 25, 2025 10:19 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; clerk@oceansideca.org; Planning-Planning Commission; City Clerk; City Council
Cc: oceansidespeaksout@gmail.com
Subject: Opposition to the Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Planning commissioners and esteemed City Council members, Mayor and Deputy Mayor,

I am opposed to the Eddie Jones Project. I am writing you today to request that the Eddie Jones Project not be allowed an exemption to the zoning requirements for the area. Having 57- 114 diesel truck bays, with trucks coming and going 24 hours per day, will undoubtedly clog up the Benet Road access to the Airport community. Highway 76 will be slower than ever without restrictions to when the trucks can travel back and forth to the Interstate 5 and 15 freeways. The pollution in the beautiful City of Oceanside will be unbearable. The noise to the Airport community will be unjustified.

Limiting the developer to the current zoning restrictions would be done in good conscience. Please vote with the citizens of this great city and deny this project as proposed.

Kind regards,

Lisa Urbach

Robert Dmohowski

From: Lisa Urbach <3landslide@gmail.com>
Sent: Saturday, January 25, 2025 10:19 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; clerk@oceansideca.org; Planning-Planning Commission; City Clerk; City Council
Cc: oceansidespeaksout@gmail.com
Subject: Opposition to the Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Mayor and Deputy Mayor, Planning commissioners and City Council members,

I am opposed to the Eddie Jones Project. I am writing you today to request that the Eddie Jones Project not be allowed an exemption to the zoning requirements for the area. Having 57- 114 diesel truck bays, with trucks coming and going 24 hours per day, will undoubtedly clog up the Benet Road access to the Airport community. Highway 76 will be slower than ever without restrictions to when the trucks can travel back and forth to the Interstate 5 and 15 freeways. The pollution in the beautiful City of Oceanside will be unbearable. The noise to the Airport community will be unjustified.

Limiting the developer to the current zoning restrictions would be done in good conscience. Please vote with the citizens of this great city and deny this project as proposed.

Regards,

Martin Urbach

Robert Dmohowski

From: Carol McConnell <cmccommel@aol.com>
Sent: Sunday, January 26, 2025 3:33 PM
To: Planning-Planning Commission
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello Planning Commission Members-

For over two years, Oceanside community members have been deeply concerned about the pollution, noise, traffic, etc. that the proposed Eddie Jones Warehouse Project will bring to the San Luis Rey Bicycle Trail, yet we've only had one simple goal:

*Please limit the Project to EXISTING Light-Industry Zoning parameters, and deny the Developer's Conditional Use Permit (CUP22-00001), and Variance (V22-00001) requests.

*Please LIMIT the Developers to an industrial distribution and storage facility that does NOT exceed 50,000 square-feet and NO MORE than 6 heavy trucks on the premises at one time.

*Please require that the 6 heavy truck terminals AND any affiliated heavy equipment use, traffic, and parking to be LIMITED TO THE SOUTH SIDE OF THE BUILDING to minimize noise.

IN CONCLUSION:

Please deny the developer's

DEVELOPMENT PLAN (D22-00001)

CONDITIONAL USE PERMIT (CUP22-00001)

VARIANCE (V22-00001)

and require them to adhere to existing Zoning regulations, with all truck terminals, truck parking AND affiliated equipment use, traffic and parking limited to the South side of the building.

Thank you!
C. McConnell

Robert Dmohowski

From: Clayton Pence <shaggy79cjp@yahoo.com>
Sent: Monday, January 27, 2025 6:38 PM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; Zeb Navarro; Eric Joyce; Jimmy Figueroa; City Manager
Subject: Eddie Jones Opposition

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To whom it may concern,

My name is Clayton Pence and I am writing you on behalf of my family of four (plus our incredible dog) in opposition of the Eddie Jones Project. I will start off by stating that I have been a resident of Oceanside since 2004, and have owned my home off Benet since 2008. That being said I have seen a tremendous growth and improvement in the city that seems to be well planned out to help increase allure to our incredible city, to promote a safer and more friendly environment for those who live within or visit our fine city, to increase revenue in a way that matches our city's future goals, and done with a mindset that will do nothing but bolster and improve what we have already become. I have seen all of this done with great success and precision and am proud to see what the future of Oceanside has to hold.

That being said, I see the Eddie Jones Project veer completely from any progress our city has made, not holding any of our city's values and goals within its mindset, and does nothing but put the city's residents as an afterthought of its future. This is seen by forgoing any thought to how this project will affect any one person that travels along HWY 76, because there WILL be a significant change in traffic patterns created by the Big Rigs traveling, merging, and taking up an already well traveled thoroughfare; this does not even take into account the traffic of the employees that would be traveling to fill many of the **593** parking spaces! Along with this, the impact that this project will have on any lure to travelers wanting to visit the up and coming Wave Park is immeasurable. The surfing and outdoor community is 100% environmentally focused. What person will want to spend their hard earned dollars to travel to a surfing destination just to have a warehouse facility being within eyeshot, earshot, as well as having to breath the contaminates while 'trying' to enjoy their outdoor excursion? I would venture to say very few. This project also snubs those that are already utilizing the bike path that runs along the San Luis Rey, deterring their health benefits because of the environmental impacts this project will have.

This project is an abhorrent use of the land that is already classified to maintain a facility 10% the size and truck capacity of Eddie Jones. In past years Oceanside has shot down similar projects that were dwarfed in size by Eddie Jones because we knew it was not a good fit for our city and our citizens. Knowing all of this we ask you to not grant the Conditional Use Permit for this project. Don't grant it for the families within the direct boundaries of the project, those that have heath issues or who are trying to utilize this area to enjoy the outdoor extracurriculars, for any of those who travel along the Hwy 76 for any reason, to maintain a clear vision for Oceanside's future, but mainly don't grant it because you know it is not the right thing to do for Oceanside and it's current zoning parameters.

Thank you for your time, and I thank you in advance for doing the right thing!

-Clayton Pence

Robert Dmohowski

From: Keri Cleeremans <keri@elliptigo.com>
Sent: Monday, January 27, 2025 3:54 PM
To: Robert Dmohowski; Peter Weiss; Zeb Navarro; esanche@oceansideca.org; Eric Joyce
Cc: OceansideSpeaksOut@gmail.com; 4dees@cox.net
Subject: Eddy Jones Project-deny--please

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello Oceanside City Representatives,

I am an 18 year resident of Oceanside and have been using the 76 to commute to work for those years. You are planning to add more housing complexes, a hotel, possible a wave park (although, I haven't heard if this was actually passed or changed), as well as another complex off Douglas and the 76.

The 76 CANNOT handle this much traffic. My commute takes me from Rancho Del Oro to the 5 freeway and just driving to the 5 can take me north of 30 minutes. (it's not even a full 6 miles from the 5 to Rancho del Oro) Taking surface streets is not practical, especially during school days. They can't handle any more traffic than the 76 can. And you opened a stadium which funnels in more traffic during event days.

Traffic and parking are the top issues with this City because the Planning Commission keeps adding and taking away open space, squeezing everyone in on every available scratch of turf and NOT providing the required infrastructure to support all of this money making decisions. Don't let the fires of LA happen to Oceanside. Please set up the proper infrastructure to be able to assist residents in an emergency.

Lets be honest. You do it for the money. But the Eddy Jones project isn't going to make the City much money. This was reviewed early on. This project makes zero sense for the city to allow. More traffic. More noise. More pollution. More complaints. Not enough money in taxes and licenses.

On that note-

It's been super depressing to see open space disappear. There are so many vacant storefronts in the new buildings approved. And, I heard the City now wants to remove the Regal theater downtown to add yet more housing and storefronts (more *vacant* storefronts).

When do you plan on creating open space, fields, dog parks, pickleball courts, picnic areas, schools? When do you plan on siding with your residents instead of planning against them?

I vote **NO** on this project. NO on the condos going in on Douglas and NO on further HOUSING/COMMERCIAL lots until you GIVE BACK to your community! **GIVE US A DOG PARK!!!!**

No on Eddy Jones.

Keri Cleeremans
92057 Resident

Vare Keck

The Eddie Jones project will be before you on 10 Feb for your *Pg 1 of 3*

consideration.

I and many of those present here today would rather have something entirely different built but you might feel that is impractical.

This project will require two Conditional Use Permits.

One is required for exceeding the 50,000 sq ft structures the other for exceeding the number of large trucks.

Both of these requests exceed the current code allowances, I would like you to hold to those allowances.

I remind you that CUP approvals are at your DISCRETION.

Just because the Planning Department recommends it, you do not have to approved it

Should this commission approve the project I would like to see a set of CC&Rs established that would put heavy restrictions and conditions to counter the negative impacts on our community.

restrict the hours of operations – reducing the impact of light, noise and vibrations during the hours of darkness

delineate all delivery truck routes - restricting the trucks utilizing Benet rd north of the project

Have the owner and/or tenants file a facility management plan Pg 203

that shall be reviewed by the public prior to operations and be negotiated in a transparent public fashion

included in that management plan shall be procedures that allow, in case of fire or other emergency situations emergency vehicles unfettered access and to allow for the speedy evacuation of all the residents north of the project

should any future modifications to the CUP be considered that all of the Airport community, including Prince of the Peace Abby, be formally notified via registered correspondence prior to the commencement of that request to the city.

This project is CLEARLY not in keeping with the city's character, quality, or beach town atmosphere.

This project is just too big and generates way too many negative impacts that will have a forever adverse impact on our community

The future of our community's health, welfare, quality of life and values rest with you.

Our quality of life includes the ability to easily move about the community – this project will seriously impact our commute

times if you use the 76 Hwy. Less time with the family,

-Pg 3 of 3

or time to enjoy our down town and recreational amenities.

Our air quality is already among the worst within north county

and will only get worse.

Think of our community and it's future.

Dave Keck
Asksd

Hello Planning Commissioners-

Carol McConnell

1 of 2
Pages

Thank you for your time, attention and your volunteer efforts to make the City of Oceanside a wonderful place to live.

Structure of EDDIE JONES WAREHOUSE OWNERSHIP:

10% = *RAF Pacifica Group - local developer, Encinitas, CA*

AND

90% = *The Carlyle Group - multinational private equity, alternative asset management & financial services corporation, Washington DC*

This parcel of land adjacent to the municipal airport is zoned for Light-Industry. It's been that way for decades and we have NO DESIRE to contest that zoning designation.

But what has evolved around that parcel during those decades is a number of outdoor recreational activities that draw both residents and tourists to the area.

As you know, the San Luis Rey River Trail, popular with families and surfers peddling to the beach, runs immediately ADJACENT to this Light-Industry parcel.

The Prince Memorial Skate Park, used by children and teens on a daily basis, and the Go Jump parachute adventure company that draws tourists to Oceanside from all over S.Cal.... Are all adjacent to this Light-Industry parcel.

And, let's not forget OceanKamp, a project coming soon.... that you know more about than I do.

Because of all these outdoor activities, and the potential impact on the young lungs of children in their prime years of development, I would like us to focus on the AIR POLLUTION impact of this project. A variable, that if we get this wrong, will have a domino effect on Oceanside's tourism today, and the health of our children tomorrow.

The OceansideSpeaksOut Community group had a GoFundMe Campaign last year to hire professionals to help us make sense of the Draft EIR. We had 132 people donate to this GoFundMe.

We hired Ray Kapahi, a professional Environmental Permitting Specialist. In Mr Kapahi's December 2023 Technical Memorandum, he pointed out several air pollution issues in the Draft EIR, NONE of which were addressed in the Final EIR. Page 3
addresses
provided
FINAL

Because air pollution is such an important variable, it's concerning that the Final EIR does NOT provide an updated analysis on the alternate 56 heavy truck terminal multi-building configuration now proposed by the builder.

AND, there is no evidence confirming that these air pollution issues comply with CEQA.

Examples of air pollution issues previously identified in the Draft EIR, that were clearly outlined in the December, 2023 Technical Memorandum by Ray Kapahi, Environmental Permitting Specialists:

- Inaccurately relies upon an OLD version of the CalEEMod Emissions model to address the Project's risk to the climate.

- Inaccurately assumes that Project-specific air pollution impacts are insignificant; then illogically states therefore, that the related cumulative impacts are insignificant.

- Fails to include the cumulative impacts of emissions adjacent to the Oceanside Municipal Airport.

- The Project's EPS Technical Report "found that emissions of NOx, PM10 and PM2.5 would violate the ambient air quality standards by a substantial margin. (EPS Technical Report at 4.)

As demonstrated in the report, the spatial distribution of pollutant concentration demonstrates that the Project exceeds the applicable air quality standards, and in no way complies with the City of Oceanside's own Climate Action Plan goals.

* Without a comprehensive analysis, proving that the alternate 56 heavy truck terminal multi-building configuration complies with CEQA Guidelines and the City's Climate Action Plan, the Planning Commission should deny the CONDITIONAL USE PERMIT (CUP22-00001), and VARIANCE (V22-00001)

The Developers, Pacifica & Carlyle Groups, should be asked to re-state their DEVELOPMENT PLAN (D22-00001) with a building design that adheres to the property's existing Light-Industry Zoning parameters which only allows for 6 heavy truck terminals, and an accompanying updated air pollution analysis.

THANK YOU FOR YOUR TIME AND CONSIDERATION.

Opposed to the proposed Eddie Jones warehouse project. 01-27-2025

As residents of the City of Oceanside, we view you, our Planning Commission, as the Gatekeepers of the "Vision for Oceanside's Future." Just because a proposed development has checked all the required boxes with the Planning Department does not mean that the proposed development should get summarily approved.

The first significant question that must be asked and answered is if a particular proposed development serves the best interests of the City of Oceanside and its residents. The proposed Eddie Jones warehouse project fails on so many levels to fulfill this requirement.

The second significant question that must be asked and answered is if the proposed project is consistent with and enhances the "Vision for Oceanside's Future." Again, the proposed Eddie Jones warehouse project fails on so many levels to fulfill this requirement.

Our understanding is that the "Vision for Oceanside's Future" is to have Oceanside become a desired vacation destination and to promote and embrace tourism as an economic engine for our City. In our airport area alone, we already have the recreational Skate Park and the Go Jump skydiving opportunity in place. And Ocean Kamp is currently under construction. These tourism and recreational activities will be adversely affected by the proposed Eddie Jones warehouse project due to increases in heavy truck traffic, constant noise, and significant air pollution.

The developer's EIR puts forth dubious conclusions that the negative effects of their project are "less than significant." This is ludicrous!

Even the most casual observer can see that the negative effects of this proposed project are quite significant, pervasive and severe. If a truly honest EIR were written, it would necessarily arrive at the obvious conclusion that this project should not be built as currently proposed because of the many significant adverse effects the project would impose on our City, and both the nearby residential and commercial communities.

Don't be misled by the developer's comments about the so-called positive economics this project will produce and how many jobs will be provided. Such statements are self-serving, but more importantly are truly just "red herrings." ANY future development on this site will produce comparable economic value and jobs.

Regarding jobs, the question is what type of jobs do we want. Oceanside does not need a massive warehouse facility which will primarily provide only minimum wage jobs which cannot provide the future employees with sufficient income to reside within our City or even the nearby surrounding communities. The “Vision for Oceanside’s Future” would be that jobs created in our City would be higher paying jobs which would create a higher economic value to Oceanside.

In summary, the proposed Eddie Jones warehouse project as currently proposed comes with so many substantial and serious negative effects that should dictate that this project NOT be approved.

I ask you to vote NO and not approve this project.

Edwin Jenkins
543 Blue Jay Court
Oceanside, CA 92058

Alan Waite

January 15, 2025

To Whom it May Concern

Subject: Eddie Jones Project ("EJP") Fire Safety

Over the past couple of years & during one of public follow-up presentations, a statement made by the developer (RPG) indicated their complete misunderstanding of our ("airport") community's fire safety concern. RPG stated, "in the event of an external fire, building occupants would not evacuate, but instead would shelter in place". RPG's response CLEARLY missed our community's primary concern, which is EGRESS OF THE ADJACENT COMMUNITIES AND INGRESS OF FIRE & SAFETY PERSONNEL, and NOT for RPG building occupants (who apparently, according to the developer, would be inherently safe due to the building's concrete construction?).

Recent devastating fires in Los Angeles, as well as our regions 2014 "Lilac fire", highlight our communities primary concern for EJP's construction & existence in Oceanside. And yet another fire was much closer to my home in the California Brisas neighborhood just a few blocks due north of the EJP site. This fire occurred 2 1/2 years ago, & I witnessed in horror with my neighbors as the raging inferno heading towards our homes, while we awaited evacuation orders*. Fortunately, in this last instance, and due to incredibly heroic efforts from several fire departments, not one life or structure was lost...I wish the same could be said for the LA communities.

In summary, with only 2 roads (Benet & Fousat) into & out of our neighborhood, safety during the above situations is of paramount importance. And I can personally attest to the difficulties in getting residents out, and fire & safety personnel into our neighborhood which was anything but smooth; As the fire raged for over 1 1/2 hours, my neighbors directly across the street were forced to walk home with their luggage in tow, having been dropped off by an Uber driver 6 blocks away at Alex Skate Park, because the 2 roads into our neighborhood were cordoned off by police to allow fire & safety vehicles in. Indeed, my next-door neighbor was also blocked from leaving our neighborhood for the same reason (many people who left shortly after the fire began were unable to return home for over 5 hours because of the congestion).

>>> There's simply NO LIMIT to our community's frustration in trying to understand why the Eddie Jones Project. IN ANY OF THEIR FOUR PROPOSED CONFIGURATIONS should be allowed to proceed! <<<

**PLEASE do not vote to allow or approve the Eddie Jones Project:
Oceanside does NOT need another warehouse,
nor does Oceanside need to further endanger our citizens!**

Thank you for your kind consideration,
Sincerely,

Alan & Robin Waite
555 Long Crest Drive
Oceanside, CA 92058
(858) 230-4711

*Photos & video footage is available for City planners and/or City council review].

Ann Marshall

There are only 2 small roads to enter and exit the Airport community: Foussat and Benet both are off the 76. This community has well over 3600 residents.

An emergency response team would need to access the community via these 2 roads and each road has only 2 lanes that cross a bridge over the San Luis Rey River.

My concern is if the road is blocked due to traffic congestion or an accident, how will they be able to get through?

Imagine a resident requiring the services of a Paramedic and was called to the top of Shadow Tree Drive in order to get to the resident in distress and in a timely manner – this could be very difficult and maybe even fatal.

In June 2022 there was a brush fire called the Wala Fire. It grew to 75 acres and prompted evacuations. The traffic congestion was a concern. This project plans to hire 500 employees on top of all the semi-trucks. Imagine 3600 residents needing to evacuate along with the 500 employees and all the trucks needing to exit on a 2-lane road. This is a concern.

I have seen trucks lost in the community. It just takes one missed turn. A common cause of truck drivers getting lost is driver fatigue or distracted driving such as using their phones while driving.

In 2022 within the community, a driver fatally hit a woman on the sidewalk. Accidents do happen. A semi-truck in the community is an accident just waiting to happen.

This is a quiet family-friendly neighborhood where people walk their dogs, babies in strollers, and children ride their bikes to the local beach.

Trucks are vital to our economy.

Unfortunately, they also present a collision risk due to their large size and the fact that truck drivers often work long hours and are susceptible to driver fatigue.

In 2021 there were over half a million large truck accidents across the US and California presents the 2nd highest risk with 364 deadly truck collisions.

And 27% of these accidents were intersection related. Both entrances to the Airport Community have intersections: Foussat & the 76 and Benet & the 76. And Foussat & 76 will have enough traffic with the upcoming OceanKamp.

Please allow the emergency response team easy and accessible entrance should their services be needed.

Thank you

Maybe: My name is Ann Marshall. I have lived in the Airport Community since 2011, and I have lived in Oceanside for 24 years

1 Minute to Planning Department

Ladies and gentlemen,

We're here today to talk about something very important—the Eddie Jones Warehouse, Manufacturing, and Distribution Facility. Now, I've seen a lot of projects, some good, some not so good. But let me tell you, folks, this one is a **disaster**. An absolute disaster for the Airport Community.

We're talking about **1,200 homes, nearly 4,800 great Americans**, who are going to be hammered by this. **Constant truck traffic, deafening noise, toxic pollution**, and traffic congestion that will cripple our roads. It's unacceptable.

For over two years, this community has been fighting. **They've gathered over 5,000 signatures**. Thousands have shown their support. But the city? **We don't know if you will stand with the people or against them.**

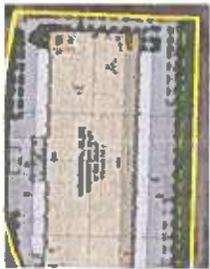
Oceanside Planning Department, the people are watching. You have a choice: protect the health, safety, and quality of life of your citizens, or cave to a project that will ruin this community. **Do the right thing. Reject this disaster.**

Thank you.



Mike Tenhover

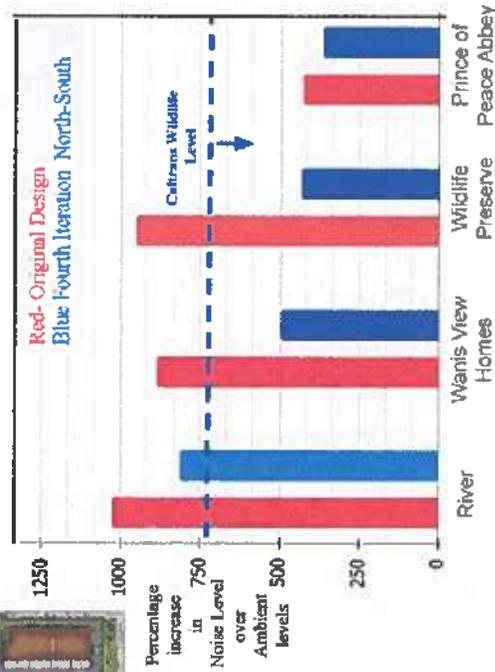
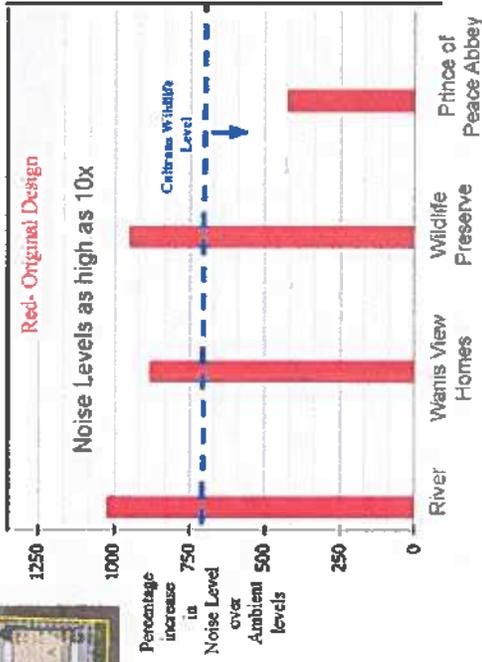
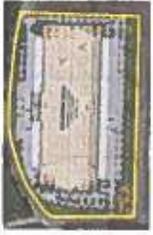
Noise Analysis: Fourth Iteration Design Eddie Jones Warehouse Project Michael Tenhover (Detailed report filed Jan 24th with OCN Planning)



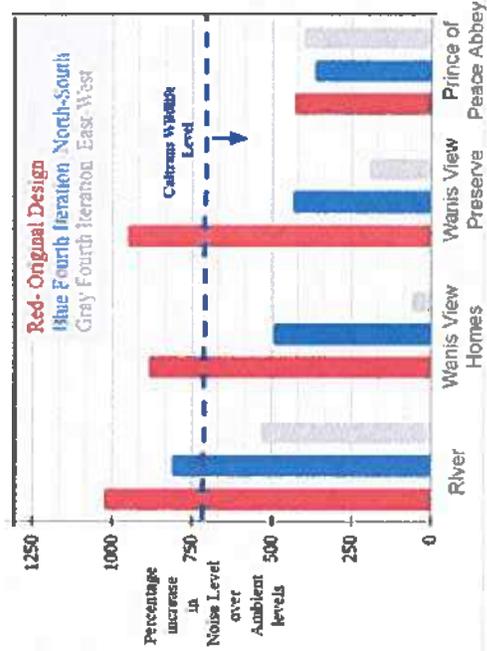
Noise sources around Perimeter



Noise sources between Buildings



East-West Version of Layout (Gray Bar)



Good evening and thank you for the opportunity to speak. I'd like to address the City's vision and goals.

One facet of the Economic Development plan is to market Oceanside as a tourism destination. The Economic Development plan is one objective of the city's General Plan. And the General Plan's purpose "is to establish a long-range vision for the community and to plan for important community issues, such as new growth, housing needs, public services, and environmental protection."

Tourism is mentioned on several different places within the City of Oceanside websites, links, and plans.

While the proposed Eddie Jones project may fulfill the economic goal of new jobs and revenue for the city, it does not fulfill the other goals of tourism, environment protection, and it certainly does not improve the lives of the Oceanside residents in the nearby neighborhoods, nor arguably anyone who traverses the 76/5 corridor.

While economic development, which includes businesses that bring in jobs and revenue, it also includes businesses that are aligned with the vision of Oceanside – tourism and a place for residents to enjoy.

I moved to Carlsbad in 1986 as a child. My parents moved to Oceanside in 1995 and I became a homeowner in Oceanside in 2011. Over the decades I have watched Oceanside transform into a premier destination – to live in and as a tourist destination. As a tourist destination, most recently we have seen Conde Nast not only feature an article in April 2022 to all things wonderful in Oceanside, the 2023 annual reader's choice awards issue noted the Mission Pacific Hotel as the top hotel in San Diego. A luxury hotel that even brought a woman who knows hotels, Kathy Hilton, to stay and highlight in RHOBH last year. Again, bringing more positive attention to our wonderful city.

The proposed Eddie Jones warehouse does not align with this vision. In fact, it is in opposition to the majority of the city's vision.

It does not benefit the residents for a place to enjoy, in fact it directly negatively affects the residents as the others have already mentioned (air pollution, noise pollution, increased traffic congestion).

I urge you to deny the proposed Eddie Jones project.

Gretchen Gary

Dear Rob Dmohowsky, members of the Planning Commission and City Clerk,

I am a resident of the Wanis View residential community that sits directly across the San Luis Rey River from the Eddie Jones project. My home sits at the end of the Tishmal cul de sac – only .15 miles from the property, that's less than 800 feet. My home is one of the closest homes to the Eddie Jones land with 180 degree views of the entire project from my master bedroom, bathroom, balcony and home office downstairs. The decision you make on this project will have a profound impact on my daily life.... Not only because I live there but also in that I work from home. I am there all day, every day and with such close proximity, I could hear and see everything. In fact, if I was to yell from my backyard, anyone on the Eddie Jones land could easily hear me.

I am asking the Planning Commission to reject this project – for the health of myself, my family, and the entire surrounding community.

I knew the land near the airport was zoned as light-commercial when I purchased, and moved to this home when the previous buildings were in operation. I would never have expected when I purchased this home that there would be a proposal for a mega industrial facility proposed on that land. I would argue that since the City approved the residential development of several thousand homes off Benet Road, this is no longer an industrial area as it once was back in the 1960s when the Eddie Jones land was first zoned as light-industrial. The presence of the Prince Skate Park further establishes this area as more of a family-oriented recreational space than an industrial park now. And there are many more than my children who are at risk of health impacts from expected air pollution from 57+ semi trucks within several hundred feet, risks that include childhood asthma, heart disease, cognitive decline and risk of stroke according to [a report from the Environmental Defense Fund](#) published in 2023. Hundreds of children gather at the Prince Skate Park every day and engage in strenuous outdoor recreational activities. Both ozone and fine particulate matter, formed from chemicals in diesel exhaust are known to be harmful to human health, [according to the EPA](#), and would have concentrated levels in the immediate area where these children are present day and night. Please don't approve a project that will bring dangerous levels of these chemicals to the area. I live only 0.15 mile from the project as the FEIR states – less than 800 feet!

Lastly, my concerns about noise pollution from the engines and backup alarms of 57 semi trucks, assisting forklifts and machinery possibly operating 24 hours a day so close to my bedroom literally keeps me up at night already. Per the [American Health Association](#), "Chronic noise, even at low levels, can cause annoyance, sleep disruption, and stress that contribute to cardiovascular disease, cerebrovascular disease, metabolic disturbances, exacerbation of psychological disorders, and premature mortality. Noise interferes with cognition and learning, contributes to behavior problems, and reduces achievement and

productivity.” I can hear the backup alarms of the trucks at the stone quarry off HWY 76 that start at 5am sometimes and that facility is at least one mile from my home. The valley of the San Luis Rey river amplifies noise in the quiet hours between sunset and sunrise, as evidenced in a video I sent to Rob Dmohowski of a single truck backup alarm at the adjacent airport when they were doing repairs on the runway in October 2022.

I reviewed the final EIR and saw that my name listed as someone who submitted feedback on the draft EIR, however I didn't see any of my concerns addressed. I would consider my above concerns on my home's value, and more importantly the impacts of air and noise pollution on the health of my family and surrounding community to be a significant environmental concern that should have been addressed in the FEIR. I am not impressed with the tactic of first presenting an obscene design for 114 truck bays and then saying that they addressed the community's concerns with a 50% reduction to 57 truck bays. That was not a negotiation, that was psychological manipulation to make it look like they were making changes to accommodate the community when 57 truck bays is still 4X the number of truck bays that was proposed in the Amazon project that was unanimously rejected by the City Council. Given the shift towards residential and recreational land uses in this area, I would argue that any development should be forced to comply with the existing zoning, which is already uncharacteristic of an area where homes are built so close by.

In closing, I am asking the Planning Commission to deny this CUP application and proposal. It is not in the best interest of the community and only selfishly benefits the developers and their financial pursuits.

Thank you for your time and consideration

Gretchen Gary

471 Tishmal Ct

760-560-6103

Planning Commission Mtg

Jan 27, 2025

→ Non item speakers

Dave Keck

Alyce Budde

*

Carol M^cConnell

Ed Jenkins

John Bucalo

*

Alan Waite

Ann Marshall

Geno O'Neill

Michael Tenbover

Windy Bravo

*

Neil Hancock

Kimberly Johnson

Gretchen Gary

Jimmy Knott

*

* No prepared speech

* Prepared speech provided prior

Robert Dmohowski

From: ALISON TOPALOGLU <atopaloglu@cox.net>
Sent: Tuesday, January 28, 2025 9:06 AM
To: clerk@oceasideca.org; rdmhowski@oceasideca.org; Planning-Planning Commission; City Manager; City Clerk
Cc: oceansidespeaksout@gmail.com
Subject: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Oceanside Elected Officials:

As a resident of the Airport Community, we are opposed to the Eddie Jones Warehouse project that will be in our backyard. The Airport Community will see a significant increase in air, noise and light pollution from this development.

The addition of semi traffic which does not exist on the 76 corridor today will put additional strain on our roads which are already in need of repair and constant maintenance. Who will be responsible for these additional costs? How will this be managed?

The recent fires in Los Angeles should be a powerful wake-up call on the effects this project may have if a natural disaster happens in Oceanside. As you already know the 76 is the only east/west route to access the I-5 and/or I-15 freeway in North County. What plans does the City have to develop new plans/implementing them for updated evacuation plans for the residents?

This project will have a long-term effect on our community. Has this project been objectivity evaluated? Who is benefiting from this project? The city council members/planners are representatives of this community, and community interests need to be a top priority. This project does not represent what our community desires. A stop needs to be put on this project to protect the residents of this city which you are elected to represent.

Robert Dmohowski

From: Bethany Storz <bethany.jodoin@gmail.com>
Sent: Tuesday, January 28, 2025 6:16 PM
To: Planning-Planning Commission
Subject: Re: Eddie Jones Warehouse Development Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To the Oceanside Planning Commission,

This email is in regards to the Eddie Jones Warehouse Development project.

I live in the Wanis View Estates neighborhood behind the Oceanside airport and I have serious concerns regarding the Eddie Jones Warehouse Development project.

I am asking the city to please adhere to the light industry zoning regulation of 6 heavy truck terminals. Any more than that will cause traffic problems, excessive pollution in this family friendly neighborhood, and noise issues day and night. With two small children the excessive pollution coming from diesel trucks puts our children at risk for childhood asthma, cognitive decline, heart disease, and a risk of stroke.

On February 10th we ask that you please vote NO to the developers request for 56-114 heavy truck terminals. Thank you for your time.

Concerned Oceanside resident,

Bethany Storz
3140 Toopal Dr
Oceanside, CA 92058
617-875-1139

Robert Dmohowski

From: Dana Nestor <dana_doo22@yahoo.com>
Sent: Tuesday, January 28, 2025 6:37 PM
To: Planning-Planning Commission
Subject: Eddie jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

To the Oceanside Planning Commission,

This email is in regards to the Eddie Jones Warehouse Development project.

I live in the Wanis View Estates neighborhood behind the Oceanside airport and I have serious concerns regarding the Eddie Jones Warehouse Development project.

I am asking the city to please adhere to the light industry zoning regulation of 6 heavy truck terminals. Any more than that will cause traffic problems, excessive pollution in this family friendly neighborhood, and noise issues day and night. With two small children the excessive pollution coming from diesel trucks puts our children at risk for childhood asthma, cognitive decline, heart disease, and a risk of stroke.

On February 10th we ask that you please vote NO to the developers request for 56-114 heavy truck terminals. Thank you for your time.

Concerned Oceanside resident,
Dana Martinez

Robert Dmohowski

From: Erin K <erin.kleefisch1@gmail.com>
Sent: Tuesday, January 28, 2025 6:47 PM
To: City Manager; City Clerk; City Council; Robert Dmohowski; Zeb Navarro
Subject: No Eddie Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

This is a concerned resident of Oceanside writing to you about the Eddie Jones Warehouse development on the 76.

Pease adhere to your own existing zoning limitation: Light Industry Zoning Regulation Limit is 6 heavy truck terminals. On Feb 10, my neighbors and I will ask that the City Planning Commission votes "No" to the Developer's request for 56-114 heavy truck terminals. That size project will destroy Oceanside and my neighborhood off Benet road.

V/r,
Erin Kleefisch

Robert Dmohowski

From: Jane Hodges <jhodges501@hotmail.com>
Sent: Tuesday, January 28, 2025 10:45 AM
To: Planning-Planning Commission
Subject: eddie jones warehouse project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Planning Commissioners:

John Malik
Louise Balma
Kevin Dodds
Tom Rosales
Thomas Morrissey
Dennis A. Anthony

Re: Eddie Jones Warehouse project

Dear Planning Commission members,

As a 40 year resident of Oceanside, I strongly urge you NOT to approve the Eddie Jones warehouse project slated for the property adjacent to Highway 76/ Mission Ave. in Oceanside. I have written previously regarding the negative impact this project will impose upon our community. Not only will our traffic flow be greatly impacted on the city streets, it will effect the response times for emergency vehicles in the area, traffic for nearby public and private schools, bus route scheduling and access to safe bus route terminals. Traffic is one area of concern, but also the noise level will increase tremendously as this project is a 24 hour business utilizing large trucks coming and going throughout the day and night. If the safety and well-being of the residents of Oceanside are your main concern, then a NO vote on this project would assure us that you truly are looking out for the welfare of the citizens rather than whatever income this atrocity will bring.

Jane Hodges
380 Islander St.
Oceanside,

Robert Dmohowski

From: Julie Matye <julie.matye@gmail.com>
Sent: Tuesday, January 28, 2025 7:41 PM
To: Planning-Planning Commission
Subject: Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

I hope this message finds you well. I am writing to you today in response to the Eddie Jones Project proposal.

My family and I currently live on Shadow Tree Dr in a housing development that would be impacted by this warehouse development. We moved here in 2020 right before the birth of our daughter. Within her first year of life we had to evacuate due to a fire in the canyon behind our house. Needless to say, that was a very scary experience and I am so glad that the police and other emergency responders were able to assist us with evacuations and access our neighborhood in a timely manner.

As we have seen these types of emergencies can get out of control in an instant. The recent fires in Maui and LA have shown us the very stark and tragic reminder of what happens when there is poor planning and the impact it has on entire communities and emergency responders in a natural disaster.

Our request is simple. We ask that you please consider the risks and dangers this project will ultimately put the community of families in and keep the existing zoning laws of 6 heavy trucks rather than the 56-114 heavy truck terminals the developer is requesting.

Thank you for everything you do.

Sincerely,
Julie and Tom

Sent from my iPhone

Robert Dmohowski

From: Karen McCallick <ksmccallick@gmail.com>
Sent: Tuesday, January 28, 2025 9:54 PM
To: Planning-Planning Commission
Subject: Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Planning Commissioners:

I am writing as a concerned resident of the Airport community. Our community's request is a simple one. We ask that you please adhere to the existing light industry zoning regulation limitation of **6** (six) heavy truck terminals.

On February 10, we ask the City Planning Commission to please vote "**NO**" on the developer's request for **56-114** heavy truck terminals.

Sincerely,

Karen McCallick
3425 Spindrift Ct, Oceanside, CA 92058

Robert Dmohowski

From: Sally Smith <salmomma@gmail.com>
Sent: Tuesday, January 28, 2025 7:35 PM
To: Planning-Planning Commission
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

To the Oceanside Planning Commission, This email is in regards to the Eddie Jones Warehouse Development project. I live in the Wanis View Estates neighborhood behind the Oceanside airport and I have serious concerns regarding the Eddie Jones Warehouse Development project. I am asking the city to please adhere to the light industry zoning regulation of 6 heavy truck terminals. Any more than that will cause traffic problems, excessive pollution in this family friendly neighborhood, and noise issues day and night.

On February 10th we ask that you please vote NO to the developers request for 56-114 heavy truck terminals. Thank you for your time. Concerned Oceanside resident,

Sincerely,

Sally Smith Vega

Robert Dmohowski

From: Shelley Rendon <sarendon@gmail.com>
Sent: Tuesday, January 28, 2025 6:31 PM
To: Planning-Planning Commission
Subject: Feb 10 Vote NO on Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Our request is simple.

Please adhere to your own existing zoning limitation:
Light Industry Zoning Regulation Limit is 6 heavy truck 🚛 terminals.

On Feb 10, we ask that the City Planning Commission votes "No" to Developer's request for 56-114 heavy truck 🚛 terminals.

Thank you,
Shelley Rendon
Local Resident in Las Brisas HOA, Oceanside
818-219-6223

Robert Dmohowski

From: Andie's Asylum <andiesalazar@gmail.com>
Sent: Wednesday, January 29, 2025 9:29 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: RE: Eddie Jones Warehouse project

Warning: External Source

Hello Oceanside!

As I understand it, The developer of the Eddie Jones Warehouse Project has proposed three different building configurations for the land between the airport runway and the river. These configuration designs include 57 - 114 Diesel Truck docks.

Further, I've learned that the existing zoning for this land only allows 15-18 Diesel Truck docks. As a resident in the community of a home adjacent and upwind of this development, I humbly request that the developer be, at the very least, limited to the existing zoning regulation.

Ideally, you would consider shutting this whole idea down! The traffic is already an issue on the 76, and this project is set to be situated smack in between the only two ways in and out of that residential area.

As we've recently been horribly reminded of, wildfires can rip through an area like ours quickly and with hardly any warning. These trucks could even potentially spark an issue themselves, with the riverbed just feet away.

Seriously hoping you reconsider and with all my best regards,
Andrea Salazar
Concerned Oceanside Resident on Soyla Dr.

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Wednesday, January 29, 2025 8:55 AM
To: Planning-Planning Commission
Subject: Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

TO: Planning Commission
RE: Eddie Jones Warehouse Development

We urge you to please adhere to the *existing* zoning limitation: the Light Industry Zoning Regulation Limit *is 6 heavy truck terminals*

Thank you,
Willie & Ann Marshall
543 Shadow Tree Drive

Robert Dmohowski

From: golddrive588 <golddrive588@yahoo.com>
Sent: Wednesday, January 29, 2025 8:47 AM
To: Robert Dmohowski
Subject: Eddie Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Stop this project.

Sent from my T-Mobile 5G Device

Robert Dmohowski

From: Debbie Lake <dlake54@yahoo.com>
Sent: Wednesday, January 29, 2025 7:09 AM
To: Planning-Planning Commission
Cc: City Clerk
Subject: Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Planning Commission,

Please adhere to Oceansides own existing zoning limitation:

Light Industry Zoning Regulations- Limit is 6 heavy truck terminals ONLY

As a resident in this community it is very concerning to our health and safety to allow Eddie Jones development change the zoning to their liking. The traffic on hwy 76 is already at a max and this will bring even more.

Please do the right thing and vote NO on Feb 10, 2025. Do Not allow a zoning exception to this Developer.

Best regards,

Debbie Lake
3431 Southwood drive, 92058

Robert Dmohowski

From: Dane Thompson
Sent: Wednesday, January 29, 2025 12:51 PM
To: Robert Dmohowski
Subject: FW: Planning - General (Internal) #16466889 has been received

fyi

From: Oceanside <noreply@publicstuff.com>
Sent: Wednesday, January 29, 2025 10:59 AM
To: Dane Thompson <dthompson@oceansideca.org>
Subject: Planning - General (Internal) #16466889 has been received

Warning: External Source



Request Received

Planning - General (Internal)

Hi Dane Thompson,

Planning - General (Internal) Request #16466889 has been received.

This request was submitted by Diana Farris [Verified Official] at Jan 29, 2025, 10:56 AM PST through gov.publicstuff.com.

View this request here:

<https://gov.publicstuff.com/request/view/16466889>

Request Details

Description

From: Oceanside Website Team <webmaster@govaccess.org> Sent: Wednesday, January 29, 2025 10:53 AM To: *Oceanside Customer Care* <CustomerCare@oceansideca.org> Subject: Feedback for Oceanside, CA
You have received this feedback from Joshua Ong <joshuaong@sbcglobal.net> for the following page:
<https://www.ci.oceanside.ca.us/government/development->

[services/planning/ceqa/eddie-jones-warehouse-manufacturing-and-distribution-facility-project-eir](#) I am a resident of Wanis View estate and my house address is 3135 Toopal, which directly face the warehouse. I am opposing to the project due to traffic and noise concern. I have read the EIR on noise and traffic and found that the mitigation is less than optimal and there is no teeth to make sure compliance. Sincerely, Joshua Ong

Priority

NONE

Status

RECEIVED

Address

3135 Toopal Drive, Oceanside, CA, USA

Due Date

Thanks!

Oceanside



About | Support

Robert Dmohowski

From: JULIETTE LE FEVRE <jsgleason@comcast.net>
Sent: Wednesday, January 29, 2025 6:24 PM
To: Planning-Planning Commission
Subject: Vote Against Eddy Jones Truck Facility ...

Warning: External Source

This email is to communicate that I am strongly against the Eddy Jones Warehouse Facility.

My understanding is that the facility goes against the existing zoning limitations by bringing in a truck terminal that will have over 100 trucks.

YUCK ... I don't want to inhale the stinky truck pollution or hear the noise from the trucks and all the traffic it will create to name a few things.

What will be the impact on the green belt behind my house and the entire ecosystem ... wildlife endangerment etc.

I hope you will vote against this facility ... it doesn't belong here less than 2 miles to the beach and next to our neighborhood. I never moved into this neighborhood to be next to such a facility ... just awful.

Juliette LeFevre
3239 Toopal Drive
Oceanside

Robert Dmohowski

From: justin sager <justin.sager@live.com>
Sent: Wednesday, January 29, 2025 2:32 PM
To: Planning-Planning Commission
Subject: Eddie Jones

Importance: High

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear, Oceanside Planning Officials.

As a resident of Oceanside, I respectfully implore all of you to adhere to the existing zoning of the Eddie Jones site as it currently stands without giving the developers ANY special consideration for up-zoning. As stated in the letter I had previously issued to all council members, this request by the developers is nothing more than an attempt to line their pockets at the expense of Oceanside residents. They are attempting to market this project as a distribution and manufacturing facility to give the impression that it would create jobs. This is false advertising as their proposal is for a distribution facility which creates virtually no new jobs but creates tremendous amounts of traffic, pollution and degradation of public infrastructure. All you need to do is look at the experience of Riverside, Fontana and other areas within the Inland Empire that have been subject to this sad type of development and all of its incumbent costs and consequences.

If our city were to grant the developers their request, it would be nothing short of a major CASH GIFT to the moneyed interests who are disconnected to our community and paid for by the City of Oceanside and its residents. This would be a major stain on our community and a disgraceful dereliction of duty committed by those in charge of promoting responsible development that should be in the best long-term interests of our community.

Again, I urge the planning commission to do the right thing by the residents and the city by rejecting this cash grab attempt.

Justin Sager
Principal of JRS Development Management, LLC
Oceanside Resident
3369 Ricewood Drive, Oceanside California 92058
858-876-5988

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Robert Dmohowski

From: Ken Mccallick <ken.mccallick76@gmail.com>
Sent: Wednesday, January 29, 2025 3:08 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Eric Joyce; rwrobinson@oceanisdeca.org; Jimmy Figueroa; Peter Weiss
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Sir,

I am writing once again to express my vehement opposition to the proposed Eddie Jones Project for a truck depot.

I am not against all development and I would support a project that benefited the neighborhood and the city of Oceanside. My neighbors and I, who live in the neighborhood adjacent to this site, can see no redeeming value for the community in this project, The only partiers who will benefit are the developers. I had suggested a glamping trailer park to enhance the travel allure of the wave park.

I belong to **OSO, Oceanside Speaks Out:** Some concerns that I and my neighbors have about this project are:

1. Ability to exit the neighborhood in an emergency. The recent fires have demonstrated how crucial it is to be able to exit safely and quickly in case of a fire or other emergency. We had a fire in our neighborhood in the last 2 years that fortunately was quickly controlled. With the opening of the wavepark on Foussat, that exit will be much busier than it is now and residents of our neighborhood will have a much more difficult time exiting there.

The only other exit is the small 2 lane Benet road where it is already congested. If large semi-trucks are exiting and entering at that intersection, it will double the danger of not being able to exit the neighborhood in an emergency, and it will just make traffic even more congested than it is now on Hwy 76 in the area from the I-5 all the way to College Blvd.

Do we know how serious the threat of fuel inside the semi-trucks catching fire during an emergency is?

2, Multiple Fatalities at Benet and Hwy 76: There have been multiple fatalities and accidents at the Benet/76 intersection in the last 2 years, also accidents and injuries. The area is poorly marked and poorly lit at night. It is an area where many homeless people live in the river bottom and around the airport. The presence of large trucks coming and going will only escalate the danger.

3. Zoning is inappropriate and outdated: The site was zoned for its use DECADES AGO! This was before the neighborhood grew to its present density of residents and homes, some of which back right up to the site. The proposed project seeks an expansion of the original zoning and triples the amount of trucks originally permitted.

Also this site is next to the San Luis River Preserve. We don't know how this project will affect that precious natural resource with its pollution from exhaust, noise, and possibly fuels and chemicals. This is a dirty project and it doesn't belong next to a beautiful neighborhood and a nature preserve!

4. Traffic pollution and congestion: Traffic at rush hour times and on weekends is already congested in the valley. It takes sometimes 3-4 minutes just to exit from Foussat or Benet. The addition of large semi-trucks to the traffic in the airport area is only going to make traffic more intolerable. This is a REALLY BAD IDEA!

5. Uncertainty about the project: As I understand it, there is no real tenant for the site, so there is no way to know what the real effect on the area will be. We don't know for sure what the exact usage will be and we can't really determine the economic effects without that knowledge. Its benefit to Oceanside is speculative at best.

I am an Oceanside homeowner, taxpayer, voter, and 20 year resident of this city. My kids went to school here and our church and community are here.

I moved here to get away from the sprawl and mess of Orange County and L.A., having lived in both places. I love the diversity, the beauty, and the ambience of our special city. I'd like to keep it special, and this project adds nothing to the good things about Oceanside. Please listen to the voices of the residents who are most directly impacted by this awful idea, and support us in keeping our neighborhood an amazing place to live.

Sincerely,

E. Kenneth McCallick
760-622-9524

Robert Dmohowski

From: Young, Bill (William) <Bill.Young@commscope.com>
Sent: Thursday, January 30, 2025 11:14 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Vote 'No'

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Rob and the Planning Commission,

As a resident owner at 575 Big Sky drive, I am opposed to the proposal plan to rezone this project to facilitate more than 6 Heavy Trucks.

Please vote NO to Developers request for 56 to 114 heavy truck terminals and adhere to the existing Light Industry Zoning Regulation. The list of reasons is extensive including Traffic on Hwy 76, Delays to ENTER/Exit neighborhood, Public Safety Access, and Environmental concerns. This Airport neighborhood only has two access roads to enter, and the East Access (N Foussat Rd) will be limited with the extensive OceanKamp buildout- stated to bring in 700-housing units, a 300 room hotel and 134,000 SqFt of retail. Perhaps look at ALT locations on Ocean Ranch Blvd. In addition, I would recommend plans to increase traffic lanes on Hwy 76 or this area will be overwhelming.

Thank you for your Consideration.

Respectfully,

**Bill Young, BICSI RCDD, OSP, TECH
2025 SKILLS CHALLENGE JUDGE**

SR GLOBAL ACCOUNT MANAGER



SAN DIEGO, CALIFORNIA (PST)

Mobile: [1.858.413.6934](tel:1.858.413.6934)

Follow me on [LinkedIn](#)

MEMORANDUM

TO: City of Oceanside Planning Commission, City Council,
City Manager and City Clerk

FROM: Joanne Pilgrim

DATE: January 30, 2025

SUBJECT: Notice of Planning Commission Public Hearing
Consideration of a Development Plan, Conditional Use Permit and Variance
Site: Eddie Jones

As indicated in previous correspondence, I am a homeowner in the Wanis View Estates community within 1,500 feet of the exterior boundary of the above-entitled site. I received the City of Oceanside's (City) Notice of Planning Commission Public Hearing on February 10, 2025 for consideration of changes to the zoning and land use requirements regarding the construction of a warehouse, manufacturing and office facility at 250 Eddie Jones Way.

Please vote "NO" to the developer's request for approval of a Development Plan, Conditional Use Permit and Variance at the Eddie Jones site as noted below:

1. Violations of Limited Industrial Zoning Ordinance and Light Industrial Land Use.
The City confirms the project has a land use designation of Light Industrial and a Zoning Designation of Limited Industrial. However, instead of following these regulations, the City proposes to skirt the zoning requirements which limit "an industrial facility for distribution and storage use" to 50,000 square feet and "to allow truck terminals with more than six heavy trucks on the premises at one time." The developer proposes to complete a project of 497,822 square feet with a total of at least 56 truck bays to be divided among four buildings and 593 parking stalls outside. This is a strict violation of the zoning regulations and will impact the land use of the site exponentially. There are many environmentally-pleasing ways to develop the site which will avoid the problems currently presented and complement the area designated.
2. Heavy Traffic Blockage.
The proposed access to the site is from both Benet and Alex Roads. The City is well aware these access points are very narrow, two-lane roads and cannot handle the heavy trucks proposed. Our community is already challenged with the current traffic conditions on Highway 76 because of recent high-density housing near the Mission San Luis Rey, not to mention the Ocean Kamp project on Foussat Road which is the other access point into our community. With any additional traffic – especially heavy trucks – the 1,200 homeowners above the site will be completely blocked to Highway 76.
3. Fire Risk and Other Emergencies.
The City is well aware of the fire danger in southern California. San Diego is projected to experience wildfire endangerment to be of equal or greater severity than in recent years. We've already had a brush fire within the last year, and with the current predictions, if this

project is confirmed, not only lives will be lost, but, also, all our homes due to the lack of ingress and egress to the communities.

4. Noise and Light, Environmental Considerations.

The proposed project will exceed any normal residential living conditions of noise and lights. Because of its size and commercial components, this type of proposed land use is for non-residential areas. Additionally, there will be water, air and other pollution concerns affecting all of the homeowners adjacent to the proposed site.

5. Military Considerations.

Many military families live within the community, mostly affiliated with Camp Pendleton. The proposed project will block these servicemen from quick ingress and egress with resulting harm to this and other communities.

These proposed changes to the zoning and associated requirements do not meet the Code restrictions for light industrial and limited industrial sites such as the Eddie Jones location and should not be granted for the reasons cited above. Please find a more suitable use of the proposed site that will complement the surrounding area and avoid the developmental problems noted.

John MacDowell
3101 Buena Hills Dr.
Oceanside, CA 92056
Johnmacdowell344@gmail.com

January 30, 2025

Planning Commission
City of Oceanside
300 N. Coast Hwy.
Oceanside, CA 92054
Planningcommission@oceansideca.org

Re: Sprinter

Dear Commissioners:

I had the opportunity to watch the Planning Commission's hearing and vote on the Olive Drive apartment project on Monday. I think you made the right decision – we desperately need affordable housing and we also need transit oriented development.

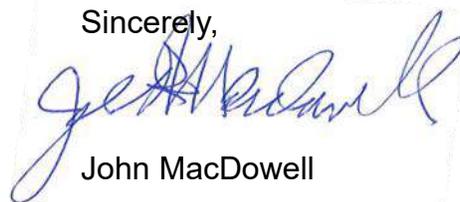
However, I was disappointed to hear commissioners make disparaging (and, frankly, gratuitous) comments about the Sprinter, even as the Commission approved a project designed to increase transit use. One commissioner referred to the "Sprinter to nowhere." Another acknowledged the project's proximity to the College Blvd. station and in the next breath suggested no one would use the Sprinter.

Increased transit use is essential, and it is the policy of the City of Oceanside. The Commission's vote supported the policy, but your comments suggested that you, yourselves, do not. To me, the comments seemed offhand, unnecessary, and a bit cynical. I would hope for better from public officials.

The Planning Commission should be leading the charge toward more transit use and development, and policies that will increase transit use. Let's encourage people to use the Sprinter instead of sneering at it. The Sprinter directly serves downtown Oceanside, downtown Vista, Palomar College, and downtown Escondido, among other places. It is hardly the Sprinter to nowhere.

Thank you for your service on the Commission, and thanks for giving some thought to my concerns.

Sincerely,



John MacDowell

Robert Dmohowski

From: Marion Donahue <mariondonahue@hotmail.com>
Sent: Thursday, January 30, 2025 7:54 PM
To: Robert Dmohowski; City Council
Cc: Dee Keck; Jim Waite
Subject: Eddy Jones
Attachments: 1000003925.jpg; 1000003923.jpg

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

At 9:30 AM, the sky was blue, but little did we know that giant plumes of smoke and flames of fire were advancing rapidly over the hill toward our house in the Santa Ana winds! The only warning we got was a fist pounding on the front door, accompanied by a voice shouting, "LEAVE NOW!! A FIRE IS AT YOU!!"

We had 15 minutes to grab a few documents and race down the ONE available exit road to get our daughters and ourselves to safety. Our oldest daughter at the time had stood on the kindergarten playground, looking up at the fiery hillside, believing that her parents were dead.

Remembering something crucial in our home, but unable to turn the car around in the impossible traffic jam, my husband handed over the wheel to me and hiked back towards our street! The year-old baby and 5-year-old daughter and I evacuated to my parents' home in another town.

Fire difficulties prevented signals and firsthand communication from the chaotic situation. So, my family and I ticked off the minutes in our heads, fearfully awaiting news about the fate of my husband. It wasn't until 6 PM that night that we finally heard his voice on a phone and knew he was alive!!

*The blaze lasted 6 days with most damage occurring on the first day. It ended with 4 dead, 286 homes destroyed, another 113 structures damaged or destroyed and the fire consumed 23,800 acres total. **The cause of the fire had been a wind-blown spark from a storage warehouse, just like the proposed Eddy Jones Warehouse!!***

This is REAL. I lived through it, and I have expressed my concerns to the city several times about this same scenario happening to my Oceanside home. I have [snail-mailed letters in bright red envelopes](#) with many photographs of the San Bernardino Panorama fire included. I [emailed](#) my concerns in a separate communication to the City. I have [voiced my concerns at a City Council meeting](#). I have [rallied, carried signs, informed strangers, and even spoke to my local Uber drivers who reside in Oceanside.](#)

I now write again to adamantly repeat that I am fully opposed to the Eddy Jones project! (Not only because of fire, but also air pollution, emergency and regular traffic concerns and 24-hour light pollution

which affects human sleep and causes birds to be unable to migrate. (*Lights Out San Diego article) Our homes are already facing 24-hour light pollution and traffic problems when Ocean Kamp is completed and sharing a crucial exit road with us. Do we need to add more of the same with Benet?

Since the tragic Palisades, Altadena, Eton and numerous other fires in Southern California have recently occurred, I am hoping that my concerns will be taken seriously!

Should this same scenario happen here, is this the legacy for which you want Oceanside to be remembered??

Unanswered questions remain:

1. There is a proposed plan for a fire **inside** the Warehouses, but **NOT** for nearby homes in a case of fire caused **by** the Warehouses?

2. We do not know what "cargo" will be carried in the diesel trucks and stored in the proposed warehouses or what "tenant" will own the trucks?

3. There is No Plan how

or -to **evacuate residents** via Benet because of downed electric lines, earthquakes, diesel accidents causing a fire at the Warehouse facility?

-to **regulate** the new **24 hour traffic** on Benet?

-to **maintain and repair the road-caused damage to Benet** by the added cars and diesel trucks (Even now repair must constantly be done on HWY 76 to repair damage from "normal" traffic and Benet is expected to add "no significant adverse results" from the hundreds of daily trips by employees and truck drivers??)

and -to **place additional police supervision** for the travel nuisance, traffic violations complaints of having to share a **crucial exit road** with the Eddy Jones facility?

-to **add space for traffic to pull off the road** when emergency vehicles must get through on regular health crises to residents?

Our hillside communities have already experienced the June 12, 2022 Cal Brisa Fire in Oceanside. A Dog Rescue location was barely saved by fast-thinking neighbors. There were plumes of smoke and 2hour+ water drops by planes. Residents who filmed the day were amazed that no lives or homes were lost at that time. (*reference letter to council by Jim Waite, a Las Brisas resident here.)

**My fire and traffic concerns are not a fantasy!
I have been there, done that, and don't ever want to do it again!
It is easy to believe it will never happen to you.....Until it does!!**

Marion Donahue
3412 Northwood Dr.
Oceanside CA 92058



...the physically safe



Robert Dmohowski

From: Andy Meyers <andy.x.meyers@gmail.com>
Sent: Friday, January 31, 2025 1:05 PM
To: Robert Dmohowski; City Council
Subject: Re: Opposition to the Proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility

Warning: External Source

Andrew Meyers
Canyon View Dr
Oceanside, CA 92058
andy.x.meyers@gmail.com <mailto:andy.x.meyers@gmail.com>

31 Jan 2025

Re: Opposition to the Proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility

Dear Mr. Dmohowski and Honorable Council Members:

I am writing as a deeply concerned parent of two children and a resident of the airport neighborhood in Oceanside. After thoroughly reviewing the proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility project, I must strongly urge the City to deny—or significantly modify—this proposal. Below, I present several key points referencing both scientific studies and regional precedents that demonstrate the risks of such large-scale warehouse developments in close proximity to family homes.

1. Significant Health Hazards from Diesel Emissions

a. Diesel Exhaust and Children's Health

Numerous peer-reviewed studies have established the harmful impacts of diesel exhaust, particularly on children's still-developing respiratory systems. The California Air Resources Board (CARB) has classified diesel particulate matter (PM2.5) as a toxic air contaminant linked to asthma, chronic bronchitis, and even heightened cancer risk. The USC Children's Health Study (which has tracked thousands of Southern California students for decades) found that children living near busy truck routes show reduced lung capacity and increased likelihood of respiratory illnesses.

Given that this proposed facility could operate 24/7, with hundreds of truck trips potentially entering and exiting daily, our neighborhood would see a substantial increase in diesel emissions. Already, we live near the Oceanside Municipal Airport, which creates its own noise and pollution burden. Additional diesel exhaust from a warehouse would compound our community's existing air quality challenges and place children—like my own two children,(and exacerbating my own injuries from military service in Iraq)—at even higher risk.

b. Regional Case in the Inland Empire

Communities in the Inland Empire (e.g., Moreno Valley, Perris) have experienced sharp increases in childhood asthma rates after large-scale warehouse complexes brought intense freight traffic to residential neighborhoods. Despite EIRs

outlining mitigation, real-world conditions have often proven more severe than projected. I fear Oceanside could face a similar outcome if we do not heed these cautionary examples.

2. Noise Impacts Comparable to Motorcycles

The trucks servicing a warehouse of this scale often emit up to 90 dB (decibels) during various operations, which is roughly on par with a motorcycle engine. Our community can already hear motorcycle noise (which California state law limits to 80db) traveling from Route 76—so if trucks operate at similar or higher decibel levels, it is almost certain the sound will carry far into our homes. Notably, the EIR itself declares a maximum decibel level of 96 dB at the boundary of a truck yard (section H5a). That constant, high-level noise, day and night, has been shown by the U.S. Environmental Protection Agency (EPA) to disrupt sleep patterns and contribute to stress-related health issues.

3. Declining Property Values

There is legitimate concern that property values in our neighborhood could drop as a direct result of increased truck traffic, noise, and around-the-clock industrial operations. In other Southern California communities—where homes adjoin large warehousing and distribution centers—residents have reported decreased market interest and lower home appraisals. Many families, especially those with young children, are hesitant to buy property near a site with ongoing noise, health, and traffic concerns. If this warehouse moves forward in its current form, the economic burden of diminished property values will fall on local homeowners, not the developer.

4. Real-World Examples of Community Pushback

a. Moreno Valley World Logistics Center

In Moreno Valley, proposals for massive logistics hubs met with extensive local protests and lawsuits (including Sierra Club v. City of Moreno Valley). Opponents argued the EIR underestimated air pollution, traffic, and noise impacts on nearby residents. While some projects moved ahead with mitigations, community members continue to report adverse health effects and quality-of-life issues.

b. Riverside County Warehouse Approvals

Riverside County has faced multiple CEQA lawsuits (e.g., Sierra Club v. County of Riverside) over “warehouse sprawl.” Even after the projects were approved, many residents felt their concerns about truck-generated pollution were validated when air quality worsened. These precedents highlight that mitigation measures on paper do not always guarantee protection in practice.

5. Less Harmful Alternatives and Meaningful Mitigation

If the City insists on this project, there are less harmful alternatives to protect local families:

1. Fully Enclosed Loading Area Nearest Residential Zones

- Enclosing the loading area closest to our homes with noise-reducing walls or a full structure would help contain diesel fumes and muffle backup alarms. Other jurisdictions have successfully implemented enclosed docks, reducing ambient noise significantly.

2. Light Roofing Across Truck Bays

- Installing partial or full roofing over truck bays can help capture and redirect both noise and emissions, reducing their reach into residential areas. This measure has been used in certain European logistics facilities to limit neighborhood disruption.

3. Zero-Emission Truck Requirements

- Mandate a progressive transition to electric or near-zero-emission trucks for all operators using the site, combined with strict no-idling rules. This approach has been championed by activists in the Inland Empire to reduce diesel pollution at its source.

4. Operational Time Limits

- Restrict overnight operations or limit truck arrivals during late-night hours (10 p.m.–6 a.m.) to safeguard residents from constant disturbance.

5. Comprehensive Monitoring and Community Oversight

- Install onsite air quality and noise monitors, with real-time public data reporting. If levels exceed safe thresholds, the developer must face enforceable penalties and promptly implement additional mitigation.

6. Conclusion

As a parent, my priority is ensuring my children grow up in a safe and healthy environment. The combination of increased diesel emissions, high-decibel noise, and the prospect of diminished property values makes this project untenable in its current form. We have ample evidence from neighboring regions that large-scale warehouses near residential areas often lead to real, long-term negative outcomes—outweighing whatever short-term economic gains developers promise.

I respectfully request that the City Planner and Council Members either deny this proposal or require major modifications that genuinely protect public health, property values, and our community's quality of life. Thank you for your careful consideration, and I trust you will uphold Oceanside's commitment to responsible, equitable development.

Sincerely,

Andrew Meyers

Concerned Resident of the Airport Neighborhood

<<https://mailtrack.io/trace/mail/f2d6cbc081faadbe32d81816506371b6677e49ba.png?u=10411487>>

Robert Dmohowski

From: Carol McConnell <cmcconnell4444@gmail.com>
Sent: Saturday, February 1, 2025 5:41 PM
To: Planning-Planning Commission
Cc: City Clerk
Subject: Balance of all stakeholders

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Hello City of Oceanside Planning Commission-

Re: EDDIE JONES WAREHOUSE PROJECT

Given the historical zoning designation of this land, the previous 60+ year tenant, and the proximity to the Oceanside Municipal Airport runway, Article 13 of the City's Zoning Ordinance should stand as is. Additionally, the General Plan land use designation of Light Industrial (LI) and a Zoning Designation of Limited Industrial (IL) is appropriate.

However, the Eddie Jones Project's Conditional Use Permit (CUP22-00001) request **SHOULD BE DENIED**.

Per Article 13 of the Zoning Ordinance an industrial facility for distribution and storage should *NOT to exceed* 50,000 square feet and allows *no more than* six (6) heavy truck terminals. Additionally, there is a limit of six (6) heavy trucks allowed on the premises at one time.

(Six (6) is a significantly lower number than the developer's CUP request for fifty-six (56).)

a. By simply following its own Zoning Regulation, the City honors its goal to balance resources for all stakeholders in the community. Holding firm to the City's EXISTING Zoning Ordinance has the following advantages:

- i. This size of development ***aligns with the historical use of the property which*** was a moderately sized industrial manufacturing facility in operation at this location for 60+ years.
- ii. This size of development ***aligns with ALL other Light Industry buildings south of the Airport runway, and south of CA Highway 76.*** The developer is welcome to build more of the same; a variety industrial buildings with production areas, steel garages to accommodate light trucks and delivery vehicles, and administrative space.
(Since Article 13 Zoning Ordinance applies EQUALLY to all industrial south of the Airport and south of CA Highway 76, why should Eddie Jones Project be the exception?)
- iii. This size of development aligns with the fact that California State CALTRANS has confirmed, **there are no future CALTRANS funds earmarked for any road / traffic improvements** at the intersection of CA Highway 76 and Benet Rd.
- iv. This size of development aligns ***with the City of Oceanside's resources and budget to maintain and service roads*** leading to / from this Project site.
- v. This size of development aligns with the ***tourist-centric recreational activities and ecologically sensitive wildlife areas immediately adjacent*** to this Project site.
- vi. This size of development and construction of the building(s) in ***an East-West orientation on the property, AND mandating that all heavy truck terminals + daily truck activity be limited to the South side of the building*** would help minimize noise.
 - a. Additionally, all affiliated equipment use, traffic and parking of service trucks, forklifts, any equipment with backup alarms, should be limited to the South side of the building.
 - b. Hours of operation of future tenants' CUP's should be limited to balance the needs of all stakeholders in the multi-use area.

** PLEASE ALSO CONSIDER:

Given the Project's *tourist-centric location upwind of key recreational activities involving children and teenagers*, we urge the City to *consider imposing a net-zero Greenhouse-Gas (GHG) significance threshold for this Project*. Requiring full or partial net-zero significance aligns with the City's own Climate Action Plan (CAP), the City's strategic goal of becoming a 'go to' Southern California tourist destination, and the California Air Resources Board's (CARB) Climate Change Scoping Plans.

Thank you for your time and consideration.

Dr Carol McConnell
Resident within 1500 feet of the Project

From: Naris Tongrin 3178 Huula Dr. Oceanside, CA 92058

To: City of Oceanside / Development Services Dept./ Planning Div.

Dear Sir,

RE: **I oppose the construction of the warehouse** located at:

250 Eddie Jones Way, Oceanside, CA 92058

My concerns are:

1)Traffic congestion, noise, and air pollution. Diesel fume is a known carcinogen. It can cause cancer in humans. Children living in the area will suffer more.

2) The proposed site is next to the Nature Preserve.

Nature Preserve contains trees, plants, and wildlife indigenous to the area. The development will certainly cause irreversible harm to the Nature Preserve.

3) The proposed site is also next to a historical site of the indigenous Indian tribe of Oceanside of the Luiseño tribe, also known as the Payómkawichum. They lived along the San Luis Rey River. They helped build the San Luis Rey Mission. Obviously, there must be plenty of uncovered historical facts left by these people who helped to build Oceanside to be what it is today.

4) Warehouses are known to be a congregation place for prostitution, vagrancy, drug, gang, & violent crime. If a warehouse is built next to a community of young families, dangerous incidents would be inevitable.

A retaining wall at Benet & Foussat Rd. is often sprayed with gang graffiti. This location is less than 200 ft from the proposed site.

5) The proposed warehouse site is, also, located next to a children's playground skateboard park. It is inevitable that the young kids will be exposed to dangerous diesel fuel & other bad elements that come with it if the warehouse is built.

Please help stop the construction that will bring harm to our families, environments, and the historical value of Oceanside.

Thank you.

Cordially,

Naris Tongrin

Robert Dmohowski

From: stuart cannes <stuartcannes@yahoo.com>
Sent: Saturday, February 1, 2025 1:02 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Peter Weiss; Zeb Navarro; Eric Joyce; Jimmy Figueroa; City Manager
Subject: Opposition to the Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Mr. Rob Dmohowski:

I am writing to express my opposition to the Eddy Jones project.
I am one of the residents within 1500 feet of the proposed project.

These large truck distribution centers have deservedly earned reputations for being gross polluters. They are noisy and they frequently attract undesirable elements and criminal activity. I hope the members of the council visit a nearby truck distribution center or even a Truckstop to get a real feel for what one of these facilities is like before voting.

I am asking the Planning Department to consider that the truck traffic will not be limited to entering and exiting via Benet from Hwy 76 and proceeding directly to the distribution center. There is no doubt that the trucks will be coming and going using the back way where they drive through a residential area and enter and exit using Fousat. When traffic backs up trying to enter or exit via Benet, the truckers will figure out a shortcut and get to and from Hwy 76 via Fousat. That means that a parade of truck traffic will be passing through the Wannis View residential area as well as the skate park and other potential business on Fousat. Truckers are smart and always looking to save even a few seconds. You need to consider the trucks using all of Benet as well as a portion of Fousat.

Highway 76 is one of the main roads leading into Oceanside. A lot of visitors use the 76 to get into town to play or do business. While the 76 may not be described as pretty through the valley, the proposed Eddy Jones project is at the beginning of stretch when the 76 does has it best face forward. This is the last mile with the river on one side and hills on both sides. Placing a large industrial complex near Oceanside's front door reflects poorly on the city's overall perception among potential visitors. Long term, this is prime area for Oceanside to spruce-up over time.

I know it's a bit farfetched, but I liken this truck terminal to the old rail switching yard that used to be in the heart oceanside. The switching yard is gone now because it's not what Oceanside is today. The Eddy Jones truck terminal has the same feel. There's a proposed water park in the immediate area. A large truck distribution center and a water park just don't mix. Oceanside should look for use on the land that complements the water park, the river and the closeness to the beach and downtown.

Thanks for your consideration.

Stuart Cannas
3216 Toopal Dr., Oceanside, CA 92058

Robert Dmohowski

From: Jeremiah Yassa <jeremiah.yassa@gmail.com>
Sent: Sunday, February 2, 2025 3:41 PM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Opposing Zoning Exception for Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Oceanside City Officials,

I am writing to express my strong opposition to the proposed zoning exception for the Eddie Jones Warehouse Project, which would allow 57 to 114 diesel truck docks—far exceeding the current limit of 6 per parcel. This large-scale increase poses significant risks to our community, environment, and public well-being, and I urge the City to reject this proposal for the following reasons:

1. Air Pollution and Public Health Hazards:

Allowing this many diesel truck docks will lead to a dramatic increase in truck traffic, diesel emissions, and air pollution. Diesel exhaust is classified as a carcinogen by the World Health Organization and has been linked to serious health conditions, including asthma, respiratory diseases, and cardiovascular issues. Communities upwind of the development—many of which are residential—will be directly exposed to elevated levels of pollutants, putting families, children, and elderly residents at high risk.

2. Noise Pollution and Quality of Life:

With up to 114 truck docks, the increase in diesel truck activity would result in 24/7 noise pollution from idling engines, loading/unloading operations, and frequent truck traffic. This level of noise intrusion will negatively affect the quality of life for nearby residents, disrupt sleep, and potentially lower property values in the surrounding neighborhoods.

3. Traffic Congestion and Safety Risks:

The increase in diesel truck traffic will significantly strain local infrastructure, leading to traffic congestion on nearby roads and highways. This raises safety concerns, especially for pedestrians, cyclists, and school zones in the area. Oceanside's roads are not designed to handle this volume of heavy-duty vehicles, which could result in an uptick in traffic accidents.

4. Environmental Impact on Nearby Habitats and Waterways:

The location of the warehouse is adjacent to sensitive ecosystems and the river, which are vulnerable to pollution from diesel emissions, fuel leaks, and runoff containing harmful chemicals. This poses a threat to local wildlife and water quality, particularly during rainfall, when pollutants could be carried into the river.

Protecting Oceanside's natural environment is essential for preserving biodiversity and maintaining a healthy ecosystem.

5. Violation of Existing Zoning Protections:

The current zoning regulations were put in place to limit environmental harm, protect residents, and ensure sustainable development. Granting a zoning exception for such a large-scale industrial project sets a dangerous precedent, undermining the integrity of the zoning process and potentially leading to similar requests in the future.

6. Alternatives Exist Without the Need for a Zoning Exception:

There are feasible development options that would be more compatible with the existing zoning and have a far lower impact on the surrounding community. The City should encourage the developer to explore warehouse designs that adhere to the current limit of 6 diesel truck docks per parcel.

In conclusion, approving this zoning exception will result in long-term, irreversible harm to the health, safety, and environment of Oceanside residents. **I strongly urge you to prioritize the well-being of the community over short-term economic gain and REJECT this proposal.**

Thank you for your time and consideration.

Sincerely,

Geneviaf Yassa

Robert Dmohowski

From: Heather Moreau <hmoreau1@gmail.com>
Sent: Monday, February 3, 2025 12:46 PM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Cc: Zeb Navarro
Subject: No on Eddie Jones by our HOMES!!! + Terrible 76 traffic idea :(((

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City of Oside Team responsible for protecting our residents,

I am writing as a concerned resident regarding the proposed Eddie Jones Warehouse Project. The developer is requesting a zoning exception to allow 57–114 Diesel Truck docks—far exceeding the handful of docks permitted under the current zoning regulations.

This development is directly adjacent to a residential community and upwind of our homes. The significant increase in truck docks would bring excessive diesel emissions, traffic congestion, and noise pollution, negatively impacting the health and well-being of local families. The existing zoning regulations were put in place for a reason, and they should not be disregarded at the expense of community safety and air quality.

I strongly urge the City of Oceanside’s Planning Commission to **uphold the current zoning regulations** and deny the developer’s request for an exception. Please prioritize the health and quality of life of Oceanside residents over corporate interests.

Thank you for your time and consideration. I appreciate your commitment to making decisions that serve the best interests of our community.

Sincerely,

Heather Moreau
Local Business Owner + Airport Neighborhood Resident



Robert Dmohowski

From: Holly Sawyer <hollylani@yahoo.com>
Sent: Monday, February 3, 2025 11:05 AM
To: Robert Dmohowski
Cc: Rick Robinson
Subject: EDDIE JONES PROJECT -no

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

As a resident behind the EDDIE JONES WAREHOUSE PROJECT, I request the developer be limited to the existing zone.

PLEASE ADHERE TO EXISTING ZONING LIMITATION. DO NOT ALLOW A ZONING EXCEPTION. We are not in favor of this project.

Imagine this in front of your own house. Fire evacuation is a problem, noise, and air pollution, traffic congestion, wildlife endangerment and the Prince of Peace Abbey.



Best,
Holly Bacci
Resident in Wanis View Estates

Robert Dmohowski

From: Kimberly Romero <joolingen@gmail.com>
Sent: Monday, February 3, 2025 11:24 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; City Manager
Subject: Eddie Jones Warehouse Development

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Regarding the Eddie Jones Warehouse Development, as a resident in District 1, I am requesting that you do NOT allow a zoning exception to the developer in this project.

Thank you,
Kimberly Romero
3236 Toopal Drive
Oceanside, CA 92058

Robert Dmohowski

Subject: FW: Eddie Jones Project

----- Forwarded message -----

From: **Miranda Palmer** <miranda@zynnyme.com>

Date: Mon, Feb 3, 2025 at 10:37 AM

Subject: Eddie Jones Project

To: <PlanningCommission@oceanside-ca.org>

My understanding is that each of the 3 parcels is currently zoned for up to 6 diesel truck docks per parcel for a maximum of 18 diesel truck docks over the 3 parcels if they remain in 3 parcels. I strongly urge the planning commission to leave the maximums as they are and not create a special circumstance. Even with 18 diesel truck docks that allows for dozens of trucks in and out of each dock on an hourly basis throughout the day which is more than enough based on the size of the parcels, the current traffic congestion and based on how close it is to protected wildlife habitat. Please vote no to this exception.

Miranda Palmer, LMFT (she/her/hers)

Co-Founder & Consultant, zynnyme

miranda@zynnyme.com

www.zynnyme.com

Robert Dmohowski

From: Terry Tovar <heartgems68@gmail.com>
Sent: Monday, February 3, 2025 1:01 PM
To: Robert Dmohowski; Planning-Planning Commission; City Council
Subject: Response to notification of hearing for consideration of a DEVELOPEMENT PLAN (D22-00001), CONDITIONAL USE PERMIT (CUP22-00001), and VARIANCE (V22-00001)

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr. Dmohowski and City Council members, Planning Commision members, and City Manager,

I wish to address several concerns:

1) The first is a simple request: Can you all please adhere to their own existing zoning limitation: Light Industry Zoning Regulation Limit is six (6) heavy truck terminals!

I am not familiar with the background/history as to how this existing zone was arrived at and accepted into regulation, but I am not an investigative researcher, just a simple Oceanside property owner since 1979 with no interest in learning about zoning, which puts me at a disadvantage: relying on the good intentions of our city officials to do the right thing and look out for the city's constituents; which leads me to my next concern:

2) What weight is given to the concerns of the residents affected by the approval of this mega Eddie Jones Project, as compared to the other criteria used for deciding whether this is approved or not?

For the sake of real perspective, are any of the deciding members within the affected area for this project? If the answer is no, then how sympathetic are they toward those affected? I do realize there are no metrics for measuring "sympathy", which results in this being unanswerable, sadly.

3) Should this project be approved, is it possible to predict the impact on property values? Can it be agreed upon that the impact will surely be negative? What weight does this real concern carry toward deciding to approve or not?

4) What are the advantages the City anticipates that, I assume, has moved this project to nearing approval?

5) Up to and including the cost of the Monday, Feb. 10, 2025 Public Hearing; how much has this whole process cost the City (or the Oceanside residents, that I assume are the main source for such city funds)?

6) I could go on listing a plethora of other concerns, but I assume other like-minded affected Oceanside residents have already done so, so feel free to add my name to those submittals.

7) [Last one (1)] Have the members had the option of comparison to other land uses (e.g.: Nature Conservancy, park, playground, et cetera)?

In conclusion: I can't say yet if I will be able to attend the Feb. 10, 2025 Public Hearing, so I pray that this correspondence will carry as much impact as if it were presented in person by this very concerned Oceanside property owner (since 1979)!

Sincerely yours,

Mr. Terry Tovar (760) 560-6607
Oceanside, CA

Robert Dmohowski

From: Benikia Kressler <taylobe@gmail.com>
Sent: Tuesday, February 4, 2025 9:03 AM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Cc: Zeb Navarro
Subject: Eddie Jones Warehouse Project Will Harm Community

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City Officials,

I am writing with an urgent request to DENY Eddie Jones Warehouse Developer's request for a zoning exception for the land between the airport runway and river. As a resident and mother at 3310 Morning View Dr. 92058 I am very concerned about the pollution and traffic their project proposals will bring. I kindly and adamantly request a denial of their request for a zoning exception. Thank you.

-Benikia Kressler
Oceanside Resident

--

To truly love we must learn to mix various ingredients-care, affection, recognition, respect, commitment, and trust, as well as honest and open communication.

-bell hooks

Robert Dmohowski

From: Debbie Kempe <debkempe@hotmail.com>
Sent: Tuesday, February 4, 2025 11:24 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

I am a resident of the California Brisas neighborhood which is adjacent to the Eddie Jones Project and request that the developer be limited to the existing zoning regulation.

Sincerely,

Deborah Kempe
3305 New Branch Ct
Oceanside, CA 92058

Robert Dmohowski

From: Lynn Harman <ddharm@aol.com>
Sent: Tuesday, February 4, 2025 4:36 PM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

I urge you to uphold the existing zoning restrictions concerning the Eddie Jones warehouse project. The existing zoning for this land only allows a handful of Diesel Truck docks. As residents in the community of homes adjacent and upwind of this development, we request that the developer be limited to the existing zoning regulation.

Respectfully,
Dennis D Harman & Patricia Lynn Harman
570 Long Crest Dr
Oceanside, Ca

[Sent from the all new AOL app for iOS](#)

Robert Dmohowski

From: Randy Hanson <randyjhanson19@gmail.com>
Sent: Tuesday, February 4, 2025 2:31 PM
To: Planning-Planning Commission
Subject: Eddie Jones Project

Warning: External Source

Dear Planning Commission,
Please reconsider the use of the land for the current project proposed with 57 18-wheeler truck bays. I live on Toopal Drive and I was sitting at the Benet and 76 signal when I had (2) cars and (2) 18-wheelers and it took me (3) signals before I was able to turn on Benet. We know Cal Trans will not be doing any improvements to Highway 76. There is a lot of property over by Rancho Del Oro in Ocean Ranch that could house what this developer is looking for. Thank you for your consideration and we will see you on the 10th.

--
Randy Hanson, RCDD OSP
760.704.2408

Robert Dmohowski

From: Donna Crosby <gigicrosby17@yahoo.com>
Sent: Wednesday, February 5, 2025 1:00 PM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Oceanside City officials,

As residents in the community of homes adjacent and upwind of this development, we request that the developer be limited to the existing zoning regulation.

Thank you.

Donna Crosby
376 Shadow Tree Dr
27 year resident

Robert Dmohowski

From: Kathy Heymann <kathysaraheyman@gmail.com>
Sent: Wednesday, February 5, 2025 10:28 AM
To: Robert Dmohowski
Subject: Eddie Jones warehouse project

Warning: External Source

Please limit the developer of the Eddie Jones project to the existing zoning regulations! Thank you in advance for your consideration in this matter to help the homeowners in this neighborhood.

Kathy
Sent from my iPhone

Robert Dmohowski

From: Kirsten and Jordi Sacher <kirstandjordi@gmail.com>
Sent: Wednesday, February 5, 2025 10:02 AM
To: Planning-Planning Commission
Subject: Eddie Jones Project and us

Warning: External Source

To Whom it May Concern,

As a resident of Wanis View, located off of Benet Rd, I have to drive the 76 at least twice a day for my kids' school drop-offs and pick-ups. While we love our quiet and pretty neighborhood, our friendly neighbors, the great sidewalks for biking and scootering, it's frustrating that we have no easy access to groceries, coffee shops, schools or parks—or anything else that would improve the quality of life—without getting on the 76. To get on and off the 76 requires sitting at very long lights, and dealing with endless traffic.

To get to the grocery store or park a mile away requires multiple long lights and crossing this quasi-freeway, making the trip an annoying 10 minutes instead of an easy 3 minutes like it could if this neighborhood connected to Mission or had a bridge to anywhere South.

But instead of thinking about ways of improving this almost-awesome neighborhood, the city is making plans to directly harm it. A truck warehouse is an amazingly short-sighted idea, only benefiting investors, and being a detriment to everyone who has spent a lot of money and savings to buy a house in this area. If the city of Oceanside allows this development, then the city might as well sell this whole housing development to big investors and developers as well, because our homes and family lives do not belong in the middle of all this massive development, traffic, and trucks. Thank you for considering the actual needs of your residents over investor's smooth talking. This does not benefit us.

Thank you for your time,

Kirsten Sacher
3111 Huula Drive
Oceanside, CA 92058

Sent from my iPhone

Robert Dmohowski

From: Mark Macfarlane <pls.markmacfarlane@gmail.com>
Sent: Wednesday, February 5, 2025 11:32 AM
To: Robert Dmohowski; Zeb Navarro; Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; Dee Keck
Subject: Eddie Jones Warehouse Project

Warning: External Source

To the Mayor, Planning Department, City Clerk, Deputy Mayor, City Manager and City Council Members,

I wish to express my deepest vehement objections to the proposed exception to zoning for the proposed 57-114 diesel truck docks at the proposed development on Eddie Jones and my objections to approval for this project in its entirety. As an Oceanside resident north of that project who will be directly affected by the traffic, noise, pollution, and decrease in property value created by this project in all three forms of it's proposed disastrous configurations I request that the City Council and all persons having the authority to do so limit any proposed project to the existing number of truck bays per current zoning regulations and disapprove this project all together. This project directly affects my family's, and all of the residents in this area's, safety and health. We have only 2 roads that go into and out of our community. The traffic on the Ocean Kamp side will already be snarled by this currently approved development, but at least it won't be a billion semi trailers going 24 hours a day. This is NOT the right place for this type of development. This project does nothing for the City of Oceanside, but only enriches a greedy developer that already tried to put in an Amazon distribution center in the middle of a residential area at the old Pepsi location. You all did the right thing by denying them before and we, as residents, expect you to treat us in our neighborhood with the same consideration. If the fact that they are purposefully hiding the future client does not tell you that this will be an Amazon distribution center I don't know what to tell you. This project should not be approved at all. I would also respectfully request that any development proposed in our neighborhood be restricted in the covenants, conditions, and restrictions to operating within normal business hours as a condition of any and all approval for any proposed future development. We, the residents in this neighborhood, will likely be pursuing legal action should this project or the zoning exception be approved. I have already contacted an attorney and would encourage all of my neighbors to join in a class action suit should this project be approved. Development more in line with the Ocean Kamp project would be far preferable than this unsafe, polluting, noisy, traffic jam in the making.

Thank you, respectfully

Mark Macfarlane PLS 8960
pls.markmacfarlane@gmail.com

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Any and all documents prepared by me or under my direct supervision, if attached to this email and unless otherwise noted, are issued in accordance with Section 8761 of the Business and Professions Code (The Professional Land Surveyors Act) and unless noted otherwise, are

"Preliminary" documents. All final surveys, reports, and drawings, issued by me will be signed and stamped in accordance with the Business and Professions Code.

Thank you for your cooperation.

Mark D. Macfarlane, PLS

California Licensed Land Surveyor 8960

Robert Dmohowski

From: Raymond Ho <raymondhho@gmail.com>
Sent: Wednesday, February 5, 2025 12:53 PM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; Zeb Navarro; Eric Joyce; Jimmy Figueroa; City Manager
Subject: Opposition to Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

"To whom it may concern,

My name is Raymond Ho and I am writing to you on behalf of my family of four (plus our incredible cat) in opposition to the Eddie Jones Project. I will start off by stating that I have been a resident of Oceanside since 2015, and have owned my home off Benet since then. That being said I have seen a tremendous growth and improvement in the city that seems to be well planned out to help increase allure to our incredible city, to promote a safer and more friendly environment for those who live within or visit our fine city, to increase revenue in a way that matches our city's future goals, and done with a mindset that will do nothing but bolster and improve what we have already become. I have seen all of this done with great success and precision and am proud to see what the future of Oceanside has to hold.

That being said, I see the Eddie Jones Project veer completely from any progress our city has made, not holding any of our city's values and goals within its mindset, and does nothing but put the city's residents as an afterthought of its future. This is seen by forgoing any thought to how this project will affect any one person that travels along HWY 76, because there WILL be a significant change in traffic patterns created by the Big Rigs traveling, merging, and taking up an already well traveled thoroughfare; this does not even take into account the traffic of the employees that would be traveling to fill many of the 593 parking spaces! Along with this, the impact that this project will have on any lure to travelers wanting to visit the up and coming Wave Park is immeasurable. The surfing and outdoor community is 100% environmentally focused. What person will want to spend their hard earned dollars to travel to a surfing destination just to have a warehouse facility being within eyeshot, earshot, as well as having to breath the contaminants while 'trying' to enjoy their outdoor excursion? I would venture to say very few. This project also snubs those that are already utilizing the bike path that runs along the San Luis Rey, deterring their health benefits because of the environmental impacts this project will have.

This project is an abhorrent use of the land that is already classified to maintain a facility 10% the size and truck capacity of Eddie Jones. In past years Oceanside has shot down similar projects that were dwarfed in size by Eddie Jones because we knew it was not a good fit for our city and our citizens. Knowing all of this we ask you to not grant the Conditional Use Permit for this project. Don't grant it for the families within the direct boundaries of the project, those that have health issues or who are trying to utilize this area to enjoy the outdoor extracurriculars, for any of those who travel along the Hwy 76 for any reason, to maintain a clear vision for Oceanside's future, but mainly don't grant it because you know it is not the right thing to do for Oceanside and it's current zoning parameters.

Thank you for your time, and I thank you in advance for doing the right thing!"

Thank you and have a great day!

Raymond

Robert Dmohowski

From: Shayna Lake <sharke.lake@gmail.com>
Sent: Wednesday, February 5, 2025 2:55 PM
To: Eric Joyce; Esther Sanchez; Planning-Planning Commission
Cc: Zeb Navarro; Robert Dmohowski
Subject: Public Comment in Opposition to Eddie Jones Warehouse Project

Warning: External Source

Mayor Sanchez, Councilmember Joyce, and Members of the Planning Commission,

I am writing to express my strong opposition to the proposed Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project (D22-00001) located at 250 Eddie Jones Way in Oceanside. I understand that the EIR Public Comment period is over, but given the Zoning Variance that the developer is requesting, the information in that document is completely relevant and informs the reasoning for voting NO against this project as proposed.

1. **Traffic and Safety Concerns**

The project is expected to generate significant truck and vehicle traffic, particularly on Benet Road and the 76. Despite proposed mitigation measures, the increase in heavy truck activity will exacerbate congestion, degrade road safety, and negatively impact nearby residential areas as well as severe congestion on the freeway on and offramps. The EIR acknowledges that the project exceeds the Vehicle Miles Traveled (VMT) threshold by 2.9%, which is a clear indication that the project does not align with sustainable transportation goals. The Camp Pendleton traffic is already dangerous when merging. Additional semi-trucks will further impede the flow of traffic, and visibility, and exacerbate the hazardous conditions, especially when merging onto the 5 FWY going north from the 76. Our community is already growing with neighbors and commuters, adding trucks will only make it more difficult and less safe for everyone on this stretch of road.

2. **Air Quality and Public Health Risks**

The project will contribute to increased emissions of criteria air pollutants, including particulate matter (PM10 and PM2.5), nitrogen oxides (NOx), and volatile organic compounds (VOCs). These pollutants pose serious health risks to vulnerable populations, including children, the elderly, and individuals with respiratory conditions. The mitigation measures outlined in the EIR, such as requiring low-VOC coatings, do not sufficiently address the long-term impacts on air quality.

3. **Environmental and Biological Resource Impacts**

The proximity of the project to the San Luis Rey River raises concerns regarding potential disruptions to local ecosystems. Although the project claims to avoid direct encroachment into the 100-foot riparian buffer, indirect impacts such as noise, light pollution, and habitat fragmentation could severely affect native wildlife and plant species. The loss of open space in this sensitive area undermines conservation efforts outlined in the City of Oceanside's Subarea Plan.

4. **Noise Pollution and Quality of Life Degradation**

With 114 initially proposed truck bays (reduced to 57 in the revised plan), the facility will generate significant noise from loading, unloading, and idling trucks. The noise levels will negatively impact surrounding residents and businesses, reducing property values and diminishing overall quality of life.

5. **Zoning Exception for Developer**

Granting a zoning exception for this project sets a dangerous precedent that prioritizes developer interests over community welfare. The site's existing zoning regulations were established to ensure balanced development that aligns with Oceanside's long-term urban planning goals. Allowing a large-scale warehouse and distribution facility in this location directly contradicts these objectives and disregards the concerns of residents who will be

most affected by increased industrial activity. The city must uphold its zoning standards to prevent unchecked industrial expansion in inappropriate areas.

6. **Lack of Community Benefits and Economic Justification**

The project primarily serves large-scale warehousing and distribution, with minimal direct benefits to Oceanside residents. While the proposal highlights job creation, it does not specify whether these positions will be well-paying, long-term, or filled by local workers. Additionally, the environmental degradation and infrastructure strain outweigh any potential economic gains.

7. **Alternative Development Strategies Not Fully Explored**

The EIR presents alternative configurations, including a multi-building design and a reduced footprint, but it does not adequately explore alternatives that would align with sustainable, community-oriented growth. A more balanced approach prioritizing mixed-use development, green infrastructure, or light industrial use with lower environmental impacts should be considered instead.

Given the significant environmental, public health, and community concerns associated with this project, I strongly urge the City of Oceanside to reject the proposed development in its current application. Instead, I encourage the City to consider alternative options that promote economic growth while protecting public health and the environment.

Your time and consideration is appreciated.

Shayna Sharke and Todd Lake
Airport Neighborhood Homeowners
Oceanside, CA. 92058

February 3, 2025

Development Services Department

Attn: Rob Dmohowski, Principal Planner

rdmohowski@oceansideca.org

Re: Eddie Jones Warehouse, Manufacturing & Distribution Facility, APNs 145-021-29-00, 145-021-030-00, and 145-021-032-00

Dear Mr. Dmohowski,

We are homeowners in the Wanis View Estates development. Our home is located on Toopal Drive and backs up to the San Luis Rey River. The site of the proposed Eddie Jones Warehouse is situated across the river from us.

We are writing to express the following objections to the proposal:

Air Quality: The varying building configurations proposed by the developer include space for 57 to 114 truck terminals. The emissions produced by this large volume of trucks will be damaging to the people and wildlife residing in the adjacent wildlife conservancy and neighborhoods.

Noise: The developer has indicated that the future occupants of this proposed warehouse are unknown, therefore the expected hours of operation are unknown, but that a 24/7 operation cannot be ruled out. If this facility will be operating through the night, it will cause significant noise (and light pollution) to the adjacent homes. Even without night-time operation the proposed level of operation and volume of truck traffic will create significant noise for the adjacent neighborhoods. Noise travels readily across the river. From our home we heard trucks, alarms and phones ringing from the TCE business which formerly occupied this site, and it was a much smaller operation. The scope of this project would significantly increase the noise experienced by our neighborhood.

Transportation: The proposals under consideration include a varying number of truck terminals ranging from 57 to 114. The proposals indicate that the truck traffic ingress/egress to this facility will take place on Benet Rd. Benet Rd. is a small two lane road. The quantity of truck traffic described in this proposal will overwhelm this small road and make exiting and entering our neighborhood very difficult. Only two roads provide access to Highway 76 for the homes north of the San Luis Rey River: Foussat Rd. and Benet Rd. The approved Ocean Kamp development at the Highway 76 and Foussat intersection will compromise access to Highway 76 via Foussat Rd. It is totally unacceptable to dramatically compromise the only other route to Highway 76 available to us. Lastly, a steady stream of tractor trailer rigs clogging Highway 76 will have a detrimental impact to our city's image and will negatively impact the efforts to increase tourism and small businesses.

For the reasons stated above we object to this proposal and urge denial of the proposal, and all requested Conditional Use Permits (CUPs).

Sincerely,

Susan and Anthony Mora

3271 Toopal Drive

Oceanside, CA 92058

Cc: Esther Sanchez
Oceanside Mayor
esanchez@oceansideca.org

Richard (Rick) Robinson
Council Member – District 2
rwrobinson@oceansideca.org

Peter Weiss
Council Member – District 4
pweiss@oceansideca.org

Zebb Navarro
Oceanside City Clerk
Znavarro@oceansideca.org

Eric Joyce
Deputy Mayor – District 1
ejoyce@oceansideca.org

Jimmy Figueroa
Council Member – District 3
jfigueroa@oceansideca.org

Jonathon Borrego
Oceanside City Manager
citymanager@oceansideca.org

Robert Dmohowski

From: Judy Fitzgerald <jfits3@gmail.com>
Sent: Wednesday, January 3, 2024 6:23 PM
To: Planning-Planning Commission
Cc: jfits3@gmail.com
Subject: Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

I am a VERY concerned resident of N. Oceanside, Judy Fitzgerald. My husband (Joseph) and I have heard about the proposed Eddie Jones building going in behind the municipal airport and are deeply concerned about the added congestion to the 76 hwy and our access to the 5 freeway N. and S. The 76 highway was not built to withstand the amount of semi truck use. The traffic is getting worse everyday with all the additional building of apartments, condominiums and Senior living developments. This obvious nightmare is enough to make us rethink our residency In N. County. If this is your objective, then you have definitely satisfied that objective. You are supposed to be working FOR the community. I realize the increase in tax revenue, but at what cost???

Is there no way there could be another road built directly from the Eddie Jones building to the 5 freeway just for the numerous SEMI TRUCKS (which is only going to increase) needing that access. Let them pay for the convenience to the 5. PLEASE, PLEASE think about your fellow Oceanside neighbors and the negative impact this will have.

Thank you in advance,
Judy & Joe Fitzgerald
325 Camino Parque
Oceanside, 92057
949 933 1351

Robert Dmohowski

From: maria.vincent@cox.net
Sent: Monday, February 19, 2024 4:40 PM
To: OceansideSpeaksout@gmail.com; Planning-Planning Commission
Subject: Opposing Eddie Jones Development

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

To the administration in question!

Please, please STOP THIS PROJECT - it is irresponsible to create a situation in our area where so many young families with small children live. Almost everyone has 1 or 2 dogs as pets who they like to walk on Benet and the surrounding area.

But the worst thing is that there is a well-appreciated bike trail and a skating area for the youngsters that will be affected as well.

It is the last open area in this overdeveloped city of Oceanside with a bit of nature preserve. It is high time that the well-being and health of the people is considered. Your Program will pollute the air, greatly increasing traffic and take away the last bit of freedom we inhabitants have here.

With friendly regards,
Maria Vincent

663 Wala Drive
Oceanside, CA 92058

April 22, 2024

To: Development Services Department

Attn: Rob Dmohowski, Principal Planner

300 N. Coast HWY.

Oceanside, CA 92057

Email= RDMOHOWSKI@OCEANSIDECA.ORG

My name is DUA. T. PHAM. I'm a resident at 3194 HUULA Dr. OCEANSIDE, CA 92058.

I also raise our voices to many other residents in the Benet Road area (KB Homes builder)

I would like to send this letter against the project Eddi Jones Warehouse, Manufacturing

And Distribution Facility (APN 145-021-29-00; 145-021-030-00).

We have many children who go to school every day and walk to high school and they ride bikes every day. If this manufacturing builds the warehouse. They will cause traffic and big trucks will be dangerous traffic to our community environment.

Please consider safety for children who are living in dangerous environments incidents will occur.

Please review the attachment OSO that I attached here as you can see the direct impact:

DIRECT IMPACT : Anyone using Hwy 76 between the I-5 and I-15 to include residents, commuters, **Prince of Peace Abbey** residents/parishioners, recreational users of **Prince Memorial Skate Park**; cyclists/pedestrians on bike path to and from Oceanside Harbor, **Ocean Kamp** visitors/employees and residents, Oceanside Airport users, **Camp Pendleton** employees/residents/visitors.

TRAFFIC IMPACT : There are NO road/traffic improvements planned, Cal Trans has not earmarked any funds. With Eddie Jones Warehouse using the ONLY two roads in and out of the community (trucks anticipated to run 24/7), anyone using Hwy 76 will be negatively impacted.

I would like to write this letter to beg you to stop this project. Please consider for few

Hundreds of thousands of residents are living here. They are in peace now. If you allow them to build this warehouse you will feel sorry in the future.

I ask you with all your power please DON'T APPROVE this project. It means you help us.

I ask your power to stop them.

I pray GOD blesses you and your family.

Thank you very much.



April 22, 2024

Robert Dmohowski

From: Jeannie Dung pham <dp999_98@yahoo.com>
Sent: Monday, April 22, 2024 4:01 PM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Zeb Navarro; Eric Joyce; City Manager; OceansideSpeaksOut@gmail.com
Subject: Letter to Oceanside City Development against the EDDIE JONES project on April24 2024
Attachments: Eddie Jones Project handout 2-7-2024.pdf; Letter to Oceanside City Development April22_202404222024.pdf

Warning: External Source

To Whom it may concern,
I would like to write this letter as myself and represent many other residents in our area Benet Rd and Foussat rd. We have been objecting this project from the beginning to now.

Could you please read my attachment to know the reason why we object this project.

We can't handle daily traffic with 113 truck in and out in our area. Plus pollution, noise and safety issue.

Thank you very much for consider us.

We appreciate your help very much.

Thank you very much.

Respectfully,



**OPPOSITION to the
Eddie Jones Warehouse,
Manufacturing & Distribution Facility**

Project Overview, Impact & Concerns

- **SITE:** Proposed to be built on 31.79 acres bordering the north side of the Oceanside Municipal Airport on Eddie Jones Way (off Benet Rd/76), the San Luis Rey River, and Alex Rd/Foussat Rd, adjacent to the residential development known as the Airport Community (1200 homes); 3 miles to ocean, 1.7 miles to I-5; 19 miles to I-15. The property is currently under review for hazardous waste remediation. Note: this location housed the former Deutsch Company.
- **SCALE:** Current plans include **114 semi-truck bays, 60 truck-trailer parking stalls and 590 auto parking spaces** with a facility of **566,905 square feet (roughly the size of 4 Costcos)**.
- **PROXIMITY:** 1/2 mile from **OceanKamp** a huge, recreational wave pool facility to be built on 90 acres to the east along Foussat Rd: includes a 300-room hotel, 670 new homes, an Airstream park, recreational facilities, retail and restaurants. Construction is estimated to begin in 2024. Along with the Airport residential community, the Prince of Peace Abbey is located immediately to the north west and established Airport industrial services to the south.
- **POLLUTION:** Air / water / light / noise. Major concerns are for the health of people, animals (22 rare, endangered, and special status species within Wanis View Preserve), Whelan Lake Bird Sanctuary and the San Luis Rey River. These concerns are due to the consequences of the LONG term exposure to diesel exhaust, and the operations of the facility.
- **ACCESS:** ALL 114 Eddie Jones semi-trucks to access site via **Benet Rd**; 590 employee/visitor vehicles to access via **Foussat Rd**. Current businesses and the Abbey, located to the northwest, already transit primarily using Benet Rd. OceanKamp and the skate park will primarily use Foussat Rd.

Benet and Foussat Roads are the **ONLY TWO ROADS** that afford access to the entire area north of Hwy 76. There are no roads out of the area leading east or west and with Camp Pendleton to the north, these three directions offer no options. This land-locked area has no alternative ways for entering or exiting and more importantly Benet and Foussat are the only roads available in an emergency.

Major concern: Timely access of fire, medical, or police vehicles in the event of an emergency.

- **DIRECT IMPACT:** Anyone using Hwy 76 between the I-5 and I-15 to include residents, commuters, **Prince of Peace Abbey** residents/parishioners, recreational users of **Prince Memorial Skate Park**; cyclists/pedestrians on bike path to and from Oceanside Harbor, **Ocean Kamp** visitors/employees and residents, Oceanside Airport users, **Camp Pendleton** employees/residents/visitors.
- **TRAFFIC IMPACT:** There are NO road/traffic improvements planned, Cal Trans has not earmarked any funds. With Eddie Jones Warehouse using the ONLY two roads in and out of the community (trucks anticipated to run 24/7), anyone using Hwy 76 will be negatively impacted.
- **COMMUNICATION/TRANSPARENCY:** Developer is only required to notify all parties within 1500 FEET of the site; many residents in the surrounding areas are unaware of the project. There continues to be a lack of knowledge regarding who the occupant will be, the intended use of facility, and hours of operations.
- **CITY VISION:** Community residents are NOT opposed to site development; however, the type and scale of business and hours of operations matter; the impact it will have on our community and our quality of life are not being seriously nor adequately addressed. What value does this development bring to Oceanside especially with the goal of preserving and promoting the area along the San Luis Rey River as a conservation corridor?

OceansideSpeaksOut2.org

OceansideSpeaksOut@gmail.com

About OSO and How to Get Involved

- OSO is a small, grassroots organization founded by residents of the Airport Community; Ocean Kamp is moving forward, but with your help we can stop development of the Eddie Jones Warehouse before it is too late; the Oceanside Community cannot withstand the impact of both projects.
- Visit OceansideSpeaksout2.org and Facebook EDDIE JONES PROJECT / OCEANSIDE
- Send an email to OceansideSpeaksOut@gmail.com to join our group and to receive updates; we ALL need to be actively involved - a few of us cannot do it all. Any help is appreciated - make copies, donate a few dollars, attend city council meetings, **put a sign in your yard**, etc. We need YOU!
- Write a letter with your concerns and send a copy to all the following: Mayor and all members of the City Council, City Clerk, City Planner, City Manager.

NOTE: Please cc or bcc OceansideSpeaksOut@gmail.com so we have a record of your correspondence.

Oceanside City Principal Planner
Rob Dmohowski
(760) 435-3563
rdmohowski@oceansideca.org

Oceanside City Clerk
Zebb Navarro, Ed. D.
(760) 435-3010
znavarro@oceansideca.org

Oceanside Mayor
Esther Sanchez
(760) 435-3057
esanchez@oceansideca.org

Councilmember - District 1
Eric Joyce
(760) 435-3032
ejoyce@oceansideca.org

Council member- District 2
Richard (Rick) Robinson
(760) 435-3033
rwrobinson@oceansideca.org

Deputy Mayor- District 3
Ryan Keim
(760) 435-3048
rkeim@oceansideca.org

Council member- District 4
Peter Weiss
(760) 435-3066
pweiss@oceansideca.org

Oceanside City Manager
Jonathan Borrego
(760) 435-3065
CityManager@oceansideca.org

Contact us:

Web Site: OceansideSpeaksOut2.org

Email: OceansideSpeaksOut@gmail.com

Facebook: [Eddie Jones Project/Oceanside](https://EddieJonesProject/Oceanside)

Robert Dmohowski

From: Thomas Le Vine <tomdlevine@gmail.com>
Sent: Friday, June 21, 2024 2:20 PM
To: City Council; Robert Dmohowski
Subject: Eddy Jones Warehouse Distribution Facility

Warning: External Source

Thank you for your service to our city.

I drive on Benet every day, as an area homeowner and local business owner. Lives are at risk if this project is approved.

Pedestrian deaths: Many unhoused people walk (or ride bikes) in the roadway every day and night, wear dark clothing, and are very difficult for drivers to see. To add over 100 trips to this residential roadway --- by giant commercial vehicles --- will likely lead to injuries and death. People climb in and out of the riverbed directly from Benet!

Health risks: Winds blow from the west, and will carry damaging fumes and particulates toward homes in the river valley situated to the east / northeast. This raises the ozone levels substantially and jeopardizes the respiratory health of the neighborhood, which has a large elderly population.

Traffic deaths: Highway 76 is already the second-most deadly road in the county (behind only Highway 67). Adding 100-120 new semi trucks onto this roadway will almost certainly increase these deaths and injuries. There is no plan to widen or improve Highway 76 itself. A warehouse district zoned commercial exists already on the

south side of 76, which is where this warehouse logically should go. As fill-in construction. That way, at least, truck traffic has a second major road nearby (Mission Road).

Benet is a road that serves a residential neighborhood, not a warehouse district. Not long ago, a fire forced evacuation of our neighborhood and Benet was the only option for safety. Adding semi traffic here is unsafe near the skate park, by a trail where neighbors ride their bikes and walk their dogs. Completely inappropriate. Proposed development in this area has all been entertainment and housing focused. A giant warehouse does not fit. If the owner wants to build houses or condominiums, I would respect that. I believe that voters in Districts 2, 3, 4 and 5 will agree with voters in District 1 that this massive, inappropriate project will cause a decline in their way of life, because of the major slowdown and new congestion on Highway 76 that Oceanside depends upon for transportation.

In the "do unto others" department, would you like to see a huge, 600,000 square-foot warehouse (open 24 hours a day serving the largest trucks on the road) right by your home? I believe that you serve because you truly want the best for the people and families who live in this city. I will tell you that this proposed project has galvanized public opinion and activism in ways I've never seen during our lifetime here in North County. I hope you see and understand that this represents a genuine safety and health hazard, especially for those living in the riverbed. Thank you for this platform to communicate with you.

Tom

Tom Le Vine

760-415-6055

TomDLeVine@gmail.com

Robert Dmohowski

From: Kimberly DeLuca <kdelucapm@gmail.com>
Sent: Monday, August 5, 2024 8:24 PM
To: Robert Dmohowski
Cc: City Council
Subject: Eddie Jones Project

Warning: External Source

Kimberly DeLuca
2809 Butler Street
Oceanside, CA 92054
Kdelucapm@gmail.com
August 5, 2024

Dear Members of the City Council,

I am writing to express my strong opposition to the proposed Eddie Jones Project as outlined in the recent article from The Coast News. The project, which threatens to disrupt the preserved nature corridor in our community, is deeply concerning and warrants a thorough reconsideration.

Our nature corridor is a vital component of our local ecosystem, providing essential habitat for wildlife and contributing to the overall environmental health of our area. The corridor not only supports biodiversity but also offers residents valuable green space for recreation and relaxation. Encroaching on this protected area for development would have significant adverse effects on both the environment and the quality of life for our community members.

The potential environmental impacts of this development cannot be overstated. The project could lead to habitat destruction, increased pollution, and loss of natural resources that are crucial for maintaining ecological balance. Furthermore, the construction and subsequent activity associated with the project could result in long-term negative effects on local wildlife and plant species.

In addition to environmental concerns, many residents have voiced worries about the increased traffic and strain on local infrastructure that the project would bring. Our community is already experiencing traffic congestion and infrastructural challenges; adding a large-scale development would exacerbate these issues, leading to further detriment of our neighborhood's character and livability.

I urge the City Council to reconsider the Eddie Jones Project and explore alternative solutions that align with our community's values and priorities. It is imperative that we protect our natural spaces and seek sustainable development practices that do not compromise our environmental and community well-being.

Thank you for your attention to this matter. I trust that you will carefully weigh the potential consequences and prioritize the preservation of our precious natural resources.

Sincerely,
Kimberly DeLuca
Oceanside Resident

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Tuesday, December 3, 2024 5:49 PM
To: Robert Dmohowski
Cc: City Clerk; City Manager; City Council
Subject: please do not approve the CUP for the revised Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hi Rob -

I'm writing to you again to ask you to reject the application for a Conditional Use Permit (CUP) for the Eddie Jones project. Please don't be deceived by the developer's "strike through" marketing tactic to cut the number of mac truck bays in half to 57 truck bays. It was ridiculous to propose 114 truck bays in the first place... I'm sure the developer did that on purpose so they could later present a 50% markdown and promote it as a sign that the developer is making accommodations to comply with community concerns. They put an absurd proposal out at first only to make a 57 truck bay proposal look good by comparison.

I'm writing to remind everyone that this is still WAY TOO MANY for a semi-industrial property that is currently only zoned for 3 truck bays.

As a reminder, the Amazon Distribution Center that was proposed for the Ocean Park Industrial Park only proposed to have 15 truck bays and it was rejected unanimously via an appeal with the City Council after the Planning Commission approved the proposal.

I was at the City Council meeting when members of the St Cloud community and other surrounding communities came out in force to voice their opposition to what 15 truck bays would mean for their quality of living in residences nearby. That was for 15 truck bays. The revised Eddie Jones Project is proposing 4X that amount! It's still outrageous considering how many homes there are in the direct vicinity, most with young children in residence.

My property directly faces the project and is within 1500 feet. My master bedroom windows face the project and provide a birds eye view of the entire property. The light, air and noise pollution of 57 mac trucks is unbearable to imagine. I implore you and the Planning Commission to reject the Conditional Use Permit that would enable the proposal.

Please consider these concerns. Most of my neighbors own their homes. As local tax paying citizens, we hope our city planners will consider our concerns as they envision the future for coastal Oceanside. I'm sure those who use the bike trail, skate park and future residents/visitors of the wave park would agree that a mega-industrial complex only 2 miles from the ocean is not a good fit for the future vision of our community.

Kind regards,
Gretchen Gary
471 Tishmal Ct.

Robert Dmohowski

From: Stephanie Dimakides <stephanie.dimakides@gmail.com>
Sent: Thursday, June 15, 2023 7:21 AM
To: Robert Dmohowski
Cc: City Manager; Planning-Planning Commission; Esther Sanchez; Eric Joyce; Rick Robinson; RICHARD KLEIN; Peter Weiss
Subject: Eddy Jones Project File Number D22-00001
Attachments: Effect on Wildlife.pdf

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

I would like to be included in the interested party list with updates on the project. If the conditional use permit is approved, this will greatly impact traffic, environment and home values.

We are a beautiful coastal community and need to build on that. There is nothing classy about a half a million square foot warehouse polluting our environment.

The wildlife objection is based on noise - multiple trucks idling, or multiple trucks pulling out. The attached pdf shows the big impact this project will have on the wildlife in the area.

This is what I imagine we will be looking forward to if this project is approved:



Stephanie Dimakides

REALTOR® | CalRE# 01752778



COLDWELL BANKER REALTY

818.321.2320

EMAIL: Stephanie.Dimakides@gmail.com

COLL

Eddie Jones Noise Issue Wildlife

Noise calculation

Federal Highway Administration: Max noise level for idling truck 85 dBa at 50 ft

Truck pulling out (accelerating): 90 dBa at 50 ft

Normal urban level: 55 dBa day 47 dBa night

6 Truck idling at Warehouse (happening 24 hours per day)

Location	Distance (ft)	Decibels	Sound increase From day background	Sound increase from night background
River bed	340	76	410%	745%
Wanis Nature walk	1179	66	215%	360%
Wanis View preserve	1790	62	160%	282%

3 Trucks pulling out

Location	Distance (ft)	Decibels	Sound Increase from day background	Sound Increase from night background
River bed	340	78	422%	823%
Wanis Nature Walk	1179	68	255%	414%
Wanis View Preserve	1790	64	188%	368%

Assume some combination of these.

68 dBa at Wanis nature: 3x as loud day 4x as loud night

64 dBa at Preserve: 2x as loud day 3 x as loud night

Robert Dmohowski

From: funjewels4u@cox.net
Sent: Tuesday, June 20, 2023 6:04 PM
To: Robert Dmohowski; City Manager; Planning-Planning Commission; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim; Peter Weiss
Cc: Randy Hanson; rawketgaming@gmail.com; eddiejoneswarehouse@gmail.com
Subject: Eddy Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Hi 😊

I am emailing on behalf of myself, My husband Randy Hanson & our adult son who lives with us Robert Hanson. We are all very concerned over the future building of the Eddy Jones Project. We are a house of interest since we live on Toopal Dr and our house backs up to the river. So we will be effected by this very large project.

We are against the plans that are being submitted and here's our reasons why-

- 1) It will greatly impact our traffic in & out of our community. With that many trucks going in & out on our tiny 2 lane road going out to the 76 we already deal with the towing company trucks, Hanson cement trucks and whatever from this area & Airport road. It will add more traffic to the 76 & 5 freeways which we really don't need if we are wanting people to come & visit us from far away or staycations.
- 2) The pollution from these trucks will be awful, and if they're using electric what will they do with the batteries once they die and we're right next to a river ??
- 3) If we have a fire, which there were 2 close ones last year, having all the employees/trucks will only prolong emergency vehicles getting in to us so we can be safe.
- 4) I don't understand how this will be appealing being close to the beach and trying to make our town a tourist destination ?? Especially with Ocean Kamp going in, who would want to stay close to a place that has this many trucks coming & going ??
- 5) Why are we even considering something like this here ?? I had asked at the very 1st meeting at the Library why we haven't changed the zoning for our area when we have the whole industrial area by El Corazon/Rancho Del Oro/College/Oceanside where something of this scale can be built ??

This project is just plain crazy for our area, it's too big & other city's that have built these building are regretting they did it now.

So please reconsider rezoning this area, stopping this project from what they are wanting to build, and keep us in the know of what is happening since we live right here & will be greatly impacted by it !!

Sincerely,

Randy, Mary & Robert Hanson
3281 Toopal Dr
Oceanside, Ca 92058

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Tuesday, June 27, 2023 10:05 AM
To: Robert Dmohowski; City Manager; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Planning-Planning Commission
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

We have lived in Oceanside for 22 years. The design of a city should include attractive public spaces that create a sense of community amongst locals. We are a community of skateboarders, bicyclists, joggers. We are active and enjoy the clean air. This is what draws tourists. Of course, they are drawn to the weather and the beaches, but more so, they are drawn to the community feel. When you get off the plane to Hawaii, you are welcomed by friendly greeters who love their island. We are drawn to their smiling faces. As tourists in Hawaii, we want what makes them smile. Structures influence this feeling and the psychological sense of community.

I believe this project as proposed is too big and will have too big of a negative impact on our north Oceanside community. Noise and air pollution created by semi-trucks running 24/7 exiting/entering the I-5 onto 76 Hwy. Ocean Kamp will be another tourist attraction requiring more car access. Driving alongside a semi-truck is intimidating, not a good community feel. This proposed warehouse, being the size of 3-4 Costco stores, and is simply the wrong business for this location. In particular, it is near a much-used bike path, a children's skateboard park, and thousands of residential homes. If residents are content, tourists will have a better experience and will likely return. Please keep the beachy community feel intact. Trucks and forklifts and traffic and pollution will not enhance our quality of life. If Oceanside residents are smiling, this will attract tourists the most.

Sincerely,
Willie and Ann Marshall

Robert Dmohowski

From: Joanne Pilgrim <joanne.pilgrim2425@gmail.com>
Sent: Saturday, July 15, 2023 12:03 PM
To: Robert Dmohowski
Cc: CityManager@oeansideca; Esther Sanchez; Eric Joyce; Ryan Keim; Rick Robinson; Peter Weiss; Planning-Planning Commission
Subject: Proposed Eddie Jones Project
Attachments: Letter re Eddie Jones Project.7.13.2023docx.docx

Warning: External Source

City Planners and Representatives:

Please see attached letter. A signed copy has been mailed to each of you. Neither I, nor any of the other homeowners involved, understand how RPG (Adam Robinson) got around the Municipal Code zoning regulations with their Conditional Use Permit. Please note: This property does not comply with the regulations as detailed more specifically in my letter.

Additionally, I believe the City should pay close attention to the location of the community involved, i.e., its proximity to Camp Pendleton and acres of open brush. Many of our properties almost back up to the base property. This makes all of the homeowners who live here particularly vulnerable to fires which happen often because of the dryness and extensive danger related to Marines practicing various forms of gunfire. In fact, insurance companies are canceling homeowners' policies in our community because of this forcing homeowners to acquire new policies where they can.

As such, none of us believe any statistical studies of traffic by the City or RPG's attorneys can overcome this potential danger as outlined by RPG (Adam Robinson) on Wednesday night. We will be trapped with the blocked ingress and egress access problems already noted resulting in probable tragic loss of life and property.

Under the circumstances, please work with the community to find an appropriate use of the property which is not a warehouse with all its accompanying trucks, noise and access problems.

Joanne Pilgrim

Robert Dmohowski

From: Adam Zack <adamz@jensensfoods.com>
Sent: Tuesday, July 25, 2023 10:49 AM
To: Robert Dmohowski; Peter Weiss; Ryan Keim; City Manager; Planning-Planning Commission; Eric Joyce; Esther Sanchez; Rick Robinson
Subject: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Hello City Leaders,

We love our city and are proud residents. We think you are doing a great job with the direction Oceanside has been going. From downtown to East Oceanside, it's a great place to live.

We strongly oppose the Eddy Jones warehouse. We live directly behind the proposed location and commute daily on Benet Road to Highway 76. The addition of so many semi trucks daily will not only deter our quality of life with additional air and noise pollution, the increased traffic to get on to Highway 76 will make our daily trips from home unbearable.

Please consider the residents that will be directly affected and deny this project.

Sincerely yours,

Adam & Linda Zack
3259 Toopal Drive
Oceanside 92058

--

Adam Zack



619-550-2097
760-831-9644 cell

955 Catalina Blvd #101
San Diego, CA 92106

Robert Dmohowski

From: Adam Zack <adamz@jensensfoods.com>
Sent: Tuesday, July 25, 2023 10:49 AM
To: Robert Dmohowski; Peter Weiss; Ryan Keim; City Manager; Planning-Planning Commission; Eric Joyce; Esther Sanchez; Rick Robinson
Subject: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

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Please consider the residents that will be directly affected and deny this project.

Sincerely yours,

Adam & Linda Zack
3259 Toopal Drive
Oceanside 92058

--

Adam Zack



619-550-2097
760-831-9644 cell

955 Catalina Blvd #101
San Diego, CA 92106

Robert Dmohowski

From: int <ptsoris@aol.com>
Sent: Thursday, August 3, 2023 10:37 AM
To: Planning-Planning Commission
Subject: Fw: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Sent: Thursday, August 3, 2023 at 10:35:41 AM PDT
Subject: Fw: Eddy Jones Warehouse

From: int <ptsoris@aol.com>
Subject: Eddy Jones Warehouse

My husband and I have lived in our present home for 14 years. We want to keep our neighborhood as much as possible residential... We are assured to have the mega truck terminal will be a hazard. We would advise you to keep the zoning and not allow a change for larger warehouse...The negatives are as follows: traffic impact, pollution, safety and emergency services...It is already a given that the Ocean Kamp will be built...That alone is enough for our neighborhood...traffic and more.

When they purchased the 31 acres...they were aware of the zoning...keep it so the truck terminal won't be a detriment to our neighborhood.. Home prices with drop if they get the mega terminal.

**Pauline Tsoris
536 Crestwood Drive**

**Oceanside,
CA 92058**

760-458-4271

August 21, 2023

City Planner, City Manager & Mayor
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92054

Re; Proposed Eddie Jones Warehouse Project

Dear Oceanside Officials;

I write to you as an interested citizen who lives in proximity to the proposed Eddie Jones project and as someone who works in the real estate development field.

I am expressing my concern over, and objection to, the developer's plan to dramatically increase the scale of the project by a multiple over that which is allowable under the standing CUP.

This pursuit, if allowed by the City, will have numerous deleterious impacts to the area and risks undermining the fundamental intent of zoning laws which are set in place to promote smart development which serves not just the developer's and investor's wallets but also considers the competing aims of positively contributing to the quality of life of the citizenry.

The proposed project represents nothing short of private profiteering at the expense of the public good. If approved, this would amount to a huge financial handout to a private party, multiplying their return on investment. The out-of-town developers and investors stand to capture massive "excess profits" due to the potential upzoning, for which the City and its citizens will pay for in perpetuity by having to bear the consequences of considerable increased traffic, pollution, noise, road damage (almost all of which is caused by trucks) and other negative impacts.

Any argument that industrial warehouses are a job generator is a fallacy. The term "manufacturing" has likely been added to the naming of the project because it conveys the idea that this facility will generate jobs. This will almost certainly be a pure trucking logistics facility which will not create any considerable employment; the high quantity of car park spaces will almost certainly be majority dedicated to trucks and trailers.

I implore each of you to read the linked articles below as you consider the ill-conceived development proposal. If you're interested in the real "costs" associated with logistics facilities, you need only look toward the nation's largest concentration of such facilities, the Inland Empire:

[Inland Empire warehouse boom risks health, climate - CalMatters](#)

[California's Inland Empire Pushes Back on Booming Warehouse Construction - The New York Times \(nytimes.com\)](#)

[Southern California Warehouse Boom Poses Environmental Costs for the Inland Empire - Times of San Diego](#)

In short, the City should uphold its responsibilities to its citizens, enforce its regulations and reject this private profiteering scheme by limiting the development strictly to what is allowable within the terms of the CUP and not one inch more.

Kind regards,



Justin Sager
858-876-5999

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Tuesday, September 19, 2023 3:45 PM
To: Robert Dmohowski; Planning-Planning Commission; Eric Joyce; Ryan Keim; City Manager; Esther Sanchez; Rick Robinson; Peter Weiss; oceansidespeaksout@gmail.com
Subject: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Within 3 miles of our beautiful beaches, can you guess *which one of these things is not like the other?*

- Luxurious hotels and highly rated restaurants for tourists to enjoy
- Residential homes for families to raise their children
- OceanKamp with a Wave Park for both residents and tourists to experience
- Warehouse the size of 3-4 Costcos utilizing 114 semi-trucks daily at peak traffic times

Easy to guess right? Let's keep our identity. Please do not allow this huge warehouse to be built. It is simply the wrong location.

Sincerely,
Ann Marshall
Resident 22 years

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Tuesday, September 26, 2023 2:46 PM
To: Robert Dmohowski; Planning-Planning Commission; Eric Joyce; Ryan Keim; City Manager; Esther Sanchez; Rick Robinson; Peter Weiss; oceansidespeaksout@gmail.com
Subject: Against Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Today my daughter was 2 hours late for work due to a car crash on the I-5, hence, entering the I-5 & SR-76 intersection moved at a snail's pace.

The I-5 & SR-76 intersection annually accommodates 5 million beach visitors and 170,000 residents. It is likely the busiest intersection in all of Oceanside's 42 square miles. I understand housing cannot be restricted by the City, therefore, there will be additional future **car** traffic.

You do have control over **semi-trucks** that will feed onto SR-76 to and from I-5. And I don't think the \$72K in tax income is worth it, do you?

I hold you accountable to say no to daily truck traffic. Let's develop sensibly to preserve our Coastal identity.

Sincerely,
Ann Marshall
Resident 22 years

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Tuesday, September 26, 2023 3:24 PM
To: Robert Dmohowski; Planning-Planning Commission; Eric Joyce; Ryan Keim; City Manager; Esther Sanchez; Rick Robinson; Peter Weiss; oceansidespeaksout@gmail.com
Subject: Against Eddie Jones project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

The Eddie Jones project's environmental impact will be huge. The natural environment will be affected by carbon emissions and air pollution.

An industrial zone in the middle of residential areas within 3 miles of the beach is simply not sustainable. The intersection at Benet & SR-76 is already being used by several companies that operate with semi-trucks, i.e. Pick Your Part and AIM Recycling. Additionally, both companies are not visually appealing off the SR-76. If we are to draw in tourists, let us at least offer a scenic route to our beaches.

The Eddie Jones project would add up to 114 trucks to this intersection daily. Businesses do bring in tax revenue but is only \$72K from this project worth it?

Environmental planning is the key to sustainable development. Let's not let Oceanside grow haphazardly. Let's work together to ensure it is developed sensibly. Please Vote NO for Eddie Jones.

Sincerely,
Willie Marshall
Resident 22 years

Robert Dmohowski

From: Edgar Arroyo <edarroyo007@gmail.com>
Sent: Thursday, October 5, 2023 2:15 PM
To: Robert Dmohowski; City Manager; Planning-Planning Commission; Esther Sanchez; Eric Joyce; Rick Robinson
Subject: Opposition to the Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear City of Oceanside Elected Officials,

I have been a residence of California Brisas in Oceanside since 1994, and it has been characterized as a quiet community with a good standard of living.

This industrial project will damage the standard of living for many families living in the area and it will only enrich outside investors of the project.

The project is too large and too industrial for the area. It will also severely affect the value of our homes. I am also a real estate broker since the 1990's and I know that valuations of residences will be affected by that project.

Please do what you can and in your power to stop this kind of project. Perhaps a different type of project like a shopping center or office/ small warehouses for small businesses would be more appropriate.

Thanks,

Edgardo Arroyo
619-883-9592

Follow us on: [OceansideSpeaksOut2.org](https://www.oceansidespeaksout2.org) and Facebook - EDDIE JONES PROJECT / OCEANSIDE
Oceanside City Principal Planner
Rob Dmohowski
(760) 435-3563
rdmohowski@oceansideca.org

Oceanside City Manager
Jonathan Borrego
(760) 435-3065
CityManager@oceansideca.org

Oceanside Planning Commission
PlanningCommission@oceansideca.org

Oceanside Mayor
Esther Sanchez
(760) 435-3057
esanchez@oceansideca.org

Councilmember - District 1
Eric Joyce
(760) 435-3032
ejoyce@oceansideca.org

Councilmember – District 2
Richard (Rick) Robinson
(760) 435-3033
rrobinson@oceansideca.org

Deputy Mayor - District 3
Ryan Keim
(760) 435-3048
rkeim@oceansideca.org

Councilmember - District 4
Peter Weiss
(760) 435-3066
pweiss@oceansideca.org

Oceanside City Clerk
Zeb Navarro, Ed.D.
(760) 435-3010
znavarro@oceansideca.org

Robert Dmohowski

From: Heather Moreau <hmoreau1@gmail.com>
Sent: Thursday, October 5, 2023 9:20 AM
To: Robert Dmohowski; Planning-Planning Commission; Zeb Navarro; City Manager; Eric Joyce; Rick Robinson; Ryan Keim; Peter Weiss
Cc: OceansideSpeaksOut@gmail.com
Subject: Please NO 'Eddie Jones warehouse' off the 76! 🙏

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Hi there,

My name is Heather and I am an Oceanside resident, local business owner, and native. I live in the airport neighborhood off the 76 and I wanted to send this email to sincerely request consideration to deny the Eddie Jones Warehouse off the 76. There are so many reasons for this project to be denied, among them huge traffic impacts, and direct controversy with the current wave Park project being built right next door to that. The wave Park project is striving to make our community walkable and to attract people from all over the world, but adding this Warehouse project would be a direct impact in the type of area we are trying to build for Oceanside. Home values would suffer.

If we share our only two 2-lane ingress/egress roads with over a hundred tractor trailers and nearly 600 employees, our fire evacuation routes will be further clogged and restricted in fire emergencies.

Noise pollution that would directly affect the Wave Park project and all the proposed restaurants and amenities being built right next door. Who would want to go on vacation next to an enormous Warehouse project? They just don't fit together. There is potential that future occupants of this project could operate 24 hours per day, which would mean round-the-clock noise pollution in our community.

City Beautification. An industrial warehouse project in the same stretch of the 76 is counter to the vision behind building a wave park and beautifying the skate park, which will draw a surge of children and teenagers on Foussat and Alex Roads, likely on e-bikes and skateboards.

A steady stream of tractor trailers on our roads would detract from the tourism appeal of our community and present unprecedented road hazards. In fact, we enjoy using the local bike path so much and always use Bennet Road to travel to the bike path, we really would not be able to do this anymore with a huge shipping facility and tons of trucks over there, it would really negatively impact the bike path that so many people enjoy.

I know I for one would definitely want to move out of this neighborhood if this project was going to happen. I sincerely hope that the city will take our concerns under advisement and reject the warehouse project. We have such a great neighborhood, please don't let them ruin it!

Thanks so much for reading this email.

All the best,
Heather

Vallentyne Photography

www.vallentynephotography.com

Robert Dmohowski

From: dave keck <dgkeck@cox.net>
Sent: Monday, October 9, 2023 10:45 AM
To: Planning-Planning Commission; Zeb Navarro
Subject: Eddie Jones Warehouse, Manufacturing and Distribution Facility D22-00001

Warning: External Source

Please forward to:

John Malik,

The subject project will be coming before you in the near future and it is my intention that you be fully aware of the community's opposition. We are not against something being built there but what is currently proposed is wrong for our community so many ways.

The Community's Concerns

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as ALL who use the 76 Hwy to and from the I-5 and I-15 Fwys and traffic on those Fwys where they intersect with the 76. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There are NO Cal Trans funds earmarked for any road / traffic improvements.

TRANSPARENCY - To date the lack of transparency/communications is very apparent when many of the Airport residents and surrounding areas are still unaware of the intended development. We are not opposed to something being built but what type of business, what level and hours of operations and what impact will it have on our community and our quality of life are not being addressed or taken into consideration.

POLLUTION - We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week 24 hours a day. Noise pollution from these vehicles is concerning and lights from the facility will have a negative impact on nearby residents and nocturnal animals living in the riverbed. Pollution of the San Luis River is a concern.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. This will be intensified due to the recently passed AB 316 which approved autonomous trucks (driverless). The same will be true on Foussat Rd due to the 590 vehicles expected for the employees and visitors at this new facility who will use that road. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development, as proposed, is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. We do not oppose development as long as it is well thought out and is compatible with the residential area that it borders and the other "light industry" type businesses already located in that area. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big with too many trucks and will have a negative impact on our nearby communities and Oceanside as a whole. What value does it bring to Oceanside?

Respectfully Submitted,

Dave Keck

760 583 2083

Resident for 22+ years



Virus-free. www.avg.com

Robert Dmohowski

From: CAROL BROELAND <memaeae@aol.com>
Sent: Wednesday, October 18, 2023 4:19 PM
To: Robert Dmohowski
Subject: Eddie Jones Project through the eyes of an 8 year old

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear Directors (of this project)

I am the great granddaughter of Carol. She told me she doesn't like what you want to build near her house. So please stop. I don't like this and I don't agree with it. Just cause you want to, you don't need to build it near houses. Houses don't need a giant warehouse right next to them. So why don't you find a nicer area that's not next to houses, to build your giant warehouse? People don't want too much traffic, it's too much pollution cause of the stink from the trucks. So, if you're gonna build it, please find another area, cause I don't want my Mema to sell her house and move away but I don't want her to live in this misery. So don't build it. I don't want to be mad at you, or to have people mad at you. So just do what's right.

Emma Grace

Great granddaughter of Carol Broeland. Aka: Mema

From Carol's iPad

Eddie Jones Warehouse, Manufacturing, and Distribution Facility

Only Two Roads Service The Entire Airport Community

Airport Community Consists Of Approx. 1200 homes, 4800+ People!

Zoning Was Done in 1960's, BEFORE There Was A Community North Of Oceanside Airport. 63 YEARS AGO!

Don't Make An Island Out Of Our Community....PLEASE!

Eddie Jones Project

Ocean Kamp Project

 Choke Points



Estimated Property Values In The Airport Community = \$1,080,000,000.00

\$12,960,000 in Tax Revenue!

Please Read On and Educate Yourself. Good decisions are informed decisions.

PROJECT DETAILS: (D-EIR – 3.2.4)

114 Semi-Tractor Truck Bays

60 Semi-Tractor/Trailer Parking Stalls

590 Automobile Parking Stalls

Operations – 7-Days/Week, 24-Hours per Day

ZONING (City of Oceanside)

Airport Zoning – Zoned Light Industrial – 1961 (62 YEARS AGO!) (Airport Established 1946/Oceanside Chamber of Commerce)

Airport Residential Community Built – 2003-2004 (20 YEARS AGO)

The Current Zoning Is No Longer Valid For This Area

The Eddie Jones Project Is Not Viable For This Location Without Adversely Affecting The Entire Airport Community

The Draft Environment Impact Report Cites The Following Pollutants Affecting The Surrounding Communities.

***Information Derived From D-EIR (Draft Environmental Report D22-00001 / #2022070365, Section 4.2 - 4.2.1, Section 4.6 and 4.7)**

OZONE

NITROGEN DIOXIDE

CARBON MONOXIDE

SULFUR DIOXIDE

PARTICULATE MATTER

LEAD

VOLITILE ORGANIC COMPOUNDS

TOXIC AIR CONTAMINANTS (TAC)

DEISEL PARTICULATE MATTER (DAM)

ODOROUS COMPOUNDS

COXXIDIOIDOMYCOSIS

SULFATES

HYDROGEN SULFATE

VINYL CHLORIDE

“These standards are designed to protect the most sensitive persons from illness or discomfort. Pollutants of concern include O3, NO2, CO, sulfur dioxide (SO2), PM10, PM2.5, and lead. These pollutants, as well as toxic air contaminants (TACs), are discussed in this section. In California, sulfates, vinyl chloride, hydrogen sulfide, and visibility-reducing particles are also regulated as criteria air pollutants.”

A Supporting Vote Will Sacrifice The Children of Oceanside To This Legacy!

Section 41700 of the Health and Safety Code states that a person shall not discharge from any source whatsoever. quantities of air contaminants or other material that cause injury, detriment, nuisance, or annoyance to any considerable number of persons or to the public; or that endanger the comfort, repose, health, or safety of any of those persons or the public; or that cause, or have a natural tendency to cause, injury or damage to business or property. This section also applies to sources of objectionable odors.

Pursuant to the 1990 federal CAA Amendments, the EPA classifies air basins (or portions thereof) as “attainment” or “nonattainment” for each criteria air pollutant, based on whether the NAAQS have been achieved. Generally, if the recorded concentrations of a pollutant are lower than the standard, the area is classified as “attainment” for that pollutant. If an area exceeds the standard, the area is classified as “nonattainment” for that pollutant. If there is not enough data available to determine whether the standard is exceeded in an area, the area is designated as “unclassified” or “unclassifiable.” The designation of “unclassifiable/attainment” means that the area meets the standard or is expected to be meet the standard despite a lack of monitoring data. Areas that achieve the standards after a nonattainment designation are redesignated as maintenance areas and must have approved maintenance plans to ensure continued attainment of the standards. The California Clean Air Act, like its federal counterpart, called for the designation of areas as “attainment” or “nonattainment,” but based on the CAAQS rather than the

NAAQS. The criteria pollutants of primary concern that are considered in this analysis are O3, NO2, CO, SO2, PM10, and PM2.5. Table 4.2-2 summarizes the SDAB’s federal and state attainment designations for each of the criteria pollutants.

Table 4.2.2. SDAB Attainment Designation (Page 103)

Pollutant	Federal Designation	State Designation
O₃ (8-hour)	Nonattainment	Nonattainment
O₃ (1-hour)	Attainment ^a	Nonattainment
CO	Attainment	Attainment
PM₁₀	Unclassifiable ^b	Nonattainment
PM_{2.5}	Attainment	Nonattainment^c
NO ₂	Attainment	Attainment
SO ₂	Attainment	Attainment
Lead	Attainment	Attainment
Sulfates	(No federal standard)	Attainment
Hydrogen sulfide	(No federal standard)	Unclassified
Visibility-reducing particles	(No federal standard)	Unclassified
Vinyl chloride	(No federal standard)	No designation

Table 4.2-4. SDAPCD Air Quality Significance Thresholds

Construction Emissions			
Pollutant	Total Emissions (Pounds per Day)		
Respirable Particulate Matter (PM10)	100		
Fine Particulate Matter (PM2.5)	55		
Oxides of Nitrogen (NOx)	250		
Oxides of Sulfur (SOx)	250		
Carbon Monoxide (CO)	550		
Volatile Organic Compounds (VOC)	75*		
Operational Emissions			
Pollutant	Total Emissions		
	Pounds per Hour	Pounds per Day	Tons per Year
Respirable Particulate Matter (PM10)	—	100	15
Fine Particulate Matter (PM2.5)	—	55	10
Oxides of Nitrogen (NOx)	25	250	40
Sulfur Oxides (SOx)	25	250	40
Carbon Monoxide (CO)	100	550	100
Lead and Lead Compounds	—	3.2	0.6
Volatile Organic Compounds (VOC)	—	75*	13.7

Regionally, project-related travel will add to regional trip generation and increase the vehicle miles traveled within the SDAB. Locally, project traffic will be added to the City's roadway system. If such traffic occurs during periods of poor atmospheric ventilation, consists of a large number of vehicles "cold-started" and operating at pollution-inefficient speeds, and operates on roadways already crowded with non-project traffic, there is a potential for the formation of microscale CO "hotspots" in the area immediately around points of congested traffic. Because of continued improvement in mobile emissions at a rate faster than the rate of vehicle growth and/or congestion, the potential for CO hotspots in the basin is steadily decreasing. Projects contributing to adverse traffic impacts at highly congested intersections may result in the formation of CO hotspots. To determine whether the project would cause or contribute to a violation of the CO standard, a screening evaluation of the potential for CO hotspots was conducted including County guidance and SCAQMD CO modeling for the 2003 Air Quality Management Plan. Per County's CO hotspot screening guidance (County of San Diego 2007), any project that would place receptors within 500 feet of a signalized intersection operating at or below LOS E (peak-hour trips exceeding 3,000 trips) must conduct a "hotspot" analysis for CO. Likewise, projects that will cause road intersections to operate at or below a LOS E (i.e., with intersection peak-hour trips exceeding 3,000) will also have to conduct a CO "hotspot" analysis. LOS

Engineering Inc. Traffic and Transportation (LOS Engineering Inc. 2023) conducted a local transportation study for the project (Appendix I to this EIR) including LOS analysis of the following nine intersections and road segments:

Intersections:

SR-76/I-5 SB Ramps
SR-76/I-5 NB Ramps
SR-76/Loretta Street
SR-76/Canyon Road
SR-76/Benet Road
SR-76/Foussat Road
Benet Rd/Airport Road
Benet Rd/Eddie Jones Way
Foussat Rd/Alex Rd

ROAD SEGMENT:

Eddy Jones to SR-76
Alex Road to SR-76
I-5 to Loretta Street

Loretta Steet to Canyon Drive
Canyon Drive to Benet Road
Benet Road to Foussat Road
East of Foussat Road

The transportation study included six traffic scenarios, which included Existing, Existing plus Project, Near Term (18 cumulative projects), Near Term plus Project, Horizon Year 2030, and Horizon Year 2030 plus Project. As presented in the Local Transportation Study (Appendix I):

- Under “Existing conditions”, the study elements were calculated to operate at LOS D or better except for segment #6: SR-76 from Benet Rd to Foussat Rd.

- Under “Existing plus Project” conditions, the study elements were calculated to operate at LOS D or better except for segment #6: SR-76 from Benet Rd to Foussat Rd.

- Under Near Term (Existing + Cumulative) conditions, the following study elements were calculated to operate at LOS E/F:

Intersection #5: SR-76/Benet Rd

Intersection #6: SR-76/Foussat Rd

Segment #4: SR-76 from Loretta St to Canyon Dr

Segment #5: SR-76 from Canyon Dr to Benet Rd

Segment #6: SR-76 from Benet Rd to Foussat Rd

Segment #7: SR-76 east of Foussat Rd

- Under Near Term (Existing + Cumulative) plus Project conditions, the following study locations were calculated to operate at LOS E/F AND the project has a transportation impact:

Intersection #5: SR-76/Benet Rd

The following roadways were calculated to operate at LOS E/F without an impact because the project traffic does not exceed the transportation impact thresholds defined in the Traffic Guidelines:

Intersection #6: SR-76/Foussat Rd

Segment #4: SR-76 from Loretta St to Canyon Dr
Segment #5: SR-76 from Canyon Dr to Benet Rd

Although much of the D-EIR states this project is “within guidelines”; **IS THIS WHAT WE WANT TO LEAVE THE CHILDREN OF OCEANSIDE?** If your children were subjected to the Federal and State Guidelines, are **YOU** okay with that? How has Federal and State Guidelines historically affected US? Subjecting our children to Lead, Carbon Dioxide, Vinyl Chloride, and the other pollutants listed in the D-EIR is **NOT THE WAY**. How much are YOU okay with when it comes to your children? This project, while acceptable **before** our residential community was conceived, is **not acceptable** now, 63 years later. Surely, Oceanside deserves better and can do better. Let’s, as a community and a City, promote family and human values ahead of commerce and blight.

While the information listed above lists factual D-EIR information, the entire process and D-EIR **ignores the “Human Factor”**. For example, there is a speed limit posted on every roadway. The law says you cannot exceed that limit, or you are violating the law, or the rule. Ask yourself: Have you ever violated the speed law? Do others violate the speed law? **THAT IS THE HUMAN FACTOR**. Please consider this too in making your decision.

Thank You,

Gene R. O’Neal Jr. / Carrie L. O’Neal and all my children and grandchildren.

478 Tishmal Court
Oceanside, CA 92058
(760) 470-4363

While the information listed above lists factual D-EIR information, the entire process and D-EIR **ignores the “Human Factor”**. For example, there is a speed limit posted on every roadway. The law says you cannot exceed that limit, or you are violating the law, or the rule. Ask yourself: Have you ever violated the speed law? Do others violate the speed law? **THAT IS THE HUMAN FACTOR**. Please consider this too in making your decision.

Thank You,



Gene R. O’Neal Jr. / Carrie L. O’Neal and all my children and grandchildren.

478 Tishmal Court
Oceanside, CA 92058
(760) 470-4363

Robert Dmohowski

From: Lauren Entler <lentler@cox.net>
Sent: Tuesday, November 7, 2023 12:35 PM
To: Planning-Planning Commission
Subject: Vote NO on Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Members of the Planning Commission,
Please vote against the Eddy Jones Warehouse project. The number of semi-trucks travelling on Benet Rd will interfere with our travel to and from our homes as well as all of the 76 highway traffic. This project is too large and will have too much of a negative impact on our community. A much smaller project would be more acceptable. In addition, other things to consider are the air pollution from trucks and water pollution of the San Luis Rey river. Also consider the safety hazard if residents had to evacuate due to fire.

Please vote against this project as it is written now.

Sincerely,

Lauren Entler

3563 Coconut Way

Oceanside, Ca

Robert Dmohowski

From: Nick Torelli <nicktorelli52@gmail.com>
Sent: Wednesday, November 15, 2023 4:54 PM
To: Robert Dmohowski; Esther Sanchez; Zeb Navarro; Eric Joyce; Rick Robinson; Peter Weiss; Planning-Planning Commission; City Manager; Ryan Keim
Cc: oceansidespeaksout@gmail.com
Subject: No on EJ project

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear Honorable Mayor, Deputy Mayor, Council Members, Manager, Clerk, and the Oceanside Planning Commission.

My name is Nick Torelli and I am a district 1 resident. I am writing today to communicate to City Personnel that I am strongly opposed to the Eddie Jones Warehouse Project. Though I am not in the immediate airport neighborhood area, I will suffer the impacts of the project, which are many.

Negative impacts:

Traffic

Noise pollution

Eye pollution

Pollution pollution

Nominal monetary benefits to city and out of step with other approved projects

Above all it is unsafe for residents in proposed effected area due to limited egress and ingress during a fire season, that essentially runs all year long, and for medical personnel to get in and out in a medical emergency.

I listened to many of the folks sporting blue explaining to Council and Planning clearly why this project is not good for Oceanside and Oceanside residents. For whom then is it good for? I, along with many residents are not against development, we just want responsible and honest developers. Oceanside is up and coming, and in Johnathan Borrego's words, it is a city that has some pretty streamlined planning processes in place in comparison to many other jurisdictions, so it is relatively easy to attract developers here. With that in mind I urge the city to deny this project when the time comes and hold out for a more on brand, resident backed

development, something light on negative impacts and weight forward in regards to job opportunity and income for the city.

Thank you all for your time and consideration.

Nick Torelli

Robert Dmohowski

From: Tam Nguyen <tn9936@gmail.com>
Sent: Friday, November 17, 2023 4:05 PM
To: Robert Dmohowski; Esther Sanchez; Rick Robinson; Peter Weiss; Planning-Planning Commission; Zeb Navarro; Eric Joyce; Ryan Keim; City Manager
Subject: OPOSITION to the Eddy Jones Project
Attachments: Eddy Jones-Benet Rd underpass.jpg

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

We are writing to you to let you know that we are homeowners of Wanis View Estates HOA in Oceanside who strongly oppose the proposed project. The current plans include 114 semi-truck bays, 60 truck-trailer parking stalls and 590 auto parking spaces with a facility of 566,905 square feet stated in the Draft EIR (roughly the size of 4 Costcos), and trucks are anticipated to run 24/7. With that project scale, and its operations, our community and all residents of surrounding areas will be negatively impacted especially on traffic and noise.

1) **Traffic:** Below are the major traffic issues on the Benet Road:

1.1) **Enormous amount of trucks to the Benet Road:** Tractor/trailer/truck ingress/egress would be designated for and limited to the Benet Road access (item 3.2.4.1 Vehicular Circulation and Access.) With 114 loading bays and 60 tractor/truck/trailer parking stalls (item 3.2.4.2 Parking), it is obvious that an enormous amount of trucks from the proposed project will share the Benet Road with local traffic between the Eddy Jones Way and the highway 76 and will run and turn slowly to exit and enter the Eddy Jones Way.

1.2) **Eddy Jones Way/ Benet Road intersection very close to Benet Road/Highway 76 intersection :** It is about 1000 feet (item 3.2.4.1 Vehicular Circulation and Access.) between 2 intersections. Clearly, any 2 intersections that are too close to each other would be avoided because it would create traffic congestion in that area especially by heavy/long trucks with their slow movement.

1.3) **California Department of Transportation (Caltrans) controls the signal timing** at the highway 76/ Benet Road intersection. Caltrans always keeps the traffic on the State highway in priority resulting in delaying local traffic on the Benet Road. Now, local traffic can get through the highway 76 with one timing cycle only. If the project is implemented, local traffic would not get through that State highway with several timing cycles due to an enormous amount of trucks sharing the Benet Road with the local traffic.

1.4) **The proposed project would contribute traffic to a failing intersection:** Appendix I admitted. If adding an enormous amount of trucks to the Benet Road, that signal intersection would get worse.

With the major traffic problems above-mentioned, below are only two tiny improvements on vehicular circulation and access that the project proposes:

- a) Enter and exit the project site via the Alex Road and the Benet Road would be improved to full commercial driveway standards (item 3.2.4.1 Vehicular circulation and access.) This improvement is very little that does not solve the huge traffic congestion on the Benet Road due to trucks from the project sharing with the local traffic on the Benet Road.
- b) **Payment of 8.5%** towards the provisions of converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction (item 4.14.4 Impact Analysis - Roadway

Facilities.) This improvement would help some degree on the State highway 76 traffic, but it definitely would not solve the root cause of the local traffic congestion due to trucks from the project sharing with local traffic on the Benet Road.

If the proposed project is implemented, it would be a nightmare for all local residents and others using the Benet Road. Also, it would put the local authorities under the pressure to solve the bottleneck/choke point at the downstream end and the upstream start traffic on the Benet Road northbound and southbound at the highway 76. The root cause of the problem is to add an enormous amount of trucks from the proposed project via the Eddy Jones Way to the Benet Road that is too close to the Highway 76 signal intersection that Caltrans controls timing.

It appears that the project would need a separate lane without interference with the local traffic on the Benet Road. Following is the idea for that lane.

- a) Depress the Benet Road as an underpass at the Eddy Jones Way, then back it up at grade at the highway 76 as it is.
- b) Extend the Eddy Jones Way westward with a new signal intersection at the highway 76 (sketch attached) for traffic in and out the Eddy Jones project including passenger vehicles. In the Draft EIR, passenger vehicles would be limited to the Alex Road access point.

2) Noise

The residential Wanis View Estates HOA in Oceanside was established about 20 years ago without any noise as the project's that it is anticipated to operate 24/7. We oppose its operations at night which is to strictly curfew its operations at night.

Our community residents are not opposed to site development; however, the type and scale of business and hours of operations that impact our community and our quality of life are not being seriously nor adequately addressed.

Respectfully

Tam Nguyen 3211 Toopal Dr Oceanside, CA 92058 tn9936@gmail.com

Minh Nguyen 3211 Toopal Dr Oceanside, CA 92058

Tuyet Nguyen 3144 Toopal Dr Oceanside, CA 92058 tuyetdn@cox.net

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Tuesday, November 21, 2023 9:29 AM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Peter Weiss; Rick Robinson; Eric Joyce; Planning Web; City Manager; Planning-Planning Commission; oceansidespeaksout@gmail.com; eddiejoneswarehouse@gmail.com; Jonathan Borrego
Subject: EDDIE JONES WAREHOUSE AND MANUFACTURING

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

VOTE NO !..

To involved city staff, council,

I have contacted the city on this project a few times in the last several months, and now the draft EIR is out and I will attempt to review its \$400+ pages. Hopefully my prior emails have been read by all and their content is understood and remembered. This is not a good project for this site.

First, I wish to address the CUP, (as far as I understand it.)

1. This project is to be constructed and zoned for a maximum of 6 trucks as per the draft EIR. . This project is requesting 114 trucks, with an additional 60 truck parking spaces. It also includes 590 additional parking spaces for personal vehicles. That is an astronomical request.
2. Projects along the river are required to be made to be above the floodplain. This project is requesting to build flood walls around the property instead. According to the developer, those will be 7 to 9 foot walls around the entire perimeter of the property. That will be a visual eyesore.
3. The site is zoned "light industrial" or "limited industrial". I am not sure how Oceanside defines those terms but these are from the internet. This project is way beyond these definitions.

Light industrial: means buildings used solely to house industrial use that are not more than one story in height and not exceeding 10,000 square feet in gross floor area, or are not more than two stories in height and not exceeding 6,000 square feet in gross floor area :

Light industrial: means uses characterized by a mix of manufacturing (small items), service, and warehouse facilities in the same building with a wide variation in the proportion of each type of use. Examples of light industrial uses include materials testing laboratories, assembly of data processing equipment, contractor offices, cabinetry work, machine shops, management services, photocopying services, software publishing/production, engineering/architectural services, and electronic/computer component production..

If this project is light industrial, you have to ask, "why is this project so large, and why are there 114 truck loading docks?"

3. Supposedly, if a project will be a detriment to the value of the properties around it, it is not approvable. This facility and its use and traffic will reduce local values.

4. There was a mention that the site redevelopment should use existing trees in the design of the site. This didn't happen. It should be noted that there were over 3 dozen trees on the site that were removed. Several were large trees that housed local animals and birds, including hawks. Many of the larger ones were on the northern property line and could have, should have, been saved.

A conversation of why this isn't a good fit.

1. This project is 566,280 square feet.. That is 13 acres of building, or in another perspective, including the end zones, around 10 football fields. It is also 45 feet tall, which could be a 5 story building, at the least a 4 story.. It will also have a 7 to 9 foot tall flood wall around its nearly 32 acres. That's nearly a mile of wall. It will be over 25 acres of asphalt and concrete. There's not another facility along the San Luis Rey River (possibly in all of Oceanside) that even resembles this monstrosity.
2. Based on the site and building/architectural plans, this project will be a warehouse and distribution center and will more than likely run 24 hours a day, 7 days a week. (Even if manufacturing, that is more than likely). So not only will we have dozens to hundreds of trucks daily and around the clock, there will be shift changes creating two to three times the traffic throughout the day. As proposed, all the trucks will be on Benet, personal vehicles, Fousat. A mess.
3. The 76 is already "not" an expressway. It has signal lights along its entire length and at any time of day you can sit through three or four, or more, light changes to move (Rancho Del Oro, College, Santa Fe to name a few), and even at Benet (the road slated for all truck traffic), traffic can be backed up west past Canyon Road, and the north I-5 exit can be backed up to the east to Loretta.
4. At Benet Road, the road for all truck traffic from the site (Eddie Jones Way), to the 76, it is only around 700 to 800 feet in distance. Trucks in California are allowed to be up to 75 feet long with truck and trailer. With a safety zone between trucks, perhaps 7 to 8 trucks will fill the road, and local traffic will be in that mix/mess or backed up across the river. And the turn lanes are even shorter.
5. The acceleration of trucks is also a large problem. If you're behind any number of trucks turning to the 76 or vice versa, there's a high likelihood you won't make the turn. And on the 76 itself, imagine dozens to hundreds of additional trucks vying for space and slowly accelerating at each light. The 76 will become more of a parking lot than it already is.
6. Realizing this will be a 24/7 facility you have to rethink not only the traffic, but the night light, the noise, the pollution.. The facility has numerous windows for use of daylight, but those will also throw light out during the night operations. The trucks and shift changes require headlights at night, as well as a well lit parking lot for employee safety. The noise at night will also be an issue as the open doors during loading or for ventilation will allow the interior work to be heard, the back-up beepers of trucks and forklifts, and the noise from any diesel vehicle will disturb the night. And, with most, if not all, the trucks being diesel, as well as some personal vehicles, plus the possible hundreds of trucks a day and personal vehicles at shift changes, the air pollution can not be ignored.
7. Having spoken to several realtors about the project, they all concur that having to deal with only two access roads and the possible hundreds of more vehicles on Fousat and especially the possible hundreds of trucks on Benet, the value of the cross river real estate will be reduced and people will look to other areas. The influence on the 76 will be a large part of this issue as well. That should be another reason this project is not approvable.
8. The area around the site is filled with open space: the river, the open space to the west of Benet, the undeveloped properties along Alex Road, the canyon north of the east end of Airport Road and all the open space surrounding the north river community (over 1,200 homes) by city open space and Camp Pendleton. How will fires and exiting be dealt with should all the citizens, trucks and employees be on the only two roads?
9. What is the criteria should the tenant utilize hazardous materials or combustible/explosive ones? Will it be allowed?
10. It still amazes me that projects are only required to notify properties within 500 feet of a development with the intent to build. For this project, due to north of river housing and the adjacent airport it was expanded to 1500 feet. That still only informed a few dozen homes north of the river and a few businesses along the airport/Airport Road.

If the remainder of north Oceanside, or the citizens of Bonsall, Fallbrook, Vista, and even San Marcos and Carlsbad, that use the 76, were informed of this project, there would be a lot more opposition.

I hope this correspondence gives light to the fact that this project should not have its CUP approved and isn't the right project for this location.

Thanks for your time. If you wish to discuss this, feel free to contact me.

Rick Taylor
735 Valley Crest Drive
Oceanside, CA 92058
rkt.sce@gmail.com
760 274 4700

Robert Dmohowski

From: Carol <memaeeee@aol.com>
Sent: Saturday, December 2, 2023 2:59 PM
To: Planning-Planning Commission
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Subject: Eddie Jones Project

I am strongly opposed to the location of the Eddie Jones Project. This project causes many negative impacts on our nearby communities.

I would like to suggest the following ideas as a better use for the property off Benet and SR76.

1) **AIRPORT HANGARS:** There is a short supply of small aircraft hangars and due to the close proximity to our Oceanside Airport, this would be a desirable location for new hangars.

Perhaps a few private homes with easy access to the airport, that would include a taxi - runway to the airport.

2) **NEW CAR DEALERSHIP:** Oceanside could use a new car dealership! Reasonable business hours, no big traffic impact, or impact on our environment, lots of revenue for the city/state. Quiet nights, so the Abby can continue serving our community as a peaceful retreat destination for locals and visitors alike.

3) **BOWLING ALLEY WITH ARCADE:** This would attract locals and visitors to our community. Reasonable business hours, little traffic impact.

Just a few ideas that would not interfere with the lives or safety of our (your) Oceanside residents. A few suggestions that would be a better alternative to the mega project proposed.

Please do what's right for our community. **HOW WOULD THE CITY OF OCEANSIDE AND ITS RESIDENTS BENEFIT FROM EDDIE JONES PROJECT? They would not. it would not bring one tourist or tourist dollar to Oceanside. It would not make the residents or city planners/ councilmen, proud of Oceanside. HOW DOES THIS MASSIVE PROJECT FIT INTO THE VISION OF OCEANSIDE MOVING INTO THE FUTURE. It does not.**

Kindly think carefully about the lives you are affecting when it's time to approve/ disapprove this project.

**SINCERELY,
A PROUD RESIDENT OF THE AIRPORT COMMUNITY,
Carol Broeland**

From Carol's iPad

Robert Dmohowski

From: Amanda Solis <amandasolis714@gmail.com>
Sent: Saturday, December 9, 2023 4:33 PM
To: Planning-Planning Commission; Sergio Madera
Subject: Concerns Regarding Negative Impacts of Proposed Eddie Jones Distribution Center on Oceanside Residents and Nature/Wildlife

Warning: External Source

Dear Members of the Oceanside Planning Committee,

I am writing to express my concerns regarding the proposed Eddie Jones Warehouse, Manufacturing, and Distribution Facility Project (D22-00001) and its potential adverse effects on Oceanside residents and the local environment.

1. Trip Generation Estimates and Mitigations:

- The trip generation estimates and mitigations in the LTS and VMT analyses raise significant concerns. Firstly, the estimates appear to be based on potentially outdated or irrelevant data, and they do not account for worst-case scenarios, particularly the maximum number of trips associated with a distribution center. This lack of specificity poses a risk of underestimating the project's true impact on traffic and greenhouse gas emissions. Please refer to Appendix 1 for detailed supporting comments on this issue.
- The VMT mitigation, in the form of a Voluntary Employer Commute Program, while addressing employee travel to and from the project site, fails to consider the broader impact of distribution center-generated trips. Furthermore, the effectiveness of such a program is uncertain, as there are no guarantees that employees will participate. Consequently, the reduction in VMT using this mitigation measure appears to be grossly overestimated.
- The LOS indicates that numerous segments and intersections along the 76 will be affected by cumulative projects, including the proposed distribution center. The proposed fair share payment as a mitigation strategy lacks assurance that it will address identified segments or intersections with an F or E operating condition. Similar concerns apply here as with the Voluntary Employer Commute Program.

2. Nature of the Facility:

- The involvement of the same architectural firm in both this project and the Amazon distribution center proposed on Ocean Ranch Boulevard prompts questions about the nature of the Eddie Jones facility. Supporting sources for this claim can be found in Appendix 2 for clarity. If the intention is for the facility to serve as an Amazon distribution center, a critical examination of its necessity and benefits for the City of Oceanside and its residents becomes imperative. Considering the already accessible quick delivery options for Amazon packages, this project could adversely affect Oceanside residents, visitors, and passersby traveling along the 76, with no evident advantages for current or future residents. The absence of project alternatives that exclude a distribution center further raises concerns about the project's intentions and potential negative impacts.

3. Ecological Significance of Proposed Location:

- The proposed location's proximity to the San Luis Rey River, a unique and ecologically significant area, is alarming. The River & Trail, cherished by residents, wildlife, and tourists, stands to suffer from the introduction of a 24/7 operation. This departure from the current serene environment could significantly impact the quality of life for residents and wildlife and diminish the area's appeal to tourists. The substantial increase in building footprint from the previous facility further emphasizes the potential detriment to the local environment.

4. Undesirable Precedent for Development:

- Approving this project may set an undesirable precedent for development along the San Luis Rey River and the 76 corridor, both of which are integral to our community's well-being. The potential for 24/7 activity near the 76 and the River and Trail raises concerns about lighting, noise, pollution, and increased traffic. Such approval could pave the way for future proposals compromising the quality of life for residents and wildlife.

I urge the Planning Committee to carefully consider these concerns, explore alternative options which exclude a distribution center, and prioritize the preservation of Oceanside's unique environment and the well-being of its residents.

Sincerely,

Amanda Solis
Oceanside, 92057
amandasolis714@gmail.com

Appendix

[1] The LOS and VMT analyses use estimates for daily car and truck trips based on guidelines from the ITE Trip Generation Manual, 11th Edition. The Land Use: 140 Manufacturing and Land Use: 154 High-Cube Transload and Short-Term Storage Warehouse data are based on sites surveyed in multiple decades in various states.

The Trip Generation Appendices on the ITE website (<https://www.ite.org/tripgenappendices/>) provide an Excel table called "Time-of-Day Distribution - Truck" for Land Use: 154 High-Cube Transload and Short-Term Storage Warehouse, with distribution truck trip numbers based on a survey of only 3 sites. The reliability of these sites as representatives of distribution center traffic in San Diego in 2023 is questionable. Clarification is needed to ensure that the data is not outdated or irrelevant.

[2]

"Oceanside council overturns approval of Amazon distribution center", San Diego Union Tribune, Phil Diehl, <https://www.sandiegouniontribune.com/communities/north-county/oceanside/story/2021-08-04/oceanside-overturns-approval-of-amazon-distribution-center>

"Will Oceanside get a stealth Amazon warehouse?", San Diego Reader, Ken Leighton, <https://www.sandiegoreader.com/news/2022/mar/18/Stringers-oceanside-get-stealth-amazon-warehouse/>

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Monday, August 29, 2022 1:29 PM
To: Esther Sanchez
Cc: Ryan Keim; Kori Jensen; Christopher Rodriguez; Peter Weiss; Robert Dmohowski
Subject: Eddie Jones Warehouse

Categories: Red Category

Warning: External Source

Subject: Proposed: Manufacturing/distribution Center at 250 Eddie Jones Way, Oceanside

I am a retired civil/structural engineer that lives in the community on the north side of the San Luis Rey River between Benet and Foussat Roads. I have never been a NIMBY as things need reasonable evaluation, and projects were my company's life blood.

But this project just doesn't fit the area. There are only two roads that access this north river community (Benet/Foussat) and Highway 76 IS NOT an expressway. Highway 76 can be a traffic nightmare most times of the day.

With the waterpark and housing development moving forward at Foussat and Mission, traffic on already busy roads just doesn't warrant adding hundreds of new vehicles, in particular semi-trucks.

Additionally, from review of the proposed plans, the existing Deutsch park buffer to the river is abandoned and the project is being allowed to have asphalt parking right up to the river trail. This area has even been posted as "protected habitat" by the city and the assumed site activity is going to disrupt the environment and animal life in the area, and likely 24 hours a day. Why would this be allowed?

The children's skate park and biker/hiker river trail access on Alex Road would be an accident ready to happen.

Therefore, my wife and I, and I know my neighbors are as well, are against this type of project being placed at that location. Something less intrusive must be found. Please do not approve this type of project at that location.

Thanks for your time.

Rick and Sue Taylor
735 Valley Crest Drive
Oceanside, CA 92058
760 274 4700

--

Rick Taylor Principal PE
SCE
(Structural Consulting Engineers)
rkt.SCE@gmail.com 760 274 4700

Robert Dmohowski

From: Mikhael Madello <mikhaelmadello@gmail.com>
Sent: Friday, August 26, 2022 4:14 PM
To: Robert Dmohowski
Subject: Eddie Jones project

Categories: Red Category

Warning: External Source

As homeowners in the neighborhood, we could not agree more that this development proposal would be detrimental. In addition to creating traffic and noise issues, our young daughters skateboard at the adjacent skate park. We are very concerned with the pollution of this facility would produce. Please consider the consequences and vote AGAINST this project.

Very respectfully,

Mikhael and Dylan Madello
732-720-7547

Sent from my iPhone

Robert Dmohowski

From: ken bahadar <kenbaha@gmail.com>
Sent: Monday, September 5, 2022 11:57 AM
To: Robert Dmohowski; ken bahadar
Subject: AGAINST EDDIE JONES DEVELOPMENT

Categories: Red Category

Warning: External Source

Please this is wrong to do.

Ken bahadar
349 rimhurst ct
Osida
760.828.8810

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

I am a member of the community immediately north of the proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility located at 250 Eddie Jones Way in the city of Oceanside. I am against the establishment of this facility for the following reasons:

1. This community has ONLY TWO ingress and egress roads; Foussat Road and Benet Road. There is no other way to access any point south of the San Luis Rey River by car or by foot. There is no other way for our community to receive emergency services. With severe congestion expected with the development of Ocean Kamp, a 79-acre development containing retail, office space, residential units, rental units, and a large wave park; this congestion is expected to choke off access to our community via use of Foussat Road.

With the introduction of the Eddie Jones Warehouse, Manufacturing & Distribution (with 114 semi-truck terminals, and 60 truck parking spaces/auto parking), allowing such project which will effectively choke off Benet Road not to mention creating severe congestion on SR-76 around Benet Road and Foussat Road. This community will effectively be cut off from Oceanside.

2. Noise easily travels into our neighborhood from the shooting range and the airport which is farther away than the proposed Eddie Jones Warehouse project. We have chosen to live in this area even with our small airport noise, but this project will inject a severity of sound we do not want. In as much, a project this large, with potentially 24-hour operations, will flood our neighborhoods with noise from forklifts, (OSHA) semi-truck/tractor engines, manufacturing noise and light pollution that will detrimentally affect the entire area north of this proposed project affecting roughly 2000+ homes.
3. It is reasonable to assume that manufacturing often uses hazardous materials which, with a facility this size (547,320 SF), could expose this entire community (roughly 2000+ homes/4000+ people) to exposure and catastrophic losses in the event of a fire or spill.
4. Air quality will also suffer with the expected semi-truck traffic (diesel exhaust/fumes) effectively harming the Alex Road Skate Park, immediately adjacent bike path, Ocean Kamp project, as well as our entire community.

These are our concerns and the concerns of our neighbors. This project is not right for this community or this area. In essence, our community will become choked off by industry. Our home values will suffer. Our health will suffer. Our children will suffer. Our very survival will suffer. Please do not let this happen.

Name (Printed) Victor Ardonin

Address: 3278 Brightwood Ct Oceanside

Signed: _____

Oceanside, CA 92058

Date: 08/31/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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With the introduction of the Eddie Jones Warehouse, Manufacturing & Distribution (with 114 semi-truck terminals, and 60 truck parking spaces/auto parking), allowing such project which will effectively choke off Benet Road not to mention creating severe congestion on SR-76 around Benet Road and Foussat Road. This community will effectively be cut off from Oceanside.

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3. It is reasonable to assume that manufacturing often uses hazardous materials which, with a facility this size (547,320 SF), could expose this entire community (roughly 2000+ homes/4000+ people) to exposure and catastrophic losses in the event of a fire or spill.
4. Air quality will also suffer with the expected semi-truck traffic (diesel exhaust/fumes) effectively harming the Alex Road Skate Park, immediately adjacent bike path, Ocean Kamp project, as well as our entire community.

These are our concerns and the concerns of our neighbors. This project is **not right** for this community or this area. In essence, our community will become choked off by industry. Our home values will suffer. Our health will suffer. Our children will suffer. Our very survival will suffer. Please do not let this happen.

Name (Printed) Eddie Avdonin

Address: 3278 Brightwood Ct, O'Side

Signed: Eddie Avdonin

Oceanside, CA 92058

Date: 08/31/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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Name (Printed) Amber Puka

Address: 328 Rimhurst Ct O'side 92058

Oceanside, CA 92058

Signed: Amber Puka

Date: 08/ /2022

9/4/22

(We've been traveling.
Hope the later reply is helpful.)

JOANNE PILGRIM
641 Tukmal Drive
Oceanside, CA 92058
Cell: (760) 712-6221

November 21, 2022

Rob Dmohowski
City of Oceanside Principal Planner
300 North Coast Highway
Oceanside, CA 92057

Re: Proposed Eddie Jones Warehouse, Manufacturing
And Distribution Facility

Dear Mr. Dmohowski:

I just learned of your disastrous proposal to build a warehouse and other leased properties, along with hundreds of parking spaces for visiting personnel, between the municipal airport and our properties north of the San Luis Rey River.

I am a homeowner in the Wanis Estates home community just north of the San Luis Rey River, along with 300 other families in the community. To the north of us is probably another 300 homes that also will be impacted by this project. Your proposed project will completely destroy our only ingress and egress exits to our homes and Highway 76 since the only choice is either Foussat Road or Benet Road. With the Ocean Kamp project now underway, both of these outlets will be completely blocked and cause numerous clogs and resulting accidents if this construction is approved.

As if this is not enough, your proposed project will destroy our land property values, bring in unwanted commercial/industrial development to a residential part of Oceanside and destroy a natural habitat environmentally. To say I am appalled you would even consider such a project is an understatement. Your proposed development will be an eyesore for Oceanside.

There are other areas in which to build a warehouse. Please do not destroy what we have worked so hard to enjoy. This will have a lasting impact on all homeowners currently near the San Luis Rey River, as well as all future investors and homeowners who want to reside in the Oceanside community, including those stationed at Camp Pendleton.

Sincerely,



Joanne Pilgrim

/jp

Robert Dmohowski

From: Jane Hodges <jhodges001@hotmail.com>
Sent: Tuesday, May 2, 2023 7:45 PM
To: Robert Dmohowski
Subject: Eddy Jones Project File Number D22-00001

Categories: Red Category

Warning: External Source

May 2, 2023

Dear Mr. Dmohowski,

I am writing in opposition to the Eddy Jones Warehouse, Manufacturing and Distribution Facility that is projected for Eddie Jones Way and Benet Road, just Northeast of the Oceanside Airport(Project File Number D22-00001). My concerns are mainly regarding traffic type and volume, safety, air and noise pollution, as well as the environmental impact and impact on emergency services to residents in the area.

As a resident of Oceanside for almost 40 years, I am deeply concerned about this proposed development that would house semi-trucks and trailers, facilitate a 24 hour warehousing operation, and negatively impact the traffic flow on Mission Ave. and Highway 76. These 2 thoroughfares are already grossly congested during peak commuting hours and have been even more so with the current construction on Highway 78. Residents, businesses and recreational facilities along that road would all be negatively impacted. The Ocean Kamp development is already expected to increase traffic issues on Fousat Rd., Mission Ave. and Highway 76!

Your support of the building of this proposed facility is very disappointing. As an active community member and voter, I keep close watch on the representatives of Oceanside. I do not feel the residents have been made aware of the impact this proposal will make on the well-being of the citizenry of our beautiful city of Oceanside. I realize our representatives are constantly looking for ways to increase the income to the city, but THIS is not the answer! The residential communities in close proximity to this proposed facility will suffer immeasurable impact, ranging from noise and air pollution to major safety concerns for the children who frequent the nearby establishments(skate park, restaurants, and retail stores) using the same thoroughfares(state route 76 and Mission Ave) that these large semi-trucks will be entering/exiting and traversing on.

I strongly urge you to withdraw your support of this disastrous proposal for development. The citizens of this city depend upon you to act in our best interests.

I look forward to hearing your response with regard to my concerns stated above. Thank you.

Jane Hodges

**Jane Hodges
380 Islander St.
Oceanside, CA 92054**

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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Name (Printed) Jeff Verryo

Address: 760 Volley Crest Dr.

Signed:  _____

Oceanside, CA 95058

Date: 8/10/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92054

E-Mail: rdmohowski@oceansideca.org

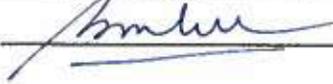
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Name (Printed) Binh Lue
Address: 740 Wala Dr. Oceanside CA 92058
Signed: 

Oceanside, CA 92058

Date: 08/25/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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Name (Printed) Steven Salado
Address: 3278 Spring Brook Ct.
Signed: [Signature]

Oceanside, CA 92058

Date: 08/___/2022

7/12/2022

Robert Dmohowski

From: Ramona McGinnis <ramona@pure-av.com>
Sent: Wednesday, August 7, 2024 10:32 AM
To: Robert Dmohowski
Cc: ramona@pure-av.com
Subject: Written Protest Against Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

August 7, 2024

Rob Dmohowski
Principal Planner
300 N. Coast Hwy.
Oceanside, CA

Dear Rob Dmohowski and Members of the City Council,

I am writing to express my strong opposition to the proposed Eddie Jones Project as outlined in the recent article from The Coast News. The project, which threatens to disrupt the preserved nature corridor in our community, is deeply concerning and warrants a thorough reconsideration.

Our nature corridor is a vital component of our local ecosystem, providing essential habitat for wildlife and contributing to the overall environmental health of our area. The corridor not only supports biodiversity but also offers residents valuable green space for recreation and relaxation. Encroaching on this protected area for development would have significant adverse effects on both the environment and the quality of life for our community members.

The potential environmental impacts of this development cannot be overstated. The project could lead to habitat destruction, increased pollution, and loss of natural resources that are crucial for maintaining ecological balance. Furthermore, the construction and subsequent activity associated with the project could result in long-term negative effects on local wildlife and plant species.

In addition to environmental concerns, many residents have voiced worries about the increased traffic and strain on local infrastructure that the project would bring. Our community is already experiencing traffic congestion and infrastructural challenges; adding a large-scale development would exacerbate these issues, leading to further detriment of our neighborhood's character and livability.

I urge the you and the City Council to reconsider the Eddie Jones Project and explore alternative solutions that align with our community's values and priorities. It is imperative that we protect our natural spaces and seek sustainable development practices that do not compromise our environmental and community well-being.

Thank you for your attention to this matter. I trust that you will carefully weigh the potential consequences and prioritize the preservation of our precious natural resources.

Sincerely,

Ramona McGinnis
Concerned Oceanside Resident
179 N. Barnwell Street
714-856-5464

Robert Dmohowski

From: Autumn Ferrante <autumnferrante@gmail.com>
Sent: Friday, September 30, 2022 11:36 AM
To: Robert Dmohowski
Subject: Re: Amazon Distribution Center

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Hi,
I am checking in to see what the status is on the project? Please let me know who to contact if it's not you.

Thank you,
Autumn

On Sun, Aug 14, 2022 at 11:48 AM Autumn Ferrante <autumnferrante@gmail.com> wrote:

Hi Mr. Dmohowki,

I am writing today to express my concern about the Amazon distribution center being proposed to take over and expand the old Duetch building on Benet and Airport rd.

We have over 600+ homes surrounding the proposed site. Hundreds of kids and adults skateboard, bike, and walk along Benet and Foussat Rd. Hundreds of houses that are close to the building that would be expanded to accommodate Amazon would be subjected to 24/7 truck and operations noise.

We also have the Oceanside Kamp project that was just approved less than a mile from the proposed Amazon distribution center. We have no idea how the Oceanside Kamp project is going to impact our area and then to add an Amazon Distribution center with the constant traffic it will bring doesn't make sense and seems very irresponsible.

We are a residential area. Amazon needs to find a home in an industrial area that will have less impact on the surrounding community.

Thank you,
Autumn Ferrante

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92054

E-Mail: rdmohowski@oceansideca.org

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1. This community has **ONLY TWO** ingress and egress roads; Foussat Road and Benet Road. There is no other way to access any point south of the San Luis Rey River by car or by foot. There is no other way for our community to receive emergency services. With severe congestion expected with the development of Ocean Kamp, a 79-acre development containing retail, office space, residential units, rental units, and a large wave park; this congestion is expected to choke off access to our community via use of Foussat Road.

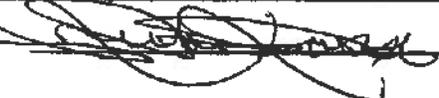
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Name (Printed) RUBEN RAJSD

Address: 3436 SOYLA DR.

Signed: 

Oceanside, CA 92058

Date: 08/7/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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Name (Printed) Poly Powers

Address: 3205 Brightwood Ct. Oceanside CA 92058

Signed: Poly Powers

Oceanside, CA 92058

Date: 08/1/2022
09/21/2022

SCOPING MEETING – EDDIE JONES INDUSTRIAL PROJECT

6:00 p.m. on August 3, 2022

Civic Center Library Community Room, located at 300 North Coast Highway in the City of Oceanside

Name (first, last)/email: Terry Tovar/heartgems68@gmail.com

Comments:

- ① Increasing traffic without expanding the infrastructure (i.e.: widening streets) will only choke access, affecting emergency support.
- ② Global warming is such an important fact that allowing for increasing greenhouse gases from so many large trucks will result in rising sea levels that will decrease the life span of this proposed project. Unless the trucks are 100% non-polluting OR a giant enclosed air purification system is included with this project, everyone's standard of living will drop unacceptably.
- ③ As an Oceanside resident since 1979, the lack of the appropriate detailed analysis of this proposed project's effect on the nearby residents' standard of living is a damning reflection on the Oceanside Planning Office.
- ④ Why not plan on expanding the airport.

Please return comments to:
Development Services Department
Attn: Rob Dmohowski, Senior Planner
300 North Coast Highway
Oceanside, CA 92054
Fax: (760) 435-2958
Email: rdmohowski@oceansideca.org

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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Name (Printed) _____

Address: _____

Oceanside, CA 92058

Signed: _____

Date: 08/___/2022

Robert Dmohowski

From: Vicki Steyn <gmavicki70@gmail.com>
Sent: Saturday, October 22, 2022 10:50 PM
To: Robert Dmohowski
Subject: RE: Eddy Jones Development.

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

I am opposed to this development unless there are improvements in Hwy 76 and Benet Road to mitigate the increased traffic. As a resident who uses 76, the traffic is already congested so adding this development without also improving the infrastructure is a bad idea! I know that we need to develop the area but PROVIDE THE INFRASTRUCTURE TO SUPPORT THE DEVELOPMENT!

Respectfully,
Vicki Steyn

Robert Dmohowski

From: cantonmichaela@gmail.com
Sent: Thursday, November 17, 2022 4:21 PM
To: Robert Dmohowski
Subject: Eddy Jones development

Warning: External Source

My name is Micaela Canton, and I live at 3382 Soyla Dr, Oceanside CA 92058. I was just received the information in regard of the warehouse and I find odd that the city planning are allowing a ~570k sf. construction near housing and a future resort/ lagoon. The existing warehouse was small in comparison of this monstrosity . I and many residents are astounded to see the city even considering this project in the vicinity of a skate park where plenty of children come to play. I guess their health and safety are not on the council and city planners mind.

A concern resident,
Micaela Canton

Sent from my iPhone

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

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1. This community has ONLY TWO ingress and egress roads; Foussat Road and Benet Road. There is no other way to access any point south of the San Luis Rey River by car or by foot. There is no other way for our community to receive emergency services. With severe congestion expected with the development of Ocean Kamp, a 79-acre development containing retail, office space, residential units, rental units, and a large wave park; this congestion is expected to choke off access to our community via use of Foussat Road.

With the introduction of the Eddie Jones Warehouse, Manufacturing & Distribution (with 114 semi-truck terminals, and 60 truck parking spaces/auto parking), allowing such project which will effectively choke off Benet Road not to mention creating severe congestion on SR-76 around Benet Road and Foussat Road. This community will effectively be cut off from Oceanside.

2. Noise easily travels into our neighborhood from the shooting range and the airport which is farther away than the proposed Eddie Jones Warehouse project. We have chosen to live in this area even with our small airport noise, but this project will inject a severity of sound we do not want. In as much, a project this large, with potentially 24-hour operations, will flood our neighborhoods with noise from forklifts, (OSHA) semi-truck/tractor engines, manufacturing noise and light pollution that will detrimentally affect the entire area north of this proposed project affecting roughly 2000+ homes.
3. It is reasonable to assume that manufacturing often uses hazardous materials which, with a facility this size (547,320 SF), could expose this entire community (roughly 2000+ homes/4000+ people) to exposure and catastrophic losses in the event of a fire or spill.
4. Air quality will also suffer with the expected semi-truck traffic (diesel exhaust/fumes) effectively harming the Alex Road Skate Park, immediately adjacent bike path, Ocean Kamp project, as well as our entire community.

These are our concerns and the concerns of our neighbors. This project is not right for this community or this area. In essence, our community will become choked off by industry. Our home values will suffer. Our health will suffer. Our children will suffer. Our very survival will suffer. Please do not let this happen.

Name (Printed)

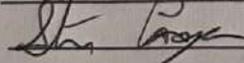
Steven Cooper

Address:

3289 Brightwood Ct., Oceanside, CA, 92058

Oceanside, CA 92058

Signed:



Date: 11/4/2022

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

I am a member of the community immediately north of the proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility located at 250 Eddie Jones Way in the city of Oceanside. I am against the establishment of this facility for the following reasons:

1. This community has ONLY TWO ingress and egress roads; Foussat Road and Benet Road. There is no other way to access any point south of the San Luis Rey River by car or by foot. There is no other way for our community to receive emergency services. With severe congestion expected with the development of Ocean Kamp, a 79-acre development containing retail, office space, residential units, rental units, and a large wave park; this congestion is expected to choke off access to our community via use of Foussat Road.

With the introduction of the Eddie Jones Warehouse, Manufacturing & Distribution (with 114 semi-truck terminals, and 60 truck parking spaces/auto parking), allowing such project which will effectively choke off Benet Road not to mention creating severe congestion on SR-76 around Benet Road and Foussat Road. This community will effectively be cut off from Oceanside.

2. Noise easily travels into our neighborhood from the shooting range and the airport which is farther away than the proposed Eddie Jones Warehouse project. We have chosen to live in this area even with our small airport noise, but this project will inject a severity of sound we do not want. In as much, a project this large, with potentially 24-hour operations, will flood our neighborhoods with noise from forklifts, (OSHA) semi-truck/tractor engines, manufacturing noise and light pollution that will detrimentally affect the entire area north of this proposed project affecting roughly 2000+ homes.
3. It is reasonable to assume that manufacturing often uses hazardous materials which, with a facility this size (547,320 SF), could expose this entire community (roughly 2000+ homes/4000+ people) to exposure and catastrophic losses in the event of a fire or spill.
4. Air quality will also suffer with the expected semi-truck traffic (diesel exhaust/fumes) effectively harming the Alex Road Skate Park, immediately adjacent bike path, Ocean Kamp project, as well as our entire community.

These are our concerns and the concerns of our neighbors. This project is not right for this community or this area. In essence, our community will become choked off by industry. Our home values will suffer. Our health will suffer. Our children will suffer. Our very survival will suffer. Please do not let this happen.

Name (Printed) Nancy Cooper
Address: 3289 Brightwood Ct.
Signed: Nancy Cooper

Oceanside, CA 92058

Date: 11/4/2022

Robert Dmohowski

From: Peter Komor <petekomor@comcast.net>
Sent: Wednesday, November 16, 2022 4:17 PM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Kori Jensen; Peter Weiss; Christopher Rodriguez
Cc: City Clerk; 4dees@cox.net
Subject: recent development

Warning: External Source

Hello,

We are very concerned regarding what appears to be the city councils recent approvals of rampant development projects in the Mission San Luis Rey/Airport areas. We moved here in July 2021 to retire and be near our children, and had we known what the city had in mind we most likely would not have purchased in Oceanside.

When we moved here we were aware of the senior housing project next to the mission itself but since that time we have heard that the city council has approved multiple massive projects in our immediate neighborhood.

1) In addition to the prior approval of a sports arena, the council has now approved a multi-story/multi-unit housing complex, and most recently a mixed use development including a biotech campus on the El Corazon property where, prior to the arrival of the bulldozers we watched Golden Eagles and Red Tailed Hawks soaring. The El Corazon property was originally a gift to the people of Oceanside for recreational use. That is has been divided up and sold off piecemeal is not in harmony with that original intent, as the development plans benefit only a few and not the population as a whole.

2) The Wave camp project (site of two private plane crashes in 2022).

3) A below market rate housing development at the intersection of Rancho del Oro and the 76.

4) The massive Eddy Jones warehouse distribution project near the airport. We will be strenuously fighting against this project.

All of this development was approved with little or no mention of infrastructure improvements. What the development plans are essentially doing is putting significantly more vehicles on our already crowded streets. The 76 is currently a nightmare much of the day given the stop lights and the mix of private cars, heavy

trucks, pedestrians and bicycles. The air quality here is worse than that of the SF Bay Area from which we moved, primarily from traffic related pollutants. Recent studies have shown a relationship between traffic related air pollution and dementia, a perfect storm for us given our age. This is not what we expected of our retirement.

In the recent election we voted to remove the current pro-development council member from our district. Hopefully the city council will begin to rethink its development policy to be more in line with the residents of Oceanside.

Peter and Carol Komor
360 Vista Marazul
Oceanside, Ca 92057

Robert Dmohowski

From: nadine ramirez <gankrn@yahoo.com>
Sent: Wednesday, November 9, 2022 3:02 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Ryan Keim; Kori Jensen; Christopher Rodriguez; Peter Weiss
Subject: OPPOSE EDDY JONES PROJECT
Attachments: EDDIE JONES WAREHOUSE CONCERNS.pdf

Warning: External Source

Dear Mr. Dmohowski,

As a resident of 710 Rivertree Dr., Oceanside, CA 92058. We strongly **oppose** the "Eddy Jones Warehouse Project" for concerns enumerated below in the attachment (please see attachment). My husband and I are **very concerned that the safety** of our neighborhood will be strongly impacted by the " EDDY JONES PROJECT" because of the increase traffic and hazardous conditions it will create. Please consider the **safety** of the residents of our community when planning your development! We hope you will listen to our voice.

Respectfully,

Nadine and George Dela Rosa

OUR CONCERNS

- THE INGRESS AND EGRESS TO THIS AREA IS LIMITED TO FOUSSAT RD AND BENET RD
- MAJOR TRAFFIC CONGESTION ON ALL CONNECTING ARTERIALS INCLUDING W/B AND E/B HWY76, N/B AND S/B I-5 WILL BE IMPACTED BY THIS PROJECT
- TRACTOR TRAILERS LEAVING THE WAREHOUSE WILL BLOCK INGRESS AND EGRESS ON BENET RD AS WELL AS CREATING HAZARDOUS CONDITIONS FOR PEDESTRIANS /BICYCLES
- SAFETY HAZARD FOR PRINCE PARK SKATEBOARDERS CREATED BY 500+ PRIVATE VEHICLES ENTERING AND EXITING AT ALEX ROAD OFF OF FOUSSAT ROAD
- HAZARDOUS AIR QUALITY CREATED DUE TO MULTIPLE TRIPS OF SERVICE/DELIVERY TRUCKS, TYPE OF FUEL UNKNOWN AT THIS TIME
- EXTREME SAFETY HAZARD FOR RESIDENTS CREATED DURING FIRE AND OR EMERGENCY EVACUATION
- EXISTING ROADWAYS AND BRIDGES ARE NOT CONSTRUCTED TO SUPPORT THE WEIGHT OF LARGE VEHICLES FOR EXTENDED USE
- PROJECT DEVELOPMENT IS NOT COMPATIBLE WITH AIRPORT NEIGHBORHOOD AND THE OCEAN KAMP DEVELOPMENT
- HOME VALUES FOR RESIDENTS WILL DECLINE DRAMATICALLY
- WE DO NOT OPPOSE DEVELOPMENT AS LONG IT IS WELL THOUGHT OUT AND IS CONDUCIVE TO THIS RESIDENTIAL AREA

Robert Dmohowski

From: Jeffrey Moore <JeffreyMoore@cox.net>
Sent: Monday, October 24, 2022 5:52 PM
To: Robert Dmohowski
Subject: Eddy Jones Project

Warning: External Source

Mr. Dmohowski,

I'm against the proposed warehouse by the airport because of diesel fumes affecting the airport community, because of noise affecting the airport community, and because of traffic affecting everyone along the 76 from the 5 clear out to Bonsall. The traffic jams should have been fixed long ago. To add to that traffic will be horrible.

Jeffrey Moore
Resident of Oceanside Airport Community

Robert Dmohowski

From: Walter Wilson <wjwreb@gmail.com>
Sent: Wednesday, October 12, 2022 5:31 PM
To: Robert Dmohowski
Subject: I OPPOSE Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Robert Dmohowski

From: Jonathan Montano <jonathan@mitchtsailaw.com>
Sent: Friday, October 21, 2022 12:13 PM
To: Robert Dmohowski
Cc: Stephanie Papayanis; Mitchell Tsai; Rebekah Youngblood; Hind Baki; Steven Thong; Malou Reyes; Barrie Brown Martinez; Maria Sarmiento
Subject: Re: SWRCC - [City of Oceanside, 250 Eddy Jones Way Warehouse Facility] - Project Status Inquiry

Warning: External Source

Good afternoon,

We are following up on the status of the **250 Eddy Jones Way Warehouse Facility Project** in Oceanside. Are there any upcoming hearings that are scheduled or any updates that are currently available?

Additionally, please let us know if there is another planner to direct our inquiries to.

On Fri, Oct 14, 2022 at 12:10 PM Jonathan Montano <jonathan@mitchtsailaw.com> wrote:

Good afternoon,

Our office would like to inquire on the status of the **250 Eddy Jones Way Warehouse Facility Project** in Oceanside. Are there any upcoming hearings that are scheduled or any updates that are currently available?

Additionally, we would like to request that the following email addresses be included in the project's Noticing List of Interested Parties:

info@mitchtsailaw.com
talia@mitchtsailaw.com
rebekah@mitchtsailaw.com
hind@mitchtsailaw.com
maria@mitchtsailaw.com
steven@mitchtsailaw.com
barrie@mitchtsilaw.com
malou@mitchtsailaw.com
jonathan@mitchtsailaw.com
mitch@mitchtsailaw.com

Thank you very much for your time.

--

Jonathan Montano
Paralegal
Mitchell M. Tsai, Attorney At Law
139 South Hudson Avenue Suite 200
Pasadena, CA 91101
Phone: (626) 314-3821
Fax: (626) 389-5414

Email: jonathan@mitchtsailaw.com
Website: <http://www.mitchtsailaw.com>

*** Our Office Has Recently Moved. Please Note New Mailing Address ****

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--

Jonathan Montano
Paralegal
Mitchell M. Tsai, Attorney At Law
139 South Hudson Avenue Suite 200
Pasadena, CA 91101
Phone: (626) 314-3821
Fax: (626) 389-5414
Email: jonathan@mitchtsailaw.com
Website: <http://www.mitchtsailaw.com>

*** Our Office Has Recently Moved. Please Note New Mailing Address ****

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Robert Dmohowski

From: Brian Samoska <briansamoska@gmail.com>
Sent: Sunday, November 20, 2022 11:41 PM
To: Robert Dmohowski
Subject: stop the building

Categories: Red Category

Warning: External Source

I am emailing you to voice my concern about the warehouse that could go in near the airport area...we DO NOT want this to happen. We already deal with a great amount of traffic on the 76 and this will make it worse. We only have 2 ways in and out for our airport community and having large trucks going through all day long, will not be an easy commute for us neighbors. We already have a great deal of traffic with the tow truck company nearby and other various things. This will also impact our property values that we work hard to stay on top of in our wanis view community.

Thank you for your time with reading this.

Sincerely,
Concerned resident
Brian

Robert Dmohowski

From: Sandy Browder <scbrowder7@gmail.com>
Sent: Wednesday, October 26, 2022 10:35 AM
To: Robert Dmohowski
Cc: City Clerk
Subject: Eddie Jones Project without another bridge on San Luis Rey

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City of Oceanside Planner Rob Dmohowski,

The project should not be proceeding without a discussion of another bridge over San Luis Rey river. Traffic caused by the planned expansion will choke off all of our access to Highway 76. There are no alternative routes for our neighborhoods. I have lived for the past 20 years directly north of the proposed Eddie Jones Warehouse Facility project. I chose to live in this area specifically because of a low density aspect of this neighborhood. I am adamantly opposed to the facility for the following reasons:

1. The traffic will be too congested for us to be able to move about freely due to only 2 ways to go in and out of the area; Foussat and Benet Roads. There are about 2000 homes in our community and possibly 3000 cars that travel on those two roads. I understand that there used to be a third road planned out of the area but I have not been able to find out what happened to that plan.
2. It is already a noisy area because of the airport and the shooting range. With 114 semi-truck terminals and 60 truck parking places, the area will be a grand central station. We will already be greatly impacted by the development of Ocean Kamp on Foussat Road.
3. Oceanside has a water treatment plant that utilizes the resources from San Luis Rey River. It doesn't sound logical to put a manufacturing plant that will most likely involve hazardous materials that would pollute the river.
4. If our area had been a high density neighborhood, this new project would not have been totally inconsistent with the surrounding areas.

I hope the city will take these serious concerns of the residents of this area and scale back the project or consider another bridge as a service road.

Robert Dmohowski

From: Judy Glasser <jglasser50@gmail.com>
Sent: Monday, October 24, 2022 6:09 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Ryan Keim; Kori Jensen
Subject: Eddie Jones Project

Categories: Red Category

Warning: External Source

I am a homeowner in the area adjacent to both the Ocean Kamp and the proposed warehouse facility at Eddie Jones. There are only 2 access roads to the neighborhoods north of these projects. Each of these are 2 lane roads across a riverbed. The increased traffic congestion will affect the residents significantly. The area west and north of the Eddie Jones Warehouse is dry brush as well as a riverbed, both of which have had wildfires in the past 2 years. If there is a limit to the egress in the event of a fire there will be a significant chance for loss of life and property. I am urging a stop to the development of the Eddie Jones Warehouse facility.

Sincerely,

Judy Glasser

3236 Shadow Tree Dr.

Oceanside, CA 92058

Robert Dmohowski

From: Leah Conroy <lconroy68@gmail.com>
Sent: Wednesday, October 26, 2022 6:53 AM
To: Robert Dmohowski
Cc: Carol Broeland
Subject: Eddie Jones Property

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello,

I'm writing you to learn more about the development of property near my neighborhood (Airport/Marlado Highlands HOA), because there are many people against it. However, as a retired scientist, I am open to learning as much as possible, before offering my voice to this constant chatter on social media.

In Northern California, where I once lived, big projects are described in detail for the neighboring public, with documents, websites and flyers with updates. Since there has been little to no contact, residents fear the worst possible outcome. Could you please answer some basic questions about how Oceanside plans to improve the 76 corridor, as a result of this development? There should be a detailed plan that shows money allocated for work such as, traffic mitigation (road and bridge improvements at Foussat and Benet), pedestrian crosswalks (or overpasses) from Loretta, Canyon Streets to the bike path, overall renovation of the bike path (parking, access, restrooms) and the Skateboard Park (a critical asset), at all locations west of Douglass, and illegal encampment enforcement throughout the beautiful riverbed.

How will your project help our community, if it overwhelms the area with its massive size? Will it be anything like the Carlsbad warehouses, many of which sit vacant?
Will there be the same type of landscaping Carlsbad includes, to hide the unsightly box buildings? If so, who will pay for the water (and where will it come from) needed to maintain the landscapes?

I realize that a warehouse is probably the most sensible use of this space, but please reach out directly to our community, to ensure the northern section of Oceanside remains nice for residents and visitors.

Sincerely,

Leah Conroy
Ashwood Ct
Oceanside, CA 92058

Robert Dmohowski

From: J Kale <judykale@gmail.com>
Sent: Tuesday, November 29, 2022 8:19 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse, Manufacturing & Distribution Facility

Categories: Red Category

Warning: External Source

Dear Mr. Dmohowski

We are writing to express our concern and displeasure over the proposed Eddie Jones Warehouse project. We have been residents of Wanis View Estates for nine years. This project will greatly impact our HOA community not only with traffic congestion, noise and pollution but a potential decrease in home value due to the proximity and nature of the proposed business.

We have two routes to enter and exit our community. One is Foussat Road and the other is Benet Road. The location of the warehouse facility would heavily impact Benet Road with the use of semi trucks, employee and business parking. For those using Benet Road to access Highway 76, there is one left turn lane, one lane to go straight and one right turn lane. These lanes can get backed up already with residential traffic, traffic from Prince of Peace Abbey, S & R Towing and other businesses in that area. The current stoplight system in place favors Highway 76. When a semi truck is stopped and the light changes, they are slow to start up making it almost impossible for multiple cars to make it through the light. Traveling east on Highway 76 and attempting to make a left turn onto Benet Road is often backed up and through traffic blocks the entrance to the left turn lane. There are many homeless individuals that use the area between Highway 76 and the bike trail to enter and exit their camps in the San Luis Rey River bed. One has to be vigilant as these folks meander their way across Benet Road.

The proposed KAMP project with the addition of 700 homes and a 300-room resort hotel will heavily impact our other entrance and exit route of Foussat Road. Foussat Road does have two left turn lanes, two straight ahead lanes and two right turn lanes. This additional traffic will certainly impact our ability to access Highway 76 in a timely manner. Again, the stoplight system favors Highway 76. The KAMP construction semi trucks alone have already caused congestion.

June 12, 2022, we experienced the Wala Fire and were told by Oceanside Police to evacuate the area. Attempting to exit via Foussat was almost impossible because emergency vehicles kept activating the stoplight to favor Highway 76. Has any consideration regarding infrastructure been given to these projects and how increased traffic will dramatically impact the daily lives for all the residents that live in this area?

We urge you to reconsider the Eddie Jones Warehouse project. We don't need 114 semi truck bays, 60 semi truck parking spots and over 500 auto parking spots as our new neighbor.

Sincerely,

Robert & Judith Kale

Robert Dmohowski

From: Brian Keith <k.brian11@icloud.com>
Sent: Tuesday, November 22, 2022 2:13 PM
To: Robert Dmohowski
Subject: Eddie Jones warehouse project...

Categories: Red Category

Warning: External Source

Hello, I was just reading about this project and it sounds like a horrible location for this project. How can I get involved in opposing this project?

Thank You
Brian Keith, concerned neighbor.

Robert Dmohowski

From: Rachel Foreman <rachpaulforeman@yahoo.com>
Sent: Thursday, December 8, 2022 10:46 AM
To: Robert Dmohowski
Subject: Eddie Jones

Warning: External Source

Dear Sir,

I have just been made aware of the proposed Eddie Jones Warehouse plan slotted for the Airport area. As a resident of this area, I am completely opposed to this business setting up operations in a semi-residential neighborhood. The traffic congestion alone with semi trucks utilizing this area is a terrible idea! We are already dealing with the homeless population living in the San Luis riverbed right across Benet from this planned site. People crossing the streets at night wearing all black, traversing up and down Benet road with bikes and shopping carts is dangerous enough to navigate while driving at night. Those of us who live in this area also utilize these roads (Benet and Fousat) to get back and forth to the San Luis Rey bike trail to walk, bike and children and families to use the Alex Skate park. We want our area to stay friendly to families, people walking their dogs and pushing kids in strollers. We do NOT want a bunch of semi-trailers coming into our neighborhood and adding traffic congestion, noise, pollution and unsafe conditions for residents that utilize the beauty of this area to get outside. I oppose this manufacturing and distribution facility coming into the airport area for all these reasons.

Respectfully,

Rachel Foreman

515 Moyla Ct.

Oceanside, CA 92058

808-220-2036

Sent from my iPhone

Robert Dmohowski

From: dave keck <dgkeck@cox.net>
Sent: Thursday, December 15, 2022 3:25 PM
To: Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Robert Dmohowski
Cc: Zeb Navarro; Dee Keck
Subject: Eddy Jones Warehouse, Manufacturing and Distribution Facility
Attachments: EDDY JONES PROJECT HANDOUT 12-15-2022.pdf

Warning: External Source

Good Afternoon Mayor Sanchez and the Councilmembers of Oceanside,

Let me congratulate the newest members and welcome them aboard. I look forward to working them as our city moves into the new year.

Many of you may already know about the pending development of the Eddie Jones Warehouse, Manufacturing & Distribution Facility being considered for construction at Eddie Jones Way and Benet Road, just north of our municipal airport.

I have, as well as many of my neighbors, some major concerns regarding the development of the Eddy Jones Project and the impact it will have on our local Airport Community as well as Oceanside in general. The project is currently being reviewed by the City's Planning Division. Rather than wait until the project advances for your consideration and deliberation I want to make sure you aware of our citizen's concerns.

I hope my perception is wrong but it seems that when a project gets to your level it is all but a done deal. Our group in being proactive by bringing this to our attention now so you are knowledgeable of these concerns and that they can be addressed by all the entities involved.

I have attached a flyer that has been distributed within our community for your information. If you have any questions please feel free to call me.

Respectfully submitted,

Dave Keck

District 1

760 760.583.2083



Virus-free. www.avg.com

Opposition to the

Eddy Jones Warehouse, Manufacturing and Distribution Facility

(sometimes referred to as EDDIE Jones)

Brief Summary

The **Eddy Jones Warehouse, Manufacturing and Distribution Facility** is being proposed to be built on the 31.79 acres between the Oceanside Municipal Airport on Eddie Jones Way and Benet Rd and the residential development known as Airport. The plans include **114 semi-truck terminals, 60 truck trailer parking stalls and 590 auto parking spaces** with a building of **566,905 square feet**.

In addition, with the **Ocean Kamp** project under construction, our ability to maneuver in and out of our community via either Benet Rd or Foussat Rd will be adversely impacted.

Our Concerns

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as ALL who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There is NO money earmarked for any road / traffic improvements.

TRANSPARENCY - To date the lack of transparency/communications is very apparent when very few of the Airport residents and surrounding areas are even aware of the intended development. We are not opposed to something being built but what type of business, what level and hours of operations and what impact will it have on our community and our quality of life are not being addressed or taken into consideration.

POLLUTION - We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24 hours a day. Noise pollution from these vehicles is concerning. Pollution of the San Luis River is also an issue to be considered.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. The same will be true on Foussat Rd due to increased traffic from the Ocean Kamp and the 590 vehicles expected for the employees and visitors at this new facility. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. We do not oppose development as long as it is well thought out and is compatible with the residential area that it borders. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on our nearby communities.

HOW YOU CAN DO:

- Contact the City Planner, all the members of the City Council, and the Mayor. Be sure to copy the City Clerk and Dee Keck so there are records of your correspondence (all addresses are below)
- Sign our petition and get signatures from your neighbors.
- Join our grassroots group. We all need to be actively involved to get this changed. A few of us cannot do it all. You will get updates as to what is going on and what we are doing via this newsletter. We need YOU! Contact Dee at 4dees@cox.net
- Follow us on Facebook - EDDIE JONES PROJECT / OCEANSIDE
- **Oceanside City Principal Planner**
Rob Dmohowski
(760) 435-3563
rdmohowski@oceansideca.org
- **Oceanside Mayor**
Esther Sanchez
(760) 435-3057
esanchez@oceansideca.org
- **Deputy Mayor - District 3**
Ryan Keim
(760) 435-3048
rkeim@oceansideca.org
- **Councilmember - District 1**
- **Eric Joyce**
(760) 435-3032
ejoyce@oceansideca.org
- **Councilmember - District 2**
Richard (Rick) Robinson
(760) 435-3033
rrobinson@oceansideca.org
- **Councilmember - District 4**
Peter Weiss
(760) 435-3066
pweiss@oceansideca.org
- **Oceanside City Clerk**
Zeb Navarro, Ed.D.
(760) 435-3010
znavarro@oceansideca.org



Additional Information

Oceanside Planning Division:
Eddy Jones Project File Number – D22-00001
Contains publicly available project documents
(TrackIT) <https://oceanside.maps.arcgis.com/apps/webappviewer/index.html?id=77db3575bf5b49f290fc050cb46cfae2>

Robert Dmohowski

From: Dane Thompson
Sent: Tuesday, November 8, 2022 8:08 AM
To: Robert Dmohowski
Subject: FW: Semi-Truck Distribution Center

Categories: Red Category

fyi

From: Thomas Le Vine <tomdlevine@gmail.com>
Sent: Monday, November 7, 2022 2:04 PM
To: Planning Web <Planning_Web@oceansideca.org>
Subject: Fwd: Semi-Truck Distribution Center

Warning: External Source

From: **Thomas Le Vine** <tomdlevine@gmail.com>
Date: Mon, Oct 24, 2022 at 9:36 AM
Subject: Semi-Truck Distribution Center
To: <dmohowski@oceansideca.org>

Dear Oceanside Planning:

We live in the large Airport neighborhood directly north of Oceanside airport with our family. Please do not approve building a giant 566,000 square foot distribution center on unimproved vacant property in this residential area, servicing semi-trailer trucks.

To the south of 76, there is an existing warehouse district that is blighted. To place it there is much better as fill-in construction, and it could be serviced by Mission Road as well as Highway 76. A warehouse is one thing. Hundreds of semi-trucks is another. Placing it in this narrow Highway 76 corridor is not appropriate for this area and the infrastructure does not exist to support it. If

Highway 76 was expanded to three lanes in both directions, it would be more palatable. But zoning laws should change, if needed, to negate this kind of project.

Thank you for the opportunity to comment.

Tom Le Vine
760-415-6055
TomDLeVine@gmail.com

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Saturday, August 27, 2022 7:08 AM
To: Esther Sanchez; Kori Jensen
Cc: City Council
Subject: Fwd: complaint on Eddie Jones project
Attachments: ComplaintOnEddieJonesProject_August2022.pdf

Warning: External Source

Hello Mayor Sanchez and Councilmember Kori Jensen -

I wanted to make you aware of my concern around the proposed Eddie Jones Project near the small airport off the 76 in District 1 of Oceanside. I feel that this project has been trying to fly under the radar so as not to get too much attention while it goes through all the critical planning steps. I myself wasn't even notified of the project and my home looks out directly at this property - instead, I heard about it via word of mouth from other neighbors who did receive a certified mail informing them of what was planned.

This project sits just 2 miles from the coast along the San Luis Rey river. I can't imagine that the City of Oceanside would want the volume of semi-truck traffic that this project seeks to accommodate clogging the 76 and 5 freeway at what is essentially the gateway to the City of San Diego. Not to mention, with the pending Ocean Kamp project planned nearby as well as all the beautiful development done in downtown Oceanside to drive recreational tourism, a massive industrial warehouse (proposed at 567K sq ft!) this close to the ocean would only harm the city's plans to beautify our city and acquire a comparable status to other north county cities like Encinitas and Carlsbad.

Thank you in advance for your consideration of my concerns as detailed in the complaint that I filed with Rob Dmohowski earlier this month.

Sincerely,
Gretchen Gary
Oceanside resident
471 Tishmal Ct, Oceanside 92058
760-560-6103

----- Forwarded message -----

From: Gretchen Gary <gretchengary@gmail.com>
Date: Fri, Aug 5, 2022 at 9:35 AM
Subject: complaint on Eddie Jones project
To: <rdmohowski@oceansideca.org>

City of Oceanside
Development Services Department
300 N. Coast Highway, Oceanside, CA 92057

August 5, 2022

Attention Rob Dmohowski,

I am a member of the Wanis View community that sits above the airport and commercial area just off of Benet Road. I am writing this letter on behalf of my sincere concern about the proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility located at 250 Eddie Jones Way in the City of Oceanside. I am formally filing a complaint to be included with the environmental impact report on this project.

I am proud of my community and how we have created an oasis in a part of Oceanside that is still rough around the edges and afflicted by a growing population of homeless people who have camps in the river basin and surrounding areas. Despite the challenges, our community is an example of how things can be turned around in Oceanside and how active members of the community can effect change. I imagine it is the presence of Wanis View that has drawn the interest of so many developers to this part of the city.

While I support the development of business and growing residential communities in this area, I am fully against the proposal outlined in the plans for the Eddie Jones Warehouse, Manufacturing & Distribution Facility. It is too massive and just not a good fit for this area where there are 2,000+ homes sandwiched in an pocket community where there are only two 2-lane ingress and egress roads. Here are my primary concerns:

1. **Noise pollution.** My house sits directly on the San Luis Rey river looking out across the property and the existing commercial structures. I moved here several years ago when the property was in operation. There was a fair amount of white noise that came from the property however there were no back up alarms or sounds of large trucks moving in and out of the property. It was strictly a manufacturing facility. We chose to live here assuming the commercial activity on that property would remain at similar levels. The new proposal seeks to add 114 semi-truck terminals and 60 truck trailer parking stalls – that’s almost 8 times more than the number of truck terminals proposed at the Amazon distribution center in Ocean Ranch that was rejected by the Oceanside City Council in summer 2021, which only had plans for 15 truck terminals. Further, the rejected Amazon distribution center was only proposed to be 142,746 square feet while the Eddie Jones project is 566,905 square feet – that is massive in comparison to the Amazon facility that was already rejected per concerns of noise pollution voiced by members of the nearby St. Cloud community.

2. **Traffic.** With only two ingress and egress roads, each only 2-lanes wide, there is no capacity for the volume of semi-trucks and cars this project is anticipating. With the above mentioned 114 truck terminals, 60 truck trailer parking stalls and 590 parking spaces, this project would introduce unmanageable congestion to an area that is already struggling with road infrastructure. Turning on to Hwy 76 is already difficult at prime peak hours – adding this kind of traffic volume to the area would be simply unsustainable without major infrastructure work to widen roads and bridges.

3. **Light pollution.** A project of this size will bring round the clock light pollution to our neighborhood. As mentioned, my house sits directly on the river looking straight out to this property. We already have filed complaints to the equipment yard west of us on HWY 76 as their tall lights shine directly into our master bedroom all night. With a parking lot fit for 114 terminals, 60 truck trailer parking stalls and 590 parking space, I image the lighting set up around this property to illuminate the lot in the evening hours will be immense. Beyond the impacts to lighting up our master bedroom all night, I imagine this will have significant impact on wildlife and sensitive bird populations that are monitored regularly in the river ecosystem.

4. **Quality of life.** As a tax-paying citizen of Oceanside, I am always thinking about ways to improve our community for the better. Oceanside has enormous potential to become one of the most beautiful coastal communities of North County San Diego. A project like this so close to the ocean would not beautify or add appeal to the city. It would only continue to degrade an area that is already in great need of city attention. Beyond the constant noise pollution of tractor trailers and fork lifts with their back up alarms and unimaginable traffic congestion that this project would introduce to the roads in the area, it would reinforce Oceanside's history of poor city planning in failing to keep industrial establishments in more appropriate areas so the coastal region can evolve to embrace the natural beauty that surrounds. The San Luis Rey river basin is one of the last remaining natural corridors for natural wildlife. And the surrounding hills of wide-open space offer a beautiful backdrop to an area I believe has incredible potential for the right kind of development. This project is certainly NOT the kind of development we want 2 miles from the ocean in the City of Oceanside.

5. **Uncertainty.** In the public scoping and community meeting on August 3, 2022, the developer was unable to answer questions about hours of operation (will the distribution facility run 24 hours per day?) or why they needed so many truck terminals and trailer parking stalls. This kind of uncertainty is unacceptable when proposing any project for development. Residents and city officials should have all the details upfront before they blindly accept construction of projects built for unknown occupants or business purposes. It appears to be a way to trick the system to avoid what happened with the Amazon project in the Ocean Ranch industrial park. I hope the city sees that this is obviously a developer tactic to avoid direct confrontation. The value of our home equity is on the line with this proposal – we have the right to know the exact details of proposed operations.

In closing, I hope that you consider my concerns and the concerns voiced by many of my neighbors. None of us are in support of this proposed project on Eddie Jones Way. The City of Oceanside deserves better. We need to think of the big picture here – what do we want Oceanside to become? I hope that our city leaders are taking time to envision a brighter, better future for the City of Oceanside.

Sincerely,

Gretchen Gary
471 Tishmal Ct, Oceanside, CA 92058

Robert Dmohowski

From: Sarah Samoska <sarahmarie325@gmail.com>
Sent: Sunday, November 20, 2022 11:34 PM
To: Robert Dmohowski
Subject: NO warehouse wanted near airport

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

I am emailing you to voice my concern about the warehouse that could go in near the airport area...we DO NOT want this to happen. We already deal with a great amount of traffic on the 76 and this will make it worse. We only have 2 ways in and out for our airport community and having large trucks going through all day long, will not be an easy commute for us neighbors. We already have a great deal of traffic with the tow truck company nearby and other various things. This will also impact our property values that we work hard to stay on top of in our wanis view community.

Thank you for your time with reading this.

Sincerely,
Concerned resident
Sarah

Robert Dmohowski

From: Jason Bailie <bailiejm@gmail.com>
Sent: Tuesday, November 22, 2022 2:26 PM
To: Robert Dmohowski
Subject: Oppose Eddy Jones Warehouse

Categories: Red Category

Warning: External Source

Mr. Dmohowski, I'm writing in strong opposition of the proposed Eddie Jones Project in Oceanside. I am a life long San Diego resident. We own two properties in Oceanside and I'm a doctor at the nearby Naval Hospital at Camp Pendleton. I have a vested interest in keeping this city strong.

The proposed Eddie Jones project clearly would cause increased congestion to the primary exits of the local residential neighborhood in addition to the hwy 76. The infrastructure wasn't built for these purposes. It also will decrease safety in the area for all pedestrians and drivers who will encounter the heavy influx of major trucking.

Oceanside is a coastal destination and inclusion of a warehouse distribution center will undermine this city's attempt at rebuilding. It will devalue property and take the allure of our beautiful city. Please join us in opposing this build.

Please let me know if I can be of any further assistance in this matter.

Dr. Jason Bailie
619-433-9916

Robert Dmohowski

From: Heather Moreau <hmoreau1@gmail.com>
Sent: Tuesday, October 18, 2022 10:30 AM
To: City Council
Subject: Please NO 'Eddie Jones warehouse' off the 76! 🏠🚚

Warning: External Source

Hi there, my name is Heather and I am an Oceanside resident and Native. I live in the airport neighborhood off the 76 and I wanted to send this email to sincerely request consideration to deny the Eddie Jones Warehouse off the 76. There are so many reasons for this project to be denied, among them huge traffic impacts, and direct controversy with the current wave Park project being built right next door to that. The wave Park project is striving to make our community walkable and to attract people from all over the world, but adding this Warehouse project would be a direct impact in the type of area we are trying to build for Oceanside.

Home values would suffer.

If we share our only two 2-lane ingress/egress roads with over a hundred tractor trailers and nearly 600 employees, our fire evacuation routes will be further clogged and restricted in fire emergencies.

Noise pollution that would directly affect the wave Park project and all the proposed restaurants and amenities being built right next door. Who would want to go on vacation next to an enormous Warehouse project? They just don't fit together. There is potential that future occupants of this project could operate 24 hours per day, which would mean round-the-clock noise pollution in our community.

City Beautification. An industrial warehouse project in the same stretch of the 76 is counter to the vision behind building a wave park and beautifying the skate park, which will draw a surge of children and teenagers on Foussat and Alex Roads, likely on e-bikes and skateboards.

A steady stream of tractor trailers on our roads would detract from the tourism appeal of our community and present unprecedented road hazards.

I know I for one would definitely want to move out of this neighborhood if this project was going to happen. I sincerely hope that the city will take our concerns under advisement and reject the warehouse project. Thanks so much for reading this email.

--

Heather Moreau
Vallentyne Photography | Owner

www.vallentynephotography.com

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Wednesday, November 2, 2022 9:30 AM
To: Esther Sanchez
Cc: Ryan Keim; Kori Jensen; Christopher Rodriguez; Peter Weiss; Robert Dmohowski
Subject: Re: Eddie Jones Warehouse

Warning: External Source

To whom...

I was under the impression that interested parties, like myself, would be notified about any developments concerning this project.

Last I heard, an EIR was being required. I would suspect a traffic study would be required as well?

I now see the trees being ripped out and the existing buildings being demolished on the site.

A neighbor said he'd heard the city had approved this project.

Is this true?

I reiterate...there are nearly a thousand homes that have only two roads for access across the river, Benet and Foussat. One will have dozens to hundreds of tractor trailers and the other hundreds of personal vehicles placed upon them. And all going onto highway 76. And likely 24/7.(And then add the new developments on Foussat as well.)

(I am still in awe that just a few dozen homes were notified of this project because the 1500 foot notification zone is basically the river and river trail.)

Highway 76 is no expressway as the name implies. Have any of you driven it? And we're going to add hundreds of trucks and cars to it?

Or has anyone traveled I-5, between 76 and 78? A section that is almost always at a standstill.

Also amazing is that an EIR would approve this facility, one that would likely be 24-7 activity and is right next to the river. A river that is posted its entire length as "protected habitat".

I am hoping Oceanside's planning department and city council had the common sense to truly study this project and its obvious issues and not approve it.

Where does it stand at present?

Rick and Sue Taylor
735 Valley Crest Dr.
Oceanside, CA 92058
760 274 4700

On Sat, Sep 10, 2022, 11:30 AM Rick Taylor <rkt.sce@gmail.com> wrote:

Mayor, city council members, Mr. Dmohowski,

Thanks for reviewing my input. Since my initial contact, I have been informed as to why myself and the majority of my neighbors did not know of this proposed project.

In a response from Mr. Dmohowski, he noted that only properties within 1500 feet of the proposed project were notified.

Depending on where that 1500 feet is measured it might not include any of the residences north of the San Luis Rey River.

Even if taken to the north (near the bike trail), because of the bike trail and the river itself, only around +/- 70 of the hundreds of homes in that community would have been notified.

That seems unfair for the rest of this community which has only two roads in and out.

As noted prior, hundreds of semi's on Benet and 76, and Foussat and 76 having the community, plus the new project employees and all the new development around the waterpark just doesn't appear to be appropriate.

Wouldn't it be appropriate to include the rest of this community in the notifications for this project? It impacts them just as much as those within 1500 feet.

Thanks for your time.

Rick Taylor
735 Valley Crest Drive
Oceanside, CA 92058

On Mon, Aug 29, 2022, 1:29 PM Rick Taylor <rkt.sce@gmail.com> wrote:

Subject: Proposed: Manufacturing/distribution Center at 250 Eddie Jones Way, Oceanside

I am a retired civil/structural engineer that lives in the community on the north side of the San Luis Rey River between Benet and Foussat Roads. I have never been a NIMBY as things need reasonable evaluation, and projects were my company's life blood.

But this project just doesn't fit the area. There are only two roads that access this north river community (Benet/Foussat) and Highway 76 IS NOT an expressway. Highway 76 can be a traffic nightmare most times of the day.

With the waterpark and housing development moving forward at Foussat and Mission, traffic on already busy roads just doesn't warrant adding hundreds of new vehicles, in particular semi-trucks.

Additionally, from review of the proposed plans, the existing Deutsch park buffer to the river is abandoned and the project is being allowed to have asphalt parking right up to the river trail. This area has even been posted as "protected habitat" by the city and the assumed site activity is going to disrupt the environment and animal life in the area, and likely 24 hours a day. Why would this be allowed?

The children's skate park and biker/hiker river trail access on Alex Road would be an accident ready to happen.

Therefore, my wife and I, and I know my neighbors are as well, are against this type of project being placed at that location. Something less intrusive must be found. Please do not approve this type of project at that location.

Thanks for your time.

Rick and Sue Taylor
735 Valley Crest Drive
Oceanside, CA 92058
760 274 4700

--

Rick Taylor Principal PE
SCE
(Structural Consulting Engineers)
rkt.SCE@gmail.com 760 274 4700

Robert Dmohowski

From: Elena Bee <helenathebrit@gmail.com>
Sent: Thursday, December 29, 2022 3:38 PM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro
Subject: Eddie Jones Project
Categories: Red Category

Warning: External Source

To whom it may concern:

I live in the airport area and am very concerned about the proposed Eddie Jones project for the following reasons...

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with travel to and from homes as well as ALL who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There is NO money earmarked for any road / traffic improvements. Why the heck not?

POLLUTION - I strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24 hours a day.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on nearby communities.

Sincerely
Elena Baker
537 Shadow Tree Dr. 92058

Robert Dmohowski

From: Jessica Ryan <Phattipita@hotmail.com>
Sent: Tuesday, December 27, 2022 2:18 PM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Distribution Facility

Categories: Red Category

Warning: External Source

Good afternoon Rob,

I live in the Brisas area above the Oceanside airport.

I strongly oppose the building of the Eddie Jones Warehouse.

Recently being evacuated because of a fire, the traffic of home owners at the 76 and Foussat or Benet was huge! With Ocean Kamp already going in. That will bring more traffic to the entrance of the neighborhood.

If Benet is constantly busy with new semi traffic, as well as along the 76 corridor, I fear that my access to my own neighborhood would be too impacted in a very negative way.

I strongly object to the Eddie Jones project. The city has had zero planning to withstand the extra traffic.

Not to mention the constant noise and extra pollution constantly streaming from semi trucks.

I embrace growth in the community but I feel that there are better places for this project to happen that would have less of a neighborhood impact.

Thanks for reading.

Jessica Ryan

Sent from [Mail](#) for Windows

Robert Dmohowski

From: DARREN SWAIN <darrenswainoside@gmail.com>
Sent: Tuesday, December 27, 2022 2:14 PM
To: Robert Dmohowski
Subject: Eddie Jones Proposed dist center

Categories: Red Category

Warning: External Source

I am a resident 490 Shadow Tree Dr, Oceanside, CA 92058.

I oppose this development.

It will destroy our home values. Please put this development I a business park. Why would you put this next to an airport and a new resort development.

Thank you for considering the thoughts of those that have invested in the the residential area adjacent.

Darren Swain
darrenswainoside@gmail.com

Robert Dmohowski

From: Thomas Schmiderer
Sent: Tuesday, October 18, 2022 3:37 PM
To: Robert Dmohowski
Subject: FW: proposed warehouse

From: Melissa Campbell <melissacampbellpac@gmail.com>
Sent: Tuesday, October 18, 2022 3:31 PM
To: City Council <Council@oceansideca.org>
Subject: proposed warehouse

Warning: External Source

Dear Council members;

My name is Melissa Campbell. I have been an Oceanside resident for over 25 years now. For the past 20 years I have lived in my home on the North side of the airport and raised my family there. I am very concerned about the proposal to place a large warehouse along the bike path and Jones road.

I would ask that you reconsider and reject this proposal. Our neighborhood currently only has two access points- one off of Benet and one off of Foussat. With the proposed waterpark development going in along the 76 and Foussat, we cannot afford to have another large development along Benet. This would hinder our ability to get in and out of our development in a timely manner. In addition, in case of an emergency, it would further restrict emergency vehicle access and our neighborhoods ability to safely evacuate.

In addition, the bike path attracts lots of people attempting to get outdoors and enjoy the beautiful scenery. Having a large warehouse along the bike path would be a detriment and prevent people from using the bike path. No one wants to ride along a large, loud warehouse. It will decrease the beauty and serenity of that area.

Please reconsider and reject this proposal. The people most affected by this warehouse do not want it. Please listen to our pleas.

Thank you for your understanding and time.

Melissa Campbell
3332 Morning View Drive
Oceanside, CA 92058
760-473-2078

Robert Dmohowski

From: Kim Stone <kimstone899@gmail.com>
Sent: Thursday, January 26, 2023 7:40 PM
To: Robert Dmohowski; Dee Keck
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Rob,
I have a question re the Regional Airport Land Use Compatibility Plan for the Oceanside Municipal Airport that provides for the orderly development of the area surrounding the Oceanside Municipal Airport so as to prevent the creation of new noise and safety problems, etc. Does this have any impact on the Eddie Jones project?
Thank you in advance,
Kim Stone

PS please add me to your contact list.

Robert Dmohowski

From: Elena Bee <helenathebrit@gmail.com>
Sent: Thursday, December 29, 2022 3:38 PM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro
Subject: Eddie Jones Project
Categories: Red Category

Warning: External Source

To whom it may concern:

I live in the airport area and am very concerned about the proposed Eddie Jones project for the following reasons...

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CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on nearby communities.

Sincerely

Elena Baker

537 Shadow Tree Dr. 92058

Robert Dmohowski

From: Jessica Ryan <Phattipita@hotmail.com>
Sent: Tuesday, December 27, 2022 2:18 PM
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Good afternoon Rob,

I live in the Brisas area above the Oceanside airport.

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Recently being evacuated because of a fire, the traffic of home owners at the 76 and Foussat or Benet was huge! With Ocean Kamp already going in. That will bring more traffic to the entrance of the neighborhood.

If Benet is constantly busy with new semi traffic, as well as along the 76 corridor, I fear that my access to my own neighborhood would be too impacted in a very negative way.

I strongly object to the Eddie Jones project. The city has had zero planning to withstand the extra traffic.

Not to mention the constant noise and extra pollution constantly streaming from semi trucks.

I embrace growth in the community but I feel that there are better places for this project to happen that would have less of a neighborhood impact.

Thanks for reading.

Jessica Ryan

Sent from [Mail](#) for Windows

Robert Dmohowski

From: DARREN SWAIN <darrenswainoside@gmail.com>
Sent: Tuesday, December 27, 2022 2:14 PM
To: Robert Dmohowski
Subject: Eddie Jones Proposed dist center

Categories: Red Category

Warning: External Source

I am a resident 490 Shadow Tree Dr, Oceanside, CA 92058.

I oppose this development.

It will destroy our home values. Please put this development I a business park. Why would you put this next to an airport and a new resort development.

Thank you for considering the thoughts of those that have invested in the the residential area adjacent.

Darren Swain
darrenswainoside@gmail.com

Robert Dmohowski

From: Alyce Budde <ajbudde@cox.net>
Sent: Thursday, January 12, 2023 9:45 AM
To: Robert Dmohowski
Cc: Dee Keck
Subject: Eddie Jones Project

Categories: Red Category

Warning: External Source

Good morning Mr Mohowski,

As the city planner I am reaching out to you regarding my concerns with this project that is slated for the property on Airport Road and Benet. A distribution center !! What in the world is the city thinking ?? The community I live in behind the airport only has two avenues of ingress and egress, which are Fousett and Benet. This project would impact our community exponentially. How would we get out in the event of a disaster. Has a study been done as to how this is going to affect the traffic in our area and on CA 76. Has an environmental impact study been completed as to how this is going to affect the health and the welfare of the community and the people living around this proposed project. I for one live right along the river bed and it would be directly in my backyard I would be subjected to 24 seven of continuous noise and during the night bright light. This would definitely affect my health and well-being not to mention how it is going to depreciate the value of my home . I have been a resident of the city of Oceanside since 1988. The two projects that have been slated for development in my area would make life miserable for all of the residence north of CA 76 at Foussat and Benet. Residents have been vocal about their disapproval of the development of where the old drive-in theater was and now on the opposite side we have to deal with the distribution center. Has anyone considered how this affects homeowners in this area? As taxpayers and community members we deserve so much more from our city.

Alyce Budde
3243 Toopal Dr
Oceanside CA 92058
ajbudde@cox.net

Sent from my iPad

Robert Dmohowski

From: Justin Floyd <justin@jfcfcapitaladvisors.com>
Sent: Thursday, January 12, 2023 12:29 PM
To: Robert Dmohowski
Cc: Justin Davis
Subject: Eddie Jones Project

Categories: Red Category

Warning: External Source

Rob,

I want to introduce myself to learn more about this proposed project and to find out the current status of approvals with the city.

I not only live in the area but also will be looking to build a consortium of HOA and homeowners that will be severely impacted detrimentally to fight this with legal representation.

Best Regards,

Justin Floyd

Mobile: (858) 945-2938

Robert Dmohowski

From: Rick Empson <rickempson@hotmail.com>
Sent: Thursday, January 12, 2023 5:55 PM
To: Robert Dmohowski
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Greetings!

I am sending this email to you to inform you that my wife and I strongly oppose the proposed Eddie Jones Development Project. We hope and pray this project does not get developed as planned, due to excessive traffic on Benet Road, which will potentially impact emergency responders into our neighborhood.

My wife and I live at 3240 Toopal Drive, just off of Benet Road.

Thank you for considering this request!

Respectfully,

Rick & Terri Empson
3240 Toopal Drive
Oceanside, Ca. 92058
(760) 529-5037

Sent from my iPad

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Friday, January 13, 2023 11:15 AM
To: Robert Dmohowski
Cc: John Bucalo
Subject: interested persons list

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hello -

I'm told I should confirm that I am on the Interested Persons list for the Eddie Jones project. Considering I am one of the closest homeowners to the project, I would hope I am already on this list, but just in case, please add the following names:

Gretchen Gary

gretchengary@gmail.com

John Bucalo

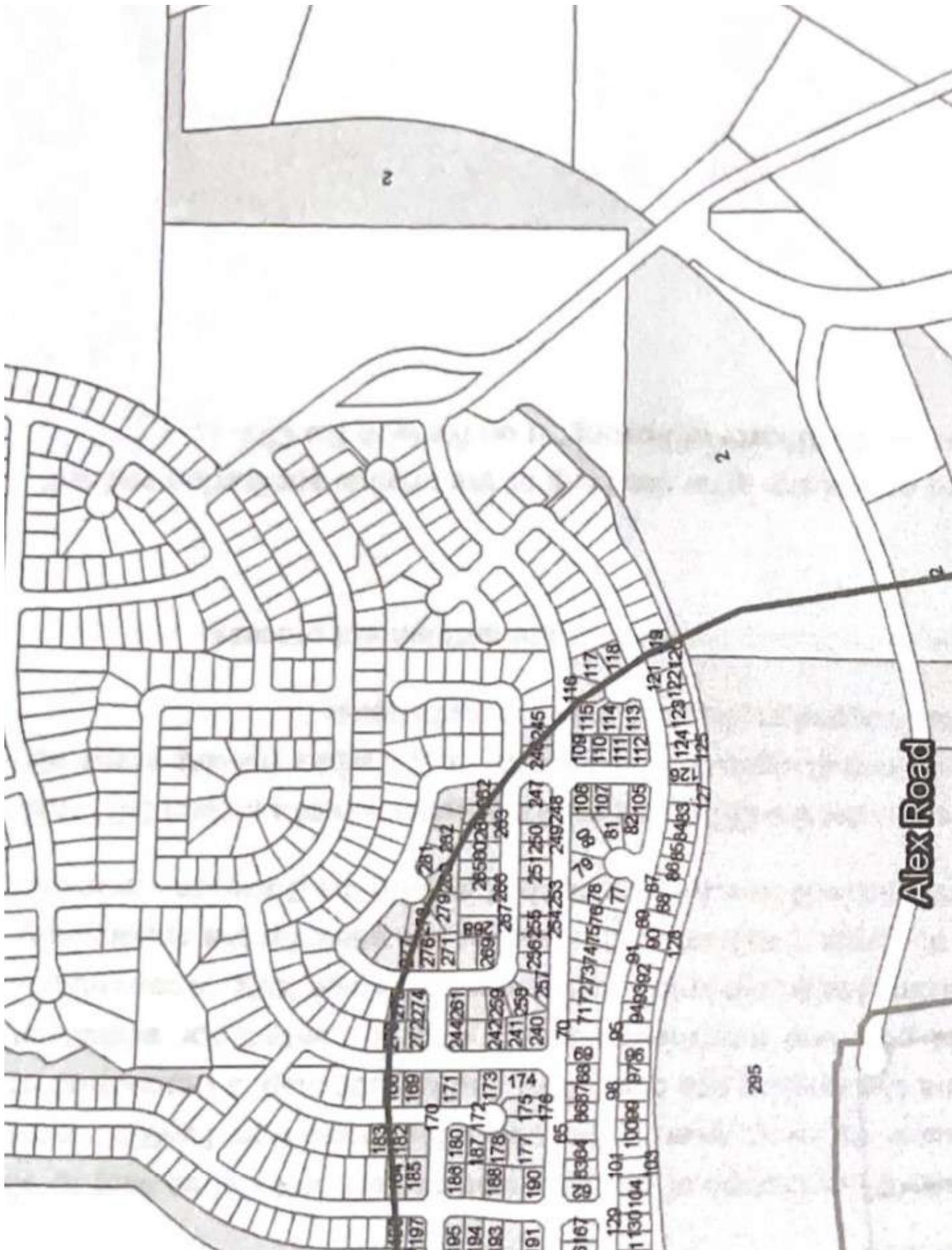
racetrackspecialties@gmail.com

I'm including a map with our house highlighted in yellow so you can see how close we are. Note that we did not receive the original mailer notifying our neighborhood of the planned project. We heard about the project through word of mouth as did all others in our cul de sac. I assume our cul de sac was deliberately excluded given how close we are to the project.

I don't see how anyone can see this project as a benefit to the city of Oceanside. The property is 2 miles from the ocean at one of the entrances to the bike trail that is celebrated as a tourist attraction. A project like the one that is proposed belongs out in remote commercial areas, not on prime coastal real estate. I would hope that the city of Oceanside has a better vision for the future of our city than a mega warehouse that will bring over 100 semi trucks to the gateway to San Diego county. The 5 freeway at the 76 is literally the first thing all southbound tourists to our city see after they pass Camp Pendleton.

Please take our concerns seriously. The city of Oceanside and the greater city of San Diego is depending on you to make the right decision on this.

Regards,
Gretchen Gary



Robert Dmohowski

From: Marion Donahue <mariondonahue@hotmail.com>
Sent: Saturday, January 14, 2023 12:28 PM
To: Robert Dmohowski
Cc: battcar@sbglobal.net; 4dees@cox.net; Richard Donahue
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Mr. Dmohowski,

I own homes in both **Las Brisas** and **Mar Lado HOA communities** and occupy the Ma Lado address presently. I am most concerned with the proposed Eddie Jones Project and the possible consequences of its development on safety, health, and light pollution.

In my lifetime, I have lived in several homes. Once, I had to evacuate because of flooding. Another time, I had to evacuate my home **within 15 minutes** because of a fire, which caused the loss of **267 homes** around me. In any situation like these, clear and **unimpeded road access** is crucial to loss of life—personal, children, and neighbors! Benet and Foussat are the **only evacuation** exits due to events that may happen on our hillside locations. Both roads will be affected by increased 24 hour traffic, even spilling out from Alex Road.

The morning I returned to my home after the "Panorama Fire" there were still 12 firetrucks stationed at the corner of our property, watching and keeping ember fires from starting new flames. The air was full of smoke and burned lumber. Our pet which could not be evacuated in the time limit survived the trauma, but **survival** of all who lived there was based upon the **fact** that the **one road** up to our development **had access** by fire equipment in the emergency. Because **another road nearby** was utilized in the evacuation process, miraculously, no life was lost!

I have asthma and believed that living in Marlado or Las Brisas would keep me away from the microscopic molecules of pollution that might come my way in everyday living. However, adding diesel trucks, and a warehouse facility of employees that will travel by vehicles around the clock will impact my health. Emission pollution carried on the wind will increase and affect my breathing. This is not speculation, it is based on facts of modern-day scientific studies.

I am also deeply concerned about light pollution around the clock. At present, we can still see the stars at night despite commercial property on Highway 76. However, with the addition of OceanKamp and the now 24-hour operation of the Eddie Jones Project, the night skies will definitely experience light changes. LED lights especially change the night skies as we saw in another Southern CA community in which we lived. Light defines the life of plants, people, and animals and even affects circadian rhythms as has been proven in studies by both health

experts, education experts, and even affects the economy.

What plans have been executed to **shield** those expected lights at night?

How will **constant, safe, road access** be provided day and night for residents, emergency vehicles for fire, heart attacks, accidents, and vehicle utility emergencies?

What provisions have been applied toward the Eddie Jones Facility to **protect from air pollution** to all residents?

Have **flood control** measures for the San Andreas river for the 100 year flood plane been observed and in place?

Marion S. Donahue
3412 Northwood Dr.
Oceanside, CA
mariondonahue@hotmail.com

Robert Dmohowski

Subject: FW: Opposition to Eddy Jones Warehouse approval and development

From: Jeff and Pati Stanford <stanfordjp@verizon.net>

Sent: Sunday, January 15, 2023 1:49 PM

To: City Council <Council@oceansideca.org>; City Council <Council@oceansideca.org>; City Clerk <CityClerk@oceansideca.org>; eddyjoneswarehouse@gmail.com

Subject: Opposition to Eddy Jones Warehouse approval and development

Warning: External Source

To: City Council, City Clerk

What little I know about the plans for the warehouse on Benet Road near the airport seems poorly done. The article reads that the semi truck traffic will increase significantly if this warehouse is built. Surely there has to be a better location than this, where traffic is going to increase anyway when Ocean Kamp is open. Please stop this development until it is vetted longer and more properly by the Council, Planner, and citizens

Jeff and Pati Stanford
Oceanside, CA

Robert Dmohowski

From: Matt Ritze <matt@championcps.com>
Sent: Tuesday, January 17, 2023 5:30 PM
To: Robert Dmohowski
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hello Mr. Dmohowski,

Please, do not allow this project to happen at this location. With only two streets in and out of our community, it would create an environment of commercial and industrial landscape. I love the drive home on Bennet and Foussat knowing that the drive will be peaceful in a residential family environment. As neighbors we wave to each other everyday and appreciate the way the City of Oceanside created this part of our community. To replace the peaceful and family oriented community with a large fleet of commercial vehicles located at the entrance and exit to our community, would be a very sad day for all of the residents impacted by such a move and for our community as a whole and our city. Please do not approve and help stop this project. Please if you have any questions, just ask.

Respectfully,

Matthew Ritze
Business Development
Champion Property Services
714-809-4750

Robert Dmohowski

Subject: FW: No on Eddy Jones Project

From: Pia Riva Woessner <piamriva@gmail.com>

Sent: Tuesday, January 17, 2023 8:53 PM

To: Esther Sanchez <ESanchez@oceansideca.org>; City Council <Council@oceansideca.org>; Ryan Keim <rkeim@oceansideca.org>; kjensen@oceanside.ca; pweiss@oceanside.ca.org

Subject: No on Eddy Jones Project

Warning: External Source

Hello,

I am writing to discuss our position on the Eddy Jones project. As I sit here and watch the developers on zoom try to explain this monstrous project my heart sinks. To think that 765 trucks will add DAILY TRAFFIC to my beloved neighborhood seems a crime. There are 200 homes in our neighborhood and they are even talking about another development. Our neighborhood is up in arms. What can we do to keep this from happening?

Sincerely,

Pia and Tom Woessner
595 Spindlewood Court
Oceanside, 92058
760.845.0381

Robert Dmohowski

From: Sandy Browder <scbrowder7@gmail.com>
Sent: Wednesday, January 18, 2023 9:33 AM
To: outreach@eddiejonesproject.com
Cc: Robert Dmohowski
Subject: Fwd: Eddie Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Begin forwarded message:

From: Sandy Browder <scbrowder7@gmail.com>
Date: January 18, 2023 at 9:24:54 AM PST
To: outreach@eddiejonesproject.com
Cc: rdmohowski@oceanside.org
Subject: Eddie Jones

Dear Mr. Adam Robinson,

This project should not be proceeding without a discussion of another bridge over San Luis Rey river. Traffic caused by the planned expansion will choke off all of the local residents access to Highway 76. There are no alternative routes other than Foussat, which will be congested due to an existing new project, for our neighborhoods. I have lived for the past 20 years directly north of the proposed Eddie Jones Warehouse Facility project. I chose to live in this area specifically because of a low density aspect of this neighborhood. I am adamantly opposed to the facility for the following reasons:

1. The traffic will be too congested for us to be able to move about freely due to only 2 ways to go in and out of the area; Foussat and Benet Roads. There are about 2000 homes in our community and possibly 3000 cars that travel on those two roads. I understand that there used to be a third bridge planned out of the area but I have not been able to find out what happened to that plan.
2. It is already a noisy area because of the airport and the shooting range. With 114 semi-truck terminals and 60 truck parking places, the area will be a grand central station. We will already be greatly impacted by the development of Ocean Kamp on Foussat Road.
3. Oceanside has a water treatment plant that utilizes the resources from San Luis Rey River. It doesn't sound logical to put a manufacturing plant that will most likely involve hazardous materials that would pollute the river.

4. If our area had been a high density neighborhood, this new project would not have been totally inconsistent with the surrounding areas.

I hope the project will take these serious concerns of the residents of this area and scale way back the project or build us another bridge as a service road.

Thank you for your attention.

Sandy Browder

Robert Dmohowski

From: RonSteffen <22287mulholland@gmail.com>
Sent: Thursday, January 19, 2023 9:16 AM
To: Robert Dmohowski
Cc: Carol Steffen; Stephanie.Dimakides@gmail.com
Subject: Eddie Jones Warehouse EJW

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Robert,

We met a while back when EJW was first announced. I own 603 Wala Dr. I'm working with the HOA to oppose the CUP request from the developer. The Massive scale of a 550,000 sf 45' clear height building with Heavy Tractor Trailer traffic will change Benet Roads atmosphere and dampen property values in Wanis View Estates.

Please inform me of the permit approval process for Oceanside. In particular what offices or committees process CUP applications.

Ronald Steffen
C: 818 425 8780

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Thursday, January 19, 2023 10:26 AM
To: Arlene Tendick
Cc: John Bucalo; Robert Dmohowski
Subject: Eddie Jones Project alternative suggestions

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hello Arlene -

Following up on the meeting this past Tuesday night, I wanted to send some suggestions as you requested when reading my comment that said you spent nearly 3 hours deflecting community questions with "this land is zoned for industrial so we're allowed to do this" and "change is hard" comments.

Personally, I am more concerned about the impact to my family's quality of life and home equity (aka our life's savings) if you build what is proposed right in front of our home, than I am with traffic. You mentioned in the meeting that this zoning pre-dates our homes... however what you failed to recognize is that the City of Oceanside allowed thousands of homes to be built within 1,500 feet when the subject property had buildings that were only 1/3 of the footprint now proposed. So to your comment, I would say our homes now pre-date your project and that is more important than some antiquated zoning that was implemented 50 years ago when city planners had little vision for what coastal Oceanside would become. There's a reason why Encinitas, Solana Beach and Carlsbad are spared this kind of development so close to the ocean. Even Adam said "my neighborhood isn't zoned for something like this" when I asked if he would build a project like this in his neighborhood. What we should ask ourselves is what do we want Oceanside to become in the future? An industrial hub or a tourist attraction? The city seems to be struggling with this question. A project like what is currently proposed at Eddie Jones belongs in a remote commercial area, not 2 miles from the breach in prime coastal real estate.

I appreciate that you asked me to send you some suggestions on how you could modify the proposed project, so here's a few for the developer to consider instead of building a convention center sized building with over 100 semi truck bays in our backyard.

1 - in the surrounding community of the Carlsbad airport, there are trampoline parks, an ice skating rink, soccer fields. Why not reduce the size of the building and put some soccer fields and other rec facilities on site?

2 - in Solana Beach, the industrial fashion/furniture Cedros district brings the community commerce and a unique attraction (rated #3 of 21 things to do in Solana Beach on TripAdvisor). Why not get more creative about what can be done with the Eddie Jones land?

3 - in Encinitas, companies like Nixon and Flock Freight are occupying buildings in the main downtown area for new age workspaces. Why not create covid-era, healthy living office buildings that encourage employees to

ride bikes to work? After all, the bike trail entrance is adjacent to the project and runs inland for 10 miles from the beach. That is a rare asset - why not leverage it?

I feel like the developer lacks creativity on what can be done in this space. This is 2023 not 1973. We should insist on more innovation in our buildings now.

Thanks for considering these suggestions.

Gretchen Gary

Robert Dmohowski

From: Jonathan Borrego
Sent: Friday, January 20, 2023 9:05 AM
To: Robert Dmohowski; Darlene Nicandro; Sergio Madera
Subject: FW: Eddie Jones Project alternative suggestions

Follow Up Flag: Follow up
Flag Status: Completed

Categories: Red Category

fyi

Jonathan E. Borrego
City Manager
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054
760-435-3918
jborrego@oceansideca.org



From: Gretchen Gary <gretchengary@gmail.com>
Sent: Friday, January 20, 2023 8:49 AM
To: Esther Sanchez <ESanchez@oceansideca.org>; Eric Joyce <EJoyce@oceansideca.org>
Cc: City Council <Council@oceansideca.org>
Subject: Fwd: Eddie Jones Project alternative suggestions

Warning: External Source

Good morning Mayor Sanchez, Council Member Eric Joyce, and members of the City Council.

As a (very) concerned tax-paying Oceanside resident, I am forwarding an email I sent to the community outreach person hired for the Eddie Jones project near the airport. For a little context, I think it's important to note that I am in favor of responsible development of this part of Oceanside, which has unfortunately seen the downside of poor city planning with too many ugly industrial buildings and a growing homeless problem. I'm looking forward to the Ocean Kamp project as I know it will bring the right kind of attention to our area. However the current proposal for the Eddie Jones project is terrifying. I own a home within 1,500 ft of the project and have a direct view of it from my bedroom. I bought here knowing there was an industrial area just in front of it and didn't have any issues with the small airport (which I was assured would never expand) or the previous commercial operation that was in this space. Had I known there would one day be a proposal for a convention center sized distribution center with 114 truck bays, I would have never considered buying here. If you and the city planners approve this project, no one will want to buy or rent the homes in our

community, especially those that face the project along Toopal and Tishmal Drive. It will be a devastating blow to all of us who live here. The building is so massive (550K sq ft) that it will literally dwarf our community.

As I say in my email below, it is 2023 not 1973. We should require more innovation and forward thinking on what we want Oceanside to become. Not more of the same over-industrialized use of prime coastal real estate.

Thank you for reading this email and the one I am forwarding. This is a very important topic for me.

Gretchen Gary

----- Forwarded message -----

From: **Gretchen Gary** <gretchengary@gmail.com>

Date: Thu, Jan 19, 2023 at 10:26 AM

Subject: Eddie Jones Project alternative suggestions

To: Arlene Tendick <outreach@eddiejonesproject.com>

Cc: John Bucalo <racetrackspecialties@gmail.com>, Robert Dmohowski <rdmohowski@oceansideca.org>

Hello Arlene -

Following up on the meeting this past Tuesday night, I wanted to send some suggestions as you requested when reading my comment that said you spent nearly 3 hours deflecting community questions with "this land is zoned for industrial so we're allowed to do this" and "change is hard" comments.

Personally, I am more concerned about the impact to my family's quality of life and home equity (aka our life's savings) if you build what is proposed right in front of our home, than I am with traffic. You mentioned in the meeting that this zoning pre-dates our homes... however what you failed to recognize is that the City of Oceanside allowed thousands of homes to be built within 1,500 feet when the subject property had buildings that were only 1/3 of the footprint now proposed. So to your comment, I would say our homes now pre-date your project and that is more important than some antiquated zoning that was implemented 50 years ago when city planners had little vision for what coastal Oceanside would become. There's a reason why Encinitas, Solana Beach and Carlsbad are spared this kind of development so close to the ocean. Even Adam said "my neighborhood isn't zoned for something like this" when I asked if he would build a project like this in his neighborhood. What we should ask ourselves is what do we want Oceanside to become in the future? An industrial hub or a tourist attraction? The city seems to be struggling with this question. A project like what is currently proposed at Eddie Jones belongs in a remote commercial area, not 2 miles from the breach in prime coastal real estate.

I appreciate that you asked me to send you some suggestions on how you could modify the proposed project, so here's a few for the developer to consider instead of building a convention center sized building with over 100 semi truck bays in our backyard.

1 - in the surrounding community of the Carlsbad airport, there are trampoline parks, an ice skating rink, soccer fields. Why not reduce the size of the building and put some soccer fields and other rec facilities on site?

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3 - in Encinitas, companies like Nixon and Flock Freight are occupying buildings in the main downtown area for new age workspaces. Why not create covid-era, healthy living office buildings that encourage employees to ride bikes to work? After all, the bike trail entrance is adjacent to the project and runs inland for 10 miles from the beach. That is a rare asset - why not leverage it?

I feel like the developer lacks creativity on what can be done in this space. This is 2023 not 1973. We should insist on more innovation in our buildings now.

Thanks for considering these suggestions.

Gretchen Gary

Robert Dmohowski

From: Nicole Douglas <nicole.nnh@gmail.com>
Sent: Thursday, January 19, 2023 12:08 PM
To: Zeb Navarro; Peter Weiss; Rick Robinson; Eric Joyce; Ryan Keim; Esther Sanchez; Robert Dmohowski
Subject: Eddie Jones and Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear Oceanside Leaders,

As an Oceanside resident and business owner, I am writing to **strongly oppose** the Eddie Jones Warehouse/manufacturing and Distribution Facility project. This area already has way too much traffic, congestion, and pollution, we do not need to further exacerbate the problems. This has a direct effect on safety and emergency services, as well as worsening the already bad traffic and pollution. The community does NOT want this, please respect the citizens and constituents of this town and do not allow this project to move forward.

Nicole Douglas
3233 Roberta Ln, Oceanside, CA 92054
760-893-6481

Robert Dmohowski

From: Anna Kasperowicz Kasperowicz <akasperowicz@yahoo.com>
Sent: Thursday, January 19, 2023 7:39 PM
To: Arlene Tendick
Cc: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim
Subject: Re: Eddie Jones Follow Up + Thank You

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hello Arlene

Thank you for reaching out. I would like say the meeting was a big disappointment. I was hoping to speak comments and so were the other homeowners. This webinar was a lecture presentation. This was not 2 hours of you collaborating with the community. Your staff made this no open forum to speak or comment. Next meeting I would hope the city council be present as they should hear what our concerns are. Let's work together and allow each member to have a few minutes same as city meetings maybe we can have sign up in advance. I included the city planner on this email. Here are my suggestions for next community meetings. We are not interested in slide presentations to take up our evenings taking time away from our families. If you care about community members dialogue between community members. Most upsetting about the meeting was that your staff skirted questions with answers not answering things such as decline in property values if this warehouse is built. New families will not want to live next to an Amazon warehouse and retirees get noises trucks to enjoying hearing. I live next to a stop sign on Benet. Just to fill you in on how this impact me. My son and daughter have windows to enjoy trucks passing by to stop and accelerate to 45 miles an hour. How does this not impact my quality of life as a homeowner. This city says trucks can drive this street. Yet you say they will not. Well how will you and the city prevent this. This project has no benefit for the community at this location and will make the quality of life detrimental to the value of the homes, safety, health of the community. The quality of life here are as owners will be terrible Lastly, the traffic report was from two years ago. This data is outdated and inaccurate now. The pollution and traffic will be must greater. I believe you should be required to update this with an independent party. You say you will increase traffic by 1 percent. But that is not accurate based on an current data as of January 2023 and you have not taken the other new very large projects into consideration.

Respectfully

Anna Floyd

On Thursday, January 19, 2023, 05:38:48 PM PST, Arlene Tendick <outreach@eddiejonesproject.com> wrote:

Hello everyone,

On behalf of our team, thank you for your time and patience during Tuesday's informational meeting on the proposed Eddie Jones warehouse project. There were more than 200 comments submitted so the Q&A portion took longer than expected, but we hope it was informative. [Here is a link](#) to the slides.

I committed to being transparent, so I did want to address one comment that has come up a couple times regarding the assumption that we omitted submissions that didn't suit us. We read all comments, even the negative ones. There were many of the same questions submitted so we made sure that all the answers responded to what we were seeing. The only comments we did not read out loud were those that were calling us names or being disrespectful. Additionally, we wanted everyone to have the same opportunity to submit questions and felt that the Q&A portal was the best way to ensure no one voice monopolized the conversation. It was not an attempt to avoid hearing from you. The sheer volume of questions is testament to that. This is just one meeting in what we hope will be an on-going and productive dialogue.

We are in the process of going back through all the questions and preparing an FAQ document that will include answers to the questions that were submitted. We will share that in the coming week.

In the meantime, I've received several emails with follow up questions or requests for meetings, so I'm happy to see the willingness to continue the conversation.

Thank you again for your participation!

Arlene

Community Outreach

Robert Dmohowski

From: Jonathan Borrego
Sent: Friday, January 20, 2023 9:05 AM
To: Robert Dmohowski; Darlene Nicandro; Sergio Madera
Subject: FW: Eddie Jones Project alternative suggestions

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

fyi

Jonathan E. Borrego
City Manager
City of Oceanside
300 North Coast Highway
Oceanside, CA 92054
760-435-3918
jborrego@oceansideca.org



From: Gretchen Gary <gretchengary@gmail.com>
Sent: Friday, January 20, 2023 8:49 AM
To: Esther Sanchez <ESanchez@oceansideca.org>; Eric Joyce <EJoyce@oceansideca.org>
Cc: City Council <Council@oceansideca.org>
Subject: Fwd: Eddie Jones Project alternative suggestions

Warning: External Source

Good morning Mayor Sanchez, Council Member Eric Joyce, and members of the City Council.

As a (very) concerned tax-paying Oceanside resident, I am forwarding an email I sent to the community outreach person hired for the Eddie Jones project near the airport. For a little context, I think it's important to note that I am in favor of responsible development of this part of Oceanside, which has unfortunately seen the downside of poor city planning with too many ugly industrial buildings and a growing homeless problem. I'm looking forward to the Ocean Kamp project as I know it will bring the right kind of attention to our area. However the current proposal for the Eddie Jones project is terrifying. I own a home within 1,500 ft of the project and have a direct view of it from my bedroom. I bought here knowing there was an industrial area just in front of it and didn't have any issues with the small airport (which I was assured would never expand) or the previous commercial operation that was in this space. Had I known there would one day be a proposal for a convention center sized distribution center with 114 truck bays, I would have never considered buying here. If you and the city planners approve this project, no one will want to buy or rent the homes in our

community, especially those that face the project along Toopal and Tishmal Drive. It will be a devastating blow to all of us who live here. The building is so massive (550K sq ft) that it will literally dwarf our community.

As I say in my email below, it is 2023 not 1973. We should require more innovation and forward thinking on what we want Oceanside to become. Not more of the same over-industrialized use of prime coastal real estate.

Thank you for reading this email and the one I am forwarding. This is a very important topic for me.

Gretchen Gary

----- Forwarded message -----

From: **Gretchen Gary** <gretchengary@gmail.com>

Date: Thu, Jan 19, 2023 at 10:26 AM

Subject: Eddie Jones Project alternative suggestions

To: Arlene Tendick <outreach@eddiejonesproject.com>

Cc: John Bucalo <racetrackspecialties@gmail.com>, Robert Dmohowski <rdmohowski@oceansideca.org>

Hello Arlene -

Following up on the meeting this past Tuesday night, I wanted to send some suggestions as you requested when reading my comment that said you spent nearly 3 hours deflecting community questions with "this land is zoned for industrial so we're allowed to do this" and "change is hard" comments.

Personally, I am more concerned about the impact to my family's quality of life and home equity (aka our life's savings) if you build what is proposed right in front of our home, than I am with traffic. You mentioned in the meeting that this zoning pre-dates our homes... however what you failed to recognize is that the City of Oceanside allowed thousands of homes to be built within 1,500 feet when the subject property had buildings that were only 1/3 of the footprint now proposed. So to your comment, I would say our homes now pre-date your project and that is more important than some antiquated zoning that was implemented 50 years ago when city planners had little vision for what coastal Oceanside would become. There's a reason why Encinitas, Solana Beach and Carlsbad are spared this kind of development so close to the ocean. Even Adam said "my neighborhood isn't zoned for something like this" when I asked if he would build a project like this in his neighborhood. What we should ask ourselves is what do we want Oceanside to become in the future? An industrial hub or a tourist attraction? The city seems to be struggling with this question. A project like what is currently proposed at Eddie Jones belongs in a remote commercial area, not 2 miles from the breach in prime coastal real estate.

I appreciate that you asked me to send you some suggestions on how you could modify the proposed project, so here's a few for the developer to consider instead of building a convention center sized building with over 100 semi truck bays in our backyard.

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I feel like the developer lacks creativity on what can be done in this space. This is 2023 not 1973. We should insist on more innovation in our buildings now.

Thanks for considering these suggestions.

Gretchen Gary

Robert Dmohowski

From: Jeannie Dung pham <dp999_98@yahoo.com>
Sent: Friday, January 20, 2023 10:39 AM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro
Subject: Fw: Please see the attached plan

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hi Ladies and Gentlemen in Oceanside city plan,

Please read the email below and understand our request in this email:

XX
XXXXXX

Hi All,

I think the zoom meeting last night (Jan 17, 2023) was fake.

We shouldn't call a Zoom meeting. It is a presentation of a group from Eddie Jones Warehouse to show their slides presentation.

Because of the definition of an open meeting. It should have 2 ways of communication. This is one way a group of people dominates the meeting. Or actually, my neighbor called a dictator of Eddie Jones warehouse and staff of Oceanside city.

I typed a request on the zoom meeting to be unmuted but they all ignored my requests.

I raised 2 questions:

1.- Safe issues. Those trucks will increase traffic and cause bottleneck traffic between HW76 and the intersection red light of Benet. I and my children ride bikes daily in Benet and HW 76.

We just want to bring up that safety issue but they don't care.

no problem solving from the OS city or Eddie Jones warehouse (Dan. Niebaun; Brian. Koshley; Adam. Robinson).

2.- If the staff of Eddie Jones Warehouse (Dan. Niebaun; Brian. Koshley; Adam. Robinson) said on the slides to show their presentation that at peak time there is only 1% of daily trucks move. We request that their employees can turn from HW 76 to Benet but those trucks should use the intersection between HW76 and Airport Road instead to use Benet.

Their staff (Dan. Niebaun; Brian. Koshley; Adam. Robinson) don't care about my question. They skipped or ignored.

I had been spending 2 hours on the meeting for nothing.

Waste my time.

Finally, I believe in GOD we trust.

I wish, whoever has the "Title as Professional Engineers" or project leader sit down and think over "Safety Issues" for residents in KB homes. "

XX
XXXXX

Please help us to solve this problem as I mentioned above is a "SAFETY ISSUE" for residents in the area of Benet Road and Fausset Road in Oceanside

Thank you very much for your help.

JP

----- Forwarded Message -----

From: Arlene Tendick <outreach@eddiejonesproject.com>

To: Jeannie Dung pham <dp999_98@yahoo.com>; RonSteffen <22287mulholland@gmail.com>; cs22620@gmail.com <cs22620@gmail.com>; Stephanie Dimakides <stephanie.dimakides@gmail.com>; Chloe Holder <chloe@avalonweb.com>; Pgrosa14@yahoo.com <pgrosa14@yahoo.com>; oceanside@avalonweb.com <oceanside@avalonweb.com>

Sent: Wednesday, January 18, 2023 at 03:45:15 PM PST

Subject: RE: Please see the attached plan

Hi Jeannie,

I'm sorry you feel that way. We received 207 comments and questions from attendees at the meeting. This format allowed everyone to have the same access – and avoid any one person dominating the meeting. The meeting went an hour past the time we'd allocated, and **every question and comment was addressed**, including the ones you listed below. To summarize, in response to comment 1, we shared that the missing segment of sidewalk will be added to Benet. We too care about safety and this new sidewalk segment will improve pedestrian access. In response to question 2 regarding your suggestion to route trucks to Airport Road – the project traffic engineer responded to this as well that it is not the most efficient circulation and wouldn't make sense with how the site is accessed. You noted that the meeting was two hours long, but it was actually 3. Perhaps these answers were given after you left the meeting.

If you'd like to dig into any of these topics in greater detail, I'd be happy set up a one-on-one meeting or a small group meeting with the people included here. As we shared, this isn't the only opportunity to learn more about the project and provide feedback. Please let me know if you'd like to get together.

Arlene

Community Outreach

From: Jeannie Dung pham <dp999_98@yahoo.com>

Sent: Wednesday, January 18, 2023 3:23 PM

To: RonSteffen <22287mulholland@gmail.com>; cs22620@gmail.com; Stephanie Dimakides <stephanie.dimakides@gmail.com>; Chloe Holder <chloe@avalonweb.com>; Pgrosa14@yahoo.com; Arlene Tendick <outreach@eddiejonesproject.com>; oceanside@avalonweb.com

Subject: Re: Please see the attached plan

Hi All,

I think the zoom meeting last night (Jan 17, 2023) was fake.

We shouldn't call a Zoom meeting. It is a presentation of a group from Eddie Jones Warehouse to show their slides presentation.

Because of the definition of an open meeting. It should have 2 ways of communication. This is one way a group of people dominates the meeting. Or actually, my neighbor called a dictator of Eddie Jones warehouse and staff of Oceanside city.

I typed a request on the zoom meeting to be unmuted but they all ignored my requests.

I raised 2 questions:

1.- Safe issues. Those trucks will increase traffic and cause bottleneck traffic between HW76 and the intersection red light of Benet. I and my children ride bikes daily in Benet and HW 76.

We just want to bring up that safety issue but they don't care.

no problem solving from the OS city or Eddie Jones warehouse (Dan. Niebaun; Brian. Koshley; Adam. Robinson).

2.- If the staff of Eddie Jones Warehouse (Dan. Niebaun; Brian. Koshley; Adam. Robinson) said on the slides to show their presentation that at peak time there is only 1% of daily trucks move. We request that their employees can turn from HW 76 to Benet but those trucks should use the intersection between HW76 and Airport Road instead to use Benet.

Their staff (Dan. Niebaun; Brian. Koshley; Adam. Robinson) don't care about my question. They skipped or ignored.

I had been spending 2 hours on the meeting for nothing.

Waste my time.

Finally, I believe in GOD we trust.

I wish, whoever has the "Title as Professional Engineers" or project leader sit down and think over "Safety Issues" for residents in KB homes.

Thanks

On Wednesday, January 18, 2023 at 09:19:20 AM PST, Chloe Holder <chloe@avalonweb.com> wrote:

Hello Everyone!

I wanted to introduce Board Member Stephanie Dimakides who is the Board point of contact regarding the Eddie Jones Warehouse. She would like to be included in these emails correspondence. Please remove Wanis@avalonweb.com and replace with Stephanie.Dimakides@gmail.com. She will then share any important information with the rest of the Board.

----- Forwarded message -----

From: **Ronald Steffen** <22287mulholland@gmail.com>

Date: Mon, Jan 16, 2023 at 4:08 PM

Subject: Please see the attached plan

To: Jeannie Dung pham <dp999_98@yahoo.com>, <Wanis@avalonweb.com>

Cc: Carol Steffen <cs22620@gmail.com>

Oceanside is bound by this plan unless they change it. The plan speaks to uses compatible with the airport and about safety. It appears that the construction of the proposed warehouse is not compatible with the plan shown above. Note pages of Exhibit IV 4-33, 4-35 and 4-37 address Safety and Airspace protection. There is a map on page 333 which shows the "RPZ" at the westerly end of the runway, which zone should be minimally impacted by traffic for safety reasons. The proposed Eddie Jones Warehouse ("EJW") would dramatically reduce safety in the RPZ by increasing heavy tractor trailer traffic through the RPZ "round the clock". 80% of Takeoffs are on Runway 24, over the Westend RPZ.

The affects of the proposed EJW on airflow and air turbulence is a factor that need to be evaluated. When there are strong onshore flows, the mass of the EJW could interfere with airflows, thus causing air turbulence that could affect the safety and operation of small light single aircraft, which make over 90% of the flights at the Oceanside Airport. It is likely that this is a major reason why prior construction at Oceanside Airport has been confined to Light Industrial buildings. It makes sense for the safety of flights in and out of Oceanside Airport to maintain the Light Industrial profile and to turn down the proposed special use permit the 550,000 EJW which will have significant height and mass adjacent to the runways.

Quoting from Page 302 of the report"

"A significant drawback of standard general plan and zoning ordinance land use designations is that they can be changed. Uses that are currently compatible may not continue to be compatible in the future. Designation of areas of special compatibility concern would serve as a reminder that airport impacts should be carefully considered in any decision to change the existing land use designation. [A legal consideration supporting the value of this concept is that down-zoning of a property to a less intensive use is becoming more difficult. It is much better not to inappropriately up-zone the property in the first place."

Basically, Constructing the proposed EJW will decrease safety at Oceanside Airport in addition to forcing a lot more heavy tractor trailer traffic on Benet Road, which is in the Westerly RPZ.

om: Ronald Steffen <22287mulholland@gmail.com>

Sent: Saturday, January 14, 2023 1:10 PM

To: 'Carol Steffen' <cs22620@gmail.com>

Cc: 'Jeannie Dung pham' <dp999_98@yahoo.com>

Subject: RE: Please view the map that I purposed those trucks from HW76 turn to Benet instead they should turn to Airport road.2

From: Jeannie Dung pham <dp999_98@yahoo.com>

Sent: Saturday, January 14, 2023 11:31 AM

To: 22620@gmail.com; 22287mulholland@gmail.com

Subject: Re: Please view the map that I purposed those trucks from HW76 turn to Benet instead they should turn to Airport road.2

Hi Ron,

I think you gave me Carol's email not correct. It got bounced back.

Please forward my email to her. Thanks

On Saturday, January 14, 2023 at 11:28:13 AM PST, Jeannie Dung pham <dp999_98@yahoo.com> wrote:

FYI

----- Forwarded Message -----

From: Jeannie Dung pham <dp999_98@yahoo.com>

To: 22287mulholland@gmail.com <22287mulholland@gmail.com>; 22620@gmail.com <22620@gmail.com>

Sent: Saturday, January 14, 2023 at 11:20:21 AM PST

Subject: Please view the map that I purposed those trucks from HW76 turn to Benet instead they should turn to Airport road.2

Hi Ron and Carol,

I stopped by this morning and talked to your neighbor at address 609 Wala Rd. He said he will attend the meeting on Jan 17, 2023, between 6:30 pm to 8:30 pm. I also gave him your phone number.

My purpose is:

Those trucks from Eddi Jones warehouse should go straight to Airport road and turn left or right from HW76 instead to turn in Benet road that will solve the bottom neck traffic

<image001.png>

Thank you.

JP

--

Chloe Holder, CMCA

The Avalon Management Group, Inc., AAMC

3618 Ocean Ranch Blvd
Oceanside, CA 92056

Phone: (760) 481-7444 x310
Website: www.AvalonWeb.com
Fax: (800) 646-1887

Planning on making an improvement to your home? [Watch our ARC Video](#)

Did you get a letter or need to report a problem about home maintenance? [Watch our CRC Video](#)

Please let us know how we are doing: [Survey](#)

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--

Chloe Holder, CMCA

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Attn: Rob Dmohowski, Principal Planner

300 N. Coast HWY.

Oceanside, CA 92057

Email= RDMOHOWSKI@OCEANSIDECA.ORG

My name is DUA. T. PHAM. I'm a resident at 3194 HUULA Dr. OCEANSIDE, CA 92058.

I also raise our voices to many other residents in the Benet Road area (KB Homes builder)

I would like to send this letter against the project Eddi Jones Warehouse, Manufacturing And Distribution Facility (APN 145-021-29-00; 145-021-030-00).

We have many children who go to school every day and walk to high school and they ride bikes every day.

If this manufacturing builds the warehouse. They will cause traffic and big trucks will be dangerous traffic to our community environment.

Please consider safety for children who are living in dangerous environments incidents will occur.

I would like to write this letter to beg you to stop this project. Please consider for few Hundreds of thousands of residents are living here. They are in peace now. If you allow them to build this warehouse you will feel sorry in the future.

I ask you with all your power please DON'T APPROVE this project. It means you help us.

I ask your power to stop them.

I pray GOD blesses you and your family.

Thank you very much.


PHAM DUA

Robert Dmohowski

From: Amanda Cockerham <acockerham@gmail.com>
Sent: Sunday, January 22, 2023 2:15 PM
To: Robert Dmohowski
Subject: No to Eddie Jones development (and Ocean Kamp)
Attachments: No to Eddie Jones Development.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hi Rob,

I want to register my voice as another who does not support the Eddie Jones project (or the Ocean Kamp)

I live just behind the river on Big Sky Drive. I bought in this area because I liked how remote and quiet it was. A huge distribution facility will add too much noise and traffic. This neighborhood will be pinned in between the facility and the "kamp".

Ocean Kamp should be required to provide at the very least a grocery store for the area. Last I heard, it was going to be only a resort and restaurants which are useless to our community. Since traffic is going to be increased in the area, the least they could do is provide the residents something closer that prevents them from having to leave the direct area.

The area has survived for a long time without development on these sites, and will continue to thrive in their absence.

Thank you,
Amanda Cockerham
Owner of 548 Big Sky Dr, Oceanside, CA 92058
650-575-3267

City Of Oceanside
Development Services Department
Attn: Rob Dmohowski, Principal Planner
300 N. Coast Highway
Oceanside, CA 92057

E-Mail: rdmohowski@oceansideca.org

I am a member of the community immediately north of the proposed Eddie Jones Warehouse, Manufacturing & Distribution Facility located at 250 Eddie Jones Way in the city of Oceanside. I am **against** the establishment of this facility for the following reasons:

1. This community has ONLY TWO ingress and egress roads; Foussat Road and Benet Road. There is no other way to access any point south of the San Luis Rey River by car or by foot. There is no other way for our community to receive emergency services. With severe congestion expected with the development of Ocean Kamp, a 79-acre development containing retail, office space, residential units, rental units, and a large wave park; this congestion is expected to choke off access to our community via use of Foussat Road.

With the introduction of the Eddie Jones Warehouse, Manufacturing & Distribution (with 114 semi-truck terminals, and 60 truck parking spaces/auto parking), allowing such project which will effectively choke off Benet Road not to mention creating severe congestion on SR-76 around Benet Road and Foussat Road. This community will effectively be cut off from Oceanside.

2. Noise easily travels into our neighborhood from the shooting range and the airport which is farther away than the proposed Eddie Jones Warehouse project. We have chosen to live in this area even with our small airport noise, but this project will inject a severity of sound we do not want. In as much, a project this large, with potentially 24-hour operations, will flood our neighborhoods with noise from forklifts, (OSHA) semi-truck/tractor engines, manufacturing noise and light pollution that will detrimentally affect the entire area north of this proposed project affecting roughly 2000+ homes.
3. It is reasonable to assume that manufacturing often uses hazardous materials which, with a facility this size (547,320 SF), could expose this entire community (roughly 2000+ homes/4000+ people) to exposure and catastrophic losses in the event of a fire or spill.
4. Air quality will also suffer with the expected semi-truck traffic (diesel exhaust/fumes) effectively harming the Alex Road Skate Park, immediately adjacent bike path, Ocean Kamp project, as well as our entire community.

These are our concerns and the concerns of our neighbors. This project is **not right** for this community or this area. In essence, our community will become choked off by industry. Our home values will suffer. Our health will suffer. Our children will suffer. Our very survival will suffer. Please do not let this happen.

Name (Printed) Amanda Cockerham

Address: 548 Big Sky Dr Oceanside CA 92058

Signed: 

Oceanside, CA 92058

Date: 08/___/2022

11/22/2023

Robert Dmohowski

From: Dylan Madello <dylanmadello@yahoo.com>
Sent: Monday, January 23, 2023 5:59 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Mr. Dmohowski,

I am writing you in regards to the Eddie Jones warehouse project. This development as proposed is not a good fit with the Airport community, nearby neighborhoods/businesses and those who travel the 76 on a regular basis. I do not oppose development as long as it is well thought out and is compatible with the residential area that it borders. I believe this project as proposed is too large and will have too big of a negative impact on our nearby communities.

Please reconsider approval for this project. Protect those who live, work and travel on the 76.

Thank you,
Dylan Madello
319 Rimhurst Ct
Oceanside, CA.

Sent from my iPhone

Robert Dmohowski

From: Justin Floyd <justin@jfcfcapitaladvisors.com>
Sent: Monday, January 23, 2023 1:18 PM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim
Cc: outreach@eddiejonesproject.com
Subject: RE: Eddie Jones Follow Up + Thank You

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

To All,

Plenty of real substantive support and issues here as stated below as to why the city must deny approval of this project.

- There should be a minimum 1,000 feet distance from any residential area and on main roads to which this proposed project is not
- Material adverse effects of diesel engine exhaust on respiratory health
- Excessive truck traffic on neighborhood streets and nearby highway access
- Property value declines

This property location has a much better land use than as proposed.

Respectfully,
Concerned Constituent
Justin

[Americans Are Pushing Back on the Warehouse Construction Boom - WSJ](#)

Opposition to distribution centers over noise, pollution and truck traffic is growing

Warehouses & Distribution Centers: Potential Adverse Effects

While warehouses or distribution centers provide vital services, **poorly planned projects can cause harm to neighborhoods and the environment**, due to:

- Adverse health effects due to diesel exhaust,
- Excessive truck traffic on neighborhood streets,
- Disturbing levels of noise, and
- Property value decline.

The vast majority of new warehouse or distribution centers are built in commercial-industrial areas far removed from homes.

We contacted **homeowners living near these two facilities who reported excessive noise due to:**

- truck engine idling,
- shouting,
- loud music, and
- backup beepers.
-

The homeowners did not report issues with excessive truck traffic on their neighborhood streets since both facilities were accessed via main roads.

We concluded that due to noise and other nuisances alone, new distribution centers and most other truck facilities should be at least a thousand feet from the nearest home and on sites where access via neighborhood streets is unlikely.

Given that most facilities are not near homes, it appears that finding low-impact locations is not that difficult.

Therefore, local governments should consider amending zoning regulations to require that distribution centers and other high-volume truck facilities locate a minimum of 1,000 feet from residential areas and on main roads (major collectors-arterials) where trucks would not pass through a neighborhood.

Diesel Exhaust & Health

There's a large and growing body of research documenting the adverse effects of diesel engine exhaust on respiratory health.

In the 2005 California Air Resources Board (CARB) [Air Quality and Landuse Handbook](#), it was recommended that homes, schools and other sensitive land use should be located at least 1,000 feet from any facility that would generate either:

- 100 diesel truck trips per day,
- more than 40 trucks per day with diesel refrigeration units, or
- where diesel Truck Refrigeration Units (TRU) would operate for more than 300 hours per week.

These recommendation begin on page 11 of the CARB Handbook.

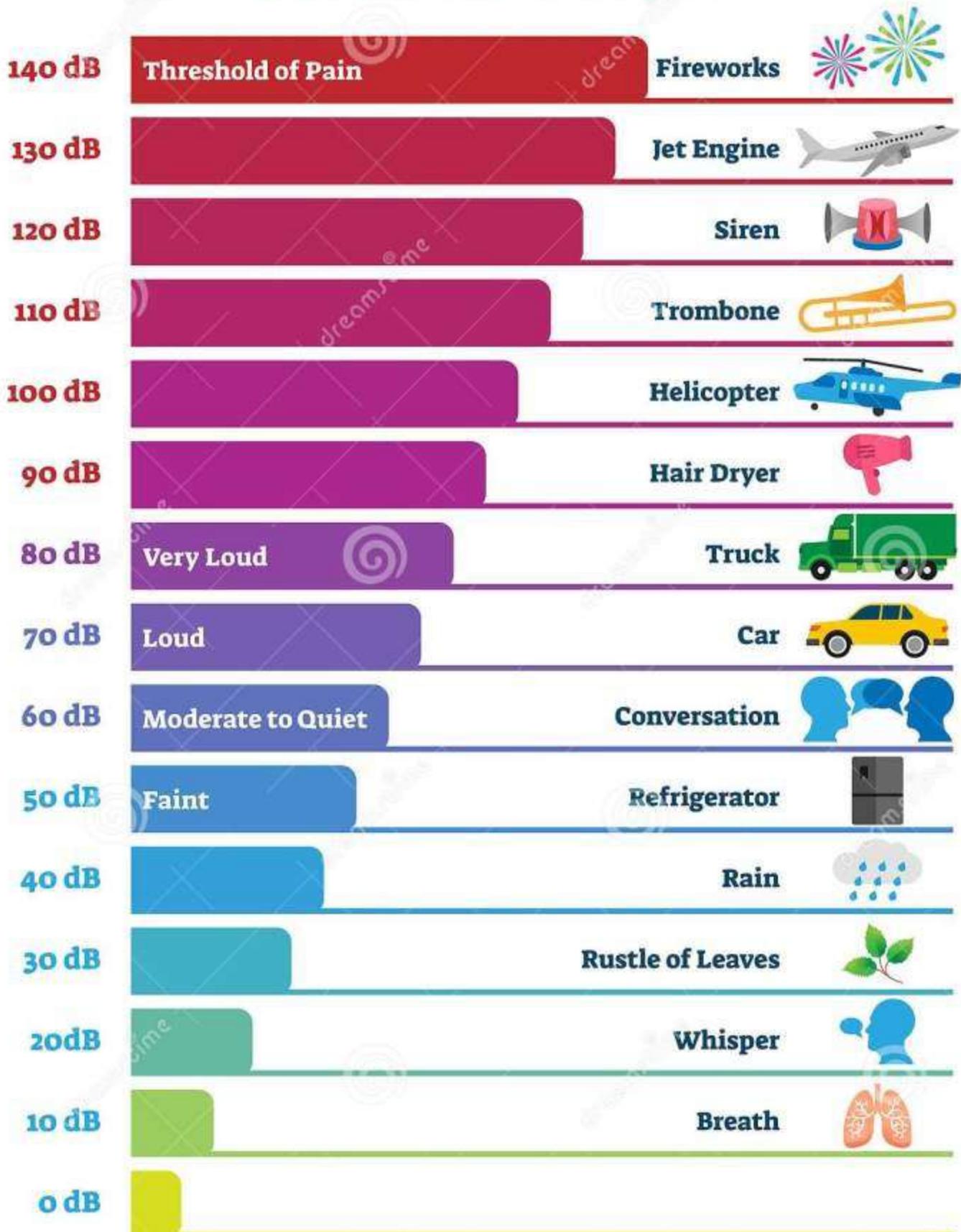
More recently, the California [South Coast Air Quality Management District](#) adopted a [regulation](#) requiring that warehouses of 100,000 square feet or more must take measures to reduce the health impact of trucks as well as other diesel-gasoline powered vehicles. The measures include [zero](#) or [near-zero emission](#) trucks. The [Socioeconomic Impact Assessment](#) for this regulation noted that emissions from a warehouse of 100,000 square feet or more can affect the health of those living 0.5- to 2.0-miles distant.

The [Clean Air Task Force](#) created [Deaths by Dirty Diesel – Mapping the health impacts of diesel nationwide](#). This interactive map will allow you to see how diesel emissions affects those living in your area. By comparing your area with others that have higher Air Pollution from Diesel you can help elected officials and your neighbors understand why it is vitally important to encourage greater use of [Zero](#) or [Near-Zero](#) emission trucks and discourage proposals that would add more conventional diesel truck traffic.

Noise

The chart below compares noise levels (in decibels) from a variety of sources, including trucks. A number of ordinances require that noise not exceed 55 decibels in residential areas (outside a home).

DECIBEL SCALE



CEDS conducted a survey of those living near facilities with a high-volume truck traffic. The neighbors reported excessive noise due to truck engine idling, shouting, loud music, backup beepers, etc. While it is possible that noise barriers or other measures might resolve noise impacts, effectiveness may depend upon maintenance or other provisions that could be difficult to enforce. Therefore, the best safeguard is to locate a new truck stop and most other truck facilities at least a thousand feet from the nearest home.

Planning officials should require a noise impact study for all proposed warehouses and other facilities with a high volume of truck traffic, especially if they are within a thousand feet of homes or trucks will pass through residential areas.

Here's a simplistic example of what such a study might show.

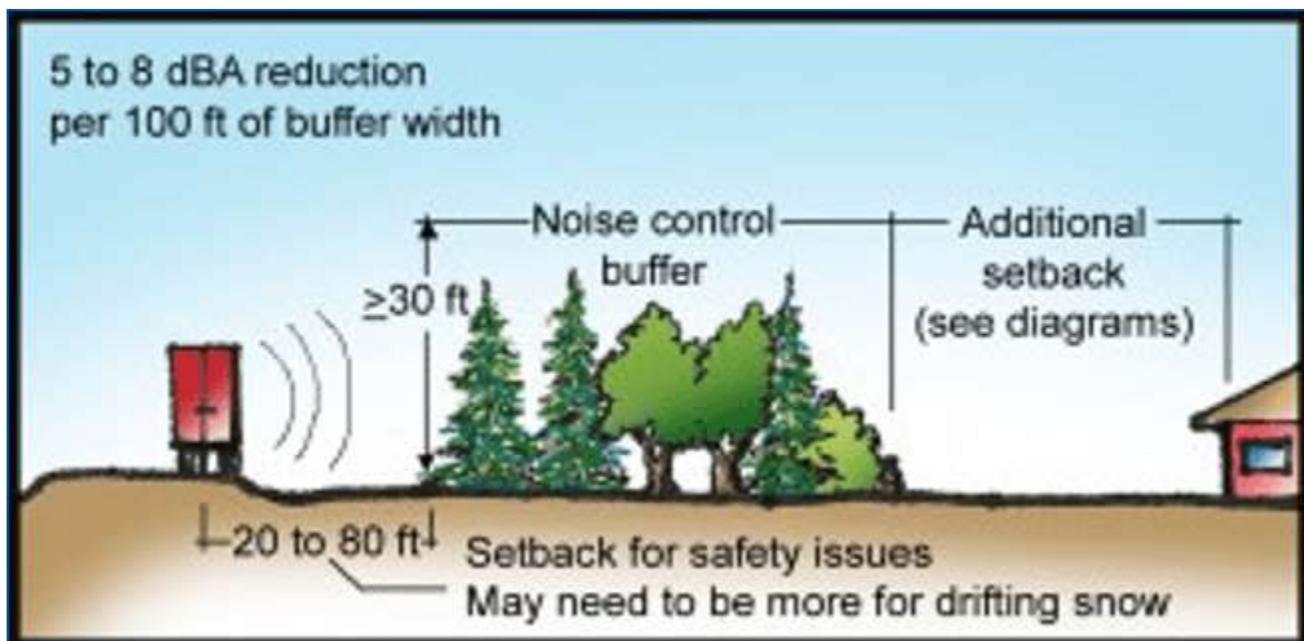
Idling diesel trucks emit noise at 85 dBA (decibels) measured at a distance of 50 feet. In general, noise decreases 6 decibels for every doubling of distance from a source. So, if truck noise level is 85 decibels at 50 feet then it would be 6 decibels lower or:

- 79 decibels at 100 feet,
- 73 decibels at 200 feet,
- 67 decibels at 400 feet and so on.

Simple noise models, such as the [Distance Attenuation Calculator](#), indicate that a separation distance of 1600 feet would be needed for the 85-decibel noise from idling diesel truck engines to drop to the residential property acceptable level of 55 decibels.

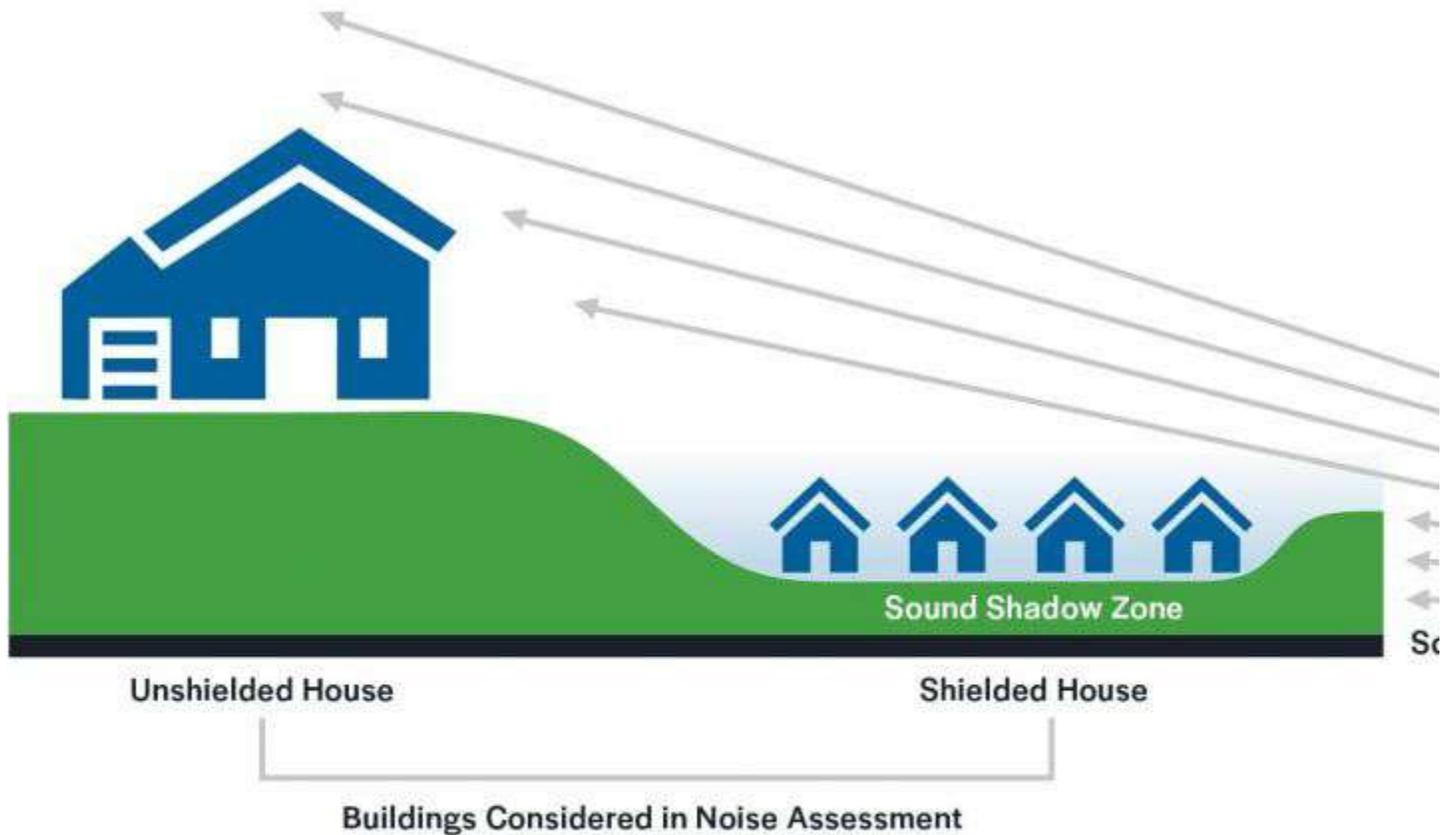
If a home is located 400 feet from the portion of a warehouse site where trucks would be idling the noise level could be 67 decibels, which is 12 decibels above the 55 decibel acceptable level for residential areas.

As shown in the following graphic, a 5 to 8 decibel decrease can be achieved for each 100 feet of densely growing evergreens and shrubs between a noise source and a home.



Earth berms and walls are sometimes used to reduce noise from highways and other sources. As shown in the next graphic, these measures only work in the area of the noise *shadow* created by the

berm or wall. In other words, if a berm or wall does not obstruct your view of a noise source from your home then it may do little to keep your home quiet.



Hilly Area Example

The effect of warehouse noise on property value is covered in the next section of this webpage.

Property Value

Truck noise may be the aspect of a warehouse with the greatest effect on residential property value. With regard to noise and property value, researchers concluded the following in a 2021 study entitled [An Analytical Framework for Evaluating Potential Truck Parking Locations](#):
“Increases in noise pollution are inevitable in such a case where dispersed idling trucks are centralized into the new or expanded truck stops...

Mandated by the Federal Highway Administration, maximum noise levels for large trucks are not to exceed 85 dBA (decibel) 50 feet away. Combined, this data can be used to approximate sound values over different distances. For every 2.5 dBA increase in noise levels above 55 dBA, residential property values are assumed to decrease by 0.2% to 1.2% with wealthier communities, containing higher willingness to pay for peace and quiet, being more sensitive to such increases in noise pollution (Palmquist, 1980). Any truck stop development project will require a noise impact study that evaluates the feasibility of installing noise barriers to remediate the noise pollution problem.”

As noted in the preceding section of this webpage, in general, noise decreases 6 decibels for every doubling of distance from a source. So, if truck noise level is 85 decibels at 50 feet then it would be 79 decibels at 100 feet, 73 decibels at 200 feet, then 67 decibels at 400 feet.

Simple noise models, such as the [Distance Attenuation Calculator](#), indicate that it would require a separation distance of 1600 feet for the 85-decibel noise from idling diesel truck engines to drop to the residential property acceptable level of 55 decibels.

If a home is located 400 feet from the portion of a warehouse site where trucks would be idling the noise level could be 67 decibels, which is 12 decibels above the 55 decibel acceptable level for residential areas. After dividing 12 decibels by 2.5 decibels we get 4.8, which could result in a $(0.2\% \times 4.8)$ 0.96% to $(1.2\% \times 4.8)$ 5.7% decline in home resale value.

If the warehouse can be seen from the home then a further decrease in property value may result.

Truck Traffic & Neighborhood Streets

Locating a facility where diesel truck traffic is likely to travel neighborhood streets can expose residents to excessive noise, air pollution, property value loss, and safety issues. Instead, these facilities should be guided to locations with direct access onto main roads that do not pass through residential areas.

Warehouse Truck Traffic Volume

The volume of truck traffic generated by a proposed warehouse can be estimated with guidance presented in *Trip Generation*, published by the [Institute for Transportation Engineers](#) (ITE). Trip generation volumes are based upon rates per unit of a proposed land use. Each *Trip Generation* land use is assigned a code. The code for a more common warehouse type is 156, which is for a High-Cube Parcel Hub Warehouse, which is described in *Trip Generation* as:

“A high-cube warehouse (HCW) is a building that typically has at least 200,000 gross square feet of floor area, has a ceiling height of 24 feet or more, and is used primarily for the storage and/or consolidation of manufactured goods (and to a lesser extent, raw materials) prior to their distribution to retail locations or other warehouses.”

For every 1000 square feet of Gross Floor Area (GFA), this warehouse type would generate 0.10 to 1.37 peak-hour trips for all vehicle types. Of these vehicles, 12% would be trucks.

So, a 200,000 square foot high-cube warehouse would generate:

$$200,000 \text{ divided by } 1000 = 200 \times 0.64 = 128 \text{ peak-hour trips of which } 15 \text{ would be trucks}$$

A table of Trip Generation 10th Edition rates is available

at: https://www.troutdaleoregon.gov/sites/default/files/fileattachments/public_works/page/966/ite_land_use_list_10th_edition.pdf

Warehouses Are Not A Light Industrial Use

According to the [USLegal.com](#) Light Industry Law and Legal Definition webpage:

“Light industries cause relatively little pollution when compared to heavy industries. As light industry facilities have less environmental impact than those associated with heavy industry, zoning laws permit light industry near residential areas. It is a criterion for zoning classification.”

The Complete Real Estate Encyclopedia contains a similar definition for Light Industrial:

“Light industry usually consists of nonpolluting users with moderate energy demands engaged in assembling products, sewing, baking, or cleaning.”

In *Putting Atlanta Back To Work: Integrating Light Industry Mixed-Use Into Urban Development*, the following distinction is made between light and heavy industry:

“Generally, to locate in a light industrial zone, a business must not produce any loud noises, vibration, noxious fumes, or other hazardous byproducts – beyond the property line. In heavy industrial districts, generally a business must not produce these negative effects beyond the boundaries of the entire district.”

Given that the:

- Air pollution emitted from warehouse diesel truck traffic can harm the health of those living well beyond a warehouse property line,
- Trucks traveling past homes to reach a warehouse can significantly lower property value,
- Warehouse noise has disturbed nearby residents

warehouses should not be allowed in light industrial zones based on the definitions and cautions above, especially when within a thousand feet or so of homes.

From: Anna Kasperowicz Kasperowicz <akasperowicz@yahoo.com>

Sent: Thursday, January 19, 2023 7:40 PM

To: Justin Floyd <justin@jfcfcapitaladvisors.com>

Subject: Fw: Eddie Jones Follow Up + Thank You

----- Forwarded Message -----

From: Anna Kasperowicz Kasperowicz <akasperowicz@yahoo.com>

To: Arlene Tendick <outreach@eddiejonesproject.com>

Cc: rdmohowski@oceansideca.org <rdmohowski@oceansideca.org>; esanchez@oceansideca.org <esanchez@oceansideca.org>; ejoyce@oceansideca.org <ejoyce@oceansideca.org>; rrobinson@oceansideca.org <rrobinson@oceansideca.org>; rkeim@oceansideca.org <rkeim@oceansideca.org>

Sent: Thursday, January 19, 2023, 07:39:18 PM PST

Subject: Re: Eddie Jones Follow Up + Thank You

Hello Arlene

Thank you for reaching out. I would like say the meeting was a big disappointment. I was hoping to speak comments and so were the other homeowners. This webinar was a lecture presentation. This was not 2 hours of you collaborating with the community. Your staff made this no open forum to speak or comment. Next meeting I would hope the city council be present as they should hear what our concerns are. Let’s work together and allow each member to have a few minutes same as city meetings maybe we can have sign up in advance. I included the city planner on this email. Here are my suggestions for next community meetings. We are not interested in slide presentations to take up our evenings taking time away from our families. If you care about

community members dialogue between community members. Most upsetting about the meeting was that your staff skirted questions with answers not answering things such as decline in property values if this warehouse is built. New families will not want to live next to an Amazon warehouse and retirees get noises trucks to enjoying hearing. I live next to a stop sign on Benet. Just to fill you in on how this impact me. My son and daughter have windows to enjoy trucks passing by to stop and accelerate to 45 miles an hour. How does this not impact my quality of life as a homeowner. This city says trucks can drive this street. Yet you say they will not. Well how will you and the city prevent this. This project has no benefit for the community at this location and will make the quality of life detrimental to the value of the homes, safety, health of the community. The quality of life here are as owners will be terrible Lastly, the traffic report was from two years ago. This data is outdated and inaccurate now. The pollution and traffic will be must greater. I believe you should be required to update this with an independent party. You say you will increase traffic by 1 percent. But that is not accurate based on an current data as of January 2023 and you have not taken the other new very large projects into consideration.

Respectfully

Anna Floyd

On Thursday, January 19, 2023, 05:38:48 PM PST, Arlene Tendick <outreach@eddiejonesproject.com> wrote:

Hello everyone,

On behalf of our team, thank you for your time and patience during Tuesday's informational meeting on the proposed Eddie Jones warehouse project. There were more than 200 comments submitted so the Q&A portion took longer than expected, but we hope it was informative. [Here is a link](#) to the slides.

I committed to being transparent, so I did want to address one comment that has come up a couple times regarding the assumption that we omitted submissions that didn't suit us. We read all comments, even the negative ones. There were many of the same questions submitted so we made sure that all the answers responded to what we were seeing. The only comments we did not read out loud were those that were calling us names or being disrespectful. Additionally, we wanted everyone to have the same opportunity to submit questions and felt that the Q&A portal was the best way to ensure no one voice monopolized the conversation. It was not an attempt to avoid hearing from you. The sheer volume of questions is testament to that. This is just one meeting in what we hope will be an on-going and productive dialogue.

We are in the process of going back through all the questions and preparing an FAQ document that will include answers to the questions that were submitted. We will share that in the coming week.

In the meantime, I've received several emails with follow up questions or requests for meetings, so I'm happy to see the willingness to continue the conversation.

Thank you again for your participation!

Arlene

Community Outreach

Ms. Ester Sanchez
Oceanside Mayor
Via email: esanchez@oceansideca.org

Rob Dmohowski
Oceanside City Principal Planner
Via email: rdmokowski@oceansideca.org

February 7, 2023

Ref: Opposition to the Eddy Jones Warehouse, Manufacturing and Distribution Facility

Dear Ms. Sanchez and Mr. Dmohowski:

Our primary concern about the Eddy Jones development is air pollution! Very recently my wife and I were traveling down Hwy 76 about ½ mile behind a single diesel truck. We were able to smell the diesel for the entire drive to near the airport.

The majority of the areas to be affected by diesel smoke lay downwind from this project and include most of Oceanside, Vista and Bonsall. We do not want to be exposed to diesel smoke 24/7 and would possibly invite these other cities affected to sue Oceanside over this matter.

Please also consider the prospect of even more traffic and the resulting pollution of our wonderful environment. In addition, let's honor the Whelan's family donation and contribution to this city by not polluting this land further.

Sincerely,

Patrick Campbell
Patrick Campbell
Whelan Lake Neighborhood

Eliana Campbell
Eliana Campbell
Whelan Lake Neighborhood

cc: **Ryan Keim, Deputy Mayor, District 3 -** rkeim@oceansideca.org
Eric Joyce, Councilmember, District 1 - ejoyce@oceansideca.org
Richard Robinson, Councilmember, District 2 - rwrobinson@oceansideca.org
Peter Weiss, Councilmember, District 4 - pweiss@oceansideca.org
Zeb Navarro, Ed.D. Oceanside City Clerk - znavarro@oceansideca.org
Eddiejoneswarehouse@gmail.com

Robert Dmohowski

From: Jochen Kressler <jkressler@sdsu.edu>
Sent: Tuesday, January 3, 2023 4:05 PM
To: Esther Sanchez; Ryan Keim
Cc: Robert Dmohowski; Zeb Navarro
Subject: NO - Eddy Jones Warehouse, Manufacturing and Distribution Facility

Categories: Red Category

Warning: External Source

Dear Mayor Sanchez And Deputy Mayor Keim,

I am writing on behalf of myself, my wife and my mother-in-law, registered voters living in Oceanside. We are vehemently opposed to the proposed Eddy Jones Warehouse, Manufacturing and Distribution Facility project. This project will severely impact the quality of life in our neighborhood, the safety of our children and elderly, and create noise and air pollution. It is simply unconscionable to build such a facility this close to a family neighborhood. I urge you to ask yourself just one question. Would you want this project built right next to where you live?

Sincerely,

Jochen Kressler

--

Jochen Kressler, Ph.D.
Associate Professor, Exercise Physiology
School of Exercise and Nutritional Sciences,
College of Health and Human Services,
San Diego State University

Office: ENS 303
Phone: [619-594-0323](tel:619-594-0323)
Email: jkressler@mail.sdsu.edu

Robert Dmohowski

From: ANDREA TARGHETTA <atargetta@comcast.net>
Sent: Tuesday, January 10, 2023 12:49 PM
To: outreach@eddiejonesproject.com
Cc: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson
Subject: Comments for Zoom Meeting January 17, 2023

Categories: Red Category

Warning: External Source

Planning Committee:

I write in opposition to the Eddie Jones Project. A great deal of planning has gone into this project by the City and the developer, yet there are areas of concern which homeowners are eager to know more about. My concerns are first and foremost, safety and security matters. Heavy equipment during construction, and tractor trailers, extra automobiles once it is finished will be amplified and our exits and entrances will be severely diminished. In times of natural disasters and national emergencies, bottlenecks will form. This is unacceptable.

With any construction, dust abatement is a must. Currently, we are experiencing Ocean Kamp dust each day in our homes even with the water truck. Allergies and headaches get aggressive. The noise so far is minimal, but the main construction has not begun.

Please send the Zoom link for this meeting. Thank you for this opportunity to express some of you concerns.

Andrea Targhetta

atargetta@comcast.net

505-280-8892

“It is only with the heart that one can see rightly; what is essential is invisible to the eye.”
— Antoine de Saint-Exupéry, [The Little Prince](#)

Robert Dmohowski

From: Anna Kasperowicz Kasperowicz <akasperowicz@yahoo.com>
Sent: Thursday, January 19, 2023 7:39 PM
To: Arlene Tendick
Cc: Robert Dmohowski; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim
Subject: Re: Eddie Jones Follow Up + Thank You

Follow Up Flag: Follow up
Flag Status: Completed

Categories: Red Category

Warning: External Source

Hello Arlene

Thank you for reaching out. I would like say the meeting was a big disappointment. I was hoping to speak comments and so were the other homeowners. This webinar was a lecture presentation. This was not 2 hours of you collaborating with the community. Your staff made this no open forum to speak or comment. Next meeting I would hope the city council be present as they should hear what our concerns are. Let's work together and allow each member to have a few minutes same as city meetings maybe we can have sign up in advance. I included the city planner on this email. Here are my suggestions for next community meetings. We are not interested in slide presentations to take up our evenings taking time away from our families. If you care about community members dialogue between community members. Most upsetting about the meeting was that your staff skirted questions with answers not answering things such as decline in property values if this warehouse is built. New families will not want to live next to an Amazon warehouse and retirees get noises trucks to enjoying hearing. I live next to a stop sign on Benet. Just to fill you in on how this impact me. My son and daughter have windows to enjoy trucks passing by to stop and accelerate to 45 miles an hour. How does this not impact my quality of life as a homeowner. This city says trucks can drive this street. Yet you say they will not. Well how will you and the city prevent this. This project has no benefit for the community at this location and will make the quality of life detrimental to the value of the homes, safety, health of the community. The quality of life here are as owners will be terrible Lastly, the traffic report was from two years ago. This data is outdated and inaccurate now. The pollution and traffic will be must greater. I believe you should be required to update this with an independent party. You say you will increase traffic by 1 percent. But that is not accurate based on an current data as of January 2023 and you have not taken the other new very large projects into consideration.

Respectfully

Anna Floyd

On Thursday, January 19, 2023, 05:38:48 PM PST, Arlene Tendick <outreach@eddiejonesproject.com> wrote:

Hello everyone,

On behalf of our team, thank you for your time and patience during Tuesday's informational meeting on the proposed Eddie Jones warehouse project. There were more than 200 comments submitted so the Q&A portion took longer than expected, but we hope it was informative. [Here is a link](#) to the slides.

I committed to being transparent, so I did want to address one comment that has come up a couple times regarding the assumption that we omitted submissions that didn't suit us. We read all comments, even the negative ones. There were many of the same questions submitted so we made sure that all the answers responded to what we were seeing. The only comments we did not read out loud were those that were calling us names or being disrespectful. Additionally, we wanted everyone to have the same opportunity to submit questions and felt that the Q&A portal was the best way to ensure no one voice monopolized the conversation. It was not an attempt to avoid hearing from you. The sheer volume of questions is testament to that. This is just one meeting in what we hope will be an on-going and productive dialogue.

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In the meantime, I've received several emails with follow up questions or requests for meetings, so I'm happy to see the willingness to continue the conversation.

Thank you again for your participation!

Arlene

Community Outreach



Contact City Staff

City of Oceanside

Submitted On:

Feb 1, 2023, 04:45PM EST

Full Name	First Name: Rick Last Name: Taylor
Email	rkt.sce@gmail.com
Specific City Staff Member's Name	
Department	City Council
Comments	I hope the city will inform ALL of its citizens who live along or utilize the 76 corridor of the Eddie Jones Warehouse (not just those within 1500 ft). It will put over one hundred semi's and hundreds of vehicles onto an already congested roadway. NO to the EDDIE JONES DISTRIBUTION CENTER.

Robert Dmohowski

From: Rick Adams <daddams@hotmail.com>
Sent: Thursday, February 9, 2023 9:15 AM
To: Robert Dmohowski
Subject: Opposition to the Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Opposition to the Eddy Jones Warehouse,

Manufacturing and Distribution Facility (sometimes referred to as EDDIE Jones)

Brief Summary

The Eddy Jones Warehouse, Manufacturing and Distribution Facility is being proposed to be built on the 31.79 acres between the Oceanside Municipal Airport on Eddie Jones Way and Benet Rd and the residential development known as Airport. The plans include 114 semi-truck terminals, 60 truck trailer parking stalls and 590 auto parking spaces with a building of 566,905 square feet. In addition, with the Ocean Kamp project under construction, our ability to maneuver in and out of our community via either Benet Rd or Foussat Rd will be adversely impacted. Our Concerns

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as ALL who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There is NO money earmarked for any road / traffic improvements. **TRANSPARENCY** - To date the lack of transparency/communications is very apparent when very few of the Airport residents and surrounding areas are even aware of the intended development. We are not opposed to something being built but what type of business, what level and hours of operations and what impact will it have on our community and our quality of life are not being addressed or taken into consideration.

POLLUTION - We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24 hours a day. Noise pollution from these vehicles is concerning. Pollution of the San Luis River is also an issue to be considered.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. The same will be true on Foussat Rd due to increased traffic from the Ocean Kamp and the 590 vehicles expected for the employees and visitors at this new facility. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. We do not oppose development as long as it is well thought out and is compatible with the residential area that it borders. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on our nearby communities.

Rick & Gail Adams
3381 Hollowtree Dr.
Oceanside Ca. 92058

Sent from [Mail](#) for Windows

Robert Dmohowski

From: Vryon Airam Rodriguez <1054408@oside.us>
Sent: Monday, February 13, 2023 10:03 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear, Mr. Dmohowski

My name is Vryon Rodriguez and I am a senior of Oceanside High School who currently resides within your district. I strongly oppose and disagree with the Eddie Jones Warehouse Project because it was never publicly announced as very few of the nearby residents of the area were not made aware of the project's development.

Though there is nothing wrong with building or developing a business or how long somebody may work there, what does matter is if it will negatively affect the surrounding community and if the opinions of said community are not accounted for.

Another reason that I oppose this project is that it will also affect the future oncoming traffic issues that will arise. Due to the overwhelming amount of semi-trucks that will be coming and going every day, this will alter the traffic pathways as the drivers who take the 76 Highway and I-5 Freeway will now have to take Mission Avenue or Oceanside Boulevard. At first there would seem to be nothing wrong or inconsequential if it was not for the fact that there is no money appropriated for road or traffic improvements.

Finally, safety and security concerns will become another big factor if the project were to be complete. Due to the constant numbers of semi-trucks arriving and leaving the area blocking Benet Road for all that travel will need to be rerouted. As for security it would need to add security measures that will handle the nearby population of homeless and indigent which is another striking issue that should be handled properly before this project has been finalized.

This issue is important to me because my close friends that live in the nearby area will be directly affected by this. This will affect how I will be able to interact with them as it will be more difficult for me to visit them with the amount of pollution and traffic that will be in the area, this will also affect both mine and other students' transportation to and from school. My hope for the outcome of this would be that you pursue a better project or one that will not as heavily affect Oceanside as the Eddie Jones project will or to fund another project that will properly benefit the area.

Sincerely, Vryon Rodriguez Oceanside Class of 2023
504 Parker Road, Oceanside, California 92058

Robert Dmohowski

From: Morgan Moyle <1040256@oside.us>
Sent: Monday, February 13, 2023 10:04 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello Mr. Dmohowski. I'm Phoenix Graven. I am a senior at oceanside high school. I am not in support of the Eddy Jones Warehouse Project because of the pollution, traffic, and how that will affect emergency services.

Semi-trucks have some of the highest rates of pollution. "Heavy-duty trucks are the fastest-growing source of climate pollution in the U.S., emitting nearly a quarter of the climate pollution from transportation, even though trucks are just four percent of all the vehicles on the road. They're also responsible for 45 percent of the nitrogen oxide pollution and 57 percent of the fine particulate (PM 2.5) pollution in the U.S. — both of which are linked to premature death and chronic illnesses like heart disease, lung cancer, stroke, and childhood asthma. These impacts are especially acute in low-income and BIPOC communities that decades of racist transportation policy placed near major roadways and freight hubs." That paragraph came from climate nexus. With the proposal of this project, we would have hundreds of semis coming in and out of this area. They're planning on building this behind the airport which is right by the San Luis Ray River. With all these semi trucks it would pollute the river and the air we breathe.

The 76 already has an obscene amount of traffic. You add semis constantly turning on and off these roads that just going to grind the traffic to a halt. When semi trucks turn they tend to take up two lanes. They also have a much larger stopping distance due to their size. This requires a larger following distance as well. Both of these halt traffic on an already traffic-riddled road.

That leads to my last point. With traffic being bumper to bumper and semis trying to get on and off the road it will be next to impossible for emergency vehicles to where they need to be in an emergency when taking this road. Within a mile from the building site are fire department station 7, Fire depo number 7, and a fire department training center. All of these would be greatly impacted by heavy traffic due to semi-trucks.

Thank you for your time and for hearing me out. Here is the link to the article I quoted.

[\(https://climatenexus.org/climate-issues/energy/electric-hdv-trucks/\)](https://climatenexus.org/climate-issues/energy/electric-hdv-trucks/)

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Best wishes,
Phoenix Graven
Oceanside High School Senior
501 N Pacific Street Unit 25

Robert Dmohowski

From: Dee Keck <4dees@cox.net>
Sent: Thursday, February 23, 2023 7:48 PM
To: Robert Dmohowski; Ryan Keim
Subject: Re sending concerns

Warning: External Source

The letter below was sent to me for our grassroots' records.
I see though that two of the addresses are incorrect and one I believe to be the City Manager, but I am uncertain. I have contacted Mr Secendorf about re-sending but I have been unsuccessful in reaching him to see if he was able to. I am forwarding to the two parties that I do recognize.

Thank you ...

Dee
Eddie Jones Grassroots group

On 2/15/2023 10:24 AM, D P Smith wrote:

----- Forwarded message -----

From: Robert Seckendorf <rob.seckendorf@att.net>
Date: Wed, Feb 15, 2023 at 10:03 AM
Subject: Eddie Jones Logisites Warehouse Project in front of Wanis View Estates
To: rdrnohowski@oceansideca.org <rdrnohowski@oceansideca.org>, varro@oceansideca.org <varro@oceansideca.org>, esanchez@oceansideca.org <esanchez@oceansideca.org>, eddiejoneswarehouse@gmail.com <eddiejoneswarehouse@gmail.com>, rkeirn@oceansideca.org <rkeirn@oceansideca.org>

Good Morning City Council Members and representatives from Eddie Jones Warehouse:

My name is Robert Seckendorf and I am a resident of Wanis View Estates, adjacent to the proposed Eddie Jones Warehouse Project.

I am asking the committee to consider the effects of such an expansion in the midst of a residential community and the overall ramifications that will have on our community and the general area at large. I would like to cite three reasons for consideration into not moving forward with this project.

1. Impact on already congested traffic:

I am a consultant and IT contractor by trade. I work in the field of Information Technology and my time on the road to and from my customer locations is substantial. The current level of traffic that exists on Highway 76 is excessive already. One needs only to travel from anywhere near the San Luis Rey Mission westbound at 7:00 AM to Interstate 5 to get an appreciation of the level of traffic that is already present on this corridor. I consider myself fortunate because I only have to travel a mile or so in order to get onto Interstate 5 heading north or south. However, the traffic in the early AM hours as well as the PM

hours trying to get home from the 76 offramp just to Benet is at times, excessive. I couldn't even imagine how impactful adding a number of big rigs in and out of a logistics center would be, but anyone with an understanding of what that corridor currently looks like will have an appreciation of how dreadful that will be if the City of Oceanside gives the go ahead for this project. Traffic congestion, however, is only one concern that most homeowners in the region are upset about. In my travels for the work I do, I have the opportunity to see many such logistics warehouses, like the large Amazon warehouse on the 15 heading towards Barstow, or other large such facilities in regions outside of the state. These warehouses are not eyesores, not by any means, however they are situated in locations that are nowhere near any residentially zoned neighborhoods. This particular region is already slated for a new surf park and trendy shopping and dining that has been already approved by the City. I submit that with all the new developments slated for the region, in the planning or development stages that there is no way the city would be able to perform an adequate environmental impact on traffic, simply because these developments are all earmarked for the same time period. It would be disastrous to find out that upon completion of the surf park, shopping and dining development in concert with the Eddie Jones Warehouse Project that Highway 76 would be in need of more lane expansion, just to cover the requirements to handle the additional vehicles in and out of the region.

2. Environmental Impact from a resident's perspective only:

What about the environmental impact that the added air and noise pollution will create for the residents of my community? When I purchased my home just about four years ago, I was always struck by why all the rooftops in the neighborhood were covered in what looked like dirt. Turns out that was dust and ash material created by airborne remnants from mortar fire at nearby Camp Pendleton. I had no idea when I bought my residence that was the case. Now we are looking at having a collection of diesel-powered cab and trailers amassing not a mile away coming in and out of the neighborhood. This is only going to reduce the air quality of the region, not to mention the sound impact of so many tractor trailers coming and going.

3. How would you want this in your backyard?

Lastly, I would like the members of the City Council to put yourselves in the shoes of those who live and have lived in this beautiful area and called it their home. The demographic here at Wanis View

is comprised of elderly people looking to make this location their last residence, others are comprised of young families just getting their start at home ownership. Perhaps some like myself who's lifelong

desire was to have a residence near the beach. Having the opportunity to get on a bicycle and ride to the harbor or to a bistro with family or friends. This has been our dream.

There are things that are within our control, but I would ask each and every one of you council members, if you were to be facing the same concern, taking place in your own backyard, how anxious would you be

to give the go-ahead for this project?

Respectfully, a humble member of the Wanis View Estates Community.

Robert Seckendorf

3280 Toopal Drive

Oceanside, CA 92058
cell: (714) 315-6310
email: rob.seckendorf@att.net



Virus-free. www.avg.com

Robert Dmohowski

From: Julie DeAnda <juliedeanda@me.com>
Sent: Thursday, April 27, 2023 10:54 AM
To: Robert Dmohowski
Cc: eddiejoneswarehouse@gmail.com
Subject: We are residents opposed to Eddy Jones Warehouse, Manf and Dist Facility

Categories: Red Category

Warning: External Source

Dear Rob,

My husband and I are members of the Group "Oceanside Speaks Out"

We live in the Wanis View Estates
Our address is 717 Wala Drive, Oceanside Ca 92058

The only access to our home is Benet Rd or N Fousat Rd.

We have signed the petition opposing the Eddy Jones Warehouse, Manufacturing and Distribution Facility plans

- 566,905 square feet
- 114 semi truck terminals
- 60 truck trailer parking stalls
- 590 auto parking spaces

Our concerns are;

Traffic

Negative impact to our local streets and the 76 Hwy to I-5 Frwy

Pollution Environmental

Negative impact to our estuary preserve directly behind our home and the wildlife that takes refuge there

I am a currently a cancer patient and clean air to breathe and frequent doctors visits are vital to my treatment.

Pollution noise 24/7

Negative impact to our quality of life, disruption due to truck and traffic noise being even closer to our home.

Safety and Emergency services

We have major concerns regarding blocked access to our roads due to heavy semi-truck and car traffic from the warehouse.

There is no money earmarked for any road or traffic improvements.

In addition to the Ocean Kamp development, these two projects combined in would undoubtedly create a traffic nightmare.

We had a ***fire behind our home last year*** that was terrifying and way too close.

We need to know that emergency services can get to our neighborhood without delay.

If I or my husband or any of my neighbors were to have a medical emergency would first responders be able to get to us in time?

Property Value

We strongly believe that having increased traffic and noise pollution in the area of our home will make our property value decrease.

This would also impact the property tax value.

Our retirement plans will also be majorly impacted and we will be forced to work much longer than planned and this will also effect our quality of life.

Transparency

To date the lack of transparency/communications is apparent.

So many of my neighbors are not aware of the intended development.

We would like to have more details such as;

- What type of business is planned to be operating?
- What are the hours of operation ?
- What is the impact it will have on our community and quality of life?

Those that do have some idea of the size, wonderful why is our quality of life not being addressed!

Cities Vision

If the idea is to bring tourism and jobs to Oceanside, one should not impede the other.

When this warehouse does creates a traffic congestion in addition to the Ocean Kamp project, who is actually going to want to go to Ocean Kamp if it takes an excessive amount of time to get to and from Ocean Kamp.

Who is going to want to live in a neighborhood that takes an excessive amount of time to pick up their kids from school or get to work or medical appointments.

Since the city's vision plans were set in place over 50 years ago before any residential development was envisioned in this area, we are requesting that the city planners review the requirements for any businesses moving into this neighborhood and reconsider the use for this area.

Since the current zoning is for light industrial, the proposed plans seem way out of range for size for both the actual building and the amount of vehicles and truck bays.

We are not opposed to development as long as it is well thought out and does not have a MAJOR NEGATIVE impact on the people that live in these neighborhoods surrounding the airport and Ocean Kamp project area.

We believe this project proposed is not a good fit with the airport community, nearby neighborhoods and the Ocean Kamp development. This project is too big and will have too large of a negative impact on our nearby communities and quality of life!

Thank you for your help in addressing our concerns for our neighborhood, property values and quality of life.

Our plea is that you take actions to see that this Eddy Jones Warehouse. Manufacturing and Distribution facility does not move forward at the above proposed size and the city reviews the zoning plans based on current residential neighborhoods and quality of life needs.

Sincerely,

Julie DeAnda
714.227.2575

Carlos DeAnda
760.429.5293

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Wednesday, February 22, 2023 3:01 PM
To: Esther Sanchez
Cc: Ryan Keim; Peter Weiss; Robert Dmohowski; Rick Robinson; Eric Joyce; eddiejoneswarehouse@gmail.com
Subject: Re: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

PROPOSED EDDIE JONES WAREHOUSE

To whom it may concern,

I contacted the city about this project on multiple occasions back in 2022 and received a few emails from council members and planning. It was stated this new project is just going through the process, with no decisions being made, and an EIR was in the works. I also received a call from Mr. Keim, about my concern that the existing Deutsch facility was being demolished, and supposedly, it had nothing to do with the new project, and that Mr. Dmohowski would contact me on the why of Deutsch's demolition.

I never received that communication and the entire Deustch site has since been reduced to rubble and every tree uprooted.

With that said, I am wondering where the approval of this proposed facility stands and am extremely hopeful that both the planning department and the city council have the common sense to deny this project. And not just for the 1,000 homes that have only Benet and Foussat streets as access, but to all Oceanside citizens that utilize the 76 roadway.

Let's hope common sense prevails.

Thanks for your time,

Rick and Sue Taylor
735 Valley Crest Drive
Oceanside, CA 92058
760 274 4700

NOTE: If anyone wishes to discuss the issues concerning this project, such as notification issues, the traffic study and existing traffic problems, issues that should be addressed in the EIR, or just the basic issue of why this facility needs to go elsewhere, please feel free to contact me.

Robert Dmohowski

From: Dane Thompson
Sent: Tuesday, February 21, 2023 10:04 AM
To: Robert Dmohowski
Subject: FW: comment on Eddie Jones Warehouse project, SCH2022070365

Categories: Red Category

fyi

From: Graham Ross <graham.ross.sd@gmail.com>
Sent: Monday, February 20, 2023 1:45 PM
To: Planning Web <Planning_Web@oceansideca.org>
Subject: comment on Eddie Jones Warehouse project, SCH2022070365

Warning: External Source

TWIMC,

I read today of the pushback heard at a public meeting from a group of local residents, dubbed the "blue crew," regarding the proposed warehouse at the intersection of Eddie Jones Way and Benet Road. I am familiar with this area and an Oceanside resident. The statements made by this group are unsubstantiated, at best, and are seemingly the standard NIMBY assertions.

A member of the group is quoted as saying "We are not a 'no' group. We are not a 'Hey, we don't want anything here." They complain that the trucks will create a traffic danger. They assert that "Nobody who lives... north of the river will be able to get in or out of our homes... let alone, in an emergency trying to get emergency services in or out or to evacuate people." The additional comments by high school students claiming the warehouse will cause "an obscene amount of pollution not only the air... and the San Luis River..." without any data or even speculation on what will be released into the river. A complaint about additional exhaust emissions at a site adjacent to a 4-lane highway and downwind from a high-traffic interstate highway is ridiculous.

Based on these statements, these residents are definitely a 'no' group who don't want anything in this location.

This is a perfect location for a warehouse. 1000 ft from I-76, so minimal impact to traffic on Benet Road. Two miles from I-5 with a single intersection along the route. It might warrant adding a second right-turn lane at Benet Lane, maybe... The proposed location is adjacent to the Oceanside Municipal Airport and other existing light industrial businesses and separated from the north-of-the-river community by a swatch of green space along the San Luis Rey River. A great combination of accessibility and separation.

The traffic concerns of this group remind me of the same type of concerns voiced by opponents of large residential developments along I-56 when it was completed in 2004 after many years of delays over "environmental concerns." Various local groups claimed that the traffic would be so bad that it would take an hour for residents to even get onto I-56. None of this came true and traffic in the areas along I-56 is typical for any residential area.

Don't let this outcry from entitled residents, which lacks any credible supporting information, delay this project. Oceanside is growing and needs to include new businesses as part of the tax base.

I trust the Planning Commission to make a complete and unbiased assessment of the impact of this project and require only those additional features that are truly important.

Best regards,

Graham Ross

858-472-6943

graham.ross.sd@gmail.com

Robert Dmohowski

From: rickbobst@gmail.com
Sent: Tuesday, February 14, 2023 6:04 PM
To: Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro; eddiejoneswarehouse@gmail.com
Subject: Negative affects on our community

Warning: External Source

Oceanside City leaders,

I urge you all to reconsider the Eddie Jones project on Benet road. Please consider the interests and quality of life of the community and residents.

This past week I had two very close encounters with Angelo's towing company. The vehicles showed no regard for me and my family and nearly hit us head on and while on my bike I was nearly ran over when the truck ran the stop sign on airport road and benet.

Angelos towing company is near our residents and near the Eddie Jones project. Just with the towing and salvage company, I see or witness dangerous situations daily.

Angelo's towing company has no regard for the residents in our neighborhood, the employees of Eddie Jones will not either and this will result in the death of an innocent citizen whether in their vehicle while going to work or drop kids off at school, walking their dog or exercising.

You could stop this project before it costs someone their life. The amount of vehicle traffic, walking and biking traffic in this vicinity; with the addition of a commercial shipping company; someone will lose their life due to negligence.

Just looked what happened last year in our neighborhood. An innocent women, a member of our community we would see walking daily, lost her life due to negligence.

You all can stop this project and invest in the community with positivity and with appreciation for your tax payers. Why not invest in a nice park, a school or something that adds value to the community and shows the city officials working to increase the happiness and wellness of our great city.

Do something and stop the Eddie Jones Project!

Thank you!

Rick

Robert Dmohowski

From: Diane Hanson <ddshanson2@gmail.com>
Sent: Tuesday, February 14, 2023 12:32 PM
To: Robert Dmohowski
Cc: Zeb Navarro; City Council
Subject: Kamp Oceanside and Eddie Jones

Warning: External Source

The last Oceanside City Plan came into being long before the growth in Oceanside. The current city plan is not looking far enough into the future. I can understand the city wanting money to fill the coffers, but you are endangering the welfare of the citizens in favor of development. Please stop doing this.

New City Plan: The "Transportation Corridor"? What is that? Buses, trains and cars. Public transportation currently is not sufficient. If you want people to use public transportation, make it efficient, reliable, and inexpensive before they move to the area. Then they will have an incentive to move here.

Vista Way is currently gridlocked from Jefferson east past Emerald. Sometimes it takes 4- or more light changes to get through the intersections. Cars that don't make it through are stuck in the intersection, then oncoming cars cannot get past. There are many accidents because of this. At peak travel times, the traffic lights on Highway 76 have the same problem. All the north/south traffic clogs up at the major east/west intersections, especially near the freeway onramps to the 76 and 78. What are the city's plans to alleviate this situation? The city should fix

this situation before approving more projects that add to the problem.

The city has approved an apartment complex on Douglas between Mission and the 76 Expressway. There is only one way in and out of that complex - which is onto Douglas! That is 6 car lengths from the 76. What are you thinking! It should be zone Retail/industrial NOT residential. In any case, PLEASE rethink this project.

Kamp Oceanside: The City Council already approved this!! The hotels and condos are ok, but the wave?? No way! We have a water shortage and are already drinking recycled wastewater, and the City wants to let this project waste millions of gallons of water on something that the citizens can't use. This "wave" is for the training of Olympic surfers! The Pacific ocean is just 3 miles west of this project. How often will the training take place? This doesn't make sense. The wave area would be better used for more housing and some mixed use. Cancel that portion of that project - PLEASE!!

Eddie Jones Warehouse - This project has not come before the city council - yet. **Please do not approve it.** The warehouse consists of almost 32 acres by the airport. An almost 567 sq.ft warehouse with 114 semi-truck terminals, 60 truck parking stalls, and 590 parking spaces for employees and visitors. The city has no plans to widen any of the major streets. Unbelievable!! So much truck traffic, noise and pollution!! There are no Fire or emergency plans for exiting and entering the property. All

this near the airport, where there have been recent accidents and deaths. This project is too crazy to consider.

Instead of the above: Put in a manufactured home park made up of tiny houses. Maybe that can take some of the homeless folks off the streets, and can fill the city's requirement for affordable housing mandated by the state.

Robert Dmohowski

From: Matthew Coachman <1048781@oside.us>
Sent: Tuesday, February 14, 2023 9:17 AM
To: Robert Dmohowski
Cc: Nathan Fristed
Subject: The Eddie Jones Project

Warning: External Source

Dear Rob Dmohowski iMy name is Matthew Coachman and I am a Senior at Oceanside High School. I oppose the Eddie Jone's project because HWY 76 is already congested. I travel on this road daily to get to school and work. Waking up early now just to be on time is already a strain. With the construction of this new warehouse, I can only imagine how the hundreds of semi trucks would slow down our commute.

There are also safety concerns that must be addressed with all of the semi-trucks We must think about, cyclists and skateboarders like myself. Just thinking about one of my friends getting run over by a truck brings a tear to my eyes A child could never be able to walk again because of a warehouse that is not necessary to our community.

There is also the threat of a fire or other emergencies and the panic this would bring would be catastrophic. All of the over five hundred employees fleeing the scene in cars would cause accidents and panic. Even worse than that would be the people running away who could be hit by a car in the hysteria.

Matthew Coachman
1 Pirate Cove
Ca 92058

Robert Dmohowski

From: Trinity Nieto-Hernandez <1035210@oside.us>
Sent: Monday, February 13, 2023 11:21 AM
To: Robert Dmohowski
Subject: Eddy Jones Warehouse Project

Warning: External Source

Disagree or Agree (About Eddy Jones Warehouse Project): **Disagree**

- 1.
- 2.
3. To much traffic/ traffic jam
- 4.

- 2.
- 3.
4. To much forms of pollution
- 5.

- 3.
- 4.
5. Homeless moving around the county
- 6.

- 4.
- 5.
6. Safety hazard (fire/earthquake evacuation, safety of children)
- 7.

- 5.
- 6.
7. More taxes
- 8.

- 6.
- 7.
8. Will people be able to live on the oceanside if this happens?
- 9.

Good Evening, Mayor Oceanside city council members and fellow community members. My name is Trinity, I am a senior at Oceanside High School and I am here to share my concerns and humble opinion in regards to the Eddy Jones Warehouse Project. I have thoroughly read the pros and cons regarding the 540,000 square ft. warehouse that is proposed to be placed off of the 76 highway. The pros that I am familiar with are that the warehouse is going to offer employment for oceanside and surrounding communities, it may also improve distribution throughout the area. The cons that I have gathered are as follows: Safety hazards (fire/ earthquake evacuations, and children safety.), homeless people moving around the county, many forms of pollution, traffic jams, more taxes, living situations for the community in the area, the lack of housing and its price point. Will the wages be enough for people to reside in those areas? These cons concern me because Oceanside is already too crowded and problematic for most people who are already living here and active members of the community. Residents are concerned about their property value and the congestion that will only follow the two exits to the neighboring community.

As a child, growing up in this town was very peaceful for me. Me and my family would go down to the San Luis rey church for festivals and gatherings not having to worry about the heavy traffic. Now I have to look both ways multiple times before I cross the street, afraid that I'm going to get hit by cars not paying attention. Recently I have gotten my driving permit and I have had many interactions with heavy traffic, destructive drivers, and heavy road ragers in oceanside. These interactions make me not want to get my licenses and knowing that another big building is being added to oceanside is going to make it worse. I also think that the extra hotels in downtown Oceanside are completely useless and it blocks the view of the beach for everyone to see. The hotels also attract more homeless people, tourists, and worst of all trash.

In conclusion, Oceanside should not keep adding buildings into places that will not work out. It costs too much, adds more pollution, and is creating an unhealthy environment for older people and the new generation. I hope this short message brings some of the council to their attention and the community can come together and make oceanside a safer place again.

Robert Dmohowski

From: Jonathan Hernandez Rosas <952349@oside.us>
Sent: Monday, February 13, 2023 10:28 AM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Rob Dmohowski,

My name is Jonathan Hernandez Rosas and I live in oceanside close to where the Eddie Jones project will be built.

I'm not too fond of the idea of this project because of the air pollution it would cause which has been proven to affect the health of people living nearby. Putting a warehouse close to where people live can affect their air and lead to health problems. It would cause traffic for everyone trying to get to school or work. I have friends who use that street to get to school and the warehouse would force them to have traffic or make them use another road that already has traffic as is and the warehouse would most likely impact the other routes making them worse.

My final reason why I don't support the Eddie Jones project is that the warehouse would make Oceanside just another city that has huge warehouses. Instead, Oceanside should move forward gentrifying while still keeping Oceanside true to itself. Instead of a warehouse, more recreational activities can be added or instead build the warehouse somewhere else where it won't negatively impact the residential area and streets. Mr. Dmohowski, I ask you to do the right thing and not let the warehouse be built on location.

Sincerely,

Jonathan Hernandez Rosas, Oceanside Highschool Senior, Class of 2023
180 Canyon Dr. Oceanside CA 92054

Robert Dmohowski

From: Matthew Brown <1033199@oside.us>
Sent: Monday, February 13, 2023 9:09 AM
To: Robert Dmohowski
Subject: Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Rob Dmohowski

My name is Matthew Brown and I am an Oceanside student who resides in the district. I oppose this project for a number of reasons. The first argument I would have against it is the traffic it would cause. With the building of the project, This would cause a traffic build up that would make my commute to stores a lot harder than they usually are. My second reason is the air and noise pollution it would cause. With all the trucks bringing in supplies for building, The noise and air pollution would be greatly increased for the usual that is expected in that area. My final reason is that I think there are more important things that could go there than a warehouse. I believe that a warehouse could be helpful, but I think maybe cheaper living areas or even shelters could be a better addition to an area that is plagued with homelessness. As a military family, I am not used to staying in one place but even because of that I would still like for our community to be thought of and taken care of. I feel that this project is important to me because it will affect how our everyday lives will change from the way they used to be. It would affect my family because my Mother usually travels with my 1 year old sister to go and take her to eat. With this project, however, the noise and air pollution from the trucks could be damaging to them and would be an inconvenience for them. It would affect our community by causing overcrowding and the nature that would change around us would be too big of a change to happen. I think it would greatly help this community if you were to vote against this warehouse or at least find a new location for it as I believe the cons outweigh the pros of building it. But I would love to hear your thoughts or opinions on the matter and how you feel that it would be a helpful product to the community.

Sincerely,
Matthew Brown 1 PirateCove Way 92058.

Robert Dmohowski

From: Jordyn Scribner <1020591@oside.us>
Sent: Monday, February 13, 2023 9:12 AM
To: Robert Dmohowski
Subject: Eddie Jones warehouse project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr Dmohowski,

My name is Jordyn Scribner from Oceanside high school and I'm a Senior. Who doesn't reside in your community. But opposes this project that is happening on highway 76 because it will cause people who take it trouble.

I am sending this letter regarding the Eddie Jones Warehouse Project, which is located on Eddie Jones Way and Benet Rd , just NorthEast of the Oceanside Airport. This 566,905 square-foot facility consists of 590 parking spaces , 60 truck trailer parking stalls and 114 semi-truck terminals.

My big concerns and everyone major concerns are the Noise, pollution and what it's going do to the environment, Traffic, Emergency services, Size, Lack of information of the uses, hours of operation and future occupation, These concerns are serving the negative impact that this facility will have on quality of life for the nearby resident as well as people who are transit the 76.

My mission and many others are to improve this property. One that will benefit mostly all of the oceanside.

The Eddie Jones Warehouse, Manufacturing and Distribution Facility, which is being proposed to be built on 31.97 acres between the Oceanside Municipal Airport on Eddie Jones Way and Benet Rd. The plan is going to cause a lot of pollution in the area and traffic. The traffic is going to be caused by all the trucks since there are 114 semi-trucks taking up all the highway. This will make it hard for people to travel and go where they need to go from all the pollution and traffic.

Robert Dmohowski

From: Manuel Martinez <1050919@oside.us>
Sent: Monday, February 13, 2023 9:14 AM
To: Robert Dmohowski
Subject: Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Rob Dmohowski

My name is Manuel Martinez, I am a Oceanside High School senior student who opposes the idea of your project. I believe that the Eddy Jones warehouse project will only cause problems for people who drive on the 76. Traffic would be more apparent with the semi trucks. This would only cause problems for everyone. So this project should be shut down or moved to a more secluded location. I use the 76 from time to time and when this project is built it will be crowded like other highways on holidays. The safety of the people is at risk with this project of yours. More car accidents are bound to happen, putting people's lives at risk. That area will be even more crowded causing lots of accidents. The semi trucks will be a big problem as well. I travel a lot, whether it be to visit family or to go to a new place, and everytime my family and I drive we always see semi trucks on the roads and we don't like driving when they are in front of us because they take up so much space and we can't see what's ahead of us. The only upside to your project would be the new houses you are building, but besides that this project is not ideal. The 114 semi trucks would produce emissions that would be so toxic to the adults and children, and not to mention the environment itself. Thank you for your time and for listening.

Sincerely
Manuel Martinez 1 pirates cove way

Robert Dmohowski

From: Zury Martinez Leon <1049818@oside.us>
Sent: Monday, February 13, 2023 9:51 AM
To: Robert Dmohowski
Subject: Eddy Jones Project Proposal

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr.Dmohowski,

My name is Zury Martinez and I am an Oceanside High School senior who attends school in your district.

I am writing this letter to share my thoughts on the proposed Eddy Jones Warehouse Project. Upon becoming aware of the wide range of disadvantages of such a project and the concerns many Oceanside residents hold, I find that I myself share the same concerns. One of them being the impact this will have on traffic. San Diego County on its own deals with very heavy traffic on our highways as our population increases year by year and I fear that such a project will only worsen this problem. Not only would this increase the amount of traffic on the nearby roads but would ultimately impact the travel times of individuals who use these roads on a day to day basis. I myself use both the 5 and 76 to get to Miracosta College in the evenings and have faced the traffic that occurs first hand which is why I make sure to leave my home 30 minutes early to ensure I make it to my evening class on time. This leads me to my next concern, pollution. Climate change is real and the addition of so many trucks that this warehouse would bring along would indefinitely impact our air quality over time. It would be unjust to bring such an effect on hundreds of people who live in the nearby communities of this prospective project. Similarly, another concern I hold is the negative impact this will have on the housing market. Buying a home near a large warehouse isn't typically something homeowners desire as warehouses are noisy and are active 24/7. Not only could we argue that this would make it difficult to sell homes in the future but could ultimately bring down the value of the homes of the already existing homeowners in the area.

In essence I wish for such concerns to be taken into consideration as it is decided whether to plead against the proposed Eddy Jones Warehouse Project and would like to give thanks for the time taken to read this letter.

Sincerely,
Zury Martinez
1 Pirates Cv, Oceanside, CA 92054

Robert Dmohowski

From: Augustine Lopez <1046490@oside.us>
Sent: Monday, February 13, 2023 9:57 AM
To: Robert Dmohowski
Subject: Eddy Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Rob Dmohowski, my name is Augustine Lopez and I am a family member who resides in your district.

I believe that going forward with the Eddy Jones Warehouse Project will have more of a negative impact on the community, the people and the economy as a whole. The community will suffer greatly because of the loud noise of the planes in the area will worsen for the community near the project will suffer the most. People residing in the area will have to wake up and go to sleep to the loud noises of the factory and semi trucks. The people driving on the I76 and the I5 already have a hard time getting to their destination due to the extremely bad driving conditions and the amount of traffic in the morning and evening. Adding a hundred plus semi trucks to the equation will only worsen the situation and traffic will take even longer than ever. A simple 10 minute drive is already taking more than 20 minutes and will worsen to a possible 30 minute or an hour drive. The economy and ecosystem will suffer greatly as well due to the amount of pollutants that will be brought by the semi truck and the amount of cars that already drive on these roads. The air quality will worsen and it won't be good for anyone living in or near the area.

I would like you to vote against this bill for the sake of the people of Oceanside and SoCal, I believe that it's the best decision for everyone.

Sincerely, Augustine Lopez 119 Dogwood St S Oceanside, CA, 92058 Oceanside High Class of 2023

Robert Dmohowski

From: Samuel Oliver <956752@oside.us>
Sent: Monday, February 13, 2023 10:00 AM
To: Robert Dmohowski
Subject: Eddy Jones Warehouse:

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mr. Dmohowski ,

My name is Samuel Oliver. I am currently a Senior at Oceanside High School class of '23. I am a resident of Oceanside, and in fact live very close to the area proposed for the Eddy Jones Warehouse Project and I would like to share my opinion and thoughts on the Eddy Jones Project. I have some concerns with the proposed project, I would first like to present the positives of the project.

Some of the positives I see with this project while being a neighboring resident of the project include, more job opportunities for the city, my packages will get here faster, and this is a very slim chance but a possibility of removing some of the nearby homeless. I think if I were selfish I would say this is a pretty good deal to me. But there are negatives of the project.

There is already an issue with the traffic around the 76 and adding 175 semi trucks day in and day out I dont think will help this ongoing problem. Another issue I personally see with this project is the amount of pollution of semi trucks to not only the residents, but the San Luis Rey river basin will possibly become more polluted and much of it may make it to the ocean. On top of this, the airport is already active and noisy with planes taking off and arriving, but adding an industrial warehouse many people would not sleep, not to mention the trails being right next to the project, and it would be weird to try and go run, jog or bike a trail to see nature and get away from the industrial buildings may have a chance of backlash.

If we compare and contrast the pros and cons of the project, very little positive comes out of this. I feel we should maybe rethink the project and I feel this is not the best idea for the city of Oceanside.

-
-

- Samuel Oliver

-

Oceanside High School '23

Robert Dmohowski

From: Estrella Torres <941401@oside.us>
Sent: Monday, February 13, 2023 10:03 AM
To: Robert Dmohowski
Subject: Eddy Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

513 Canyon Dr Apt 1 Oceanside, CA 92054

Dear Mr Dmohowski,

My name is Estrella Torres, I am a student at Oceanside High School and a regional center consumer community member who resides in your district. I have concerns about the warehouse project that they plan on building.

One of my first concerns about this project is the traffic impact it will have on a part of Oceanside. There will be an increase in traffic which will make people late to their destination and that will make people very upset. My second concern is pollution. If there are multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles we will see much of an increase of pollution which can affect the Pollution of the San Luis River and can affect human health. My 3rd concern is the lack of communication they have with the community. No one is informing us the details of the project which people are having a hard time comprehending and are having a lot of questions/concerns. This issue is important to me because if i ever have to go near there often it will affect me with the rise of traffic. I wouldn't wanna waste my fuel just sitting and waiting around just to also be late at my destination.

Respectfully, I would like for the representatives to take in consideration the community's concerns about the effects it will have on part of the Oceanside community. I would recommend for them to create a project that is not a warehouse project and have something else in mind that is environmentally friendly.

Sincerely, Estrella Torres
Oceanside High School Senior

Robert Dmohowski

From: Dianna Ruiz <1001153@oside.us>
Sent: Monday, February 13, 2023 10:03 AM
To: Robert Dmohowski
Subject: Eddie Jones manufacturing project file-D22-00001 is a bad idea and I'm strongly against it.

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Dmohowski

My name is Dianna Ruiz and I am a Senior at Oceanside High School

I oppose this project because I'm concerned about how it will affect Oceanside's traffic, air, and safety, since semi trucks produce a lot of air pollution due to their big design structure and the type of fuel it takes.

With the project being built so close to San Luis Rey Elementary School we should be concerned about the air our future breathes. Mission and Highway 76 gets heavy traffic as is and with semi trucks coming and going 24/7 that will only worsen the problem we already have.

Coming back from school I usually take the 76 and the traffic is utterly horrendous and with this project being built it will just get worse.

This project will bring more harm than good, so I beg you to vote no on this project.

Sincerely,

Dianna Ruiz
127 Flamingo dr
Oceanside, California, 92054

Robert Dmohowski

From: Johnny Perez Hernandez <1001151@oside.us>
Sent: Monday, February 13, 2023 10:07 AM
To: Robert Dmohowski
Subject: Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear City Principal Planner Dmohowski, My name is Johnny Perez and I am a senior attending Oceanside High School. I am reaching out to state my disagreement with the upcoming Eddy Jones Warehouse project and the reasons I disagree with this project are simple.

For one being the traffic, the traffic in the designated area for the project is already very busy with students coming to school, kids coming off Camp Pendleton to come to school along with normal people on their way to work. The amount of traffic this project will bring would devastate the people of oceanside and make it nearly impossible for them to effectively cross through the area without being faced with several semi trucks coming and going at all times.

Another reason is the noise pollution that it would bring. The designated area the project is supposed to take place is nearby neighborhoods. The people there like people anywhere, try to live in places where its quiet. With this project, semi trucks would be coming in and out near this neighborhood 24/7. While some people may be used to it, the people living there don't deserve to be forced to deal with all this noise already bugged by the traffic.

The final reason why I oppose this project is the property value of the nearby homes. On top of the heavy traffic, the noises going off 24/7 no matter what. This will leave the property value to depreciate tremendously. Nobody would want to live in the nearby areas and this project would push all the people away. The property value of all the homes in the nearby area are going to be heavily affected solely over a project that would bring more harm than good.

Although I do not live in the nearby area, I know for certain if I lived in the areas close to the project, I would never want to live or even travel nearby again in fear of the heavy traffic, blaring noises and constant flow of trucks. My friends living nearby the area have already stated the nightmare that would unfold as a result of this project and its approval and they are enraged. I as a citizen and student in this city hope that you make the right decision and oppose this project because of all the negatives that will come from the approval of this project.

Sincerely,

Johnny Perez Hernandez
Oceanside High School Senior

Robert Dmohowski

From: Priscilla Palacio <1054978@oside.us>
Sent: Monday, February 13, 2023 10:08 AM
To: Robert Dmohowski
Subject: Eddy Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

It a bad idea because there is going to be hard to get to school with all the traffic and and it going to get kids are going to be late for school and people are going to be late to work and to doctor's appointments so i don't think that's a good thing to do there is going to be a lot of problems to get to work and to school for them they think it a good i idea but it not .

It not a good idea becomes of the the area and the pollution and they smell and its bad for the for the kids and people and it it bad for the health and it not good to smell that becomes it has lot of breathing issues and it is bad for kids that already sick .

Dear Mr. Dmohowski

Rob Dmohowski

It is going to be bad for the pollution and a lot of smog and it is going to be loud for the people that live around that area. The smog is not okay for the air and the grass and for the plants. This could kill plant life. And if we small it we can get sick and dog can die and get sick

Priscilla Palacio
Oceanside High class of 2023

Robert Dmohowski

From: Matias Castaneda <1038239@oside.us>
Sent: Monday, February 13, 2023 10:23 AM
To: Robert Dmohowski
Subject: Eddy Jones Warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Rob Dmohowski, my name is Matias Castaneda and I am a high school student who lives in Oceanside.

I oppose the building of the Eddy Jones Warehouse because it's going to cause traffic on the 78 and I5, it will cause pollution, and it will bring depreciation to the local houses.

I live on base and traffic getting on base is already crazy, now with the possibility of semi-trucks going in and out of the area will cause more traffic getting on base and it will be even crazier. Not only that, but it also affects everyone else who lives on base.

The area it'll be built in will be hit with pollution. First, the smog that semi-trucks produce will affect the lives of the people in and around the area. It'll make it harder to breathe and can affect the human body. Noise pollution can also drive people away and make housing cheaper because no one would want to live there.

It's these reasons why I ask if you could vote against the proposal and if you could email me back if you do.

-
-
- Sincerely, Matias Castaneda, 502 Gillingham Ct. Oceanside, CA, 92058
-

Oceanside High School, Class of 2023

Robert Dmohowski

From: Gus Hawthorn <gushawthorn@gmail.com>
Sent: Saturday, February 11, 2023 6:33 PM
To: City Council
Cc: Robert Dmohowski; Zeb Navarro; City Manager
Subject: Eddie Jones Warehouse & Distribution Facility Concerns

Warning: External Source

Dear City Council Members,

Before moving to Oceanside a few years ago we lived in the Inland Empire. The air and quality of life in the area improved steadily over the 40 years we were there. We still have connections there and have recently learned that conditions have become dramatically worse as numerous warehouse distribution centers have been built next to schools and homes. Health and quality of life have been badly impacted by the increased truck traffic and air pollution. EIRs notwithstanding, peoples' health and quality of life have seriously worsened. The situation is documented in the attached articles. Please do not let the same irreversible disaster happen to Oceanside.

Please stop the Eddie Jones Warehouse, Manufacturing & Distribution Facility. Just as happened inland, and in spite of what the developer says, the project will negatively impact the immediate area and adjacent roads. Importantly too, do not let this project provide a starting point for further warehouse development.

Instead of building warehouses encourage businesses that employ more people with better paying jobs that don't ruin our quality of life and destroy the city's continuously improving image. Oceanside has a good start with companies like Genentech, Nitto Hydraulics, Gilead and others. Attract quality businesses by providing a supply of skilled employees through career training, like the City supported Machinist Technology Program, with additional high school, community college and university partnerships.

Thank you,

Henry (Gus) and Terri Hawthorn
110 Sherri Lane, Oceanside

San Bernardino Sun

<https://www.sbsun.com/2022/10/13/warehouses-pose-a-dilemma-for-the-inland-empire/amp/>

NBC News

<https://www.nbcnews.com/news/amp/ncna1265420>

University of California Riverside

<https://insideucr.ucr.edu/stories/2021/06/02/poor-air-quality-and-warehouses-linked-inland-empire-covid-19-inequities>

Sierra Club

<https://www.sierraclub.org/sierra/are-warehouses-inland-empire-blessing-or-curse?amp>

LA Times

<https://apple.news/AaDIeuwBdR1-xUfGG7aF-9A>

Guardian

<https://www.theguardian.com/us-news/2022/dec/29/e-commerce-warehouses-amazon-ups-fedex-california-pollution>

Jan. 15, 2023

Oceanside City Principal Planner
Rob Dmohowski
300 North Coast Highway
Oceanside, CA 92054

Dear Rob Dmohowski,

I am a local resident and I am very much opposed to the Eddie Jones truck terminal industrial site. I purchased my beautiful home in the Ca Brisas development in 2010 in large part because it backs against undeveloped land of Camp Pendleton, and its proximity to the beach. I've always enjoyed the beach lifestyle and it was a primary factor in my choice to live here. Unfortunately, I just learned about this proposed Eddie Jones terminal and it's a very upsetting prospect to consider I may be living so close to a major truck terminal. I understand they are planning 114 semi-truck terminals, 60 truck trailer parking stalls and 590 auto parking spaces. This is very disturbing as it would be located right around the corner from my home, and situated on the only west side entry and exit road to this community.

A large industrial trucking terminal is not the type of development I would think City Council would consider compatible with a long-term vision for Oceanside. This type of trucking terminal is typically found in Fontana, Chula Vista, and other large scale commercial cities with similar industrial parks, warehousing, and manufacturing.

I'm not opposed to development, but would hope and expect those given the responsibility of representing Oceanside residents and taxpayers, would guide us towards something more in line with a future becoming Oceanside. I would also expect those representatives to protect the majority interests of City residents, and not special interest groups.

Sincerely,

Chris Weinold

Chris Weinold
760 547-4615
chrisw2100@gmail.com

Residence:
Ricewood Dr., Oceanside, CA 92058

Mailing address:
603 Seagaze Dr., #503, Oceanside, CA 92054

Robert Dmohowski

From: CAROL BROELAND <memaeae@aol.com>
Sent: Saturday, March 4, 2023 12:09 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Dee Keck
Subject: Health and Safety concerns re: Eddie Jones Project

Categories: Red Category

Warning: External Source

My husband and I are seniors, we live in the airport community, on the little hill behind the Oceanside Airport. A major concern of ours, regarding the proposed Eddie Jones Project is our SAFETY. There have been two fast-spreading fires in the 6 years we have lived here. Some neighbors were evacuated, some started to voluntarily evacuate out of fear of being trapped. Evacuation of ALL homes on our hillside is difficult, if not impossible. Our ONLY WAY OFF THE HILLSIDE ARE two streets! FOUSSAT AND BENET! Dozens of cars, trying to leave our area, clogged our narrow streets, as we headed over the FOUSSAT Bridge. It was frightening. The sky was black with smoke, we could see flames, and the reality of not being able to get out, became very real. Behind us is Camp Pendleton, we are unable to exit through the long fence line that borders many of our streets! Stuck. We felt STUCK. Can you imagine if OceanKamp was up and running at that time, how the visitors and employees would add to the chaos? More cars, trucks, residents and visitors to the wave pool, hotels, campsite, restaurants. Very frightening to imagine. The Eddie Jones Project, even if scaled back would add hundreds of cars and trucks to this mix. A disaster waiting to happen.

This project is not in the best interests of our community. It is not a good fit. We need something that would actually benefit this great community, not doom it. PLEASE, don't compromise our safety by allowing this monstrosity of a project to be built in that location.

Respectfully,
Carol and Walt Broeland

From Carol's iPad

Robert Dmohowski

From: CAROL BROELAND <memaeeee@aol.com>
Sent: Saturday, March 4, 2023 1:25 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro
Subject: Diesel Particulate Matter Health Impacts | California Air Resources Board

Categories: Red Category

Warning: External Source

Please read this informative article and consider what impact the Eddie Jones project would have on our community. Prevailing winds would distribute these pollutants easily over our entire coastline community and inland. Adults, children, animals and all living things would be impacted! Please don't let this project happen. Who would BENEFIT from THIS PROJECT??? Not the residents of Oceanside. It would have a direct impact on our pets, and the animals at the local humane society. Not to mention the thousands of marines on Camp Pendleton. Not our planet. PLEASE CONSIDER NOT ALLOWING THIS PROJECT TO MOVE FORWARD.
Carol Broeland

<https://us-east-2.protection.sophos.com?d=ca.gov&u=aHR0cHM6Ly93dzluYXJiLmNhLmdvdi9yZXRvdXJjZXMvc3VtbWFyeS1kaWVzZWwtcGFydGljdWxhdGUtbWF0dGVyLWlhYWx0aC1pbXBhY3Rz&i=NWI3ZWYwYmI4YmY4ZjUxNmJOTJhN2Q2&t=RXhtb0pZMnlpcUc0MkJoMIE3bmp1d0psd0NsTXF4WjdrUUg1RIVxcmVRUT0=&h=42ae91305d1744a386f294d8bac5cdaf&s=AVNPUEhUTONFTkNSWVBUSVZQ5bxYNMcR-GOb5jzCoCo>

>From Carol's iPad

Robert Dmohowski

From: Stephanie Dimakides <stephanie.dimakides@gmail.com>
Sent: Sunday, March 5, 2023 2:05 PM
To: Robert Dmohowski; City Manager; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss
Subject: Opposition to the Eddy Jones Warehouse
Categories: Red Category

Warning: External Source

I am a resident of the Wanis View HOA living on 3402 Soyla Drive which is in the area known as Airport. The construction of the proposed Eddy Jones Warehouse, manufacturing and distribution facility will greatly impact our quality of life in this area.

The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as all who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Camp development will soon add traffic issues on Foussat and onto the 76. Foussat and Benet are the only access roads for the Airport communities. Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. The same will be true on Foussat Rd due to increased traffic from the Ocean Camp and the 590 vehicles expected for the employees and visitors at this new facility. If there was an fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area. Home values will most definitely decrease in the area due to the semi-truck traffic on Benet Rd.



Stephanie Dimakides

REALTOR® | CalRE# 01752778



COLDWELL BANKER REALTY
818.321.2320

EMAIL: Stephanie.Dimakides@gmail.com

WEBSITE: StephanieDimakidesRealty.com



COLDWELL BANKER
REALTY

Robert Dmohowski

From: Jennifer Bray <jenbray08@yahoo.com>
Sent: Monday, February 13, 2023 5:59 PM
To: Robert Dmohowski
Subject: Object to Eddy Jones warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Our Concerns with this warehouse are listed below. Please pass along to those who are involved to hear our community voice. Thanks

TRAFFIC IMPACT - The potential number of semi-trucks coming and going 24/7 on Benet Rd will interfere with our travel to and from our homes as well as ALL who use the 76 Hwy and I-5 Fwy. Additionally the traffic generated from the Ocean Kamp development will soon add traffic issues on Foussat and onto the 76. There is NO money earmarked for any road / traffic improvements.

TRANSPARENCY - To date the lack of transparency/communications is very apparent when very few of the Airport residents and surrounding areas are even aware of the intended development. We are not opposed to something being built but what type of business, what level and hours of operations and what impact will it have on our community and our quality of life are not being addressed or taken into consideration.

POLLUTION - We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24 hours a day. Noise pollution from these vehicles is concerning. Pollution of the San Luis River is also an issue to be considered.

SAFETY/EMERGENCY SERVICES - Safety concerns will be created by semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists and skateboarders. The same will be true on Foussat Rd due to increased traffic from the Ocean Kamp and the 590 vehicles expected for the employees and visitors at this new facility. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

CITY'S VISION - This development as proposed is not a good fit with the Airport community, nearby neighborhoods and the Ocean Kamp development. We do not oppose development as long as it is well thought out and is compatible with the residential area that it borders. The city's vision plan was set in place over 50 years ago before any residential development was envisioned on property to the north of this area. We believe this project as proposed is too big and will have too big of a negative impact on our nearby communities.

Jen Bray
Sent from my iPhone

Robert Dmohowski

From: Dan De Flon <dandeflon@yahoo.com>
Sent: Friday, May 12, 2023 12:08 PM
To: Robert Dmohowski
Subject: Eddie Jones warehouse Project

Categories: Red Category

Warning: External Source

My name is Daniel De Flon and I bought a house north of the San Luis Rey river on Rivertree drive in oceanside in 1988. Before the highway 76 was built Mission ave was the closest road to access the interstate 5 and get around this part of oceanside. The building of highway 76 in the 1990's greatly improved accessing the 5 fwy and getting to the 15 fwy. The Eddie jones warehouse project would destroy all the improvement highway 76 gave us. The big trucks from the Eddie jones warehouse project would make using benet rd extremely difficult when your leaving or returning to your home. The left hand turn lane on highway 76 east to Benet rd would be jammed packed with big trucks from the Eddie jones warehouse project all the time making it hard for cars to use the left hand turn lane to access Benet rd. The on/off ramps to the 5 freeway will be congested with big trucks from the Eddie jones project causing dangerous driving situations. Cars exiting the 5 freeway to Camp Penelton will be met with extremely heavy big truck traffic entering the 5 freeway causing dangerous driving conditions for motor vehicles. The Eddie jones warehouse project is a terrible idea and will have adverse effects on people who live locally inland or far away. The big trucks from the Eddie jones warehouse project will cause accidents from fender benders to fatalities. I hope you do the right thing and stop the Eddie jones warehouse project from happening. Thanks for your time. Oceanside residents Daniel and Rhoda De Flon

Robert Dmohowski

From: funjewels4u@cox.net
Sent: Tuesday, June 20, 2023 6:04 PM
To: Robert Dmohowski; City Manager; Planning-Planning Commission; Esther Sanchez; Eric Joyce; Rick Robinson; Ryan Keim; Peter Weiss
Cc: Randy Hanson; rawketgaming@gmail.com; eddiejoneswarehouse@gmail.com
Subject: Eddy Jones Warehouse Project

Categories: Red Category

Warning: External Source

Hi 😊

I am emailing on behalf of myself, My husband Randy Hanson & our adult son who lives with us Robert Hanson. We are all very concerned over the future building of the Eddy Jones Project. We are a house of interest since we live on Toopal Dr and our house backs up to the river. So we will be effected by this very large project.

We are against the plans that are being submitted and here's our reasons why-

- 1) It will greatly impact our traffic in & out of our community. With that many trucks going in & out on our tiny 2 lane road going out to the 76 we already deal with the towing company trucks, Hanson cement trucks and whatever from this area & Airport road. It will add more traffic to the 76 & 5 freeways which we really don't need if we are wanting people to come & visit us from far away or staycations.
- 2) The pollution from these trucks will be awful, and if they're using electric what will they do with the batteries once they die and we're right next to a river ??
- 3) If we have a fire, which there were 2 close ones last year, having all the employees/trucks will only prolong emergency vehicles getting in to us so we can be safe.
- 4) I don't understand how this will be appealing being close to the beach and trying to make our town a tourist destination ?? Especially with Ocean Kamp going in, who would want to stay close to a place that has this many trucks coming & going ??
- 5) Why are we even considering something like this here ?? I had asked at the very 1st meeting at the Library why we haven't changed the zoning for our area when we have the whole industrial area by El Corazon/Rancho Del Oro/College/Oceanside where something of this scale can be built ??

This project is just plain crazy for our area, it's too big & other city's that have built these building are regretting they did it now.

So please reconsider rezoning this area, stopping this project from what they are wanting to build, and keep us in the know of what is happening since we live right here & will be greatly impacted by it !!

Sincerely,

Randy, Mary & Robert Hanson
3281 Toopal Dr
Oceanside, Ca 92058

Robert Dmohowski

From: Linda Middleton <lindaincarlsbad@yahoo.com>
Sent: Saturday, June 24, 2023 8:59 AM
To: Planning Web; Robert Dmohowski
Subject: Eddie Jones Warehouse

Categories: Red Category

Warning: External Source

I recently viewed the CBS news report on the proposed Eddie Jones Warehouse and I was disappointed by the untruths shared by the developer's spokesperson.

I understand her job is to say whatever is necessary to get this development approved but, she chooses to share the same mistruths every time she speaks. I am sure you are aware of what she is sharing with the public to support her multimillion dollar project but here are just a few of her misstatements.

1. She stated the industrial area was there before us - this is true and I would be fine with something similar to the Deutsch plant that was there when I moved here 20 years ago. The footprint was much smaller than what this warehouse would be and it entailed employees only arriving in cars in the morning and leaving in the evening. The noise, traffic and pollution were not similar to what this warehouse will allow. As for her argument, we can use the same one - we bought our homes before this massive warehouse was proposed and many might have chosen to move elsewhere if it had been there.
2. She continues to state the one concession they will make us is a right turn lane from Benet. That already exists so that will cost them nothing.
3. She states there will only be 9 - 13 trucks entering at the peak times. How does she know this since she doesn't know who the tenet will be. And what are the peak times she is referring to - is this during a 10 minute window of time? She says it's from a traffic study - that may be true currently since there is no warehouse there and never has been. This is a completely made up number to support her claim that we should not be concerned about traffic.

She continues to support the development with false claims and she will be long gone when the warehouse is there and we are stuck with the noise and traffic.

Please listen to the residents in this area who will be negatively affected by this development.

Respectfully,

Linda Middleton
601 Tukmal Dr
Oceanside

Robert Dmohowski

From: Julie DeAnda <juliedeanda@me.com>
Sent: Thursday, April 27, 2023 10:54 AM
To: Robert Dmohowski
Cc: eddiejoneswarehouse@gmail.com
Subject: We are residents opposed to Eddy Jones Warehouse, Manf and Dist Facility

Categories: Red Category

Warning: External Source

Dear Rob,

My husband and I are members of the Group "Oceanside Speaks Out"

We live in the Wanis View Estates
Our address is 717 Wala Drive, Oceanside Ca 92058

The only access to our home is Benet Rd or N Foussat Rd.

We have signed the petition opposing the Eddy Jones Warehouse, Manufacturing and Distribution Facility plans

- 566,905 square feet
- 114 semi truck terminals
- 60 truck trailer parking stalls
- 590 auto parking spaces

Our concerns are;

Traffic

Negative impact to our local streets and the 76 Hwy to I-5 Frwy

Pollution Environmental

Negative impact to our estuary preserve directly behind our home and the wildlife that takes refuge there
I am a currently a cancer patient and clean air to breathe and frequent doctors visits are vital to my treatment.

Pollution noise 24/7

Negative impact to our quality of life, disruption due to truck and traffic noise being even closer to our home.

Safety and Emergency services

We have major concerns regarding blocked access to our roads due to heavy semi-truck and car traffic from the warehouse.

There is no money earmarked for any road or traffic improvements.

In addition to the Ocean Kamp development, these two projects combined in would undoubtedly create a traffic nightmare.

We had a ***fire behind our home last year*** that was terrifying and way too close.
We need to know that emergency services can get to our neighborhood without delay.

If I or my husband or any of my neighbors were to have a medical emergency would first responders be able to get to us in time?

Property Value

We strongly believe that having increased traffic and noise pollution in the area of our home will make our property value decrease.

This would also impact the property tax value.

Our retirement plans will also be majorly impacted and we will be forced to work much longer than planned and this will also effect our quality of life.

Transparency

To date the lack of transparency/communications is apparent.

So many of my neighbors are not aware of the intended development.

We would like to have more details such as;

- What type of business is planned to be operating?
- What are the hours of operation ?
- What is the impact it will have on our community and quality of life?

Those that do have some idea of the size, wonderful why is our quality of life not being addressed!

Cities Vision

If the idea is to bring tourism and jobs to Oceanside, one should not impede the other.

When this warehouse does creates a traffic congestion in addition to the Ocean Kamp project, who is actually going to want to go to Ocean Kamp if it takes an excessive amount of time to get to and from Ocean Kamp.

Who is going to want to live in a neighborhood that takes an excessive amount of time to pick up their kids from school or get to work or medical appointments.

Since the city's vision plans were set in place over 50 years ago before any residential development was envisioned in this area, we are requesting that the city planners review the requirements for any businesses moving into this neighborhood and reconsider the use for this area.

Since the current zoning is for light industrial, the proposed plans seem way out of range for size for both the actual building and the amount of vehicles and truck bays.

We are not opposed to development as long as it is well thought out and does not have a MAJOR NEGATIVE impact on the people that live in these neighborhoods surrounding the airport and Ocean Kamp project area.

We believe this project proposed is not a good fit with the airport community, nearby neighborhoods and the Ocean Kamp development. This project is too big and will have too large of a negative impact on our nearby communities and quality of life!

Thank you for your help in addressing our concerns for our neighborhood, property values and quality of life.

Our plea is that you take actions to see that this Eddy Jones Warehouse. Manufacturing and Distribution facility does not move forward at the above proposed size and the city reviews the zoning plans based on current residential neighborhoods and quality of life needs.

Sincerely,

Julie DeAnda
714.227.2575

Carlos DeAnda
760.429.5293

Robert Dmohowski

From: Jane Hodges <jhodges001@hotmail.com>
Sent: Tuesday, May 2, 2023 7:45 PM
To: Robert Dmohowski
Subject: Eddy Jones Project File Number D22-00001

Categories: Red Category

Warning: External Source

May 2, 2023

Dear Mr. Dmohowski,

I am writing in opposition to the Eddy Jones Warehouse, Manufacturing and Distribution Facility that is projected for Eddie Jones Way and Benet Road, just Northeast of the Oceanside Airport(Project File Number D22-00001). My concerns are mainly regarding traffic type and volume, safety, air and noise pollution, as well as the environmental impact and impact on emergency services to residents in the area.

As a resident of Oceanside for almost 40 years, I am deeply concerned about this proposed development that would house semi-trucks and trailers, facilitate a 24 hour warehousing operation, and negatively impact the traffic flow on Mission Ave. and Highway 76. These 2 thoroughfares are already grossly congested during peak commuting hours and have been even more so with the current construction on Highway 78. Residents, businesses and recreational facilities along that road would all be negatively impacted. The Ocean Kamp development is already expected to increase traffic issues on Fousat Rd., Mission Ave. and Highway 76!

Your support of the building of this proposed facility is very disappointing. As an active community member and voter, I keep close watch on the representatives of Oceanside. I do not feel the residents have been made aware of the impact this proposal will make on the well-being of the citizenry of our beautiful city of Oceanside. I realize our representatives are constantly looking for ways to increase the income to the city, but THIS is not the answer! The residential communities in close proximity to this proposed facility will suffer immeasurable impact, ranging from noise and air pollution to major safety concerns for the children who frequent the nearby establishments(skate park, restaurants, and retail stores) using the same thoroughfares(state route 76 and Mission Ave) that these large semi-trucks will be entering/exiting and traversing on.

I strongly urge you to withdraw your support of this disastrous proposal for development. The citizens of this city depend upon you to act in our best interests.

I look forward to hearing your response with regard to my concerns stated above. Thank you.

Jane Hodges

**Jane Hodges
380 Islander St.
Oceanside, CA 92054**

Robert Dmohowski

From: Vicki Steyn <gmavicki70@gmail.com>
Sent: Saturday, October 22, 2022 10:50 PM
To: Robert Dmohowski
Subject: RE: Eddy Jones Development.

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

I am opposed to this development unless there are improvements in Hwy 76 and Benet Road to mitigate the increased traffic. As a resident who uses 76, the traffic is already congested so adding this development without also improving the infrastructure is a bad idea! I know that we need to develop the area but PROVIDE THE INFRASTRUCTURE TO SUPPORT THE DEVELOPMENT!

Respectfully,
Vicki Steyn

Robert Dmohowski

From: Dane Thompson
Sent: Tuesday, June 20, 2023 8:57 AM
To: Robert Dmohowski
Subject: FW: STOP this crazy warehouse!

fyi

From: Leah Conroy <lconroy68@gmail.com>
Sent: Monday, June 19, 2023 7:38 PM
To: Planning-Planning Commission <PlanningCommission@oceansideca.org>
Subject: STOP this crazy warehouse!

Warning: External Source

Dear Representative;

Now that the land is clear, it seems appropriate to address the future of this expansive space in the planning stages for a massive warehouse near Oceanside's municipal airport. The location, size and business schedule of this type of development does not add to our beautiful city's character nor, will it benefit Oceanside's local and visiting population. This busy corridor of HWY 76, between the 5 and College Blvd is highly residential and ecologically vulnerable, as it includes the SLR watershed.

Oceanside made mistakes in the past such as, permitting the shopping center (Kohls), to build just feet above beautiful, (once sacred to native people), El Salto Waterfall in the Buena Vista Creek. What a disaster, never to be corrected or improved. Tents and toilet paper litter the small space between the building and the falls, now. The short trail system (off Hacienda) east of this shopping center offers little to no compensation for destroying a lovely, natural resource.

Right now, we have time to stop this ridiculously huge warehouse project (Eddie Jones), before we lose sight of better options. Please consider an eco-tourism type of plan, similar to Ocean Kamp, for the entire, SLR riverbed region. This huge Eddie Jones warehouse would ruin any future for a potentially lucrative business with a more progressive, healthful theme. The EJ project belongs far away from residential areas and even farther from our city's shoreline asset. Oceanside deserves a better development plan, for this highly visible, naturally elegant, location.

I hope the owner of the property and the City of Oceanside, consider a more forward-thinking approach to the San Luis Rey riverbed district that would benefit our community's longterm health. Let's reduce pollution, reduce traffic, improve our neighborhoods, reach out to visitors with fun outdoor options, by encouraging smart, clean growth.

Thank you, for your commitment to making Oceanside a healthful, fun place to reside and visit.

Regards,

Leah Conroy

3445 Ashwood Ct
Oceanside, CA 92058

Robert Dmohowski

From: Jeanette <jweeks0400@att.net>
Sent: Saturday, August 17, 2024 11:47 AM
To: Robert Dmohowski
Subject: NO to the Eddie Jones warehouse Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

We Oppose:

The Eddie Jones Warehouse, Manufacturing and Distribution Facility. We do not want this in our community.

Jeanette Weeks

John Whitford

"Carry out a random act of kindness, with no expectation of reward, safe in the knowledge that one day someone might do the same for you" - Princess Diana

Robert Dmohowski

From: Stephanie Dimakides <stephanie.dimakides@gmail.com>
Sent: Thursday, June 15, 2023 7:21 AM
To: Robert Dmohowski
Cc: City Manager; Planning-Planning Commission; Esther Sanchez; Eric Joyce; Rick Robinson; RICHARD KLEIN; Peter Weiss
Subject: Eddy jones Project File Number D22-00001
Attachments: Effect on Wildlife.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

I would like to be included in the interested party list with updates on the project. If the conditional use permit is approved, this will greatly impact traffic, environment and home values.

We are a beautiful coastal community and need to build on that. There is nothing classy about a half a million square foot warehouse polluting our environment.

The wildlife objection is based on noise - multiple trucks idling, or multiple trucks pulling out. The attached pdf shows the big impact this project will have on the wildlife in the area.

This is what I imagine we will be looking forward to if this project is approved:



Stephanie Dimakides

REALTOR® | CalRE# 01752778



COLDWELL BANKER REALTY
818.321.2320

EMAIL: Stephanie.Dimakides@gmail.com

WEBSITE: StephanieDimakidesRealty.com



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REALTY

Eddie Jones Noise Issue Wildlife

Noise calculation

Federal Highway Administration: Max noise level for idling truck 85 dBa at 50 ft

Truck pulling out (accelerating): 90 dBa at 50 ft

Normal urban level: 55 dBa day 47 dBa night

6 Truck idling at Warehouse (happening 24 hours per day)

Location	Distance (ft)	Decibels	Sound increase From day background	Sound increase from night background
River bed	340	76	410%	745%
Wanis Nature walk	1179	66	215%	360%
Wanis View preserve	1790	62	160%	282%

3 Trucks pulling out

Location	Distance (ft)	Decibels	Sound Increase from day background	Sound Increase from night background
River bed	340	78	422%	823%
Wanis Nature Walk	1179	68	255%	414%
Wanis View Preserve	1790	64	188%	368%

Assume some combination of these.

68 dBa at Wanis nature: 3x as loud day 4x as loud night

64 dBa at Preserve: 2x as loud day 3 x as loud night

Robert Dmohowski

From: rickbobst@gmail.com
Sent: Tuesday, April 11, 2023 6:11 AM
To: rickbobst@gmail.com
Cc: Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro; eddiejoneswarehouse@gmail.com
Subject: Why Eddie Jones and not a new school?
Categories: Red Category

Warning: External Source

We are struggling to educate the generations to come with adequate infrastructure. Neighbors are leaving because options are so limited for elementary/middle school availability in our town. Why Eddie Jones and not a school?

All the new homes being built, the closing of schools, location to 76, SLR Trail, skate park, future water park, and an empty lot that would accommodate a beautiful school...why not!!!

Build a school; not a trucking facility that adds no value to our community.

Cheers,

Rick

Robert Dmohowski

From: Arness James <deal9977@yahoo.com>
Sent: Tuesday, February 14, 2023 5:48 PM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse congestion

Categories: Red Category

Warning: External Source

Hello, I am hoping that you will vote against this project that will impact my quiet community with semi tractor truck noise and congestion. Benet Rd. is a two lane road poorly suited for the numerous trucks that this project will place upon it. Please vote no. Thank you, Dean

Robert Dmohowski

From: Jachin Cook <1048769@oside.us>
Sent: Tuesday, February 14, 2023 9:33 PM
To: Robert Dmohowski
Subject: Eddie

Categories: Red Category

Warning: External Source

Dear (**Rob Dmohowski**):

My name is Jachin Cook, I am an Oceanside High School student who resides in the district. I oppose this project for the following reasons. The first argument I have against this project is traffic. Traffic in Oceanside is already terrible and the construction of this wave-pool project would add considerably more after its completion. The current commute from the 76 to the I-5 would build up much more than it is already. If anything, the best thing to do instead of adding more traffic is to build another lane, or fewer traffic lights, which would allow traffic to flow better before adding Ocean Kamp into Oceanside. The volume of traffic is already bad in rush hour, not to mention, any time of the weekend or day that people are traveling more frequently when not working.

My second reason to back my opposition to Ocean Kamp is the noise neighborhoods will hear from all the trucks and construction required to build something of this magnitude. Furthermore, the potential pollution of the sea and surrounding area wouldn't be the best. Adding more trucks entering and exiting the highway would cause massive backups on the 76 and I-5 let alone all the debris, trash, and other harmful waste to the environment this construction project would add to the existing pollution we already have in Oceanside. Also, the ocean would get affected by runoff and any leaks, spills, or trash that flies away and lands in the sea would harm the surrounding ecosystem.

My final and most important reason for the opposition to this project is that as a semi-new member of the Oceanside community, and a previous resident in Florida. I have seen what this industry does specifically to Orlando, which is very industrial, and they still struggle with keeping up with population influx due to attractions. The outcome of this project would add more tourists which means more people, and that is good for the economy through financial growth coming into our city, which will help build more things in the future, what we as a community should be more focused on is what we have right now and managing the problems we can fix now instead of adding more into an already busy and booming area. In contrast, we should focus on more affordable housing, mitigating road traffic, and modernizing public transportation assets resulting in faster and more efficient transportation and less traffic. It takes 30-40 minutes to get to the beach currently for most residents due to traffic and we could use some of this 200 millions= dollars to bolster public transportation or add in bike lanes/bridges, resulting in a more feasible ten to fifteen-minute commute. People wouldn't have to speed or worry about being late to work or emergency vehicles struggling to get through areas on freeways or even local lanes because of traffic. Overall, we could use some of this money to enhance and encourage residents and visitors to travel to our beautiful beaches faster and in a manner healthier for our environment while ensuring future generations enjoy the natural beauty of Oceanside. Electric buses, pedestrian and bike routes and bridges over the highways could really bolster tourism and protect the natural beauty of our city and beachfront.

I believe that the Ocean Kamp project should be pushed back or overturned as there are a lot of people in our community who would benefit from the ideas I have presented. The younger generation of Oceanside

residents, who will be able to vote soon, would love to be able to visit our beaches more frequently and surf real waves, with less traffic. The money for this project could be better used, or at least saved for later, and not used at this moment. Our economy and community will continue to grow rapidly, mitigating traffic and providing residents and tourists with easier and environmentally friendly transportation means, which will undoubtedly protect Oceanside for years to come. The natural beauty of our city should be preserved and adding in this expensive wave pool project could prove to hurt our city in the long run.

Sincerely,

Jachin S. Cook
3019 Arnies Alley
Oceanside, CA, 92056

Robert Dmohowski

From: Tami Montague <tmontague153@gmail.com>
Sent: Monday, April 24, 2023 9:02 AM
To: Robert Dmohowski; City Manager; Esther Sanchez
Subject: Eddie Jones Project

Categories: Red Category

Warning: External Source

Good Morning

I just wanted to voice my opposition to the Eddie Jones Project proposed over near the airport. I don't feel that area can handle the additional traffic and especially the trucks. As a concerned Oceanside resident, I hope the concerns of those opposed will be considered.

Thank you for your time,

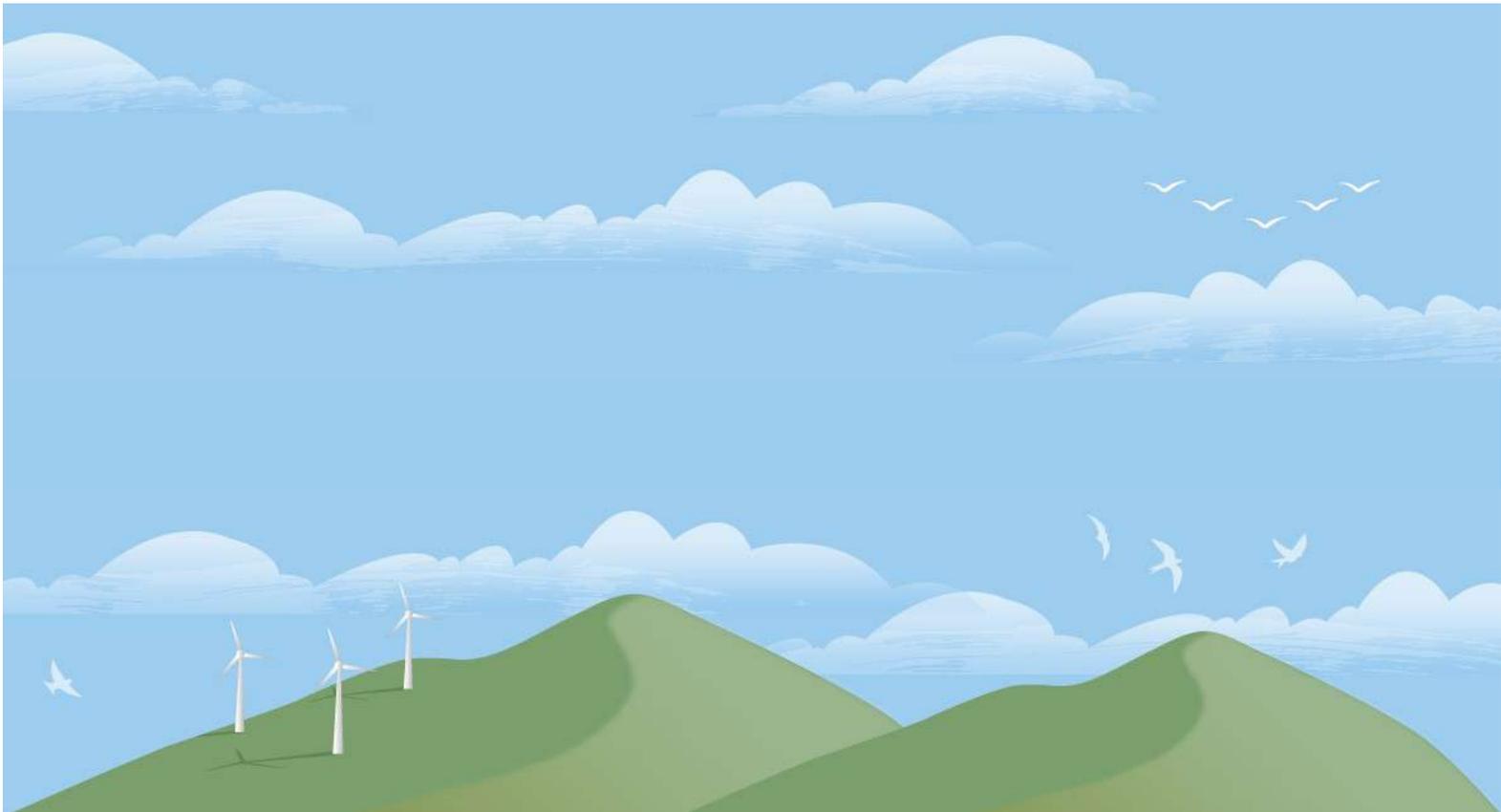
Tamara Montague
153 Roadrunner Ln.
Oceanside, CA 92057

Robert Dmohowski

From: int <ptsoris@aol.com>
Sent: Thursday, August 3, 2023 10:39 AM
To: Robert Dmohowski
Subject: Fw: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source



AOL Mail Stationery

Subject: Fw: Eddy Jones Warehouse

From: int <ptsoris@aol.com>
Subject: Eddy Jones Warehouse

My husband and I have lived in our present home for 14 years. We want to keep our neighborhood as much as

possible residential... We are assured to have the mega truck terminal will be a hazard. We would advise you to keep the zoning and not allow a change for larger warehouse...The negatives are as follows: traffic impact, pollution, safety and emergency services...It is already a given that the Ocean Kamp will be built...That alone is enough for our neighborhood...traffic and more.

When they purchased the 31 acres...they were aware of the zoning...keep it so the truck terminal won't be a detriment to our neighborhood.. Home prices with drop if they get the mega terminal.

Pauline Tsoris
536 Crestwood Drive
Oceanside,
CA 92058

760-458-4271

Robert Dmohowski

From: Matthew Brown <1033199@oside.us>
Sent: Monday, February 13, 2023 8:54 AM
To: Robert Dmohowski

Warning: External Source

Dear Rob Dmohowski

My name is Matthew Brown and I am an Oceanside student who resides in the district. I oppose this project for a number of reasons. The first argument I would have against it is the traffic it would cause. With the building of the project, This would cause a traffic build up that would make my commute to stores a lot harder than they usually are. My second reason is the air and noise pollution it would cause. With all the trucks bringing in supplies for building, The noise and air pollution would be greatly increased for the usual that is expected in that area. My final reason is that I think there are more important things that could go there than a warehouse. I believe that a warehouse could be helpful, but I think maybe cheaper living areas or even shelters could be a better addition to an area that is plagued with homelessness. As a military family, I am not used to staying in one place but even because of that I would still like for our community to be thought of and taken care of. I feel that this project is important to me because it will affect how our everyday lives will change from the way they used to be. It would affect my family because my Mother usually travels with my 1 year old sister to go and take her to eat. With this project, however, the noise and air pollution from the trucks could be damaging to them and would be an inconvenience for them. It would affect our community by causing overcrowding and the nature that would change around us would be too big of a change to happen. I think it would greatly help this community if you were to vote against this warehouse or at least find a new location for it as I believe the cons outweigh the pros of building it. But I would love to hear your thoughts or opinions on the matter and how you feel that it would be a helpful product to the community.

Sincerely,
Matthew Brown 1 PirateCove Way 92058.

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Tuesday, February 21, 2023 8:50 AM
To: Arlene Tendick
Cc: Robert Dmohowski; John Bucalo
Subject: innovation in building

Categories: Red Category

Warning: External Source

Hi Arlene -

Following up on your suggestion to send alternative ideas for the Eddie Jones project, I thought I'd send some solid examples of innovation and creativity in development that presents a more sustainable and acceptable approach to commercial development than what is currently proposed at the Eddie Jones project.

One of my primary concerns with the current proposal is quality of life living so close. The Eddie Jones land was zoned in the 70s before any of the housing projects existed off Benet road, and before recreational attractions like the bike trail and skate park existed. It doesn't make sense to put an industrial warehouse and distribution center with over 100 bays for diesel-pollution-spewing big rigs right next to attractions that Oceanside touts as outdoor recreational attractions as much as it doesn't make sense to build something like this just hundreds of feet from homes where many resident families (including my own) have young children who spend most of their time riding bikes in our quiet cul de sacs or playing in backyards facing the project.

Our homes pre-date your project thus I would argue, the project must consider the impact as a top priority. The community surrounding this project does not accept the project as it is proposed. We want you to come back with a different proposal that takes our concerns seriously.

The developer has to know this project lacks creativity and innovation, but gives the impression that he is just pushing for as much as he can take ("this project is zoned for a project like this" he reminded everyone several times throughout the zoom meeting). As a good point of comparison, the rejected Amazon project in Ocean Ranch (also zoned for the project that was proposed) was a fraction of the size of the Eddie Jones project at 142K sq ft with 15 truck terminals, and still rejected 5-0 by the Oceanside City Council.

We are living in 2023, not 1973. It's time to come up with something that brings value to the community given all the changes in the surrounding area. This should be an opportunity to stand out as an innovator of land development, rather than follow old-school models of industrial exploitation of one of the last remaining large plots of coastal real estate in Southern California.

See below for a few examples of great work in the world in Industrial zoned areas as well as some examples of creative uses of industrial structures. Oceanside deserves more creativity in one of the last remaining large plots of land just 2 miles of the beach. The proposed building of 550K sq ft has no place in coastal San Diego. Beyond the impact to the quality of life and equity value of the homes in the immediate area, the number of semi-trucks clogging the 76 and nearby 5 freeway at the gateway to the city of San Diego will have negative implications for all those who live and visit San Diego.

1) Bjarke Ingles Group "Miami Produce" - Miami, FL

<https://big.dk/projects/miami-produce-6292>

"Located at the center of Allapatah, an industrial district of northwest Miami, BIG's design for Miami Produce aims to create a new destination for the city... The site will be activated by urban farming, restaurants, store fronts, co-working offices, apartments and educational programming."

2) Bjarke Ingles Group (BIG) "The Plus" - Denmark

<https://big.dk/projects/the-plus-3837>

"...the factory doubles as a public park for hiking and camping..."

4) MVRDV Hoowave Water Factory - Taiwan

<https://www.mvrdv.nl/projects/908/hoowave-water-factory>

"The masterplan gives the town a comprehensive strategy for flood resilience while improving the quantity and quality of the town's available water, as well as integrating mobility networks such as cycle paths, cultural amenities, and ecological systems to create new connections between Huwei, its citizens, nature, and the river."

5) House of Air - San Francisco

Housed in one of the Presidio's historic airplane hangars

<https://houseofair.com/san-francisco/>

6) Cedros District - Solana Beach

<https://socallandmarks.com/index.php/2021/11/06/cedros-design-district/>

"William S. Jack established the Bill Jack Scientific Instruments Company in Solana Beach in 1949. He built the row of Quonset huts on South Cedros Avenue "because he needed a place for people to work quickly." The huts have been transformed, over time, to house shops, showrooms and restaurants in the Cedros Design District."

<https://socallandmarks.com/index.php/2021/11/06/cedros-design-district/>

Your assistant said she was taking notes of the residents' concerns at the City Council meeting and would take them back to the design team. Please feel free to forward this email to your design team as well. There are many more examples of innovation in industrial buildings out there. Let me know if you would like me to send more.

Regards,
Gretchen

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Wednesday, March 1, 2023 11:27 AM
To: Arlene Tendick
Cc: Robert Dmohowski; John Bucalo
Subject: Sustainable transport as an opportunity for innovation

Warning: External Source

Hello Arlene -

I was in a business class this morning and someone mentioned the importance of sustainable transportation in the future of ESG in corporate governance and it got me thinking. Per [this Yahoo Finance article](#) dated February 2023, the 2023 Sustainable Transport Award recognized Paris, France for the citywide cycling plan and investments in building infrastructure for cycling in the city as a way to reduce emissions, alleviate traffic, and eliminate dependencies on fossil fuels as part of a broader climate plan.

This got me thinking about the Eddie Jones land and how unique it is with the adjacent bike trail. Why not build a commercial center or business district with cafes and bike trails focused on promoting cycling to work? Imagine a corporate campus where employees ride bikes between buildings and to onsite cafes and shops. Or a new-age Cedros District with cycling infrastructure between stores and attractions to create a cycling attraction in Oceanside.

Something like this would not only create a unique and innovative attraction for Oceanside as a whole, but could also help the city achieve climate plan objectives.

No one wants a 550K sq ft warehouse/distribution center built in coastal Oceanside. No matter how much community outreach you do, you will not convince our community that your project is a good thing for Oceanside. Our request is that the developer come back with a different proposal, not add a lane to Benet Rd as I have heard you mention a few times now.

In the meantime, I'll keep sending the ideas on alternative business concepts for the land as you suggested in the Zoom call.

Gretchen

Robert Dmohowski

From: Kathy Stark <kathy.stark1@gmail.com>
Sent: Saturday, March 25, 2023 10:49 AM
To: Robert Dmohowski
Subject: Airport Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Sent from my iPad Mr. Dmohowski, I am writing to request you put me on the "interested" list when it comes to this proposed project. I could not be more opposed than I am and I know my neighbors feel the same way. Really hoping this horrible project will not be approved, there could be so many more enhancing projects there for the City of Oceanside. There is only now so much property located close to water anymore. Why put in something that ruins the look of the land, pollutes the air and will make traffic much worse than it already is? Thank-you for your consideration, Kathy Stark

Robert Dmohowski

From: Kathy Stark <kathy.stark1@gmail.com>
Sent: Wednesday, August 16, 2023 1:03 PM
To: Robert Dmohowski
Subject: Eddy Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Sent from my iPad Concern over the Eddy Jones project

Dear Mr, Dmohowski

My name is Kathy Stark and I live in the airport community and believe this project threatens my safety. As we have seen with Maui, if a fire rolls down the San Lewis Rey and into our neighborhood, we are in danger with extreme traffic at both ends of our community. Ocean Camp and then this huge project this developer wants to build which is able to house over a hundred trucks and 560 cars is potentially life threatening. This project is not necessary in this city. The traffic, the pollution and the size of this thing is very frightening to me. We only have 2 ways in and 2 ways out, and in an extreme emergency there would be deaths. I know we can't prevent him building but this project is terrible. Please, for once, can we not make is "all about the money" but about the safety and well being of my community as well as anyone else who travels the 76? I know the city wants money, but at what cost? Something else can also make money and jobs.

Thank-you for your consideration, Kathy Stark

Kathy Stark
3323 Morning View Dr
Oceanside, Ca 92058

Robert Dmohowski

From: David Sacher <dave@vitalclimbinggym.com>
Sent: Monday, March 6, 2023 11:39 AM
To: Robert Dmohowski
Subject: Eddy Jones Project

Categories: Red Category

Warning: External Source

Dear Rob,

I am writing to express my concerns about the proposed Eddy Jones Warehouse at the Benet Road and Highway 76. I believe this project is not in line with the goals of the city and could have significant negative impacts for the city and its residents.

The proposed Eddy Jones Warehouse would bring significant truck traffic and noise to the area. This would have a negative impact on the quality of life for our residents and businesses, as well as on the local environment. Additionally, the large size of the warehouse would take away potential development opportunities for the area, such as the construction of a business park or other use that would draw talented, high paying workers to the area.

I understand the importance of ensuring that businesses can operate efficiently and effectively in the city, but we feel that the issuance of a CUP for the project is not in line with the intent of the CUP process. The CUP process is intended to ensure that businesses are in line with the goals of the city, and we believe that the proposed Eddy Jones Warehouse does not meet this criteria.

I urge the City of Oceanside to consider the potential negative impacts of the Eddy Jones Warehouse before approving the project.

Sincerely,
David Sacher

Robert Dmohowski

From: Dominic Galliani <domgalliani@yahoo.com>
Sent: Monday, March 20, 2023 8:20 AM
To: Robert Dmohowski
Cc: Citymanger@oceansideca.org; Esther Sanchez; Ryan Keim; ejoyce@oceanside.org; Rick Robinson; pweiss@oceanside.org
Subject: Concerns About the Eddie Jones Project and Its Impact on Our Community

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear Rob,

I hope this email finds you well. I am writing to express my concerns about the proposed Eddie Jones project, a 500,000 square-foot warehouse project in our community. As a concerned resident, I believe it is crucial to evaluate the potential impacts of this development on our community, environment, and quality of life.

The following are critical concerns I've identified, supported by relevant articles and studies:

- **Traffic Congestion and Safety:** The project may bring significant truck traffic, leading to congested roads and decreased safety. A study published by the Urban Institute found that communities with increased truck traffic experienced a higher rate of accidents and fatalities [3]. This is particularly concerning for pedestrians, cyclists, and local infrastructure.
- **Air Quality and Noise Pollution:** Increased truck traffic may result in poorer air quality and health risks. Manta and Pucci's study (2020) found that logistics sprawl led to increased traffic emissions, contributing to poorer air quality in urban areas [2]. Additionally, noise pollution from increased traffic could disrupt the quality of life for residents, particularly those living near the proposed development site.
- **Environmental Impact:** Warehouse developments can contribute to environmental degradation, such as the loss of native vegetation and potential water quality issues. Browne et al. (2005) highlighted the impacts of urban freight movements on traffic congestion, air quality, noise, and the urban environment [4]. These potential environmental consequences warrant a thorough investigation and consideration.
- **Community Character:** The construction of a massive warehouse project could fundamentally alter the character of our community. Shearer (2017) discussed the warehouse boom's effects on communities, providing examples of how increased traffic, noise, and air pollution have impacted residents' quality of life and property values [1].

I urge you to carefully consider the potential impacts of the Eddie Jones project on our community and to conduct a thorough environmental review process that includes extensive community input. By doing so, we can ensure that any future development in our community is responsible, sustainable, and reflective of our shared values.

Thank you for your time and attention to this important matter.

Sincerely,
Dominic & Sheila Oceanside residents

[1] Shearer, C. (2017). Warehouse Boom and Its Effects on Communities. Urban Land Institute. Retrieved from <https://urbanland.uli.org/industry-sectors/industrial/warehouse-boom-effects-communities/>

- [2] Manta, V., & Pucci, L. (2020). The Impact of Logistics Sprawl on the Urban Environment: A Case Study on Air Quality in Rome. *Sustainability*, 12(3), 1020. Retrieved from [Community-Driven Sports Events as a Vehicle for Cultural Sustainability within the Context of Forced Migration: Lessons from the Amsterdam Futsal Tournament](#)
- [3] Urban Institute. (Year). [The impact of truck traffic on urban communities](#)
- [4] Browne, M., Sweet, M., Woodburn, A., & Allen, J. (2005). Urban Freight Consolidation Centers: Final Report. Transport Studies Group, University of Westminster. Retrieved from http://westminsterresearch.wmin.ac.uk/4595/1/Browne_Sweet_Woodburn_Allen_2005_final.pdf

Robert Dmohowski

From: Alyce Budde <ajbudde@cox.net>
Sent: Thursday, March 30, 2023 4:58 PM
To: Robert Dmohowski
Subject: Eddy Jones Project

Categories: Red Category

Warning: External Source

Greetings,

Recently I received my water bill and included in the envelope were two inserts, 1) STAY CLASSY O'SIDE and 2) RESIDENT INPUT WANTED TO HELP SHAPE THE FUTURE OF OCEANSIDE TOURISM. The timing of these couldn't have been any more on point.

I have been out in the community trying to bring attention to residents of Oceanside the proposed EDDY JONES PROJECT. Some residents have met as a group to express our concerns about this project. We feel this would have such a NEGATIVE IMPACT on our beautiful coastal city. I felt so hopeful seeing the slogan, STAY CLASSY O'SIDE. We need to promote Oceanside as a "Destination City" and this absurdly huge distribution center does not promote that image.

I hope the recent closure of the 78 freeway was a real eye-opener to all the Oceanside residents and city officials. If this Eddie Jones project proceeds the increase to traffic on the 76 freeway will be insurmountable !! As a city official, what would you like your legacy to be? One of being a champion for sustainable growth, clean air, and coastal living? Or, as being seen as a sellout to developers?

PLEASE, I urge you to put the future of our city first and foremost. There is great potential here, let's not miss this window of opportunity, a distribution center will NOT KEEP OCEANSIDE CLASSY !! NO TO EDDY JONES PROJECT !!!

Alyce Budde (35 year resident)
3243 Toopal Dr
Oceanside, CA

Sent from my iPad

Robert Dmohowski

From: Keri Cleeremans <keri@elliptigo.com>
Sent: Friday, March 31, 2023 11:32 AM
To: eddiejoneswarehouse@gmail.com; Robert Dmohowski; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Zeb Navarro
Subject: Please halt the Eddy Jones Project and the KB Homes Project- REJECT
Categories: Red Category

Warning: External Source

Hi All,

I appreciate the few seconds of your time. I will keep this short. As an 18 year resident of Oceanside, I am begging our elected officials to please support your constituents whom voted for you! Please stop building in areas that should not be built just because you like their money. Please consider the environment, the traffic, the parking, the stress put on residents impacted by adding such structures like the Eddy Jones Manufacturing/Semi-Truck facility or the new KB Homes on a marsh set of land! (why is this project even allowed???)

Mental stress is real. Road rage is real. My commute to Solana Beach (24 miles) takes me 1.25 in morning and evening. Most of which I'm sitting on the 76 trying to get the freeway. Please, think of residents. Please consider the environment. Please consider the infrastructure it takes to support a massive building and new residential homes.

Please don't be that council woman who said in a City Council meeting to us; well, it's already bad, what difference does it make if we add more?. (I'm not kidding, she said that-it's in your minutes). Oceanside is becoming NOSide- no parking, no surfing, no swimming, no dogs, no fun, ????

This is from the 4th District Court in Riverside County who turned down a proposed village because they felt it was the right thing to do.

This ruling reminds us why California's environmental protections are so critical to helping us make smarter land-use decisions and avoid irresponsible sprawl, [said J.P. Rose, a senior attorney at the Center for Biological Diversity, in a press release.](#) A wet winter in California doesn't mean we can ignore the potential harm to wildlife and existing communities from supplying water to a new 2,800-acre development.

Sincerely,
Keri Cleeremans
Oceanside Resident

Robert Dmohowski

From: Jonathan Borrego
Sent: Sunday, April 9, 2023 9:00 PM
To: Robert Dmohowski
Subject: Fwd: Manufacturing not warehouses

Categories: Red Category

FYI

Sent from my iPhone

Begin forwarded message:

From: Bill Roth <william_roth@icloud.com>
Date: April 9, 2023 at 12:45:42 PM PDT
To: City Council <Council@oceansideca.org>
Subject: Manufacturing not warehouses

Warning: External Source

Our city again has a choice...we can enable manufacturing or warehouses. Gilead is a prime example.

Thank you for not approving the proposed Amazon warehouse in Ocean Ranch. By doing so you enabled Gilead to acquire this property for future biotech manufacturing.

Your votes also enabled Ionis Pharmaceutical to locate in El Corazon.

The link below is to a Wall Street Journal article that references FutureStitch in Oceanside as an example of how America is rebuilding its manufacturing capabilities:

<https://www.wsj.com/articles/american-manufacturing-factory-jobs-comeback-3ce0c52c>

“Best and highest use” is a basic land-development principal. Excitingly, this now applies to Oceanside’s remaining under developed land. Your vote will determine if a warehouse really is the best and highest use for land located next to a regional airport with direct access to a California highway or is Oceanside best served by offering this land as our city’s next higher tech manufacturing location?

Your answer to that question will shape our city’s future growth in city revenues and a future where Oceanside citizens can both work and live in Oceanside.

Bill Roth
3377 Golfers Drive
Oceanside

Robert Dmohowski

From: Elizabeth Anderson <eanderson3828@gmail.com>
Sent: Monday, April 17, 2023 3:22 PM
To: City Manager; Eric Joyce; Esther Sanchez; Peter Weiss; Robert Dmohowski; Ryan Keim; Rick Robinson
Subject: Eddie Jones Project / Oceanside
Categories: Red Category

Warning: External Source

Dear Oceanside City Planners, Manager, Mayor and Council Members,

I have just become informed on the Eddie Jones Warehouse Manufacturing and Distribution facility proposed to be built in Oceanside near the airport on Eddie Jones Way. Let it be known that I disagree with this proposal. This project and the resulting facility will negatively impact the traffic not only on Highway 76 but on Benet Road and Foussat Road as well.

I live very near to the airport. How is it that the residents here have not been made acutely aware of this intended development? The extra traffic and the pollution of the air, the San Louis Rey River, and the increased noise pollution 24/7 will very negatively impact our neighborhoods.

With pick ups and deliveries being made 24/7 with semi trucks coming and going at all hours, every day of the week, even firetrucks and ambulances will have trouble getting into and out of my neighborhood no matter the time of day. This is just not a good fit for our community as it is not well thought out and not compatible with the residential area that it borders. The extra traffic will put our bicycling and walking children and pets at increased risk of injury and/or death. This project is entirely too large and will ruin life as we know it in my residential area.

Please make sure that you vote against this project as this is what your constituents desire.

Sincerely,
Elizabeth Anderson
N. Oceanside resident and registered voter

--

Liz Sent from Gmail Mobile

Robert Dmohowski

From: Julie DeAnda <juliedeanda@me.com>
Sent: Thursday, April 27, 2023 10:54 AM
To: Robert Dmohowski
Cc: eddiejoneswarehouse@gmail.com
Subject: We are residents opposed to Eddy Jones Warehouse, Manf and Dist Facility

Categories: Red Category

Warning: External Source

Dear Rob,

My husband and I are members of the Group "Oceanside Speaks Out"

We live in the Wanis View Estates
Our address is 717 Wala Drive, Oceanside Ca 92058

The only access to our home is Benet Rd or N Foussat Rd.

We have signed the petition opposing the Eddy Jones Warehouse, Manufacturing and Distribution Facility plans

- 566,905 square feet
- 114 semi truck terminals
- 60 truck trailer parking stalls
- 590 auto parking spaces

Our concerns are;

Traffic

Negative impact to our local streets and the 76 Hwy to I-5 Frwy

Pollution Environmental

Negative impact to our estuary preserve directly behind our home and the wildlife that takes refuge there
I am a currently a cancer patient and clean air to breathe and frequent doctors visits are vital to my treatment.

Pollution noise 24/7

Negative impact to our quality of life, disruption due to truck and traffic noise being even closer to our home.

Safety and Emergency services

We have major concerns regarding blocked access to our roads due to heavy semi-truck and car traffic from the warehouse.

There is no money earmarked for any road or traffic improvements.

In addition to the Ocean Kamp development, these two projects combined in would undoubtedly create a traffic nightmare.

We had a ***fire behind our home last year*** that was terrifying and way too close.
We need to know that emergency services can get to our neighborhood without delay.

If I or my husband or any of my neighbors were to have a medical emergency would first responders be able to get to us in time?

Property Value

We strongly believe that having increased traffic and noise pollution in the area of our home will make our property value decrease.

This would also impact the property tax value.

Our retirement plans will also be majorly impacted and we will be forced to work much longer than planned and this will also effect our quality of life.

Transparency

To date the lack of transparency/communications is apparent.

So many of my neighbors are not aware of the intended development.

We would like to have more details such as;

- What type of business is planned to be operating?
- What are the hours of operation ?
- What is the impact it will have on our community and quality of life?

Those that do have some idea of the size, wonderful why is our quality of life not being addressed!

Cities Vision

If the idea is to bring tourism and jobs to Oceanside, one should not impede the other.

When this warehouse does creates a traffic congestion in addition to the Ocean Kamp project, who is actually going to want to go to Ocean Kamp if it takes an excessive amount of time to get to and from Ocean Kamp.

Who is going to want to live in a neighborhood that takes an excessive amount of time to pick up their kids from school or get to work or medical appointments.

Since the city's vision plans were set in place over 50 years ago before any residential development was envisioned in this area, we are requesting that the city planners review the requirements for any businesses moving into this neighborhood and reconsider the use for this area.

Since the current zoning is for light industrial, the proposed plans seem way out of range for size for both the actual building and the amount of vehicles and truck bays.

We are not opposed to development as long as it is well thought out and does not have a MAJOR NEGATIVE impact on the people that live in these neighborhoods surrounding the airport and Ocean Kamp project area.

We believe this project proposed is not a good fit with the airport community, nearby neighborhoods and the Ocean Kamp development. This project is too big and will have too large of a negative impact on our nearby communities and quality of life!

Thank you for your help in addressing our concerns for our neighborhood, property values and quality of life.

Our plea is that you take actions to see that this Eddy Jones Warehouse. Manufacturing and Distribution facility does not move forward at the above proposed size and the city reviews the zoning plans based on current residential neighborhoods and quality of life needs.

Sincerely,

Julie DeAnda
714.227.2575

Carlos DeAnda
760.429.5293

Robert Dmohowski

From: Jane Hodges <jhodges001@hotmail.com>
Sent: Tuesday, May 2, 2023 7:45 PM
To: Robert Dmohowski
Subject: Eddy Jones Project File Number D22-00001

Categories: Red Category

Warning: External Source

May 2, 2023

Dear Mr. Dmohowski,

I am writing in opposition to the Eddy Jones Warehouse, Manufacturing and Distribution Facility that is projected for Eddie Jones Way and Benet Road, just Northeast of the Oceanside Airport(Project File Number D22-00001). My concerns are mainly regarding traffic type and volume, safety, air and noise pollution, as well as the environmental impact and impact on emergency services to residents in the area.

As a resident of Oceanside for almost 40 years, I am deeply concerned about this proposed development that would house semi-trucks and trailers, facilitate a 24 hour warehousing operation, and negatively impact the traffic flow on Mission Ave. and Highway 76. These 2 thoroughfares are already grossly congested during peak commuting hours and have been even more so with the current construction on Highway 78. Residents, businesses and recreational facilities along that road would all be negatively impacted. The Ocean Kamp development is already expected to increase traffic issues on Fousat Rd., Mission Ave. and Highway 76!

Your support of the building of this proposed facility is very disappointing. As an active community member and voter, I keep close watch on the representatives of Oceanside. I do not feel the residents have been made aware of the impact this proposal will make on the well-being of the citizenry of our beautiful city of Oceanside. I realize our representatives are constantly looking for ways to increase the income to the city, but THIS is not the answer! The residential communities in close proximity to this proposed facility will suffer immeasurable impact, ranging from noise and air pollution to major safety concerns for the children who frequent the nearby establishments(skate park, restaurants, and retail stores) using the same thoroughfares(state route 76 and Mission Ave) that these large semi-trucks will be entering/exiting and traversing on.

I strongly urge you to withdraw your support of this disastrous proposal for development. The citizens of this city depend upon you to act in our best interests.

I look forward to hearing your response with regard to my concerns stated above. Thank you.

Jane Hodges

**Jane Hodges
380 Islander St.
Oceanside, CA 92054**

Robert Dmohowski

From: Sarah Costi <sarah.j.costi@gmail.com>
Sent: Friday, March 10, 2023 3:37 PM
To: Robert Dmohowski
Cc: eddiejoneswarehouse@gmail.com
Subject: Opposed to the Eddie Jones Warehouse

Warning: External Source

Mr. Dmohowski,

I am opposed to the Eddie Jones warehouse being built in my neighborhood, because of increased traffic and safety concerns. The last time a wildfire approached my home, I had my pets and important documents packed up, but I almost couldn't get out of my neighborhood because Foussat Road was blocked by everyone trying to evacuate. Between the Ocean Kamp development and the Eddie Jones project, I'm worried about being able to get to work on a normal day, much less evacuating during an emergency. If you have not been to my neighborhood, I invite you to drive on Foussat north of the 76 and see just how many people live here. The Eddie Jones warehouse poses a danger to us.

Thank you,
Sarah Costi
773 Rivertree Drive, Oceanside

Robert Dmohowski

From: Sandy Young <sdchargerfamily@gmail.com>
Sent: Monday, April 24, 2023 6:59 PM
To: Robert Dmohowski
Cc: City Manager
Subject: Eddy Jones Warehouse

Warning: External Source

Good evening,

I live behind the airport and am an interested party concerned with this plan. I would like to be updated as this progresses. I am completely opposed to the prospect of this warehouse at this location. This is so bad on so many levels. The traffic, the noise, and the pollution, to name a few. Please let me know if I need to do anything else to be included in future information regarding this project.

I have a family that lives here and I am concerned for our future with this project. We LOVE using the bike path to ride to the beach and to Mance Buchanon. We LOVE living in Oceanside and this project will change everything.

Thanks,
Sandy Young
Big Sky Resident
760.576.4785

Robert Dmohowski

From: Chris W <chrisw2100@gmail.com>
Sent: Wednesday, March 15, 2023 11:54 AM
To: Robert Dmohowski
Subject: Eddie Jones Truck Terminal

Categories: Red Category

Warning: External Source

I am a local resident and I am very much opposed to the Eddie Jones truck terminal industrial site. I mailed a letter to your office on Jan. 15 and have not received a reply, even a courtesy note acknowledging my letter.

Please advise me as to your position on this matter.

I purchased my beautiful home in the California Brisas development in 2010 in large part because it backs against undeveloped land of Camp Pendleton, and its proximity to the beach. I've always enjoyed the beach lifestyle and it was a primary factor in my choice to live here. Unfortunately, I just learned about this proposed Eddie Jones terminal and it's a very upsetting prospect considering that I may be living so close to a major truck terminal. I understand they are planning 114 semi-truck terminals, 60 truck trailer parking stalls and 590 auto parking spaces. This is very disturbing as it would be located right around the corner from my home, and situated on the only west side entry and exit road to this community.

A large industrial trucking terminal is not the type of development I would think the City Council would consider compatible with a long-term vision for Oceanside. This type of trucking terminal is typically found in Fontana, Chula Vista, and other large scale commercial cities with similar industrial parks, warehousing, and manufacturing.

I'm not opposed to development, but would hope and expect those given the responsibility of representing Oceanside residents and taxpayers, would guide us towards something more in line with a future becoming Oceanside. I would also expect those representatives to protect the majority interests of City residents, and not special interest groups.

Sincerely,

Chris Weinold

Chris Weinold

760 547-4615

chrisw2100@gmail.com

Residence: Ricewood Dr., Oceanside, CA 92058

Mailing address: 603 Seagaze Dr., #503, Oceanside, CA 92054

Robert Dmohowski

From: battcar@sbcglobal.net
Sent: Tuesday, May 2, 2023 9:34 AM
To: Robert Dmohowski
Subject: RE: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Sorry, I forgot to include this one:-)

https://us-east-2.protection.sophos.com?d=pagesuite.com&u=aHR0cHM6Ly9lZGl0aW9uLnBhZ2VzdWI0ZS5jb20vcG9wb3ZlcnMvZHluYW1pY19hcnRpY2xlX3BvcG92ZXluYXNweD9ndWlkPTRiOTI3NmUwLTU3NTAtNGRkMy1hYmZmLWNjYWZiODY1YTlxNyZ2PXNkaw==&i=NWI3ZWYwYmI4YmY4ZjUxNmJjOTJhN2Q2&t=RU84WmJlSUNLS1loQjJqZmM3VmFWYzV0SlhWRG5xNnFyTHgrbEtmRUI3MD0=&h=a543b10029fc40c9849f4f353d3dc8ad&s=AVNPUEhUTONFTkNSWVBUSVZPrIjKb72agvw9mhqwIT43bEG_wrzDSVnN3eJ_6nXY1g

-----Original Message-----

From: battcar@sbcglobal.net <battcar@sbcglobal.net>
Sent: Tuesday, 2 May, 2023 9:30 AM
To: 'Robert Dmohowski' <RDmohowski@oceansideca.org>
Subject: RE: Eddie Jones Project

Hi again Rob,

I've now read Oceanside's Economic Development Element (or EDE) more than once & candidly, the more I think about the Eddie Jones Project, the more frustrated I get... :- (What I know is what I don't want and why we (Oceanside) do not need this Project!

This, however, has already been fairly well covered by others, but frankly it seems completely out of line with the EDE.

That said, you've rightfully requested viable alternatives be provided. This has become a challenge for me, partly because of the project's current zoning: It appears that revising the property's current zoning designation from "Light Industrial" to either "Commercial" or "Residential" would include certain pitfalls, UNLESS "special" provisions or contingencies could be applied or included (if that's even possible?).

- Revising zoning to a Commercial designation "could" provide an additional but "limited" number of specialized small businesses (such as bike rental/repair), dry cleaners, auto/tire repair, etc.) that would unfortunately also lead to an increase in vehicular traffic. The potential overriding benefit would be additional jobs created within close proximity of services to the local neighborhood being served. The required "contingency" needed to limit additional traffic would be to limit either the numbers or the size of businesses, i.e., less than 5-6 businesses, or less than "X" employees/business. However, I'm not sure these types of business limitations could be imposed.

- A “Residential” designation "could" also lead to increased vehicular traffic: Therefore, the required “contingency” needed to limit traffic would be to identify & restrict the “type” of residential facilities allowed.

My point: In accordance with the City of Oceanside’s need to meet (California State Mandated) needs for increased residential housing (current shortfall numbers for Oceanside is 5,000 to 6,000 units), I believe our community would HIGHLY benefit & welcome additional senior housing community development(s). This would of course require rezoning, however, here are some examples;

- Oceanside’s Villa Trieste https://us-east-2.protection.sophos.com?d=dreamwellhomes.com&u=aHR0cHM6Ly93d3cuZHJlYW13ZWxsaG9tZXMuY29tL3ZpbGxhLXRyaWVzdGUv&i=NWI3ZWYwYmI4YmY4ZjUxNmJjOTJhN2Q2&t=QUZXU004SWd3Rm41Sno0QIRURWhmZGpBaU1RVU5TMIFoSmRDRE5IVDhqYz0=&h=a543b10029fc40c9849f4f353d3dc8ad&s=AVNPUEhUTONFTkNSWVBUSVZPrIjKb72agvw9mhqWIT43bEG_wrzDSVnN3eJ_6nXY1g
- “Themed” Retirement Communities https://us-east-2.protection.sophos.com?d=seniorsguide.com&u=aHR0cHM6Ly93d3cuc2VuaW9yc2d1aWRlLnNvbS9yZXRpcmVtZW50LWNvbW11bml0aWVzLzEwLXVuaXF1ZS1yZXRpcmVtZW50LWNvbW11bml0aWVzLw==&i=NWI3ZWYwYmI4YmY4ZjUxNmJjOTJhN2Q2&t=cTFZY0dKb3ZWMM1RejFoWTcvS0N1Q3laTnI3TWFWYm9UcHIBOWxVeGtDbz0=&h=a543b10029fc40c9849f4f353d3dc8ad&s=AVNPUEhUTONFTkNSWVBUSVZPrIjKb72agvw9mhqWIT43bEG_wrzDSVnN3eJ_6nXY1g
- Senior Living: 4 Themes & B 9 Trends https://us-east-2.protection.sophos.com?d=bdcnetwork.com&u=aHR0cHM6Ly93d3cuYmRjbWV0d29yay5jb20vc2VuaW9yLWxpdmLuZy00LXRoZW1lcy05LXRyZW5kcw==&i=NWI3ZWYwYmI4YmY4ZjUxNmJjOTJhN2Q2&t=SORSa2p3RzQrQ1FnajZjak5seEFvTWFGS1BXZkZjRDN4WGxend1VDg3cz0=&h=a543b10029fc40c9849f4f353d3dc8ad&s=AVNPUEhUTONFTkNSWVBUSVZPrIjKb72agvw9mhqWIT43bEG_wrzDSVnN3eJ_6nXY1g

In Summary, instead of smothering Oceanside with unneeded/unnecessary/unsightly warehousing, with attendant & severely increased traffic congestion, AS WELL AS highly reduced emergency services availability, why not showcase our region and make our community proud by increasing both the livability and viability?!

Many thanks again for your kind & dedicated attention to the needs of our community.
Best regards,

Alan & Robin Waite

-----Original Message-----

From: Robert Dmohowski <RDmohowski@oceansideca.org>
Sent: Monday, 17 April, 2023 3:40 PM
To: 'Jim Waite' <battcar@sbcglobal.net>
Subject: RE: Eddie Jones Project

Hi Alan,

You're welcome and I appreciate the acknowledgment. I'm glad I was able to provide answers to the questions and help guide everyone through the development process. Mechanical engineering sounds much more challenging than planning, but I do appreciate that you can relate. I have added your name to the interested parties list for future notices regarding the release of the draft EIR and public hearing notices. Please feel free to reach out if you have any questions.

Regards,

Robert Dmohowski, AICP

Principal Planner
City of Oceanside
Planning Division
Development Services Department
760.435.3563
rdmohowski@oceansideca.org

-----Original Message-----

From: Jim Waite <battcar@sbcglobal.net>
Sent: Monday, April 17, 2023 1:38 PM
To: Robert Dmohowski <RDmohowski@oceansideca.org>
Subject: Eddie Jones Project

Warning: External Source

Hi Rob,

First I want to thank you for your participation & taking massive amounts of your time to answer questions submitted thru Dee & Dave Keck's grassroots group (now known as OSO). As a retired mechanical engineer I truly appreciate the volume of technical information you wade through on a daily basis & having to find time to address these questions & the impact it has on your already busy schedule: MANY thanks again!

Second, could you please add my name / email to your "interested parties list" for the Eddie Jones Project.

Best regards,
Alan Waite
Battcar@sbcglobal.net

Robert Dmohowski

From: Tom Yatteau <tyatteau@gmail.com>
Sent: Friday, June 16, 2023 10:53 AM
To: Robert Dmohowski; City Manager; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss
Cc: Julie Matye
Subject: Concerns and Questions re: the Proposed Eddy Jones Development

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear All,

We are writing to each of you to express our profound concerns regarding the proposed development known as the Eddy Jones Warehouse, Manufacturing, and Distribution Facility.

My wife and I moved into this community in November of 2020. My wife was 6-months pregnant when we bought our home, and we had been looking for somewhere to lay down our roots as we started our family. What attracted us to the Airport neighborhood of Oceanside was how quiet, peaceful, and family friendly it was, while offering quick and easy access to the burgeoning downtown, with its beaches, restaurants, and shops. The neighborhood offered both comfort and excitement, and it was easy for us to imagine how Oceanside was going to continue to grow and develop as we raised our family.

In the two and a half years since moving here, our dreams have come true. Our home is our oasis, and we continue to enjoy and explore all of the great amenities that Oceanside has to offer. The only scare we've had since moving here happened last summer. On a regular Sunday afternoon, we smelled smoke, and as soon as we stepped out of our house, we could see that the fire was close by. Within an hour, fire engines were driving up the hill to protect the neighborhood, and not long after, police officers began going door-to-door to tell people they needed to leave, while helping those in need. We packed what we could into our car and drove to my wife's parent's house in Murrieta. The rest of the day seemed to move in slow motion, and we weren't sure if we would ever get to go back to the home and life that we had worked so hard to build. Thanks to the quick response from the firefighters and police officers, our home and neighborhood were saved.

Since then, life returned to its normal rhythms, and we settled back into our quiet and peaceful life. This changed once we learned about the Eddy Jones Warehouse. It's hard to imagine a proposed development that is more out of character with what the Airport neighborhood is, and what we thought Oceanside aspired to be. These concerns have only grown since we've learned more about the proposed developers, and the questionable decisions you as a group have already made, which threatens to turn our dream neighborhood into a noisy, clogged industrial nightmare.

The City of Oceanside's mission is to enhance the quality of life through outstanding service to its diverse community. The City's vision is to provide a safe, culturally diverse community that empowers its citizens to provide an environment that promotes economic development, supports quality education, fosters the cultural arts, and preserves its natural resources. It's hard to imagine a proposed development that is more out of step

with that mission and vision.

Why did the City approve two Conditional Use Permits that allow this development to go from a 150,000 square foot facility with 18 semi-truck terminals, into a 567,000 square foot facility with 114 semi-truck terminals, 60 truck trailer parking stalls, all operating 24-hours a day? How does that fit into the mission and vision for Oceanside? Is the City planning on turning the Airport neighborhood into an industrial area? If that is the plan, how does that enhance the quality of life for our family, our neighbors, and the surrounding residents, the people you represent?

This proposed development would negatively impact every aspect of our lives, and the lives of our neighbors. There are only two ways of entry and egress into our neighborhood. With the Ocean Kamp project already under construction on the east side of the neighborhood, the Eddy Jones Warehouse would add another choke point on the west side. Is there a safety plan for evacuations when the next fires arise? Will the first responders be able to react in time to protect the neighborhood? Likewise, if our family has to evacuate again, will we be able to do so safely with the increased traffic and congestion? Surely, you know that this will create more chaos in those situations.

Beyond that, there are the obvious environmental impacts. A facility of that size operating 24-hours a day will produce air pollution and noise pollution. Gone are the quiet Sundays in the backyard, only to be replaced by the constant racket of semi-trucks and endless traffic. Where will the money be coming from to fix all the potholes and catastrophes this will create?

Financially, what will happen to the property values of the homes in the neighborhood? Overnight, this neighborhood would become less desirable. We worked hard and saved for years to buy our first home. Our investment is now threatened by developers who do not live here, so that our loss can become their financial gain.

Beyond the obvious negative effects this would have on our neighborhood, this proposed development would negatively impact the City as a whole. Have you notified the forward thinking businesses, both small and large, such as the Sea Bird, Mission, and Cohn Group restaurants that bring in tourists that help the economy thrive? What about the effect this development would have on small businesses? How will their employees get to work? How will any of us get around the community, to go to school, to the market, grab a coffee, go to the beach, or enjoy the bike trails? Who will want to voluntarily come to a clogged up industrial neighborhood to relax, or to have family and friends over? How will the homes that border the proposed development site sleep with a huge trucking facility operating 24-hours a day?

As an attorney and a Social Worker for the County of San Diego, we know that the answers to these questions will have a long-term impact on people's lives. Our concern, and our neighbor's concern, is that the City and developer appear to be working together while ignoring the foreseeable and substantial negative effects this would have on the citizens in our community. How will you take the resident's concerns into consideration? Have the first responders been informed of this development? How will they have access to our community in emergency situations? Beyond that, has the Coastal Commission been informed of this proposed development?

For the elected officials receiving this letter, why would you ever consider prioritizing the financial gain of developers that don't live in Oceanside, while risking the health, wellness, and safety of the citizens you are supposed to represent? Our concerns are not hyperbolic or unknown. They are foreseeable and they are a certainty. A government's baseline duty is to protect the health and safety of its citizens. It's hard to imagine a clearer abdication of your duties and responsibilities to your community. This proposed development simply does not fit with the City's mission and vision, and it is not in line with the way the residents of Oceanside desire to live. It's time to thrive just like our neighbors North in Orange County and South from Carlsbad to La Jolla.

Your constituents are watching you closely, and we expect you to represent the interests of Oceanside, not some developers who would never build this warehouse where they live.

Sincerely,

Tom Yatteau and Juliana Matye

Robert Dmohowski

From: Leah Conroy <lconroy68@gmail.com>
Sent: Monday, June 19, 2023 7:18 PM
To: Robert Dmohowski
Cc: OceansideSpeaksOut@gmail.com
Subject: STOP the Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear Representative;

Now that the land is clear, it seems appropriate to address the future of this expansive space in the planning stages for a massive warehouse near Oceanside's municipal airport. The location, size and business schedule of this type of development does not add to our beautiful city's character nor, will it benefit Oceanside's local and visiting population. This busy corridor of HWY 76, between the 5 and College Blvd is highly residential and ecologically vulnerable, as it includes the SLR watershed.

Oceanside made mistakes in the past such as, permitting the shopping center (Kohls), to build just feet above beautiful, (once sacred to native people), El Salto Waterfall in the Buena Vista Creek. What a disaster, never to be corrected or improved. Tents and toilet paper litter the small space between the building and the falls, now. The short trail system (off Hacienda) east of this shopping center offers little to no compensation for destroying a lovely, natural resource.

Right now, we have time to stop this ridiculously huge warehouse project (Eddie Jones), before we lose sight of better options. Please consider an eco-tourism type of plan, similar to Ocean Kamp, for the entire, SLR riverbed region. This huge Eddie Jones warehouse would ruin any future for a potentially lucrative business with a more progressive, healthful theme. The EJ project belongs far away from residential areas and even farther from our city's shoreline asset. Oceanside deserves a better development plan, for this highly visible, naturally elegant, location.

I hope the owner of the property and the City of Oceanside, consider a more forward-thinking approach to the San Luis Rey riverbed district that would benefit our community's longterm health. Let's reduce pollution, reduce traffic, improve our neighborhoods, reach out to visitors with fun outdoor options, by encouraging smart, clean growth.

Thank you, for your commitment to making Oceanside a healthful, fun place to reside and visit.

Regards,

Leah Conroy

3445 Ashwood Ct
Oceanside, CA 92058

Robert Dmohowski

From: Leah Conroy <lconroy68@gmail.com>
Sent: Thursday, June 29, 2023 7:13 PM
To: Esther Sanchez; Ryan Keim; Peter Weiss; Eric Joyce; Rick Robinson; Planning-Planning Commission; Robert Dmohowski; outreach@eddiejonesproject.com
Cc: Dee Keck
Subject: EJones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear City Representative and EJProject Managers;

Now that the land is clear, it seems appropriate to address the future of this expansive space in the planning stages for a massive warehouse near Oceanside's municipal airport. The location, size and business schedule of this type of development does not add to our beautiful city's character nor, will it benefit Oceanside's local and visiting population. This busy corridor of HWY 76, between the 5 and College Blvd is highly residential and ecologically vulnerable, as it includes the SLR watershed.

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Right now, we have time to stop this ridiculously huge warehouse project, before we lose sight of better options. Please consider an eco-tourism type of plan, similar to Ocean Kamp, for the entire, SLR riverbed region. This huge warehouse would ruin any future for a potentially lucrative business with a more progressive, healthful theme. The EJ project belongs far away from residential areas and even farther from our city's shoreline asset. Oceanside deserves a better development plan, for this highly visible, naturally elegant, location.

I hope the owner of the property and the City of Oceanside, consider a more forward-thinking approach to the San Luis Rey riverbed district that would benefit our community's longterm health. Let's reduce pollution, reduce traffic, improve our neighborhoods, reach out to visitors with fun outdoor options, by encouraging smart, clean growth.

Thank you, for your commitment to making Oceanside a healthful, fun place to reside and visit.

Regards,

Leah Conroy

3445 Ashwood Ct Oceanside, CA 92058

June 29, 2023

Robert Dmohowski

From: Linda Middleton <lindaincarlsbad@yahoo.com>
Sent: Saturday, June 24, 2023 8:59 AM
To: Planning Web; Robert Dmohowski
Subject: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

I recently viewed the CBS news report on the proposed Eddie Jones Warehouse and I was disappointed by the untruths shared by the developer's spokesperson.

I understand her job is to say whatever is necessary to get this development approved but, she chooses to share the same mistruths every time she speaks. I am sure you are aware of what she is sharing with the public to support her multimillion dollar project but here are just a few of her misstatements.

1. She stated the industrial area was there before us - this is true and I would be fine with something similar to the Deutsch plant that was there when I moved here 20 years ago. The footprint was much smaller than what this warehouse would be and it entailed employees only arriving in cars in the morning and leaving in the evening. The noise, traffic and pollution were not similar to what this warehouse will allow. As for her argument, we can use the same one - we bought our homes before this massive warehouse was proposed and many might have chosen to move elsewhere if it had been there.
2. She continues to state the one concession they will make us is a right turn lane from Benet. That already exists so that will cost them nothing.
3. She states there will only be 9 - 13 trucks entering at the peak times. How does she know this since she doesn't know who the tenet will be. And what are the peak times she is referring to - is this during a 10 minute window of time? She says it's from a traffic study - that may be true currently since there is no warehouse there and never has been. This is a completely made up number to support her claim that we should not be concerned about traffic.

She continues to support the development with false claims and she will be long gone when the warehouse is there and we are stuck with the noise and traffic.

Please listen to the residents in this area who will be negatively affected by this development.

Respectfully,

Linda Middleton
601 Tukmal Dr
Oceanside

Robert Dmohowski

From: M Bash <Prtymrty@hotmail.com>
Sent: Saturday, June 24, 2023 9:25 AM
To: Robert Dmohowski
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hi Rob,

Will you please add me as an interested party. I want to be aware of the mega warehouse purposal.

I object to its size and pollution it will create.

Thank you,

Marty Basham

Sent from my T-Mobile 5G Device
Get [Outlook for Android](#)

Robert Dmohowski

From: Gretchen Gary <gretchengary@gmail.com>
Sent: Wednesday, August 23, 2023 10:35 AM
To: Robert Dmohowski
Cc: John Bucalo; City Manager; City Clerk; Eric Joyce
Subject: please reject CUP for Eddie Jones Project

Warning: External Source

Good Morning Rob D and others,

Following up on my last email with the subject Conditional Use Permit, I wanted to help illuminate why I am asking that you DO NOT approve the conditional use permit as requested by the developers of the Eddie Jones Project.

This video was taken in October 2022 when the Oceanside airport was conducting repair work on the runway. There was a single truck on the runway that was working out there with a single backup alarm. Please watch the video with sound on. Volume does not need to be high to understand how easily noise travels across the river. And this is on the runway, which is further away from our house than the Eddie Jones land.

Approving the conditional use permit so the Eddie Jones project can accommodate 50+ big rigs and all the required supporting forklifts will be detrimental to the welfare of all those living in the surrounding community, including hundreds of adults and children in the Wanis View community as well as the retreat visitors and residents of the nearby Abbey which also overlooks this project. If just one backup alarm is this intrusive to our environment, imagine what dozens of backup alarms will sound like on any given day or night. There will be no peace in our neighborhood - only constant irksome noise, possibly round the clock.

https://www.youtube.com/shorts/N_m6xHwgoDA

Please do not approve something that would have such a profound impact on the quality of life for my family and other nearby residents.

Thanks in advance for your consideration.

Regards,
Gretchen Gary
471 Tishmal Court

Robert Dmohowski

From: Jonathan Borrego
Sent: Sunday, June 25, 2023 7:40 PM
To: Robert Dmohowski; Darlene Nicandro; Sergio Madera
Subject: Fwd: Eddie Jones "project"

Categories: Red Category

Just an FYI

Sent from my iPhone

Begin forwarded message:

From: Jamie Hesseling <jhesseling@icloud.com>
Date: June 25, 2023 at 1:40:28 PM PDT
To: City Council <Council@oceansideca.org>
Subject: Eddie Jones "project"

Warning: External Source

To the Oceanside Chamber, City Council and all politicians that might be considering allowing this, think twice. Think about us. If the San Louis Rey natural waterway and it's inhabitants aren't a priority to protect, or the health of our neighbors isn't important to you , hopefully the communities best interest will be a priority over \$. No one needs more traffic and exhaust. NO ONE! The traffic is already a cluster mess! As a born and raised North County resident I'm fed up with the \$\$\$ grabs disregarding those who pay taxes and your paychecks. We will find out how you vote and plaster it so all know know who is a sell out/trader and we will do what we can as a community to see you loose your position and ability to ruin our quality of life going forward will see who isn't working for \$\$\$.

Sent from my iPhone

Robert Dmohowski

From: Ann Marshall <willann543@gmail.com>
Sent: Tuesday, June 27, 2023 10:05 AM
To: Robert Dmohowski; City Manager; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss; Planning-Planning Commission
Subject: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

We have lived in Oceanside for 22 years. The design of a city should include attractive public spaces that create a sense of community amongst locals. We are a community of skateboarders, bicyclists, joggers. We are active and enjoy the clean air. This is what draws tourists. Of course, they are drawn to the weather and the beaches, but more so, they are drawn to the community feel. When you get off the plane to Hawaii, you are welcomed by friendly greeters who love their island. We are drawn to their smiling faces. As tourists in Hawaii, we want what makes them smile. Structures influence this feeling and the psychological sense of community.

I believe this project as proposed is too big and will have too big of a negative impact on our north Oceanside community. Noise and air pollution created by semi-trucks running 24/7 exiting/entering the I-5 onto 76 Hwy. Ocean Kamp will be another tourist attraction requiring more car access. Driving alongside a semi-truck is intimidating, not a good community feel. This proposed warehouse, being the size of 3-4 Costco stores, and is simply the wrong business for this location. In particular, it is near a much-used bike path, a children's skateboard park, and thousands of residential homes. If residents are content, tourists will have a better experience and will likely return. Please keep the beachy community feel intact. Trucks and forklifts and traffic and pollution will not enhance our quality of life. If Oceanside residents are smiling, this will attract tourists the most.

Sincerely,
Willie and Ann Marshall

Robert Dmohowski

From: Ken Mccallick <ken.mccallick76@gmail.com>
Sent: Thursday, July 6, 2023 12:44 PM
To: Robert Dmohowski
Cc: Dee Keck; Alyce Budde; Karen Mccallick
Subject: Eddie Jones proposal

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hello sir.

I'm Ken McCallick, member of OSO, resident and citizen, Our family has owned our home in Oceanside for 35 years.

I am upset that the developer has set a meeting in a room that holds only 67 people.

I responded immediately to the developer's invitation. Yet I'm told today that there's no room for me.

If 1200 invitations were sent out, there should have been a bigger room set aside. In my opinion, this is an attempt by the developer to limit attendance at the physical meeting.

I request that the meeting be postponed or a bigger room should be made available.

I've written you before citing that our household (3) is against this project as it is proposed.

It's a huge expansion of the approved zoning, as I understand it, from 18 to 100 trucks.

This project does nothing to improve our home values, solve housing issues, beautify the area, make it safer or more sustainable.

The Teamsters don't think it will provide good jobs, but no one knows, since they don't have a tenant.

I disagree with the developer that it is safe to leave trucks filled with diesel sitting in place in the parking lot during a fire. (The developer made this comment in an interview on Channel 8 recently)

I don't trust traffic figures from the developer. Since they have no tenant, how could they realistically estimate traffic at 76 & Benet?

As I understand the city's long term plan, this type of industrial development, with lots of heavy vehicles, pollution, traffic congestion, doesn't fit. The zoning was approved long before the neighborhoods were here. The environment is different now.

I have proposed a project that would help establish the airport area as an eco/ adventure tourism area. Assuming OceanKamp gets built, you have an airport, a hi-caliber skate park, skydiving, biking trails, and plenty of entertainment nearby.

I propose an Air-Stream Hotel with biking and walking trails connecting to the river-trail, or something similar, yurts, glamping tents. This would attract hi-dollar tourists to the area and create jobs.

I ask you and your staff to consider my recommendation or something that fits into tourism and recreation. We could make the airport corridor a world-wide tourist attraction. A bunch of dirty diesel trucks doesnt help anybody but the developer, and he doesn't live in Oceanside. My idea is healthier, safer, and more profitable for Oceanside.

Thanks for your support

Ernesto Kenneth McCallick
3425 Spindrift Ct.
760-622-9524

Robert Dmohowski

From: MONICA GARCIA <mgarcia157@cox.net>
Sent: Thursday, July 13, 2023 7:07 PM
To: Robert Dmohowski
Subject: Eddy Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Oceanside, CA

July 13, 2023

Dear Rob D. Mohowski,

Oceanside City Principal Planner

My name is Monica Garcia. I am writing to you to ask for your help. I attended the meeting for the Eddy Jones Warehouse Project, and I cannot tell you how hopeless I feel! This project is huge, and the developers are set on the path to building this site without any regard to how terribly it will affect us (the residents of this area) as well as all those who travel on the 76.

They have stated over and over that they are working within the standards of our beautiful Oceanside city and for this reason I am contacting you. You (our representatives, the ones who look out for us) are the only ones who can help us. As a resident of this area, I cannot express enough concern how terrible it would be to have the only TWO access points to our residential community clogged with traffic congestion at each access. One with Ocean Kamp and the employees from the Eddy Jones project and the other with this Eddy Jones semi-trucks. Image your home having two exits and having hundreds of visitors blocking these exits. How will you manage to keep your family safe in case of emergency? How will you manage coming and going to work every day? How will you keep your family safe and healthy (both physically and mentally)?

The impact of this project will affect noise congestion, light congestion, traffic congestion, destruction of the natural environment, our way of life! Sleep patterns will be disrupted, commute times will increase (not only for us residents but also for anyone traveling 76 to and from I-5).

After yesterday's meeting, it is obvious that the developer is not interested in in how we will have to live with all of these drastic changes because they are only looking at their profits (with little or no benefit to our city).

I am begging you to please stop this massive project! I feel helpless in making any impact with the developer. I can only hope that YOU, our citizen's voice in this matter, have the courage to see our needs and support our efforts to keep our beautiful community a great place to live. This destructive project does so little for Oceanside and our community and yet, so harmful to us, the citizens. Please, please don't let them destroy our way of life!

Sincerely,

Monica Garcia

Robert Dmohowski

From: Joanne Pilgrim <joanne.pilgrim2425@gmail.com>
Sent: Saturday, July 15, 2023 12:03 PM
To: Robert Dmohowski
Cc: CityManager@oeansideca; Esther Sanchez; Eric Joyce; Ryan Keim; Rick Robinson; Peter Weiss; Planning-Planning Commission
Subject: Proposed Eddie Jones Project
Attachments: Letter re Eddie Jones Project.7.13.2023docx.docx
Categories: Red Category

Warning: External Source

City Planners and Representatives:

Please see attached letter. A signed copy has been mailed to each of you. Neither I, not any of the other homeowners involved, understand how RPG (Adam Robinson) got around the Municipal Code zoning regulations with their Conditional Use Permit. Please note: This property does not comply with the regulations as detailed more specifically in my letter.

Additionally, I believe the City should pay close attention to the location of the community involved, i.e., its proximity to Camp Pendleton and acres of open brush. Many of our properties almost back up to the base property. This makes all of the homeowners who live here particularly vulnerable to fires which happen often because of the dryness and extensive danger related to Marines practicing various forms of gunfire. In fact, insurance companies are canceling homeowners' policies in our community because of this forcing homeowners to acquire new policies where they can.

As such, none of us believe any statistical studies of traffic by the City or RPG's attorneys can overcome this potential danger as outlined by RPG (Adam Robinson) on Wednesday night. We will be trapped with the blocked ingress and egress access problems already noted resulting in probable tragic loss of life and property.

Under the circumstances, please work with the community to find an appropriate use of the property which is not a warehouse with all its accompanying trucks, noise and access problems.

Joanne Pilgrim

JOANNE PILGRIM
641 Tukmal Drive
Oceanside, CA 92058
Cell: (760) 712-6221

July 13, 2023

Rob Dmohowski
City of Oceanside Principal Planner
300 North Coast Highway
Oceanside, CA 92057

Re: Proposed Eddie Jones Warehouse, Manufacturing
And Distribution Facility

Dear Mr. Dmohowski:

I attended the Eddie Jones Public Meeting yesterday evening and, like everyone else in the room, am concerned at the direction this project seems to be taking. As you may recall, I have corresponded with you on two former occasions; i.e., November 21, 2022 and January 6, 2023.

The purpose of this letter is to request that you reconsider the legal ramifications of the property, which is currently zoned as "Light Industrial." I do not believe the project complies with the legal guidelines the Oceanside Code requires and needs further review by outside attorneys for the following reasons:

1. Article 12, Section 1200 of the Code defines "Light Industrial Zone" as one that will "allow a wide diversity of industrial uses . . . where such uses will not have an adverse effect upon adjacent residential areas . . .". The proposed project will definitively have an adverse effect upon all the adjacent residential areas north and east of the site, as well as along Highway 76 and Interstate 5, respectively, for reasons of: (1) traffic congestion; (2) safety concerns; (3) noise and light concerns; and (4) blocked egress and ingress access to our homes and Highway 76. Among other things, with the Ocean Kamp in progress off Foussat Road access will be completely unavailable to any additional traffic, especially large trucks, when completed. This only leaves Benet Road as an outlet for the warehouse vehicles and ours, which, obviously, is insufficient.
2. Article 12, Section 1201(b) of the Code mandates that all "(M)anufacturing areas shall be located with primary access to a major or secondary street . . .". There is no primary access to a major or secondary street as I believe you are aware, and the proposed side street by the facility does not solve the problem since their vehicles will hold up any traffic behind them.
 - a. At yesterday's meeting several of the audience participants asked why there were so many proposed bays (57)? Adam Robinson's response was that "flexibility" was needed for future planning purposes. Audience participants challenged this statement based on the fact the high number of proposed bays would not be included unless they were going to be used. Moreover, Mr. Robinson stated the main time frame for the vehicles would be at 9:00 a.m. and 4:00 p.m., respectively, which are main time frames for most families. As suspected, residents will be blocked from access to Highway 76 and their homes at the most important times of the day.

3. Finally, Article 12, Section 1215(5) of the Code notes that the “industrial activities” shall not “cause damage or nuisance to the health, safety, peace or general welfare of persons residing or working in the vicinity of the industrial park.” [Emphasis added.]. This section is especially noteworthy since it covers major concerns shared by those residing near projects such as the Eddie Jones in order to protect the residents. Emergency vehicles including fire trucks, ambulances, police and military personnel called to duty, will be unable to gain access to the communities of more than 1,200 homes with the number and size of road-blocking vehicles coming from the proposed facility. Such will undoubtedly result in loss of lives as well as problems with military blockages because of the time delays. Not only will any life-saving vehicles be impeded from residential area access, but will, also, be subject to major delays up and down Highway 76 and along Interstate 5 because of the increased congestion. Does the City of Oceanside want to be responsible for people dying because the emergency vehicles couldn’t get to their homes in time? What happens if we are surrounded by a fire here? This is, unfortunately, very possible with the high temperatures we are facing, not to mention the fact there was a fire here a year ago in which we were blocked from getting out. I should also mention that with Camp Pendleton so near to us, a lot of their personnel reside in our communities and will be blocked from getting out in cases of emergency. How will the City respond in cases of military emergencies where personnel cannot get to their assigned destinations in time to save lives?
 - a. This summary doesn’t even include the fact our property land values will undoubtedly diminish with all the noise, congestion, and lights, not to mention the fact that the peace and enjoyment to which we, as homeowners, are entitled to legally will be extinguished.

For the foregoing reasons, we request that you cancel the proposed warehouse project and any other proposed similar projects, that would have the same adverse effects upon the Oceanside community as the Eddie Jones. Such action may help to avoid costly lawsuits that will undoubtedly result if this project proceeds along the lines the developer envisions. Mr. Robinson’s explanation that the project will bring in 500 additional jobs to Oceanside only compounds the existing problems noted, especially safety and congestion. We, therefore, ask that you consider the consequences of further construction in this area to meet the needs of those residing in the Oceanside area.

Sincerely,

Joanne Pilgrim
/jp

Cc: Oceanside City Clerk
Esther Sanchez, Mayor
Jonathan Borrego, City Manager
Eric Joyce, Councilmember
Rick Robinson, Councilmember
Ryan Keim, Councilmember
Peter Weiss, Councilmember

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Saturday, July 22, 2023 4:19 PM
To: Robert Dmohowski
Cc: Planning Web; Esther Sanchez; Ryan Keim; Rick Robinson; Eric Joyce; Peter Weiss; City Manager; oceansidespeaksout@gmail.com; eddiejoneswarehouse@gmail.com
Subject: Eddie Jones Warehouse and Distribution Center

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

SUBJECT: Traffic

Mr. Dmohowski, planning, city council,

I attended the most recent meeting, July 12, with the developers on this project.

And yes, we've all been informed they've cut the truck docks down to 57 (which is still more than 6 (or the 18 allowed if it was considered three parcels) and still more than Benet and 76 can or should handle. With that, there will still be multiple conflicts with all the trucks using Benet Road and the 1,000+ homes that have to use it (with only one other access road across the river, Foussat) on a daily basis.

As the developer kept stating, traffic issues will be "the tenant", "the tenant", "the tenant" !

And the Benet/76 intersection (turn lanes, signals, road widening, etc.) is out of their control and that of Caltrans...so live with what's there?

Additionally, the traffic report individual was there and gave information about his report, Later, upon request from the developer, asking where the input came from. The developer answered via email, it is based on studies and statistics "from the city".

Statistics such as trucks are loaded or unloaded in 2 to 6 hours, and that up to 13 trucks per hour in the early morning peak traffic and 9 trucks per hour in the afternoon peak will be utilizing Benet and the 76 expressway(76 is not an expressway).

Since the city knows where and when this statistical information comes from, I'd like to ask how those are/were determined and what year(s) these were formulated. With present modern technology and automated systems of all types, are these studies still realistic? Are those statistics based on worst case tenant, or just averages?

From reading various articles, the truck loading today is typically 1 to 2 hours with up to 6 a far and away exception. A load that is palletized and with an experienced forklift operator might be loaded in less than 1 hour.

So depending on "the tenant", the technology and the efficiency of the warehouse, this could place trucks on Benet and 76 on a totally constant basis.

Additionally, reviewing studies on large trucks, the 76 will become more of a nightmare than it already is. Yes, there are times that 76 functions, but more often than not you can sit through some lights 3 to 4 times. And getting behind a loaded semi, or semis, is never good.

From basic studies of types of semis, from small, to medium, to big engines, how far does it take to get to 40 miles per hour? Near 1100 ft, near 800 ft and near 600 ft, respectively. This will have a major impact on traffic flow, not only the 76, but don't forget the interstates, especially I-5. To get to 55 miles per hour, it's 2,700 ft, 2200 ft, and 1930 ft, per engine size, and looking at the I-5 north and south entrances off 76. Of course, I-5 south is already a parking lot during the day.

With that, I'd also like to mention that the distance from Eddie Jones Way (the site truck exit) there's only about 800 feet to 76, and the left turn lane on 76 to Benet is around 1,000 feet. California allows trucks to be up to 75 feet maximum long. Several of those on 76, Benet or the turn lanes, and with their limited acceleration, will not be good for local traffic.

So with limited access for the 1,000+ homes across the river, the poor truck acceleration, turn signals and lanes being limited by Caltrans, possibly several trucks taking up much of the road/turn lanes, and trucks possible continuously on Benet/76/Interstates, this project is not a good fit.

Thanks for your time. A response to the initial questions would be appreciated.

Rick & Sue Taylor
735 Valley Crest Drive
Oceanside, CA 92058
760 274 4700

PS: Don't forget this project is over 566,000 square feet of structure that's primarily 45 feet tall, nearly 26 acres of asphalt and concrete and has to have a flood wall 7 to 9 feet tall (per the developer) around its perimeter, and its CUP request is far beyond reasonable limits.

Robert Dmohowski

From: Haley Diaz <haleydiaz3@gmail.com>
Sent: Saturday, July 29, 2023 10:32 AM
To: Robert Dmohowski
Subject: Oceanside Resident - Against Eddie Jones

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Good Morning,

My name is Haley Diaz and I am an Oceanside resident living in District 1 near the proposed Eddie Jones site. I am extremely against this project. While the proposed plans for the site are set to bring in millions for the owners of the property, there is nearly no benefit to Oceanside residents and/or the city as Adam Robinson and RPG are only estimating about \$70-\$80k going to the city annually. Meanwhile, the air pollution and environmental impacts of having a manufacturing and distribution site with 50+ semi-stalls will solely be paid for by the residents in the near by community.

Furthermore, placing the site at the opening of the valley/ocean connector is geographically going to likely cause the smog and pollutants to blow into and sit within the valley, which already houses a good majority of the financially disadvantaged residents within Oceanside.

The risks of polluting the San Luis Rey River — and thus ocean — are great.

RPG was quick to pat themselves on the back during the community meeting for the contaminated sole cleanup they have already done, yet fail to provide any of the reports listing what contamination was found. In addition, there appeared to be no consideration of avoiding an airborne toxic event by mitigating contamination plumes during their building teardown and soil “cleanup”. Driving past the the site during these phases soil could be seen drastically kicked up into the air, which then releases the contaminants into the surrounding community.

The latest community meeting that RPG held was less than convincing, nor did they seem to understand the likelihood that their new warehouse is likely to cause significant environmental impact. When community members discussed other business options that would benefit residents, the response was that they need high-paying “life science” rent in order to make the types of profits they are looking for. They state there are no real areas of land along the coast that can host a similar project and mentioned areas like the Imperial Valley and south of San Diego. I do not want the air pollution of either of these places and if that’s what we should all expect and accept is coming with these warehouses is alarming.

I am not against companies making a profit, but when there are no real positives and only consequences for the community it no longer makes sense for Oceanside.

I am hopeful that you are against this project and would like to know where you stand.

Thank you,
Haley Diaz

Robert Dmohowski

From: Kim Whittemore <kimwhit@gmail.com>
Sent: Saturday, July 29, 2023 6:11 PM
To: Robert Dmohowski
Cc: Eric Joyce; Mission Cove Seniors; Eric Ligare; Esther Sanchez
Subject: Re: Eddie Jones Warehouse Notification Requirements

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Robert,

After leaving a lengthy voicemail message, I checked all the relevant sites and of course, found what you have stated herein. I do want to strongly suggest to the Mayor, City Council, Planners, AND, in this case, [Eddie Jones](#) / Lightfoot that:

- Hundreds of low-income and senior residents have moved into what was predominantly an industrial area and should also be notified even if not required per se. That includes [Mission Cove](#), [La Misión](#), [Community Housing Works / Mission Cove Seniors](#), etc. all within .8 miles from the site by foot (walking).
- We aren't buyers who were informed ahead of time. In fact, we are all renters. We "fought" for highly competitive, low-income housing. It isn't as if the residents have choices about living here. Our mere presence COMPLETELY changes the dynamic of the area. We WILL be affected in every way imaginable.
- Prince of Peace Abbey had yet to be notified as of the last meeting and should also have a voice/presence, IMO.

I appreciate your consideration and your thoughtful and informative reply to my voicemail.

Kim Whittemore
781-258-1116
Mission Cove Seniors

On Fri, Jul 28, 2023 at 9:48 AM Robert Dmohowski <RDmohowski@oceansideca.org> wrote:

Hi Kim,

Thanks for reaching out about the Eddie Jones industrial project and the notification requirements. The City Council adopted an Enhanced Notification Program as a policy to increase public awareness of and promote public participation in proposed development projects within the City. Key provisions include:

- Notice of Application mailed to property owners within a 1,500-foot radius of a project site and occupants/tenants within 100 feet of the site
- A 24' x 36' yellow project notification sign posted on the project site that is visible from the street (two signs are required for larger sites)
- At least 15 days prior to the hearing, a public hearing notice and notice of Environmental Impact Reports mailed to property owners within a 1,500-foot radius of a project site and occupants/tenants within 100 feet of the site
- Per state law, notice of public hearing advertised in newspaper
- A community outreach plan is also required and the applicant must conduct outreach before the project can be scheduled for public hearing.

The City's notification policy goes beyond the minimum notification requirements per state law. The legal requirement is to notify surrounding property owners within 300 feet of a project site. I'm happy to add your email address to the project's interested parties list so that you will receive notification of the upcoming release of the Environmental Impact Report and the Planning Commission hearing date.

Please let me know if you have any additional questions about the notification requirements.

Regards

Robert Dmohowski, AICP

Principal Planner

City of Oceanside

Planning Division

Development Services Department

760.435.3563

rdmohowski@oceansideca.org

Robert Dmohowski

From: Anthony Vincent <anthony.vincent@cox.net>
Sent: Monday, July 31, 2023 8:42 AM
To: Robert Dmohowski
Cc: oceansidespeaksout@gmail.com
Subject: Concerning the Eddy Jones Warehouse Manufacturing and Distribution Facility

Warning: External Source

Dear City Planner Mr. Rob Dmohowski,

Oceanside has been my home since 2005 and I am blessed to have owned a house in the airport community for the last six years. As an interested person, I am following the discussions, planning and future development of the area closely.

The prospect of a humongous warehouse being planted into our neighborhood is quite worrisome. The Ocean Kamp project alone will, as projected, add 18K – 20K cars a day to our neighborhood. Semi-trucks and hundreds of workers frequenting a warehouse herby clogging up our limited space and access roads even more is the last thing we need in our community. Concerns regarding noise and air pollution as well as safety issues have also been raised numerous times.

During the pandemic it became clear we lack public recreational space in our airport community. Without a baseball, football or soccer field to speak of children had to play on the street. The skate park really is the only available option and it is often overcrowded. To me a sign more needs to be done here. We all understand that something will be built in the proposed area, why can't it be a combination of housing, recreational space and perhaps light industrial buildings instead of an insanely large warehouse?

Please keep "Quality of Life" in mind during the planning and development phase and oppose the proposed Eddy Jones Warehouse project. Don't let the will of the people go unheard!

Thank you.

Respectfully,
Anthony Vincent

663 Wala Dr.
Oceanside, CA 92058
(760) 529-9140
anthony.vincent@cox.net

Sent from [Mail](#) for Windows

Robert Dmohowski

From: Julie Lewis <kastler.julie@gmail.com>
Sent: Monday, July 31, 2023 12:18 PM
To: Robert Dmohowski
Subject: Eddie Jones Warehouse

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Hi Rob-

Can you add me to the interested party notification list? I do not want the Eddie Jones warehouse project going up in our community. I fear it will disrupt traffic, cause air pollution and the safety to our community. Please stop this if possible. I hope we have a voice and say about the neighborhood we live in.

Robert Dmohowski

From: Dane Thompson
Sent: Friday, August 4, 2023 8:53 AM
To: Robert Dmohowski
Subject: FW: Eddy Jones Warehouse

fyi

From: int <ptsoris@aol.com>
Sent: Thursday, August 3, 2023 10:37 AM
To: Planning-Planning Commission <PlanningCommission@oceansideca.org>
Subject: Fw: Eddy Jones Warehouse

Warning: External Source

Sent: Thursday, August 3, 2023 at 10:35:41 AM PDT
Subject: Fw: Eddy Jones Warehouse

From: int <ptsoris@aol.com>
Subject: Eddy Jones Warehouse

My husband and I have lived in our present home for 14 years. We want to keep our neighborhood as much as possible residential... We are assured to have the mega truck terminal will be a hazard. We would advise you to keep the zoning and not allow a change for larger warehouse...The negatives are as follows: traffic impact, pollution, safety and emergency services...It is already a given that the Ocean Kamp will be built...That alone is enough for our neighborhood...traffic and more.

When they purchased the 31 acres...they were aware of the zoning...keep it so the truck terminal won't be a detriment to our neighborhood.. Home prices with drop if they get the mega terminal.

**Pauline Tsoris
536 Crestwood Drive**

**Oceanside,
CA 92058**

760-458-4271

Robert Dmohowski

From: Madison Lang <madisonlng@yahoo.com>
Sent: Tuesday, August 8, 2023 12:05 PM
To: Robert Dmohowski
Subject: Plans for a 76 Fire escape route

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Mr Mohowski,

This was my response to you on the Nextdoor app. I was responding to a woman named Gretchen.

“Gretchen, I agree with your statement 100%. In addition to your list I would like the City Planners to show us, via a map, the escape route if a fire happens on Hwy 76? Particularly either of the two locations on Benet or Fousat. All the trucks would try to escape. All the residents and visitors at the Ocean Kamp will pour onto the 76! Plus everyone trying to reach the 5 Freeway, which will include our entire neighborhood.

Obviously they don't care about us or they haven't thought it through. Sent from my iPhone

August 21, 2023

City Planner, City Manager & Mayor
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92054

Re; Proposed Eddie Jones Warehouse Project

Dear Oceanside Officials;

I write to you as an interested citizen who lives in proximity to the proposed Eddie Jones project and as someone who works in the real estate development field.

I am expressing my concern over, and objection to, the developer's plan to dramatically increase the scale of the project by a multiple over that which is allowable under the standing CUP.

This pursuit, if allowed by the City, will have numerous deleterious impacts to the area and risks undermining the fundamental intent of zoning laws which are set in place to promote smart development which serves not just the developer's and investor's wallets but also considers the competing aims of positively contributing to the quality of life of the citizenry.

The proposed project represents nothing short of private profiteering at the expense of the public good. If approved, this would amount to a huge financial handout to a private party, multiplying their return on investment. The out-of-town developers and investors stand to capture massive "excess profits" due to the potential upzoning, for which the City and its citizens will pay for in perpetuity by having to bear the consequences of considerable increased traffic, pollution, noise, road damage (almost all of which is caused by trucks) and other negative impacts.

Any argument that industrial warehouses are a job generator is a fallacy. The term "manufacturing" has likely been added to the naming of the project because it conveys the idea that this facility will generate jobs. This will almost certainly be a pure trucking logistics facility which will not create any considerable employment; the high quantity of car park spaces will almost certainly be majority dedicated to trucks and trailers.

I implore each of you to read the linked articles below as you consider the ill-conceived development proposal. If you're interested in the real "costs" associated with logistics facilities, you need only look toward the nation's largest concentration of such facilities, the Inland Empire:

[Inland Empire warehouse boom risks health, climate - CalMatters](#)

[California's Inland Empire Pushes Back on Booming Warehouse Construction - The New York Times \(nytimes.com\)](#)

[Southern California Warehouse Boom Poses Environmental Costs for the Inland Empire - Times of San Diego](#)

In short, the City should uphold its responsibilities to its citizens, enforce its regulations and reject this private profiteering scheme by limiting the development strictly to what is allowable within the terms of the CUP and not one inch more.

Kind regards,



Justin Sager
858-876-5999

Robert Dmohowski

From: Elena Bee <helenathebrit@gmail.com>
Sent: Tuesday, August 8, 2023 7:41 AM
To: Robert Dmohowski
Subject: Eddie Jones

Warning: External Source

Please do not to approve the conditional use permit needed for this project to advance. This will impact the welfare, personal finances and health of people residing nearby and I am one of them - I live on Shadow Tree Dr.

Thank you for the consideration of my request
Elena Baker

Robert Dmohowski

From: Rick Taylor <rkt.sce@gmail.com>
Sent: Tuesday, July 11, 2023 5:01 PM
To: Robert Dmohowski
Cc: Esther Sanchez; Ryan Keim; Peter Weiss; eddiejoneswarehouse@gmail.com; Rick Robinson; Eric Joyce; City Manager; Planning Web; oceansidespeaksout@gmail.com
Subject: EDDIE JONES WAREHOUSE, MANUFACTURING & DISTRIBUTION FACILITY

Warning: External Source

To Oceanside city officials,

I will try to be brief and to the point, but there are just so many things wrong with this project and its location.
JUST SAY NO !!

I truly hope common sense prevails in the matter of this project and there's a no on its approval. It is actually a little hard to fathom that a project of this size and type might ever be considered in this location; the monstrous warehouse, the gigantic parking areas, the flood walls, the traffic, the river and its wildlife, etc.

The CUP application is so beyond allowables it is unimaginable that just it alone hasn't killed the project. Just the basics: 566,280 square feet of building, 113 loading docks, 139 additional truck parking spots and 400 vehicle parking spaces. And a high probability of running 24/7, seven days a week.

A review of the proposed project is nearly 26 acres of concrete and asphalt. And let's not forget, they aren't going to raise the site for flooding (they're actually exporting 20,000 cubic yards of soil), but building a flood wall around the entire project. That will not be a favorable sight.

Let's also not forget the thousands of birds that utilize the trees on the north side of the river right along the site, or the hundreds of swallows that make their home on the Benet bridge. The abundant squirrels, rabbits and reptiles everywhere (& the birds of prey patrolling), and the coyotes and occasional deer seen along the riverbed and on the existing and surrounding site.

And if this facility is allowed to run 24/7, this wildlife will be negatively affected... as well as all the area residents and businesses...plus the traffic nightmare noted below.

Then there's over 1,000 homes that have only Benet and Foussat roads for access. Add in this warehouse/manufacturing/trucking facility and the Ocean Kamp project to those two roads and these residents will be left out. And note: the traffic study was a single day event, yes, a single day, without regard to possible Camp Pendleton or city events, and it should be noted certain lights and access points weren't even addressed.

Has anyone involved ever driven the 76 daily? It is ever changing. You can sit through 3 to 4 lights at Rancho del Oro, College, or Santa Fe just to name the obvious on various days, but not others, at various times, and not others. Or the back-up on I-5 north entrance, usually in the morning, when marines and civil servants head to work. Or just the traffic snarl on I-5, especially from the 76 to the 78, and vice versa, day in and day out.

Let's add hundreds of slow accelerating trucks to the mix.

And BTW, it is supposedly proposed that all the trucks use Benet Road, and all cars, Foussat Road. The distance from Eddie Jones Way (the truck entrance/exit road on Benet) to 76 is between 700 to 800 feet. Semi's can be over 70 feet long. So let's add that to the vehicle/truck traffic study. Trucks backed up and homeowners having to suffer through it all..

Let's put this project in its proper place, **JUST SAY NO !**.

Thanks for your time.

Rick Taylor PE
735 Valley Crest Drive
Oceanside CA 92058
rkt.sce@gmail.com
760 274 4700

Robert Dmohowski

From: Tam Nguyen <tn9936@gmail.com>
Sent: Saturday, August 26, 2023 11:51 PM
To: Robert Dmohowski; City Manager; Esther Sanchez; Ryan Keim; Eric Joyce; Rick Robinson; Peter Weiss
Cc: eddiejoneswarehouse@gmail.com; tuyetdnguyen; Tam Nguyen
Subject: Vote No on Eddie Jones

Warning: External Source

To Whom It May Concern,

We are writing to you to let you know that we are homeowners of Wanis View Estates HOA in Oceanside who strongly oppose the proposed Eddy Jones project. The Eddy Warehouse, Manufacturing and Distribution Facility (the Eddy Jones) is being proposed to be built on 31.79 acres between the Oceanside Municipal Airport and the residential Wanis View Estates HOA established about 20 years ago. The Eddy Jones includes **114 semi-truck terminals, 60 trailer parking stalls and 590 auto parking spaces** with a building of **566,905 square feet**.

Our ability and also the surrounding residents' to maneuver in and out the community via either Benet Rd or Foussat Rd will be adversely impacted.

Our concerns:

- 1) **Traffic Impact:** The potential number of semi-trucks coming and going 24/7, Benet Rd/76 Hwy's intersection will be a choke point for travelling from our community and the surrounding residents as well as all who use 76 Hwy and I-5 Fwy. Additionally, the traffic generated from the Ocean Kamp development that was approved will soon add traffic issues on Foussat Rd and onto 76.
- 2) **Transparency:** To date, the lack of transparency/communications is very apparent when very few of our community's and surrounding areas' residents are even aware of the intended development.
- 3) **Air Pollution:** We strongly oppose the air pollution created by multiple trips of semi-trucks, service trucks, forklifts, and employee vehicles for an operation that can run 7 days a week/24hours a day.
- 4) **Noise Pollution:** We also strongly oppose the noise pollution from the operation above mentioned.
- 5) **Pollution of the San Luis River** is also an issue to be considered.
- 6) **Safety/Emergency Services:** Safety concerns will be created by multiple trips of semi-trucks arriving and leaving the warehouse blocking access to Benet Rd and impeding cars, pedestrians, bicyclists on bicycle route and bicycle trail. If there was a fire or other emergency this would cause an extreme safety hazard for residents to evacuate and emergency vehicles to enter the area.

This development as proposed is not a good fit with our residential community, nearby neighborhoods, and the Ocean Kamp development. This project is not compatible with the abutting residential area that was established about 20 years ago. We believe this project as proposed is too big and will destroy the quality of life of our community and surrounding areas' residents.

Sincerely,

Tam Nguyen, 3211 Toopal Dr, Oceanside CA 92058, (949)933-7375

Minh Nguyen, 3211 Toopal Dr, Oceanside CA 92058, (949)677-1793

Tuyet dinh Nguyen, 3144 Toopal Dr, Oceanside CA 92058, (858)354- 9290

Robert Dmohowski

From: Madison Lang <madisonlng@yahoo.com>
Sent: Wednesday, September 27, 2023 2:10 PM
To: Robert Dmohowski
Subject: Fw: Eddie Jones Project

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

From: Madison Lang <madisonlng@yahoo.com>
Sent: Wednesday, September 27, 2023 at 02:02:06 PM PDT
Subject: Eddie Jones Project

From: Madison Lang <madisonlng@yahoo.com>
To: Rob Dmohowski
Sent: Wednesday, September 27, 2023 at 01:59:08 PM PDT
Subject: Eddie Jones Project

Madison Moorehead-Lang
834 Rivertree Drive
Oceanside California 92058
September 27, 2023

Oceanside City Principal Planner
Mr. Rob Dmohowski
Oceanside, CA

Subject: Concerns Regarding Warehouse Eddie Jones Project

Dear Mr. Dmohowski,

I am writing to let you know about my deep concerns about the proposed Warehouse Eddie Jones project near the intersection of California Highway 5 and 76 in Oceanside. This project has raised several issues that require careful consideration and action.

1. Traffic Congestion:

The potential increase in traffic due to warehouse operations could exacerbate congestion on Highways 5 and 76. This could lead to accidents and prolonged delays for daily commuters.

2. Diesel Exhaust Pollution:

The use of large diesel trucks servicing the warehouse will contribute to air pollution, adversely affecting air quality and public health.

3. Air Pollution:

Prevailing westerly winds in the area will carry polluted air inland, impacting nearby residential areas and causing health concerns. Diesel exhaust, dust from tires and brakes are known to be carcinogenic.

4. Fire Hazard:

There are concerns that warehouse operations may pose a fire hazard, potentially blocking evacuation routes during emergencies.

5. Noise Pollution:

The round-the-clock warehouse operations will result in noise disturbances for nearby residents, impacting the overall quality of life. The serenity of the two churches on the hills immediately above, known as havens for contemplation and meditation, will be destroyed.

6. Scenic Attractiveness:

The presence of a warehouse in this scenic area, adjacent to the San Luis Rey river, will detract from the city's efforts to beautify Oceanside.

7. Integration with City Plans:

It is crucial that the warehouse project aligns with the city's urban planning goals and vision for Oceanside's development.

8. Coordination with Other Warehouses:

Consideration should be given to how this warehouse project fits into the broader plan for warehouses in Southern California to minimize cumulative impacts.

9. Support from Local Representatives:

I plan to reach out to our representative in the US Congress, Mike Levin, to talk about these concerns and to ask for help to address them. Governor Newsom will also be requested to oppose this blight.

10. Involvement of Transportation Secretary:

I also believe involving Transportation Secretary Pete Buttigieg is vital to ensure that transportation-related issues are adequately considered and addressed.

Additionally, I would like to highlight another concern:

11. Lack of Railway Connection:

Despite the presence of passenger and freight railways just a mile away, the absence of a railway connection for heavy trucks carrying containers to and from the warehouse is a missed opportunity for economic efficiency and pollution reduction.

12. Hwy 76 is not a big highway only 2 lanes on each side. The Eddie Jones warehouse can send 114 semi-trucks 24/7 onto the 76 plus Ocean Kamp Wave Park 2.6 miles from the warehouse with 92 acres, 300 room hotel, 700 condos, 3.5 acre surf lagoon and a trailer park. The wave park is expecting 9,000 vehicles per day

Potential Solutions:

- Advocate for the integration of a railway connection into the warehouse project's design and operations.
- Emphasize the benefits of railway transport, such as reduced congestion, lower emissions, and cost savings.
- Collaborate with local authorities, transportation agencies, and the warehouse developer to explore the feasibility of establishing a railway connection.
- Engage with railway companies to assess their interest and potential support for such a connection.
- Raise awareness about the environmental and economic advantages of railway transport within the community and among relevant stakeholders.

I believe that addressing these concerns and exploring the possibility of a railway connection would contribute to the sustainability and efficiency of warehouse operations while mitigating some of the associated problems.

I urge you to consider these concerns seriously and take appropriate actions to ensure that the Warehouse Eddie Jones project aligns with the best interests of Oceanside's residents and the broader community.

Thank you for your attention to this matter. I look forward to a positive response and am willing to engage in further discussions to find practical solutions to these challenges.

Sincerely,

Madison Moorehead-Lang
Alexander Craig Lang PE

CC:
Mike Levin
Pete Buttigieg
Governor Newsom

Robert Dmohowski

From: nicole rayta <nicolerayta@hotmail.com>
Sent: Wednesday, October 4, 2023 6:52 AM
To: Robert Dmohowski
Subject: The Eddie Jones Warehouse/Manufacturing and Distribution Facility

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Red Category

Warning: External Source

Dear Mr. Dmohowski,

I am writing to express my opposition to the Eddie Jones Warehouse/Manufacturing and Distribution Facility. On behalf of myself and my neighbors, our mission is to preserve the quality of life in North Oceanside, California by advocating for responsible land use and development. We believe that the proposed mega warehouse, with its 24/7 operation and 114 truck bays, would create severe traffic congestion and pollution, negatively impacting our community's safety, health, and well-being. We aim to work with local officials and community members to transform the area into a recreational space/corridor that aligns with the future vibrant vision of our city, promoting outdoor activities and healthy lifestyles while preserving the natural beauty of the San Luis Rey River. Our vision is to create a sustainable, vibrant, an inclusive community where residents can live, work, and play with pride and joy. Thank you.

Nicole Rayta
Resident of Oceanside San Luis Rey

Sent from my iPhone

Robert Dmohowski

From: Shauna Fanella <shaunafanella99@gmail.com>
Sent: Monday, November 27, 2023 12:42 PM
To: Robert Dmohowski
Subject: I vote NO/ Current Oceanside Resident not in favor of Eddy Jones warehouse plan.

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Hello Robert Dmohowski, I am contacting you regarding the proposed plan of the Eddy Jones Warehouse. I am a resident of Oceanside for 24 years now and I do not agree with this plan. I live on mission cove way in oceanside California and currently take the 76 freeway in order to get to work and back. I believe that this warehouse will negatively impact the traffic for current residence of the area as well as air quality.

Thank you

Shauna Fanella

Shaunafanella99@gmail.com

760-783-6346



CITY OF OCEANSIDE

DEVELOPMENT SERVICES DEPARTMENT
PLANNING DIVISION

Memorandum

Date: February 7, 2025

To: Chair and Planning Commissioners

From: Rob Dmohowski, Principal Planner

Subject: **Eddie Jones Warehouse Final EIR**
Response to Caltrans comment letter dated February 4, 2025

On February 4, 2025, the California Department of Transportation (Caltrans) submitted a comment letter regarding the Local Transportation Study dated August 25, 2023 and Response to Comments section of the Final Environmental Impact Report (FEIR) for the Eddie Jones Warehouse, Manufacturing, and Office Facility project.

Staff finds that Caltrans comments were adequately addressed in the FEIR and offers the following responses in consultation with the LOS Engineering, Inc., who prepared the Local Transportation Study dated August 4, 2025.

1. **Bullets #1, 2 and 5 Response:** For purposes of performing the LOS calculations, the Synchro program relies solely on the actual data shown in the tables and files. As Caltrans points out, a hiccup exists with the map graphics produced by Synchro. Those map graphics were not utilized in performing the substantive LOS analysis so nothing further is required. See also Responses to comments (RTC) A3-4, A3-6, and A3-7.
2. **Bullet #3 Response:** This is a repeat comment. The Synchro software works as intended. See RTC A3-5.
3. **Bullet #4 Response:** Caltrans is mistaken. The I-5 and SR-76 interchange was properly analyzed and the project's contributions disclosed. See also RTCs A3-2 and A3-6.
4. **Bullet #6 Response:** The basis for the fair share percentage is already discussed and disclosed in Final EIR Sections 4.10 and 4.14, the LTS and RTCs O5-15 and I1-3.



CITY OF OCEANSIDE

DEVELOPMENT SERVICES DEPARTMENT PLANNING DIVISION

5. **Bullet #7 Response:** The comment is correct that the MMRP does not include such an improvement.
6. **Bullet #8 Response:** The Final EIR's Section 4.14 and VMT Report explain the VMT methodology supporting the percentage reduction, including the use of the SANDAG Mobility Management VMT Reduction Calculator Tool. See also RTCs Thematic Response 2, O5-17, and I60-27 addressing the same topic.
7. **Bullet #9 Response:** The Final EIR properly disclosed the baseline signal timing conditions in accordance with CEQA. Caltrans' request for use of a new baseline is not consistent with CEQA.
8. **Bullet #10 Response:** As previously explained in RTC A3-11 and elsewhere, the requested Synchro queuing analysis is not useful or required to analyze project impacts. As further evidence, the chart provided by Caltrans is internally flawed and includes inconsistent information about queue lengths. Applying Caltrans' own queuing methodology found at Index 405.2(e) of the current 7th Edition Highway Design Manual, further demonstrates that adequate capacity exists (e.g. the project queue at the left turn lane is far less than the 450 feet of storage).
9. **Bullet #11 Response:** For the reasons previously noted, no additional queuing analysis is required.
10. **Design Comment #1 Response.** RTC A3-23 already responded to the comment about a new crosswalk.

Attachments:

- a. Caltrans Letter dated February 4, 2025
- b. FEIR Response to Comment - Letter A3 (Caltrans Dec. 28,2023)

California Department of Transportation

DISTRICT 11
4050 TAYLOR STREET, MS-240
SAN DIEGO, CA 92110
(619) 985-1587 | FAX (619) 688-4299 TTY 711
www.dot.ca.gov



February 4, 2025

11-SD-5, 76
PM VAR

Eddie Jones Warehouse, Manufacturing & Distribution Facility
Response to Comments/SCH#2022070365

Mr. Rob Dmohowski
Principal Planner
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92057

Dear Mr. Dmohowski:

Thank you for including the California Department of Transportation (Caltrans) in the environmental review process for the Response to Comments for the Eddie (Eddy) Jones Warehouse, Manufacturing & Distribution Facility project located near Interstate 5 (I-5) and State Route 76 (SR-76). The mission of Caltrans is to provide a safe and reliable transportation network that serves all people and respects the environment. The Local Development Review (LDR) Program reviews land use projects and plans to ensure consistency with our mission and state planning priorities.

Safety is one of Caltrans' strategic goals. Caltrans strives to make the year 2050 the first year without a single death or serious injury on California's roads. We are striving for more equitable outcomes for the transportation network's diverse users. To achieve these ambitious goals, we will pursue meaningful collaboration with our partners. We encourage the implementation of new technologies, innovations, and best practices that will enhance the safety on the transportation network. These pursuits are both ambitious and urgent, and their accomplishment involves a focused departure from the status quo as we continue to institutionalize safety in all our work.

Caltrans is committed to prioritizing projects that are equitable and provide meaningful benefits to historically underserved communities, to ultimately improve transportation accessibility and quality of life for people in the communities we serve.

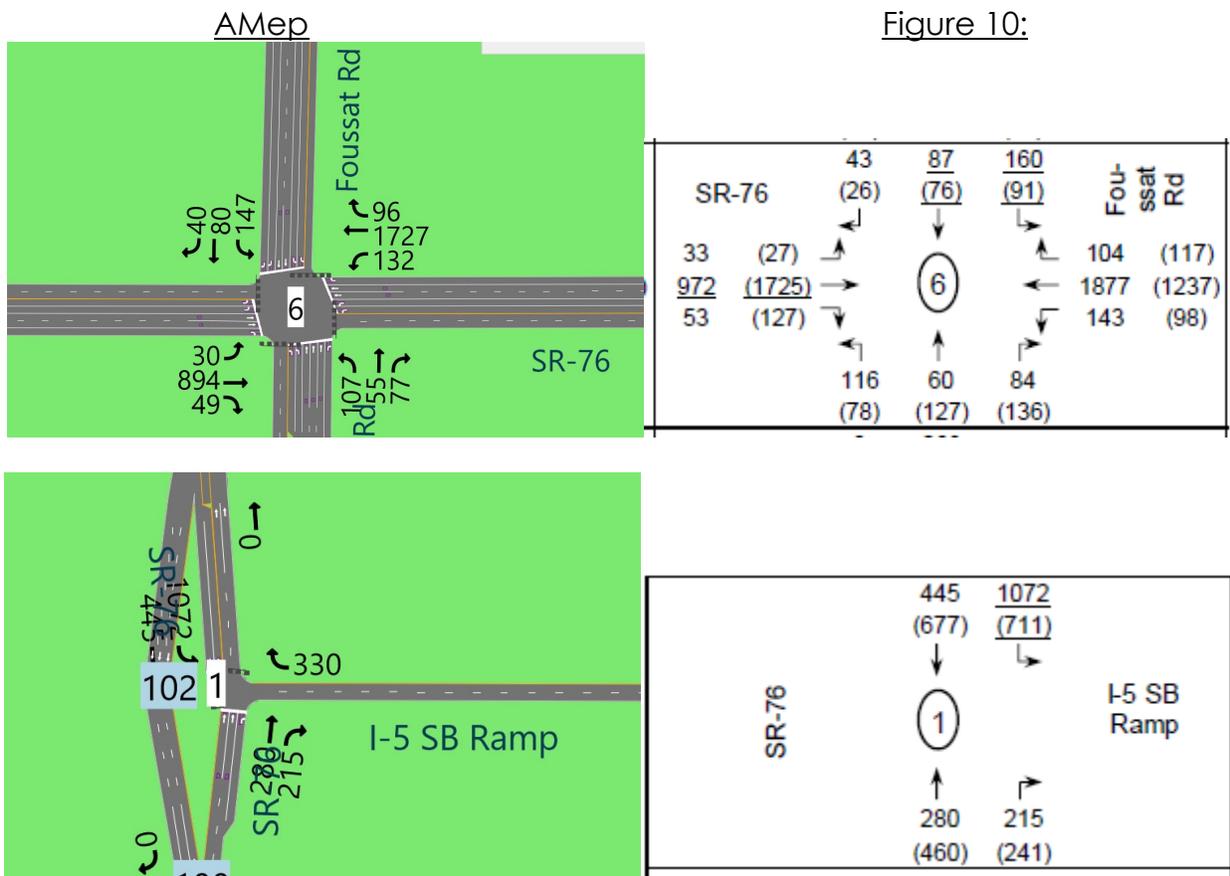
We look forward to working with the City of Oceanside in areas where the City and Caltrans have joint jurisdiction to improve the transportation network and connections

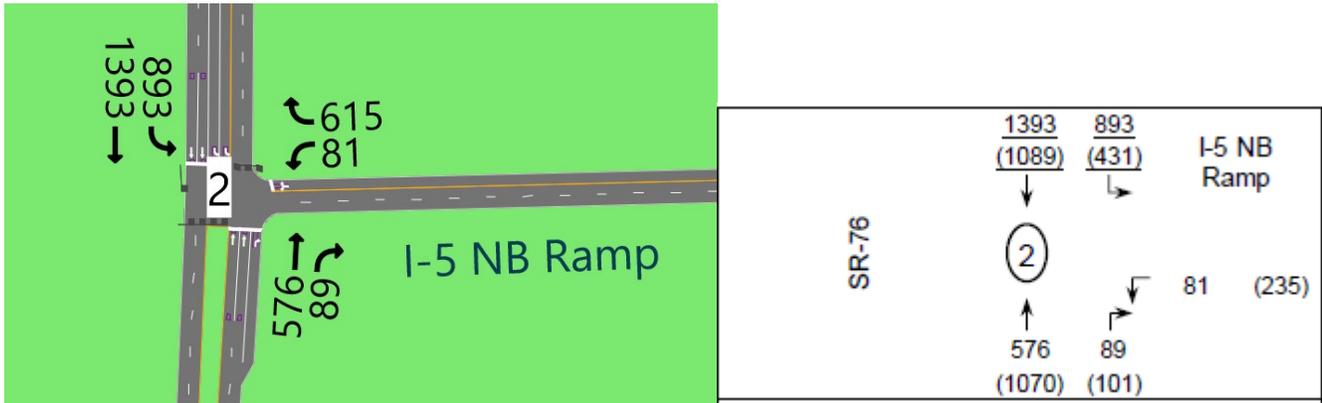
between various modes of travel, with the goal of improving the experience of those who use the transportation system.

In response to the Draft Local Transportation Study dated August 25, 2023 and the other technical documents that were submitted to Caltrans published in the project's final environmental document on January 10, 2025, Caltrans has the following comments:

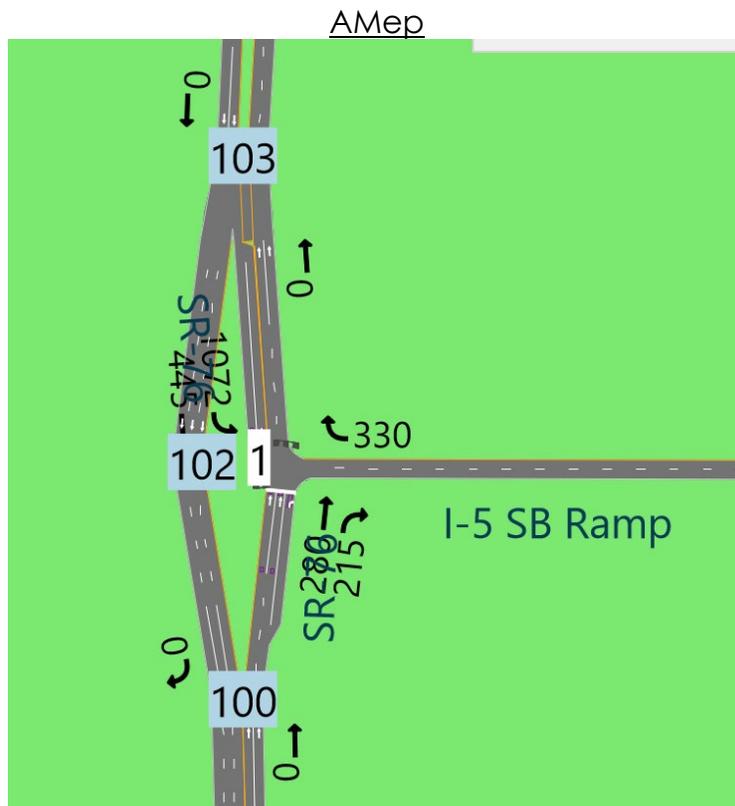
Traffic Engineering and Analysis

- The AM existing plus project Synchro file does not match Figure 10 of the Local Transportation Study for the SR-76 and Fousat Road intersection, the SR-76 and I-5 southbound ramps intersection, and the SR-76 and I-5 northbound ramps intersection. Please revise accordingly.





- The AM existing Synchro file shows a volume of 0 for the I-5 southbound to SR-76 eastbound movement. Please revise accordingly.



- The AM existing and existing plus project Sim Traffic does not run due to a fatal program error. Please revise accordingly.

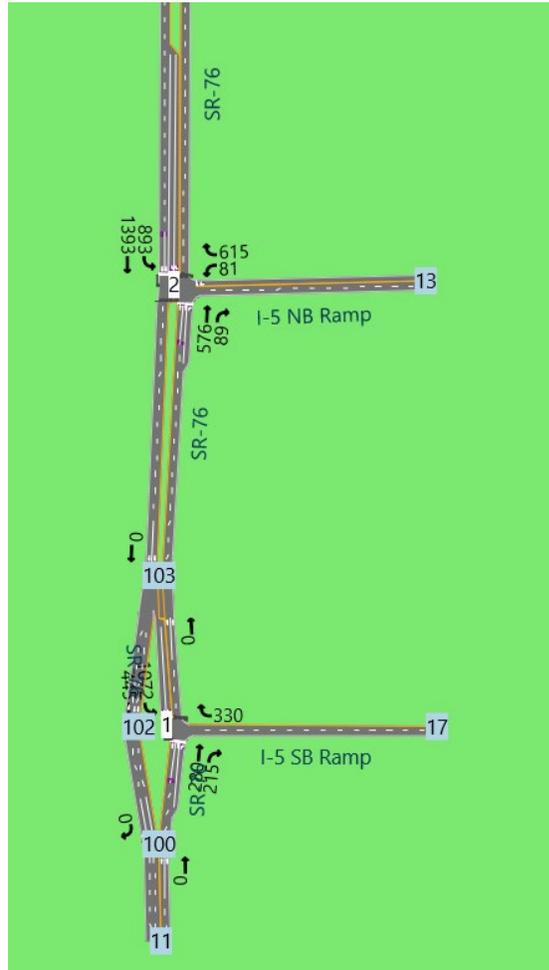
AMep

SimTraffic: Errors and Warnings		
Type	Node	
Error	Nd. 103	SBT : 2 input lanes, but only 1 receiving lanes on Nd. 1 SB
Error	Nd. 1	SBL : Storage length exceeds link length.
Fatal	Nd. 1	WBL : Volume with no capacity.
Fatal	Nd. 1	SBT : Volume with no capacity.
Error	Nd. 5	Phase 8 NBT : Ped timing + Y + AR exceeds max split.
Error	Nd. 6	NBT : 3 input lanes, but only 1 receiving lanes on Nd. 14 NB
Warning	Nd. 100	Angle between SB and SE approaches less than 25 degrees.
Fatal	Nd. 1	SBT : 1 input lanes, but no receiving lanes on Nd. 100 SB
Warning	Nd. 103	Angle between NE and NB approaches less than 25 degrees.

- The Synchro files need to be revised to include the I-5 and SR-76 interchange connected to the SR-76 segment to show the entire network and how it interacts. Please revise accordingly.



- The Synchro files incorrectly model the I-5 and SR-76 interchange including not showing the correct lane configurations and having volumes of zero. Please revise accordingly.



- Per section 4.14.4 Traffic and Circulation Impacts Analysis of the project *Draft Environmental Impact Report*, the project proposes a fair share payment of 8.5%. This is for converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction resulting in three through lanes in each direction. However, further analysis and discussions with Caltrans will be required to determine if this proposed mitigation is beneficial before implementation.
- Per the Mitigation Monitoring and Reporting Program (MMRP) dated January 2025 the proposed mitigation does not propose any improvements or mitigation on state Right-of-Way, therefore at this point no Encroachment Permit is anticipated.
- Please provide what analysis determined the 6.2% reduction of the Voluntary Employer Commute Program.
- The signal timing at SR-76 and Benet Road does not match the Caltrans Signal Timing sheets. Please revise accordingly.

Phasing Settings	1-WBL	2-EBT	3-NBL	4-SBT	5-EBL	6-WBT	7-SBL	8-NBT
Minimum Initial (s)	12.0	25.0	12.0	11.0	13.0	25.0	12.0	12.0
Minimum Split (s)	17.7	33.0	17.7	20.0	18.7	33.0	17.7	35.0
Maximum Split (s)	17.7	99.6	17.7	35.0	22.0	95.3	17.7	35.0
Yellow Time (s)	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1
All-Red Time (s)	2.0	2.5	2.0	2.0	2.0	2.5	2.0	2.0
Lagging Phase?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Allow Lead/Lag Optimize?	<input checked="" type="checkbox"/>							
Optimize Phs Weights - Delays	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Minimum Gap (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Time Before Reduce (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Time To Reduce (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Recall Mode	None	C-Max	None	Min	None	C-Max	None	None
Pedestrian Phase	<input type="checkbox"/>	<input checked="" type="checkbox"/>						
Walk Time (s)	-	-	-	-	-	-	-	7.0
Flash Dont Walk (s)	-	-	-	-	-	-	-	37.0
Pedestrian Calls (#/hr)	-	-	-	-	-	-	-	5

© 2017 California Department of Transportation. All Rights Reserved. Location: 76 @ Benet Rd - Airport Rd TSCP 3.10

Phase (2-2)	-1-	-2-	-3-	-4-	-5-	-6-	-7-	-8-
--- Walk 1 ---	0	0	0	0	0	0	0	1
Flash Don't Walk	0	0	0	0	0	0	0	37
Minimum Green	12	25	12	11	13	25	12	12
Det Limit	0	0	0	0	0	0	0	0
Max Initial	0	0	0	0	0	0	0	0
Max Green 1	18	50	18	24	27	50	28	23
Max Green 2	8	80	3	4	2	80	0	3
Max Green 3	3	80	0	4	0	80	0	3
Extension	2.0	3.5	2.0	3.0	2.0	3.5	2.0	2.0
Maximum Gap	2.0	8.3	2.0	3.0	5.0	8.3	5.0	2.0
Minimum Gap	2.0	3.5	2.0	3.0	2.0	3.5	2.0	2.0
Add Per Vehicle	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Reduce Gap By	0.0	0.1	0.0	0.0	0.1	0.1	0.1	0.0
Reduce Every	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Yellow	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1
All-Red	2.0	2.5	4.7	3.9	2.0	2.5	4.3	4.1
Ped Bike (2-3)	-1-	-2-	-3-	-4-	-5-	-6-	-7-	-8-
--- Walk 2 ---	0	0	0	0	0	0	0	0
Delay/Early Walk	0	0	0	0	0	0	0	6
Solid Don't Walk	0	0	0	0	0	0	0	0
Bike Green	0	0	0	0	0	0	0	0
Bike All-Red	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0

- The revised Synchro file results in Sim Traffic queuing for the PM existing + cumulative + project at the SR-76 and Benet Road intersection exceeds the storage capacity for the east bound left, eastbound through right, and westbound through right. Please provide further analysis and a safety analysis for this queuing issue.

Queuing and Blocking Report
 PM Existing + Cumulative + Project

Intersection: 5: Benet Rd & SR-76

Movement	EB	EB	EB	EB	B28	B28	WB	WB	WB	WB	B27	B27
Directions Served	L	T	T	TR	T	T	L	T	T	TR	T	T
Maximum Queue (ft)	475	588	591	475	2357	2353	415	539	551	455	126	500
Average Queue (ft)	320	537	540	452	1171	1189	96	474	472	427	11	25
95th Queue (ft)	606	656	659	562	2646	2669	336	560	550	517	56	252
Link Distance (ft)		488	488		2575	2575		461	461		1306	1306
Upstream Blk Time (%)	0	33	37	4	1	1		15	11		3	0
Queuing Penalty (veh)	0	381	428	0	6	10		122	90		0	0
Storage Bay Dist (ft)	450			450			390			430		
Storage Blk Time (%)	0	34	40	6				29	20		2	
Queuing Penalty (veh)	0	73	313	41				12	111		9	

- Please provide a revised Synchro and Sim Traffic queuing analysis after the comments above are addressed.

Design

Caltrans commends the improvements on Alex Road and the nearby east side of Benet Road and its contribution to the City of Oceanside's goals of additional bicycle and pedestrian network upgrades. As an addition to this improved network, we recommend a pedestrian crossing to be installed on the north side of the SR-76 and Benet Road intersection. With the increased vehicular trips through this intersection

due to the construction of the warehouse and the permittance of pedestrians and bicyclists on the non-sidewalk shoulders of the SR-76.

Right-of-Way

Per Business and Profession Code 8771, perpetuation of survey monuments by a licensed land surveyor is required, if they are being destroyed by any construction.

Any work performed within Caltrans' Right-of-Way (R/W) will require discretionary review and approval by Caltrans and an encroachment permit will be required for any work within the Caltrans' R/W prior to construction.

Additional information regarding encroachment permits may be obtained by visiting the website at <https://dot.ca.gov/programs/traffic-operations/ep>. Projects with the following:

- require a Caltrans Encroachment Permit.
- have completed the Caltrans Local Development Review (LDR) process.
- have an approved environmental document.

need to have documents submitted for Quality Management Assessment Process (QMAP) process via email to D11.QMAP.Permits@dot.ca.gov. Early coordination with Caltrans is strongly advised for all encroachment permits.

If you have any questions or concerns, please contact Shannon Aston, LDR Coordinator, at (619) 992-0628 or by e-mail sent to shannon_aston@dot.ca.gov.

Sincerely,

Kimberly D. Dodson

KIMBERLY D. DODSON, GISP
Branch Chief
Local Development Review

CALIFORNIA STATE TRANSPORTATION AGENCY

GAVIN NEWSOM, GOVERNOR

California Department of Transportation

DISTRICT 11
4050 TAYLOR STREET, MS-240
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(619) 985-1587 | FAX (619) 688-4299 TTY 711
www.dot.ca.gov



December 28, 2023

11-SD-5, 76
PM VAR
Eddie Jones Warehouse, Manufacturing & Distribution Facility
DEIR/SCH#2022070365

Mr. Rob Dmohowski
Principal Planner
City of Oceanside
300 N. Coast Highway
Oceanside, CA 92057

Dear Mr. Dmohowski:

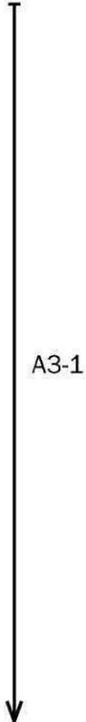
Thank you for including the California Department of Transportation (Caltrans) in the environmental review process for the Draft Environmental Impact Report (DEIR) for the Eddie (Eddy) Jones Warehouse, Manufacturing & Distribution Facility project located near Interstate 5 (I-5) and State Route 76 (SR-76). The mission of Caltrans is to provide a safe and reliable transportation network that serves all people and respects the environment. The Local Development Review (LDR) Program reviews land use projects and plans to ensure consistency with our mission and state planning priorities.

Safety is one of Caltrans' strategic goals. Caltrans strives to make the year 2050 the first year without a single death or serious injury on California's roads. We are striving for more equitable outcomes for the transportation network's diverse users. To achieve these ambitious goals, we will pursue meaningful collaboration with our partners. We encourage the implementation of new technologies, innovations, and best practices that will enhance the safety on the transportation network. These pursuits are both ambitious and urgent, and their accomplishment involves a focused departure from the status quo as we continue to institutionalize safety in all our work.

Caltrans is committed to prioritizing projects that are equitable and provide meaningful benefits to historically underserved communities, to ultimately improve transportation accessibility and quality of life for people in the communities we serve.

We look forward to working with the City of Oceanside in areas where the City and Caltrans have joint jurisdiction to improve the transportation network and connections

"Provide a safe and reliable transportation network that serves all people and respects the environment!"



A3-1

Mr. Rob Dmohowski, Principal Planner
December 28, 2023
Page 2

between various modes of travel, with the goal of improving the experience of those who use the transportation system.

Caltrans has the following comments:

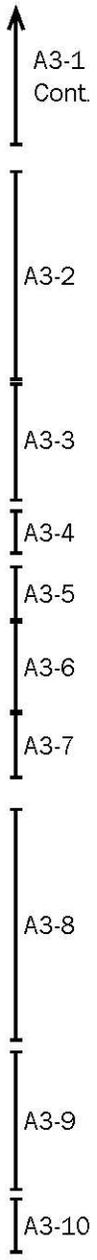
Traffic Engineering and Analysis

Please revise the following:

- The project distributions in figure 8 of the Local Transportation Study do not add up. There are 60% project trips going out at the SR-76 and I-5 interchange, but only 30% project trips going in.
- The AM existing plus project Synchro file does not match figure 10 of the Local Transportation Study for the SR-76 and Foussat Road intersection, the SR-76 and I-5 southbound ramps intersection and the SR-76 and I-5 northbound ramps intersection.
- The AM existing Synchro file shows a volume of 0 for the I-5 southbound to SR-76 eastbound movement.
- The AM existing and existing plus project Sim Traffic does not run due to a fatal error.
- The Synchro files need to be revised to include the I-5 and SR-76 interchange connected to the SR-76 segment to show the entire network and how it interacts.
- The Synchro files incorrectly model the I-5 and SR-76 interchange including not showing the correct lane configurations and having volumes of zero.

Please see the following comments:

- Per section 4.14.4 Traffic and Circulation Impacts Analysis of the Draft Environmental Impact Report, the project proposes a fair share payment of 8.5% towards the provisions of converting the existing eastbound and westbound right turn lanes to a combination through-right lane in the eastbound and westbound direction resulting in three through lanes in each direction. However, the three through lanes are only temporary, then followed by merge tapers. Additionally, the provided Synchro models with this mitigation do not accurately depict what is proposed in Appendix P of the Local Transportation Study. Please revise the Synchro files accordingly. Further analysis and discussions with Caltrans functional units is needed to determine if this proposed mitigation will be beneficial.
- The signal timing at SR-76 and Benet Road does not match the Caltrans Signal Timing sheets. Please revise accordingly.



“Provide a safe and reliable transportation network that serves all people and respects the environment”

Mr. Rob Dmohowski, Principal Planner
 December 28, 2023
 Page 3

- Please provide a Synchro and Sim Traffic queuing analysis after the comments are addressed above regarding Synchro and Sim Traffic.

A3-11

Electric Vehicle

- It is recommended that Electric Freight/Fleet Vehicles be utilized as much as possible for the proposed fleet/freight trips.
- Please clarify if the proposed 89 Elective Vehicle parking spaces includes freight vehicles.
- Please clarify if the proposed 45 electric vehicle chargers include access for freight vehicles.
- If electric vehicle fleets are not currently proposed for this site, please identify approximately a time frame when electric freight/fleet vehicles would be incorporated into the fleets at this location.

A3-12
 A3-13
 A3-14

Hydrology and Drainage Studies

- Please clarify what is the impact due to the proposed project to the Federal Emergency Management Agency (FEMA) defined Floodplain and associated water surface elevations at the Caltrans' Right-of-Way (R/W) along SR-76.
- The Hydrology and Hydraulics Report states that levees exist on both sides of the San Luis Rey River, but the Federal levee project has not yet been certified by FEMA. Please provide more information on why the levees have not yet been certified by FEMA. Please provide a date as to when will the levees be certified.
- The National Levee Database describes the levees next to the project as the San Luis Rey River 3 (SLR3) Levee System. It states that in future the responsibility will be turned over to the City of Oceanside for post-construction operation and maintenance. Please confirm if the City is currently maintaining the levees.
- The Hydrology and Hydraulics Report shows an exhibit with a proposed floodwall around the project. Provide more details on this floodwall and how it will impact the floodplain.
- The Hydrology and Hydraulics Report states that the Manning's roughness coefficient used for the 2D model was 0.06. Please verify if this coefficient used in all models - effective, existing, proposed. Please provide copies of models/backup to verify.
- The proposed project features may significantly alter the FEMA defined Floodplain and associated water surface elevations through the project area. Please confirm if the FEMA Flood Insurance Rate Map (FIRM) be remapped. Caltrans requests proof of coordination with the

A3-15
 A3-16
 A3-17
 A3-18
 A3-19

"Provide a safe and reliable transportation network that serves all people and respects the environment"

Mr. Rob Dmohowski, Principal Planner
December 28, 2023
Page 4

- City of Oceanside acting as the Local FEMA Administrator.
- Caltrans requests that formal notification be sent for review when the City approves the permit to alter the floodplain and/or when the Developer applies for the Conditional Letter of Map Revision (CLOMR) and a Letter of Map Revision (LOMR). Please provide this information to Caltrans when it is available.

A3-19
Cont.
A3-20

Design

Vehicle Miles Travelled Analysis (Appendix J) for this project calculated trip generation using the Institute of Transportation Engineers 11th Edition Trip Generation, September 2021. However, the City of Oceanside *Traffic Impact Analysis Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment*, August 2020, requires that the consultant for the project shall identify the number of trips generated by the project using SANDAG's "Not so Brief Guide of Vehicular Traffic Generation Rates for San Diego Region" (2002) trip generation rates. Using SANDAG's guide results in 2,670 Average Daily Trips (ADT). According to the Cities' guidelines, the SANDAG Regional Travel Demand Model will be used for any project that generates over 2,400 ADT (Appendix J, pg. 7). Therefore, the regional model should be used for determining VMT impacts. This may not substantially alter the results of the analysis, but perhaps the City can authorize the use of the 2021 Institute of Transport Engineers (ITE) trip rates if other goals and policies from the Cities' stated guidelines are not implemented.

A3-21
A3-22

One of the City of Oceanside's goals is to require new developments to provide connections and/or extensions of the bicycle and pedestrian networks where applicable. The proposed manufacturing/distribution facility will increase large truck traffic at the intersection at Benet Road and SR-76. Pedestrians and bicyclists are permitted along the shoulders of SR-76, and this intersection does not currently have a crosswalk to cross Benet Road on the northern side of the state route. Caltrans prioritizes the safety of vulnerable users of the transportation system; therefore, it is recommended that a crosswalk be added as part of the project at this location.

A3-23

Right-of-Way

- Per Business and Profession Code 8771, perpetuation of survey monuments by a licensed land surveyor is required, if they are being destroyed by any construction.
- Any work performed within Caltrans' R/W will require discretionary review and approval by Caltrans and an encroachment permit will be required for any work within the Caltrans' R/W prior to construction.

A3-24

Additional information regarding encroachment permits may be obtained by contacting the Caltrans Permits Office at (619) 688-6158 or emailing

"Provide a safe and reliable transportation network that serves all people and respects the environment"

Mr. Rob Dmohowski, Principal Planner
December 28, 2023
Page 5

D11.Permits@dot.ca.gov or by visiting the website at <https://dot.ca.gov/programs/traffic-operations/ep>. Early coordination with Caltrans is strongly advised for all encroachment permits.

If you have any questions or concerns, please contact Shannon Aston, LDR Coordinator, at (619) 992-0628 or by e-mail sent to shannon_aston@dot.ca.gov.

Sincerely,

Kimberly D. Dodson

KIMBERLY D. DODSON, G.I.S.P.
Acting Branch Chief
Local Development Review

↑
A3-24
Cont.
|
A3-25

"Provide a safe and reliable transportation network that serves all people and respects the environment"

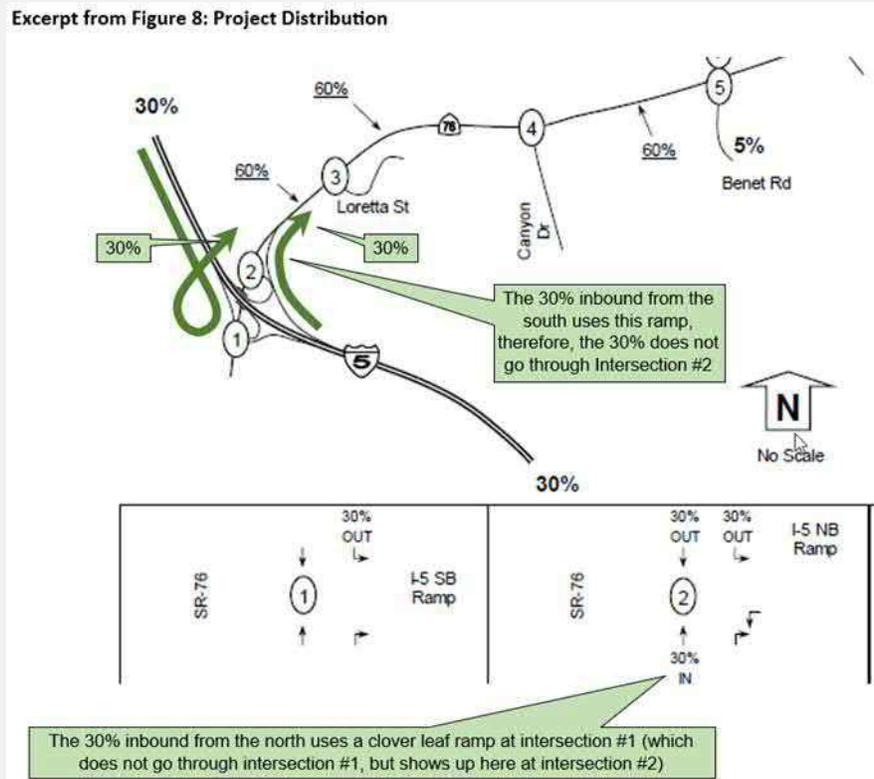
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Response to Comment Letter A3

California Department of Transportation
Kimberly D. Dodson
December 28, 2023

- A3-1** The California Department of Transportation (Caltrans) expresses appreciation for being included in the environmental review process for the DEIR for the project located near Interstate (I) 5 and State Route (SR) 76. The comment provides a summary of Caltrans' mission. The comment provides an introduction to comments that follow. The comment does not raise any specific issues related to the adequacy of the DEIR, and no further response is required.
- A3-2** The comment states that the project distribution within Figure 8 of the Local Transportation Study (LTS) (DEIR Appendix I) do not add up because it shows 60% of the project trips going out at the SR-76 and I-5 interchange, but only 30% project trips going in. In response, the 60% inbound from I-5 includes 30% from I-5 north and 30% from I-5 south. The 30% inbound from I-5 north uses a clockwise clover leaf ramp from I-5 southbound travel to SR-76 eastbound travel and does not pass through the traffic signal at intersection #1 because the clover leaf ramp connects to SR-76 approximately 140 feet northeast of intersection #1. This is why intersection #1 correctly shows 30% outbound and does not show the 30% inbound from I-5 north. Further to the northeast, and for the reasons just noted, the 30% inbound from I-5 north is shown at intersection #2 as 30% inbound. The 30% inbound from I-5 south uses a northbound ramp from I-5 to SR-76 eastbound and does not pass through the traffic signal at intersection #2 because the I-5 northbound to SR-76 eastbound ramp connects approximately 840 feet northeast of intersection #2. This is why intersection #2 correctly shows 60% outbound and only 30% inbound. The combined inbound 30% from I-5 north and 30% inbound from I-5 south add up to the 60% inbound traffic as shown on the segment on SR-76 east of intersection #2. The distribution in Figure 8 (excerpted below as Exhibit A3-1), shows how the noted ramps at the SR-76/I-5 interchange do not pass through intersections #1 and #2 as described above, where each approach leg only shows the movements that are going through the actual intersection. Therefore, the 60% in question is correctly shown on Figure 8 as called out in green and was correctly analyzed in the LTS.

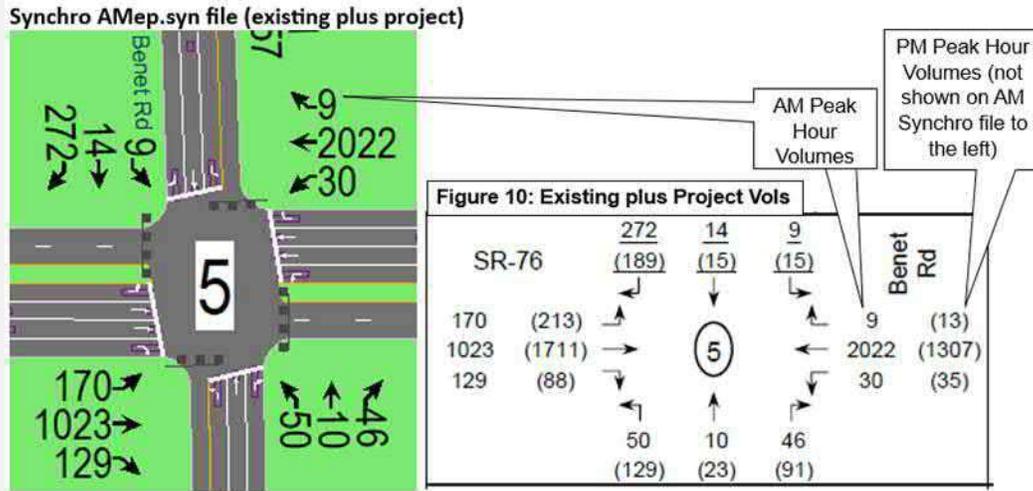
Exhibit A3-1: Excerpt from Figure 8 of the Local Transportation Study.



Source: DEIR Appendix I.

A3-3 The comment states that the AM Existing Plus Project Synchro file does not match Figure 10, Existing plus Project Volumes, of the Local Transportation Study for the SR-76 and Foussat Road intersection, the SR-76 and I-5 southbound ramps intersection, and the SR-76 and I-5 northbound ramps intersection. The commenter is mistaken. The orientation of the two figures referenced in the comment is different, but the peak volume counts are the same. To illustrate that point, excerpts from the AM existing plus project Synchro file match excerpts from Figure 10, as shown below in Exhibit A3-2.

Exhibit A3-2: Excerpts from Synchro (left) and Figure 10 of the Local Transportation Study (right).

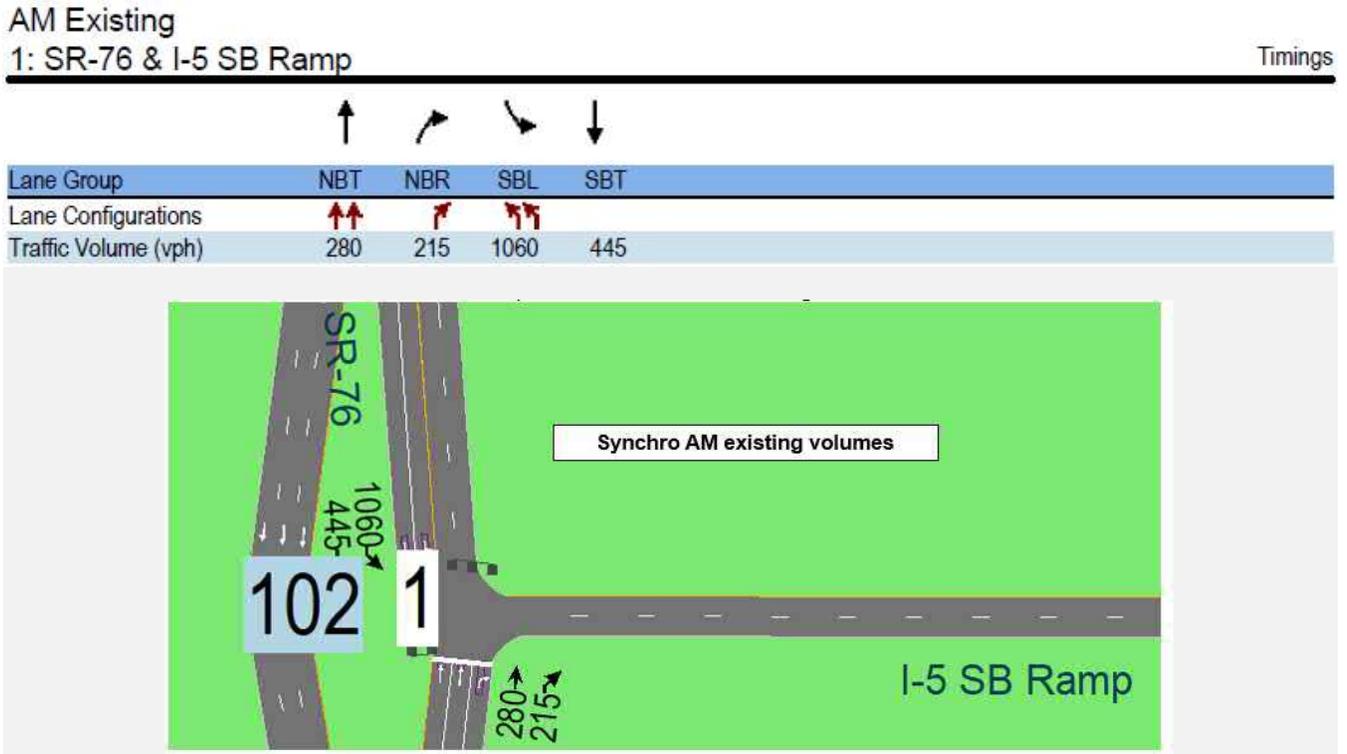


Source: Source: DEIR Appendix I.

A3-4

The comment states that the AM existing Synchro file shows a volume of 0 for the southbound to SR-76 eastbound movement. Please see Response to Comment A3-2 explaining how intersection #1 has a clockwise clover leaf ramp from I-5 southbound travel to SR-76 eastbound travel and does not permit vehicles to pass through this traffic signal because the clover leaf ramp connects to SR-76 approximately 140 feet northeast of intersection #1. The AM existing Synchro output and screenshot correctly show a volume of 1,060 AM trips on the southbound left turn onto the I-5 southbound ramp; this is documented in the Synchro output included in the LTS (DEIR Appendix I, p. 86) (Exhibit A3-3).

Exhibit A3-3: Synchro output worksheet from DEIR Appendix I (p. 86) (top) and Synchro screenshot (bottom).



Source: DEIR Appendix I.

A3-5 The comment states that the AM existing and existing plus project Sim Traffic does not run due to a fatal error. In response, as reflected in the DEIR, the Synchro software does work and Synchro output was generated. The comment appears to be the result of a software issue with Caltrans' computer equipment.

A3-6 The comment states that the Synchro files need to be revised to include the I-5 and SR-76 interchange connected to the SR-76 segment to show the entire network and how it interacts. Please see Response to Comment A3-2 explaining how the I-5 and SR-76 interchange is correctly coded in Synchro to reflect the actual approach lanes that go through intersection #1 (I-5 southbound ramp at SR-76) and through intersection #2 (I-5 northbound ramp at SR-76). The interchange has ramps that bypass the traffic signals along with southbound fixed green arrows that are always on to permit vehicles to travel south without stopping at intersection #1. Below is a screenshot of the Synchro file provided to the commenter along with an aerial of the interchange that show which travel lanes go through the intersections and the ramps that bypass the intersections (Exhibit A3-4). For context, a street view of the roadway circulation is also included that shows what is graphically depicted in the Synchro screenshot (Exhibit A3-5). The SR-76 southbound lanes have solid green arrows to proceed and do not cycle through yellow or red; thus, they are coded in Synchro as bypass lanes.

Exhibit A3-4: Synchro file (left) and aerial image (right).



Sources: DEIR Appendix I; Google Maps 2024.

Exhibit A3-5: Street view image of SR-76 southbound lanes.

Source: Google Maps 2024.

A3-7 The comment states that the Synchro files incorrectly model the I-5 and SR-76 interchange including not showing the correct lane configurations and having volumes of zero. As shown in Responses to Comments A3-2 and A3-6 (above), the interchange is correctly coded and modeled. Synchro measures vehicle delay and level of service (LOS) caused by a traffic signal cycling through multiple phases of green, yellow, and red, thereby stopping some vehicles and permitting other vehicles to travel without conflict. Therefore, vehicles that do not have to stop at a red light at an intersection are coded in the Synchro model using nodes that avoid incorrectly calculating the vehicle delay for the remaining vehicles that have to stop for a red light. The intersection of I-5 southbound ramp at SR-76 has three southbound through lanes with solid green arrows that do not cycle through yellow and red. Therefore, the vehicles that do not stop are coded with the nodes. Those nodes have zeros because the traffic signal does not cause any vehicle delays as the traffic signal for those lanes is always green. Thus, the nodes are not reported in the Synchro output like other traffic signal intersections because they are not intersections where the traffic signal could result in a calculated LOS delay.

A3-8 The comment notes that in Section 4.14.4, Impacts Analysis, of the traffic and circulation section of the DEIR, the project proposes a fair share payment towards the conversion of the existing eastbound and westbound right-turn lanes to a combination through-right lane in the eastbound and westbound direction, resulting in three through lanes in each direction. The comment incorrectly describes those three proposed lanes as “only temporary,” followed by merge tapers. The proposal is to provide three full width, through lanes for a distance of approximately 910 linear feet (250 feet of a new through lane and 660 feet of merging taper in the eastbound direction and westbound directions). This change would start before the intersection as a restripe of the near side right-turn lane to a through-right lane and then have the additional lane in each direction taper back down after the intersection. The additional lanes (one in each direction) would not be temporary in the temporal sense. The improvement would remain in place unless and until the relevant agencies construct a different configuration. As demonstrated by the analysis in the DEIR, the proposed improvements that start before the intersection and end after the intersection with the tapers would increase the capacity at the intersection of SR-76 and Benet Road such that the project would not cause a cumulative contribution to an LOS deficiency.

A3-9 The comment states that the provided Synchro models with mitigation do not accurately depict what is proposed within Appendix P of the LTS (Appendix I to the DEIR). The commenter is correct that the information within the model provided to Caltrans included initial taper lengths and not the final length that was determined when the conceptual drawing was completed. In response, the Synchro files have been revised to match the final taper length for the proposed improvement of converting the existing eastbound and westbound right-turn lanes to a combination through-right lane in the eastbound and westbound direction. The Synchro outputs with those improvements are shown in Table A3-1. As depicted, the LOS and delay at the intersection of SR-76/Benet Road with the refined lane taper lengths are consistent with the results disclosed in the DEIR. Therefore, the conclusions regarding this issue adequately disclosed the project’s cumulative contribution to an LOS delay with the proposed improvement.

Table A3-1. Intersection #5 SR-76/Benet Rd Operations with Improvement

Intersection and Scenario	Movement	Study Period	Delay ¹	LOS ²	Delay ¹	LOS ²
<u>Near Team + Project</u>			<u>As Analyzed in the LTS</u>		<u>With Refined Synchro Inputs</u>	
5) SR-76 at Benet Rd (S)	All	AM	25.7	C	25.7	C
	All	PM	43.0	D	43.0	D
<u>Horizon Year + Project</u>			<u>As Analyzed in the LTS</u>		<u>With Refined Synchro Inputs</u>	
5) SR-76 at Benet Rd (S)	All	AM	69.8	E	69.8	E
	All	PM	63.9	E	63.9	E

Notes: (S) Traffic Signal 1) Delay – HCM Average Control Delay in seconds. 2) LOS: Level of Service. 3) Delta in the increase in delay from project. 4) Impact if project traffic exceeds threshold.

The new Synchro output files are included as Attachment A to this Responses to Comments document.

A3-10 The comment states that the signal timing at SR-76 and Benet Road does not match the Caltrans Signal Timing Sheets and requests this information to be revised. In response, the Synchro timing matches the available Caltrans timing sheets as shown below in Exhibit A3-6. It is not clear from the comment why Caltrans staff believes the relevant information does not match. It is true that, for example, the nomenclature is not the same between Caltrans timing sheets and Synchro inputs and that Synchro has other phase settings that are not shown on Caltrans timing sheets. However, for purposes of conducting the required CEQA analysis, the timing information is substantively the same and the DEIR adequately disclosed and analyzed any of the project’s potentially significant impacts.

Exhibit A3-6: Synchro timing settings (top) and Caltrans timing sheet (bottom).

5 Benet Rd & SR-76								
PHASING SETTINGS								
Minimum Initial (s)	12.0	25.0	12.0	11.0	13.0	25.0	12.0	12.0
Minimum Split (s)	17.7	33.0	17.7	20.0	18.7	33.0	17.7	50.1
Maximum Split (s)	17.7	74.5	27.1	40.7	22.6	69.6	17.7	50.1
Yellow Time (s)	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1
All-Red Time (s)	2.0	2.5	2.0	2.0	2.0	2.5	2.0	2.0
Lagging Phase?	<input type="checkbox"/>	<input checked="" type="checkbox"/>						
Allow Lead/Lag Optimize?	<input checked="" type="checkbox"/>							
Optimize Phs Weights - Delays	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Minimum Gap (s)	2.0	3.5	2.0	2.0	2.0	3.5	2.0	2.0
Time Before Reduce (s)	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Time To Reduce (s)	0.0	0.1	0.0	0.0	0.1	0.1	0.1	0.0
Recall Mode	None	C-Max	None	Min	None	C-Max	None	Min
Pedestrian Phase	<input type="checkbox"/>	<input checked="" type="checkbox"/>						
Walk Time (s)	—	—	—	—	—	—	—	7.0
Flash Dont Walk (s)	—	—	—	—	—	—	—	37.0

CALTRANS timing sheet

LOCATION: RTE 76 EXPRESSWAY @ BENET ROAD - AIRPORT ROAD (WB)
 CALTRANS C8 Version 3 DATE: 10/1/2015
 F PAGE

INTERVAL	PHASE TIMING								9	PRE-EMPTIO
	1	2	3	4	5	6	7	8		E
0 WALK	1	1	1	1	1	1	1	7	CLK RST	EV SEL
1 DONT WALK	1	1	1	1	1	1	1	37		RR1 CLR
2 MIN GREEN	12	25	12	11	13	25	12	12		EVA DLY
3 TYPE 3 DET	0	255	0	0	0	255	0	0		EVA CLR
4 ADD/VEH	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0		EVB DLY
5 PASSAGE	2.0	6.3	2.0	2.0	3.0	6.3	3.0	2.0		EVB CLR
6 MAX GAP	2.0	8.3	2.0	2.0	5.0	8.3	5.0	2.0		EVC DLY
7 MIN GAP	2.0	3.5	2.0	2.0	2.0	3.5	2.0	2.0		EVC CLR
8 MAX EXT	18	50	18	24	27	50	28	23		EVD DLY
9 MAX 2	8	80	3	4	2	80	0	3	YR	EVD CLR
A MAX 3	3	80		4		80		3	NO	MAX EV
B									DAY	RR2 CLR
C REDUCE BY	0.0	0.1	0.0	0.0	0.1	0.1	0.1	0.0	DCW	
D EVERY	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	HR	
E YELLOW	3.7	5.5	3.7	4.1	3.7	5.5	3.7	4.1	MIN	
F RED	2.0	2.5	2.0	2.0	2.0	2.5	2.0	2.0	SEC	

Sources: DEIR Appendix I; Caltrans 2015.

A3-11 The comment requests a revised Synchro and Sim Traffic queuing analysis after comments A3-2 through A3-10 are addressed above. As addressed in the Responses to Comments A3-2 through A3-10, the Synchro inputs correctly represent the intersections, except for the clarification provided in Response to Comment A3-9. The commenter is mistaken that additional Synchro and Sim Traffic data are needed. As requested in this comment, a Synchro queuing analysis has been completed for the eastbound left-turn lane that shows the existing left-turn lane has capacity to accommodate the “Existing + Cumulative + Project” and “Horizon Year + Project Conditions;” this is shown below in Table A3-2. These updated Synchro files are included as Attachment A to this Responses to Comments document.

Table A3-1. Synchro Queuing Analysis

Intersection	Park Hour	Approach	Storage Length in feet	Existing+Cumulative +Project		Horizon Year + Project	
				95 th Percentile Queue ¹ (ft)	Exceeds Storage?	95 th Percentile Queue ¹ (ft)	Exceeds Storage?
5) SR-7 at Benet Rd.	AM	EBL	450	211	No	297	No
	PM	EBL	450	265	No	368	No

Notes: EBL: Eastbound Left. (1) Queue 95th percentile from Sim Traffic analysis.

A3-12 The comment recommends that electric freight/fleet vehicles be utilized as much as possible. This comment has been considered by the City, but the comment does not raise an issue of CEQA compliance. No revisions are required.

A3-13 The comment requests clarification on whether or not the 89 electric vehicle spaces include freight vehicles and whether or not the 45 electric vehicle chargers include access for freight vehicles. In response, the EV charging spaces shown on the project site plans are only for passenger vehicles, not freight vehicles. However, the service and transformer would be sized to handle the additional capacity of the future chargers.

A3-14 The comment states that if electric fleet vehicles are not currently proposed, a timeframe of when electric fleet vehicles would be incorporated should be provided. The comment does not raise an issue of CEQA compliance. Further, it is not possible to forecast a time frame because the future tenants of the project are unknown, as are future government programs, laws, and incentives as related to electric freight/fleet vehicles.

A3-15 The comment asks for clarification on the project’s impact to the FEMA defined floodplain and associated water surface elevations at the Caltrans’ right-of-way along SR-76. In response, Section 4.9, Hydrology and Water Quality, of the DEIR discloses that the project would not result in significant adverse hydrological impacts based on the information available at the time of the preparation of the DEIR. Tory R. Walker Engineering recently received an effective hydraulic model from FEMA for the San Luis Rey River. Applying that hydraulic model to the project with existing and proposed conditions confirms that the proposed project would have a less than significant impact on the floodplain and associated water surface elevations at the Caltrans right-of-way along SR-76. Please refer to the revised Hydrology and Hydraulics Report included as Appendix E to the Final EIR.

A3-16 The comment requests information on why the levees on both side of the San Luis Rey River have not been certified by FEMA, including a date as to when the levees will be certified. The comment also asks

for confirmation of whether the City is currently maintaining the levees. In response, the comment does not raise any issues with the adequacy of the DEIR, and therefore no further response is required.

- A3-17** The comment requests additional information on the proposed floodwall around the project and how it will impact the floodplain. The Hydrology and Hydraulics Report (Appendix E to the Final EIR) explains that the proposed floodwall would follow FEMA design guidelines, and the top would be designed to be higher than the base flood elevation (approximately 8-foot-tall floodwall). The Hydrology and Hydraulics Report also shows that the proposed conceptual floodwall would not have a significant adverse impact under CEQA. The final floodwall has not yet been designed, but the project approvals would require that the final design substantially conform to the floodwall conceptually described in the hydraulic report prepared for the project.
- A3-18** The comment references the Hydrology and Hydraulics Report and requests verification that the Manning's roughness coefficient of 0.06 used for the 2D model was used in all models (effective, existing, proposed), as well as copies of the models to verify. In response, the 2D model has been revised to a 1D model with the recent receipt of a FEMA effective 1D hydraulic model. The Mannings coefficients are described in the updated report. They follow the coefficients that were in the effective model, with some variations as the vegetation of the channel and banks changes as the model nears the Pacific Ocean. Please see the revised Hydrology and Hydraulics Report included as Appendix E to the Final EIR. The revised modeling described above does not change the impact significance conclusions in the EIR, specifically in Section 4.9 of the EIR.
- A3-19** The comment states that the project features may significantly alter the FEMA defined floodplain and requests confirmation that the FEMA Flood Insurance Rate Map is being remapped. The comment also requests proof of coordination with the City of Oceanside acting as the local FEMA administrator. In response, under applicable law, the project applicant would be required to apply for a CLOMR and LOMR through FEMA as a condition of development. Coordination with the City and FEMA would occur as part of that process. Under the law, given that the project is only at the discretionary permitting stage, it would be improper and premature to commence the FEMA application process now. As described in Section 4.9 of the EIR, the project site is located in Special Flood Hazard Area Zone A99, as designated by FEMA. Zone A99 designates areas "within the 100-year floodplain that will be protected by a Federal flood protection system under construction." In this case, the federal flood protection system are the levees that have already been constructed along the San Luis Rey River, but this project (levees) has yet to be certified by FEMA. As demonstrated in the Hydrology and Hydraulics Report (Appendix E of the Final EIR), with the levees in place, the water surface elevation at the project site during a 100-year flood remains the same in both the existing and proposed conditions (22.39 feet, North American Vertical Datum of 1988 [NAVD 88]). The proposed project would not increase water surface elevation. As an additional flood protection feature, an approximately 8-foot tall perimeter flood wall would also be incorporated around the boundary of the entire project site.
- A3-20** The comment requests that formal notification be sent to Caltrans for review when the City approves the permit to alter the floodplain and/or when the developer applies for a CLOMR and a LOMR. The City acknowledges this comment and formal notification will be provided to Caltrans once a CLOMR/LOMR application is submitted.
- A3-21** The comment states that trip generation was calculated using Institute of Transportation Engineers (ITE) 11th Edition Trip Generation Manual (ITE 2021a), but the City of Oceanside Traffic Impact Analysis

Guidelines for Vehicle Miles Traveled (VMT) and Level of Service Assessment (Traffic Guidelines) (City of Oceanside 2020) requires that the consultant for the project identify the number of trips generated by the project using SANDAG's Not so Brief Guide of Vehicular Traffic Generation Rates for San Diego Region (SANDAG 2002) trip generation rates. In response, the author is mistaken. Section 6.0 of the Traffic Guidelines (City of Oceanside 2020) states the trip generation shall be calculated using either SANDAG or ITE if approved by the City traffic engineer. The SANDAG trip rates for industrial land use are based on the 6th Edition of the ITE Trip Generation Manual, which is calculated from data that are over 27 years old. Therefore, the current ITE 11th Edition Trip Generation Manual trip rates were used, reviewed, and approved by the City traffic engineer.

A3-22 The comment states that using SANDAG's Not so Brief Guide of Vehicular Traffic Generation Rates for San Diego Region (SANDAG 2002), the project would result in 2,670 average daily trips (ADT), and states that the City's Traffic Guidelines require any project that generates over 2,400 ADT to use the SANDAG Regional Travel Demand Model for determining VMT impacts. The commentor is mistaken in their understanding of the Traffic Guidelines (City of Oceanside 2020). Page 9 of the City's Traffic Guidelines states only that a SANDAG Regional Travel Demand Model is used for the purpose of determining the trip distribution and assignment (not VMT) when a project has more than 2,400 ADT. Please refer to Response to Comment A3-21 for an explanation as to why ITE trip rates were applied and not SANDAG trip rates. The methodology used for the project's VMT analysis complies with the City's formally adopted guidance and the City's engineering judgment. The additional and different analysis suggested in the comment is neither necessary nor appropriate for this specific project.

A3-23 The comment states that the City has goals related to pedestrians and cyclists and requests that a crosswalk be added as a part of this project on the north side of the intersection of SR-76 and Benet Road. The LTS (Appendix I of the DEIR) included a pedestrian network analysis consistent with the Traffic Guidelines (City of Oceanside 2020). The LTS correctly analyzed the sidewalk network consistent with the Traffic Guidelines by documenting the missing and existing sidewalk segments on both the east and west side of Benet Rd within a 0.5-mile walking distance of the project site. The comment fails to disclose that the project would provide pedestrian improvements to Alex Road and the east side of Benet Road that would allow for direct pedestrian access from the project site to the San Luis Rey River Trail. The comment also does not acknowledge that an existing contiguous sidewalk already exists on the east side of Benet Road from the project site to the SR-76 and that a north/south crosswalk exists on the east side of Benet Road across SR-76. As there are no sidewalks allowed along the SR-76, and given the project's pedestrian improvements connecting the project site to the San Luis Rey River Trail, nothing further is required under the City's Traffic Guidelines, the General Plan, or CEQA as it relates to this issue.

A3-24 The comment states that per Business and Profession Code Section 8771, perpetuation of survey monuments by a licensed land surveyor is required if they are being destroyed by construction. Additionally, the comment states that any work performed within the Caltrans right-of-way will require discretionary review and approval by Caltrans and an encroachment permit prior to construction. The City acknowledges this comment, and the project would comply with applicable requirements. No survey monuments would be destroyed as a result of project construction. The comment does not raise any specific issues related to the adequacy of the DEIR, and no further response is required.

A3-25 The comment includes concluding remarks. The comment does not raise any specific environmental issues related to the adequacy of the DEIR, and no further response is required.

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Response to Robinson's email concerning Eddy Jones Warehouse Project

From Marion Donahue <mariondonahue@hotmail.com>

Date Wed 2/5/2025 11:40 AM

To RWRobinson@oceansideca.org <RWRobinson@oceansideca.org>

Cc Robert Dmohowski <rdmohowski@oceansideca.org>; council@oceansideca.org <council@oceansideca.org>; znavarro@oceansideca.org <znavarro@oceansideca.org>; esanchez@oceansideca.org <esanchez@oceansideca.org>; ejoyce@oceansideca.org <ejoyce@oceansideca.org>; //fire.ci.oceanside.ca.us <//fire.ci.oceanside.ca.us>; Dee Keck <4dees@cox.net>; Jim Waite <battcar@sbcglobal.net>; memaeeee@aol.com <memaeeee@aol.com>; kimberlyan#2@hotmail.com <kimberlyan#2@hotmail.com>

Councilman Robinson,

I am very appreciative of your timely response to my letter concerning my non-approval of the Eddy Jones Warehouse project. However, the main reason for your email was to correct an error in the description of the Panorama Fire in which I had to evacuate my home. I am hoping that you continued to thoroughly read the rest of my letter as well.

Yes, the Panorama fire was later investigated by the Fire Inspection Department in Orange County and deemed as "arson." However, most people understand **arson** to mean that someone deliberately starts the fire. The fire did indeed arise near Panorama Point and that spot is a "look-out" view of the San Bernardino Valley. It is Located on a Highway, SR18, but is also the **site of the County Equipment Storage Depot**. (I am very familiar with Panorama Point from having been a *Master-Degreed Educator* in San Bernardino for over 33 years. Though our students were discouraged from the practice, they often used Panorama Point as a "make-out site." (which sometimes resulted in a birth, nine months later.)

From verified eyewitnesses that morning, the **actual starting of the Panorama arson fire was an unidentified homeless person who had been trying to cook a meal**. It was not set *deliberately*, but the results were the same as if it had been. **50-90 mph Santa Ana Winds** whipped the fire out of control and the final statistics recorded were 4 Lives Lost, 325 structures destroyed (including 310 homes, and a burned area of 28,800 acres total.)

While all of these statistics can be found in records, the **purpose** of all my previous correspondence to the City has been to **document what evacuating on an exit road, in heavy traffic and with fast arriving emergency vehicles will mean, if a similar scenario occurs to homes near a Warehouse Complex**. This last letter you referred to also contains unanswered questions I ask that are crucial to our **sharing the same road with over 100 diesel trucks, over 500 Warehouse employees, and in a 24/7 operation run by an-as-yet unnamed Tenant**.

In order to clarify that my chosen position on this vote is not based on just an emotion from having gone through a fire, I will explain the **background** of some of my other knowledge about emergencies. **My husband who worked briefly as a HOT SHOT "Pulaski" during a fire, is also a retired Railroad Engineer of 33 years and knows firsthand what emergencies are like**. When people commit suicide, walk on the track, pull their vehicle around a barrier in front of an oncoming engine, and the car becomes stuck in front of the train, the world changes for **everyone** involved. **Preventing** these still ongoing tragedies is the best path forward and Cities are presently making decisions based upon

knowledge that has **been proven to work, instead of being based on paper "Statistics" that a tragedy "probably" won't happen.**

Our daughter who was standing on the kindergarten playground that fateful Fire evacuation day is now a Physician in Seattle WA. After Graduation from University of Washington Medical School, **Dr. Rachel Beda MD** chose to serve her first 10 years at the main hospital in the **Emergency Medical Department**, then worked at the Seattle Pike Peak Clinic for many years being their "go-to" Narcan Staff Physician during emergencies.

Councilman Robinson, **you have served as a Paramedic**, you can relate to the fact that during part of Dr. Beda's service, **she Certified EMTs** under Dr. Copass, head of University of Washington Seattle Hospital's Emergency Medicine. (Coincidentally, Dr. Copass later designed and implemented airplane service to Islands when Ferrys could not operate after he endured a fire-related tragedy in which he could not reach his dying patients.) **Dr. Beda not only lectured to candidates, but also rode with them in the big Medic One vehicle, supervising their initial trial period, and evaluating their fitness for service as an EMT in the State of Washington.**

So, other than the fact that she is my daughter, **how does that give me knowledge?** I have attended some of her lectures and have received useful information for myself. (i.e. if encountering a person who has collapsed and drunk, the individual needs to be turned on his side to prevent choking on his own vomit. If he has a head-bleed, or is injured in any other way, call 911 for help and remain with the victim until help arrives.) **I actually performed this maneuver on a man.** Having fallen on a hot sidewalk, the man was barefoot and lying immobile while inebriated. When I turned him over, I observed that he was bleeding from his head. While other walkers stepped over him, declaring him to be "OK because his eyes were open", I did what I had learned, remaining by him after calling the 911 number. Was I a "hero?" No. Were the other walkers "callous?" No. I just happened to have some **knowledge** which they had never been given.

Those residents who speak before you on **February 10** will be sharing information regarding the proposed construction of Warehouses near our homes. It is my hope that all Council Chamber members will thoughtfully **pay attention to the knowledge presented**, rather than gamble that the risks to safety never occur.

On the occasion of your retirement as Oceanside's Fire Chief on July 29, 2022, you were interviewed by Roundtable host Kent Davy on KOCT. Councilman Robinson, I would like to use **information** given by you in that segment **to elucidate other reasons for rejecting the Eddy Jones Complex near our home.** Mr. Davy asked you about your service from the beginning to the present day and how your experiences had evolved and changed with time.

Some of the specific innovative changes implemented for service today were listed as more protective PE for fighters on the line, improvements in vehicles, and an emphasis on health issues like physical fitness and becoming a non-smoker—also increasing longevity expectation by using Cancer prevention methods. You also defined factors affecting our now present environments. *In 2014, you yourself were employed by the Fire Prevention Bureau in 2014, you explained the knowledge of air quality hazards that Firemen face.* In early years, the substances that fueled fires were wood, furniture, cotton materials, and/or other natural derivatives. You explained that **today**, our houses, our vehicles, and most other products are made of **synthetic materials** and in oil-based combustion they are **toxic to humans** who breathe them. *(I will address this issue in the "questions unanswered" portion of my letter.)

Another change is the **usage of our 911 number.** 2/3 of calls to staff are **Emergency Medical**

related calls and they have grown by 80% over time. Unless key words like "Heart Attack," are used immediately in the call, the screening takes a little longer, and information must be obtained by the answering staff in order to make decisions for the caller's needs. (i.e. an advanced BLS ambulance vs needing a Paramedic resource) You stated that residents should not hesitate however, to use the 911 resource because it is **"their emergency, not ours."**

Because of the vast and complex problems of very large emergencies, the need for cooperation among agencies across the state was explained. *"None of our fire departments, even our largest fire departments, have the ability to respond for multiple days on end on a major wild land fire, or maybe it's not even a wild fire, maybe it's an earthquake or maybe it was a tsunami or maybe it's a...so, it has transitioned from firefighting to all-risk...we respond to almost anything."*

Two agency **System Response** programs were defined by you in the interview. There is State Mutual Aid ("Neighbor helping neighbor") and there is Automatic Aid. **"State Mutual Aid is a system that we have to request aid...you have to ask for it...we can get whatever we need if you want to pay for it...aircraft, dozers, personnel, etc."** **"Automatic Aid just happens...we send the closest resource available for that particular type of emergency."** (i.e. Carlsbad firetruck with Oceanside EMT vehicle)

A new **Community Risk Reduction Program** has been instituted in Oceanside, now overseen by *Dave Parsons, present Oceanside Fire Chief*. You voiced that during an emergency visit, the department will observe other factors in the house or locations which may prevent emergency calls in the future. (i.e. smoke alarms, trip hazards for seniors, and the need to create "safe spaces") **Offering advice based on knowledge can avert the necessity for emergency calls. It provides the possibility that people will be better informed to protect themselves.**

Councilman Robinson, Kent Davy asked what was the toughest fire in terms of fire suppression you ever had to fight. You named the **Freeway Complex Fire in 2008**. Internet articles reveal these statistics: The **Freeway fire, (caused by a faulty catalytic converter)** merged with the **Landfill fire, (caused by faulty power lines)**. Unlike the **Panorama fire** which I experienced, no lives were lost, but the ultimate damage was similar—314 homes, 47 other structures, and a total of 30,305 acres consumed.

I very specifically bring up this Fire to show my confidence in your understanding about Fire responses and how the decisions that are made in the face of a fire are extremely complicated but vital. Although you were not physically in the field, you were a crucial part of how the Freeway Complex was battled. You were employed as a **Chief Officer in Orange County and one of your jobs was to oversee the Dispatch Center...** Department of Operating Center (DOC). "There was a Fire Chief, but **during a fire, responsibility lay on the Chief of DOC information...to make sure of coverage in the fire, to make sure needed protection for residents was still provided by fire Depts surrounding areas of the fire, to send resources, and hold (*back) others...How do you make those decisions!"** You continued, **"I felt I was truly tested as a leader and a commander so to speak."**

(My personal hope is that this experience will be remembered when residents provide their knowledge on February 10. If the Eddy Jones Warehouses are approved and built, that decision will not only complicate our daily lives, but actually endanger them in the event of a medical emergency or a natural disaster!)

At the time of your interview, you said that the Oceanside Fire District was the 3rd largest in the county, having 8 stations, and that the City was growing fast. "As the city grows...I see us now reach the limit of some of our capabilities—for our engine responses, our paramedic responses..." But you did say that a **Standard of Cover Study** had begun. The study would be using data to determine the future of **"what we really need."** *(to be addressed)

Councilman Robinson, I understand that you have been a **resident of Oceanside for over 34 years**, and that you **care deeply about your community**. Upon retirement you continue to help fellow residents learn information about the city in which they live through your **O'side De-Mystified KOCT TV programs**. I do not doubt your illustrious **career in serving people** in myriad ways has been sincere throughout all of your years.

Therefore, I ask you (and all Oceanside Officials) to see your City in a new way-- through the eyes of fellow residents who sincerely believe that constructing the Eddy Jones Warehouses serves NO ONE's best interest.

.....
To the previous Unanswered Questions section of my letter sent , I would like to add that we still do not know what will be transported to, and stored at, the Warehouses.

However, given the information that most items produced today contain synthetic materials, there would be massive amounts compared to what is in a house or vehicle and in case of a natural disaster, arson, or downed power lines that spark a fire or fire firefighters, emergency teams (and local residents) would be exposed to severe hazardous toxins.

Did the Standard of Cover study include evacuation plans for residents sharing exit roads, with Diesel trucks, and with little or no space to pull off for advancing emergency vehicles?

Were the Benet Traffic Studies impact reports done by Independent Contractors, or within City Agencies?

Will the impact of Ocean Kamp on Foussat, the only other exit from our homes be a factor in the decision of Warehouse Construction? (Ocean Kamp includes 667 homes, a 300-room hotel, and 134,000 square feet of retail and office space) Does Oceanside even want a Warehouse facility which will bring little contribution in terms of money or benefit to our local economy?

Perhaps a more advantageous project could be proposed for this site by the present City Council rather than proceeding with a developer encouraged by previous City Officials.

We could argue semantics and numbers forever, but the bottom line is that the Weather Climate has changed, emergencies will always occur, and we residents have placed our trust in our Fire Department, Police Department, and Officials we have elected to keep us safe from unnecessary risk.

I believe that the Eddy Jones Warehouse Project is one of those risks that can be prevented.

Robert Dmohowski

From: Joseph Kaman <ninjaew@att.net>
Sent: Friday, February 7, 2025 10:34 AM
To: Robert Dmohowski; Esther Sanchez; Eric Joyce
Cc: oceansidespeaksout@gmail.com; memaeae@aol.com; Zeb Navarro
Subject: Eddie Jones Project -comments

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Mayor Sanchez: Firstly, I'd like to thank you for the road overlay done on portions of Oceanside Blvd. Back in November 2025. I support and voted for Measure X even though its road funding level (\$3.5~\$4.2 MUSD) is quite inadequate. I planned to speak at a November City Council meeting but chickened out. I was and am very concerned over the degrading condition of our main laterals (Oceanside Blvd, and Mission Ave.) owing to developments past and future:

- El Corazon Ventana 2020
 - El Corazone Luma (268 units) Phase 1 of 3 open (2024)
 - Front Wave Arena
 - TTLC Oceanside Garrison (140 homes over 21 x 3 story buildings (planning in-process))
 - Utah based developers plan submitted for 900 homes at Moon Valley Nursery site (O'side Blvd).

My overall concern is serious as too much development is ongoing/planned without proper thought (budgeting) given to the significant load on our current infrastructure. Namely traffic road width and congestion. All impacted greater should we ever have a natural/man-induced disaster.

I live in the Sea Mesa development and constantly realize a greater congestion on my main roads of Oceanside Blvd, Mission Ave, El Camino Real, and Fousat Road which ties in "directly" to SR76.

I am not in the "immediate" area of the Eddie Jones Project but very near and to be impacted. I'm not anti-development. However, I am pro- responsible development. I read through the original exhaustive 400+ page EIR report on this project. I was appalled at the disregard and "dismissal" of all traffic impacts stated at the key intersections of SR76 & Benet, SR76 & Airport Rd, SR76 & Fousat Road. I realize the project scope has changed 2-3 times since then but my opposition remains primarily the same based upon great traffic impact and load factors that will degrade the quality of life as we know it in Oceanside.

On any given day just stand at one of the 4 corners of SR76 and Airport Road at the morning or evening rush hour. **Currently** it's ugly and quite congested. OceanKamp hasn't even been built yet. The increased traffic flow from Benet and Airport to SR76 with a new truck /auto traffic warehouse will be near gridlock "at time", not always.

The adjacent traffic load of autos/truck avoiding this will spill onto Fousat Rd., over to Oceanside Blvd, and Mission Ave. I don't want the Sea Mesa development to become a gridlock sample of Orange County congestion.

Of utmost concern for our Airport neighborhoods directly impacted by the Eddie Jones Project is the safety hazard/catastrophe that realistically could occur should a fire develop and escalate in the San Luis Rey riverbed area. They would be nearly trapped owing to the only escape roads of Mission Ave and SR76. Any widening or improvement of traffic flow on SR76 is NOT likely as its a State Road not under any jurisdiction/influence by the City of Oceanside, respectfully.

In Summary: I'm not anti-development, rather responsible development and am in opposition to the the Eddie Jones Projects' "advancement". It's NOT RESPONSIBLE. Oceanside is a most beautiful City I cherish and have lived in for approximately 30years. The San Luis Rey riverbed to me is a "natural wonder" that we'll never have once its developed industrially. I am very much in support of a long range plan to develop the river bed as a largely natural development with limited industrial/residential development for those that live here long after we're gone.

Thank you for your consideration.

Regards,

Joseph Kaman
Citizen of Oceanside
3583 Seafarer Drive
Oceanside, CA. 92054

Robert Dmohowski

From: stuart cannes <stuartcannes@yahoo.com>
Sent: Wednesday, February 26, 2025 12:26 PM
To: Esther Sanchez; Rick Robinson; Peter Weiss; Zeb Navarro; Eric Joyce; Jimmy Figueroa; City Manager
Cc: Robert Dmohowski; planningcommission@oceansideca.com
Subject: Eddy Jones - Benet Road Tractor Trailer Traffic Photos
Attachments: 20221221_110148 (1).jpg; 20230223_080735.jpg

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Dear Oceanside Council Members:

I am not in favor of the Eddy Jones Project even in the reduced form of 37 bays. I am a 20-year Oceanside resident, and I own a home within the 1500 feet of the Eddy Jones project. I attended the Planning Commission meeting on February 10th.

This was my first Planning Commission meeting, and I have to tell you I was surprised at how the Planning Department staff was so strongly in favor of the project. Maybe this is normal. If the staff supports the project, they speak in favor of it.

However, I want to relay to you how upsetting it was to me for the Planning Department to just steamroll over every legitimate issue there is with this project. Traffic, pollution, environmental issues and more were all just discounted like they didn't exist.

Truck traffic using Benet Road through a residential neighborhood and exiting the distribution center via Fousat was also brushed aside.

Yes, the Planning Department says "semi-truck traffic will be prohibited on Benet Road." Well, I challenge Rob Dmohowski to explain to the Planning Commission and the Council members just what exactly does that mean?

Will there be a gate on the bridge keeping the trucks off Benet? No. Will there be Oceanside police waiting to ticket trucks using Benet Road. No, that's not going to happen. My best guess is that the Planning Department will hang a small "No Thru Trucks" sign on an existing light pole and consider their job done. Well, in case you haven't been paying attention, motorists ignore all kinds of traffic rules and signs with great regularity. A sign isn't going to keep the trucks from using Benet Road...which has a 45 and 40 mph speed limit through a narrow residential neighborhood.

If the Eddy Jones project as a truck distribution center is built, there will be a parade of tractor trailer traffic using Benet Road and the Fousat intersection in order to shave a few seconds off their trip. It's a given.

The residents currently have some experience with a limited number of large semi-trucks using Benet Road. Moving vans for new residents and delivery trucks loaded with tile or sod are rare but not uncommon. These are all one-time events and not an issue.

We've also had experience with a tractor-trailer driving school regularly using Benet Road. A truck and trailer from the driving school would sometimes drive through Benet Rd. every 15 minutes. You can hear

the tractor trailer rig approaching from a hundred yards away. The metal parts of the trailer clanging, the air brakes hissing, the diesel engine revving as it downshifts at the stop sign and it all ends with a screech of the brakes. This is a residential neighborhood with people walking dogs and children riding bikes on a sidewalk literally a few feet away.

Attached are two photos of how large and intimidating a tractor trailer rig looks like to the residents along Benet Road as it rolls by at 40 mph. These photos were taken from inside a resident's home.

Please do not approve this project.

Sincerely,
Stuart Cannes

Robert Dmohowski

From: Tim Cribbs <tmcribbs@gmail.com>
Sent: Sunday, February 16, 2025 3:15 PM
To: Esther Sanchez; Eric Joyce; Rick Robinson; Jimmy Figueroa; Peter Weiss; City Manager; Zeb Navarro; Robert Dmohowski
Subject: Eddie Jones Warehouse Project (CUP22-00001)

Follow Up Flag: Follow up
Flag Status: Flagged

Warning: External Source

Oceanside City Council,

I am writing to ask you to **reject** the Conditional Use Permit (CUP22-00001) for the Eddie Jones Warehouse Project that was approved by the planning commission on Feb 10, 2025.

I live within the 1500 ft boundary and am highly concerned about many aspects about this project. I actually cannot find one reason why this project benefits our community.

I have signed the appeal that will be presented this week. This serves as a detailed testimony to accompany my appeal signature.

I am asking that we **maintain the specifications listed in the current/existing zoning parameters**: industrial facility for distribution and storage should not exceed 50,000 square feet and allows no more than six (6) heavy truck terminals - and no more than six (6) heavy trucks allowed on the premises at one time.

Article 13 Zoning Ordinance applies to all industrial areas south of the airport and south of Hwy 76. Why should the Eddie Jones Project be the exception? Additionally, this sets a precedent that can and will be **quickly exploited** for future neighborhood-busting developments.

SAFETY: There are 1,200 homes that have only two (2) exits: Foussat and Benet. I see **no funding** earmarked from CALTRANS for any road/traffic improvements at the intersection of Hwy 76 and Benet. That intersection has a 4-minute light and will be completely subsumed with trucks coming and going, effectively taking 50% of our exits away from our daily commutes. Going to get bananas, dropping kids off at Jui Jitsu, and of course the daily trek to school and work will ALL be affected for those who leave and return via Benet. Simply re-routing our daily commutes to Foussat is a **highly unreasonable request** - adding several miles per trip to each of our lives is NOT beneficial to us residents in any way. Staggering the trucks' arrival and departure is **virtually unenforceable** and a ridiculous solution. And a dedicated lane that I saw referenced as a solution is going to go where?? Cut into the riverbed? Cut into the airport property? Funded by whom (since it isn't CALTRANS)? CALTRANS will not build a dedicated lane on the 76 for this so what exact lane are they proposing taking over completely?? (the answer is the only lane available: one lane left, one center, and one right with one lane on 76 turning into Benet)

I lived here during the 2022 fire that evacuated Wanis View Estates. Evacuations happen fast and are quite a chaotic scenario. Imagine 1200 families trying to evacuate all at once. Having only two exits is bad enough, let alone one exit being jammed up by trucks and the other jammed up with everyone else! **It is inexcusable to deliberately approve this scenario.**

HOME VALUES: I did not invest my hard earned money on a home that now is worth over a million dollars in a beautiful community less than 3 miles from the beach to have 34 trucks constantly interrupting my daily life, and neither would the potential future buyers. This will drop our home values in a significant way no matter what the Eddie Jones Project says. I saw no offer to compensate us for the drop in home values in the proposal. Dropping my home value on purpose via City Council deliberate approval is quite literally taking my modest life savings and **directly stealing** from it. I love Oceanside. Oceanside is a working class community who takes pride in being working class. Don't punish us!

Why do they need to be less than **3 miles** from the beach?? Why here, disrupting 1200 homes, specifically?

I'm not being NIMBY. We are not asking to change the zoning to ZERO trucks (though we could if we were NIMBY). We are okay with the approved 6 trucks zoning - but **not a single truck more**. Definitely not 34!!! It is mind boggling that the planning commission unanimously approved that despite clearly expressed opposition.

ROAD MAINTENANCE: The City of Oceanside has a difficult enough time maintaining the roads as they are. Mission Rd is a prime example that even when we passed Measure X over 7 years ago and passed it again making it permanent, Mission Rd still cannot get fixed. Instead, they just touch up paint the lines right over the potholes like putting lipstick on a pig.

I see it now... 2026 ballot: "We need to **expand** measure X because the increase in road usage is exceeding our ability to maintain them with the current revenues." -- and people will vote for it -- and viola... Eddie Jones just taxed us as well as slashed our home prices. Utter insanity.

TRAFFIC IN CONJUNCTION WITH OCEAN KAMP: I did not see any assessment that took into consideration the current traffic plus the increased future traffic from the approved Ocean Kamp project. This was **deceptive** because that project was already known and approved, but deliberately left out of the Eddie Jones Project assessment. There will be massive demands of the city council for many years to come to **expand Hwy 76!** Imagine City Council having to deal with escalating that community argument for the next 20-30 years....

Please reject CUP22-00001. For us, your constituents. Just keep the zoning as is. Please.

Concerned,
Timothy Cribbs
Wanis View Estates Homeowner
553 Tukmal Drive
Oceanside CA 92058

Robert Dmohowski

From: Lele Y <huxieally@hotmail.com>
Sent: Tuesday, May 6, 2025 5:44 PM
To: Planning-Planning Commission
Cc: nhojuhc@yahoo.com
Subject: Oppose to Eddie Jones

Follow Up Flag: Follow up
Flag Status: Completed

Warning: External Source

Dear City Planner,

I am writing to express my strong opposition to the proposed Eddie Jones Warehouse project in Oceanside.

As a resident, I am deeply concerned about the increased noise and air pollution this development would bring to our community. Warehouses typically mean a significant rise in truck traffic, which leads to more emissions and constant noise—both of which negatively impact our health and quality of life.

I urge the city to reconsider this project and prioritize developments that support a cleaner, quieter, and more sustainable Oceanside.

Thank you for your time and consideration.

Sincerely,

Lele Kyles & John Chu
3263 Toopal Dr
Oceanside CA 92058